TOWN OF TRUMBULL, CT

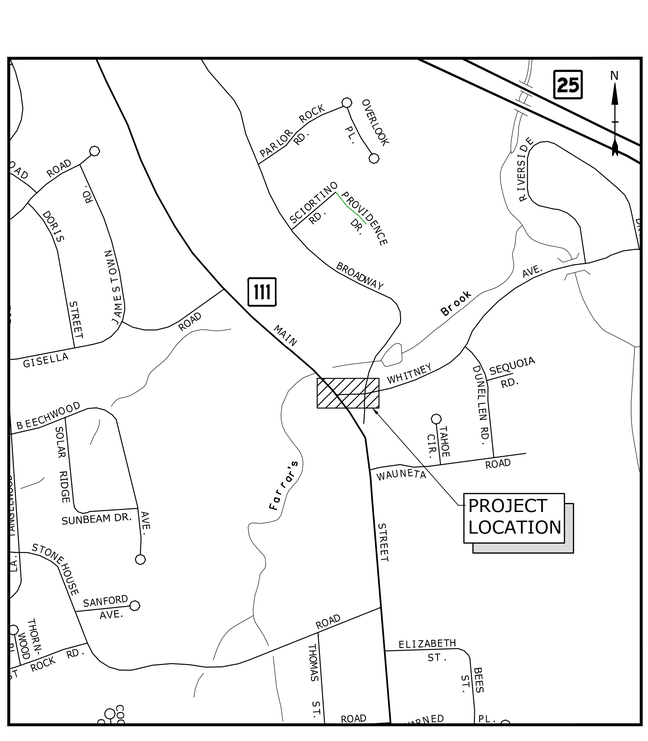
WHITNEY AVENUE AT MAIN STREET (ROUTE 111)

INTERSECTION IMPROVEMENTS

AUGUST 2018

LIST OF DRAWINGS	
SHEET NO.	SHEET TITLE
	COVER SHEET
1	TOPOGRAPHIC AND RIGHT OF WAY SURVEY (MARTIN SURVEYING)
2	TYPICAL SECTIONS AND GENERAL NOTES
3	CONSTRUCTION PLAN
4	GRADING & SOIL EROSION AND SEDIMENT CONTROL PLAN
5	SIGNING AND PAVEMENT MARKING PLAN
6	MAINTENANCE AND PROTECTION OF TRAFFIC PLAN DETAILS
7	DETAILS
8	CROSS SECTIONS STA 0+50 TO 1+00
9	CROSS SECTIONS STA 1+25 TO 1+75
10	TRUCK TURN MOVEMENTS

CTDOT STANDARD SHEETS	
SHEET NO.	SHEET TITLE
11	SIDEWALK RAMP SHEET 1
12	SIDEWALK RAMP SHEET 2
13	SIDEWALK RAMP SHEET 3
14	SIDEWALK RAMP SHEET 4
HW-507_01	TYPE "C", "C-L" & DROP INLET CATCH BASIN
HW-507_07	TYPE "C" & "C-L" CATCH BASIN TOPS AND CURBS
HW-507_10	MANHOLE - FRAME & COVER
HW-815_01	BITUMINOUS CONCRETE CURBING
HW-921_01	DRIVEWAY RAMPS AND SIDEWALKS
TR-1208_01	SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS
TR-1208_02	METAL SIGN POSTS AND SIGN MOUNTING DETAIL
TR-1210_04	PAVEMENT MARKING LINES AND SYMBOLS
TR-1210_08	PAVEMENT MARKINGS FOR NON FREEWAYS
TR-1220_01	SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS
TR-1220_02	CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES



LOCATION MAP
SCALE: 1" = 500'

PREPARED BY:

Tighe&Bond
www.tighebond.com

ALFRED J. MASCIA JR., P.E.

DANA C. HUFF, P.E.

PREPARED FOR:

TOWN OF TRUMBULL

Last Saved: 11/29/2018
Plotted On: Nov 29, 2018 - 2:13pm By: sansonem
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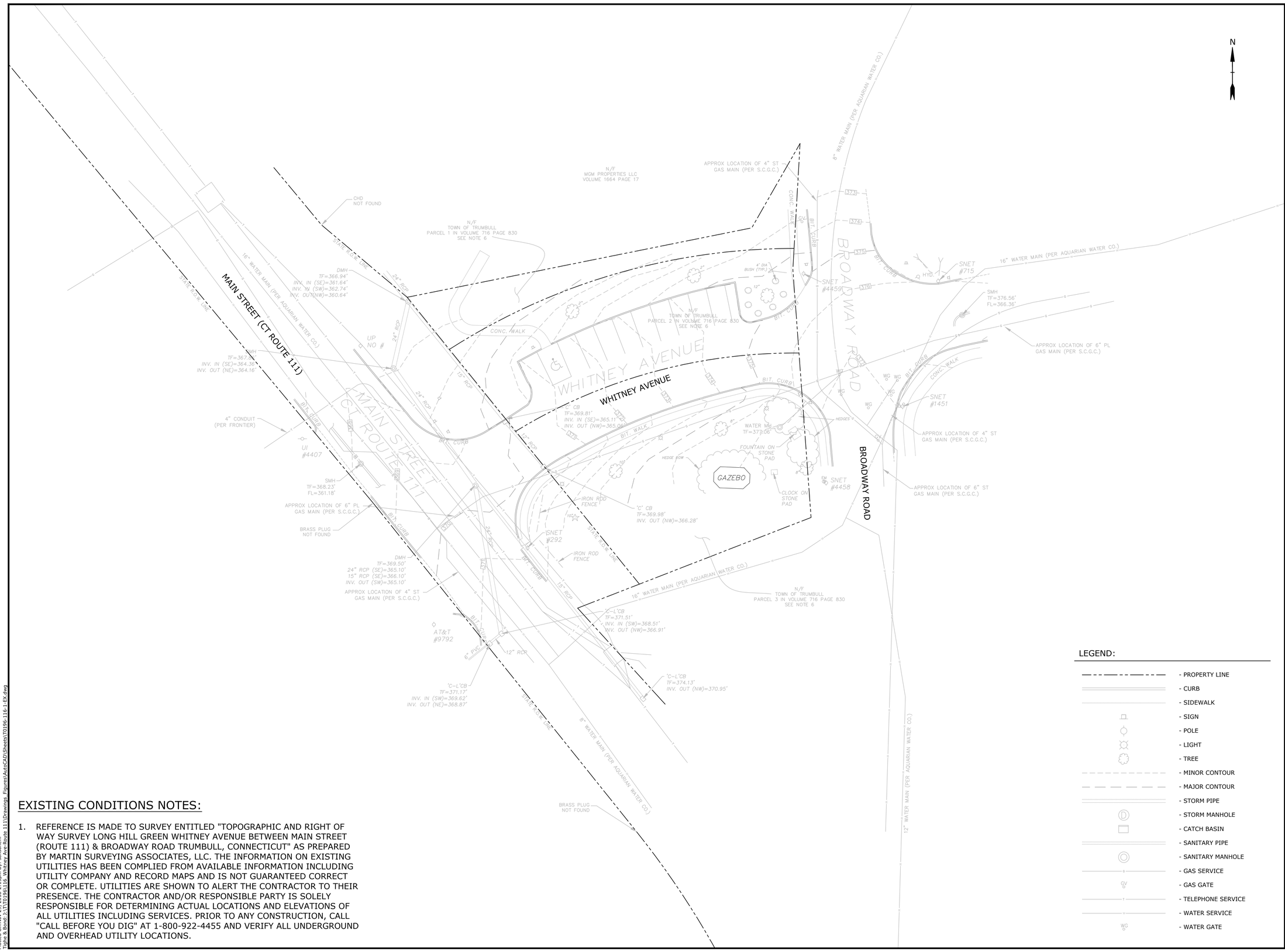

EXISTING CONDITIONS NOTES:

1. REFERENCE IS MADE TO SURVEY ENTITLED "TOPOGRAPHIC AND RIGHT OF WAY SURVEY LONG HILL GREEN WHITNEY AVENUE BETWEEN MAIN STREET (ROUTE 111) & BROADWAY ROAD TRUMBULL, CONNECTICUT" AS PREPARED BY MARTIN SURVEYING ASSOCIATES, LLC. THE INFORMATION ON EXISTING UTILITIES HAS BEEN COMPLIED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY AND RECORD MAPS AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR AND/OR RESPONSIBLE PARTY IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES INCLUDING SERVICES. PRIOR TO ANY CONSTRUCTION, CALL "CALL BEFORE YOU DIG" AT 1-800-922-4455 AND VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS.

LEGEND:

- PROPERTY LINE
- CURB
- SIDEWALK
- SIGN
- POLE
- LIGHT
- TREE
- MINOR CONTOUR
- MAJOR CONTOUR
- STORM PIPE
- STORM MANHOLE
- CATCH BASIN
- SANITARY PIPE
- SANITARY MANHOLE
- GAS SERVICE
- GAS GATE
- TELEPHONE SERVICE
- WATER SERVICE
- WATER GATE

SCALE IN FEET
0 20' 40'
GRAPHIC SCALE

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Town of Trumbull

Whitney Avenue at Main Street Intersection Improvements

Trumbull, CT

MARK	DATE	DESCRIPTION
4	11/29/2018	TOWN COMMENTS
3	8/30/2018	CTDOT COMMENTS
2	8/15/2018	CTDOT COMMENTS
1	7/2/2018	CTDOT & TOWN COMMENTS

PROJECT NO:	T0196-116
DATE:	08/15/2018
FILE:	T0196-116-1-EX.dwg
DRAWN BY:	ALW
CHECKED:	COG
APPROVED:	DCH

EXISTING CONDITIONS PLAN

SCALE: 1" = 20'

C1.1

Last Saved: 11/29/2018
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EXISTING CONDITIONS NOTES:

1. REFERENCE IS MADE TO SURVEY ENTITLED "TOPOGRAPHIC AND RIGHT OF WAY SURVEY LONG HILL GREEN WHITNEY AVENUE BETWEEN MAIN STREET (ROUTE 111) & BROADWAY ROAD TRUMBULL, CONNECTICUT" DATED 12/20/2017, AS PREPARED BY MARTIN SURVEYING ASSOCIATES, LLC. THE INFORMATION ON EXISTING UTILITIES HAS BEEN COMPLIED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY AND RECORD MAPS AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR AND/OR RESPONSIBLE PARTY IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES INCLUDING SERVICES. PRIOR TO ANY CONSTRUCTION, CALL "CALL BEFORE YOU DIG" AT 1-800-922-4455 AND VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS.

TYPICAL SECTION MATERIAL NOTES:

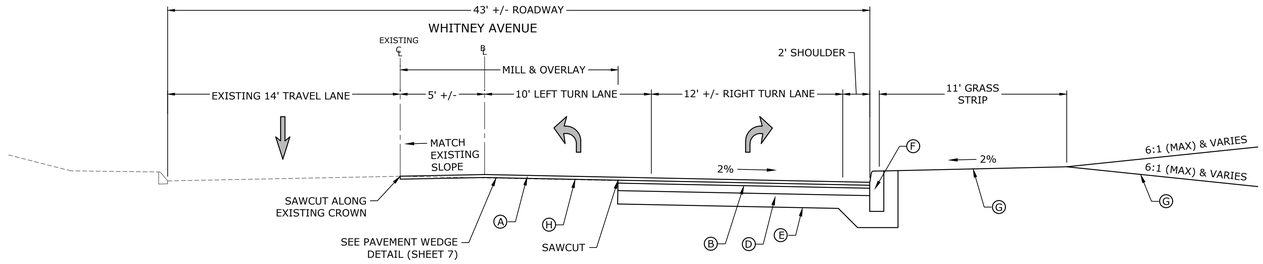
1. FORMATION OF SUBGRADE SHALL BE CONDUCTED IN ACCORDANCE WITH SECTION 2.09 OF THE CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 817.
2. PROCESSED AGGREGATE BASE SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 3.04 OF THE CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 817.
3. BITUMINOUS CONCRETE SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 4.06 OF THE CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 817.
4. TACK COAT MATERIAL SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 4.06 OF THE CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 817.
5. BITUMINOUS CONCRETE CURBING SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 8.15 OF THE CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 817.
6. CONCRETE SIDEWALKS AND RAMPS SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 9.21 OF THE CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 817.
7. BITUMINOUS CONCRETE SIDEWALK SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 9.22 OF THE CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 817.
8. TOPSOIL AND TURF ESTABLISHMENT SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 9.44 OF THE CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 817.

CONSTRUCTION NOTES:

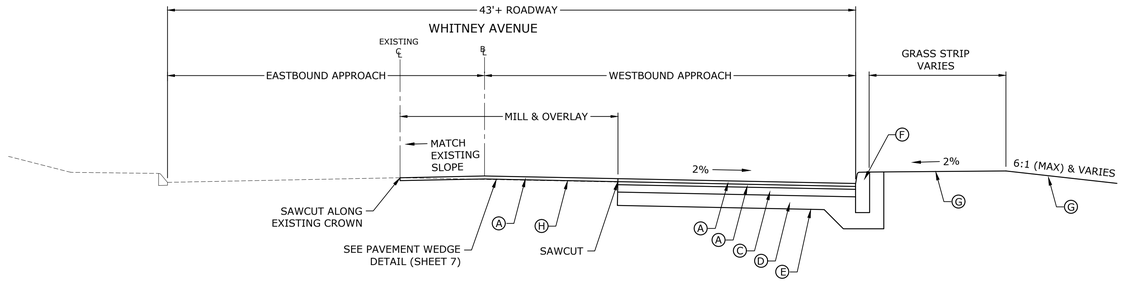
1. CONSTRUCTION SHALL COMPLY WITH THE TOWN OF TRUMBULL STANDARDS & CONNDOT FORM 817.
2. THE CONTRACTOR SHALL OBTAIN A TOWN OF TRUMBULL ROADWAY ENCROACHMENT PERMIT PRIOR TO ANY WORK BEING PERFORMED WITHIN TOWN RIGHT-OF-WAY.
3. THE CONTRACTOR IS RESPONSIBLE FOR THE MAINTENANCE AND PROTECTION OF TRAFFIC. THE CONTRACTOR SHALL PROVIDE A PLAN SHOWING PROPOSED MAINTENANCE AND PROTECTION OF TRAFFIC TO THE TOWN OF TRUMBULL AND THE ENGINEER FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION.
4. ALL UNDERGROUND UTILITIES SHOULD BE CONSIDERED APPROXIMATE LOCATIONS. CONTRACTOR SHALL CONTACT "CALL BEFORE YOU DIG" PRIOR TO ANY EXCAVATION.
5. EXCAVATION OF ANY TYPE SHALL BE ACCOMPLISHED IN SUCH A MANNER THAT UNDERGROUND UTILITIES OR STRUCTURES ARE NOT DAMAGED. IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY FOR ANY DAMAGED INCURRED DURING EXCAVATION OPERATIONS. ALL EXCAVATION SHALL BE IN CONFORMANCE WITH THE LATEST OSHA REQUIREMENTS.
6. ALL WORK TO BE CONFINED WITHIN THE CONTRACT LIMITS WITH NO CLEARING, EXCAVATION, OR DEPOSITION OF FILL TO TAKE PLACE WITHIN REGULATED AREAS OUTSIDE OF THE PROJECT LIMITS.
7. ALL DRIVEWAYS, ROADS, STAIRS, AND SIDEWALKS DISTURBED BY THE CONSTRUCTION IN OR OUTSIDE THE PROJECT LIMIT LINE SHALL BE RETURNED TO THEIR ORIGINAL CONDITION OR BETTER AND SHALL BE GRADED TO MEET THE PROPOSED CONSTRUCTION AS DIRECTED BY THE OWNER'S REPRESENTATIVE AT NO ADDITIONAL COST TO THE OWNER.
8. ANY DAMAGE TO EXISTING CURB, SIDEWALK OR ANY OTHER HIGHWAY APPURTENANCES SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE TOWN OF TRUMBULL.
9. PROVIDE A CONSTRUCTION SCHEDULE, PRIOR TO THE BEGINNING OF WORK, TO THE TOWN OF TRUMBULL ENGINEERING DEPARTMENT AND INSPECTOR. PROVIDE UPDATED SCHEDULE ON A BI-WEEKLY BASIS THROUGHOUT CONSTRUCTION.
10. IT IS THE RESPONSIBILITY OF EACH BIDDER IN EVALUATING THESE PLANS TO MAKE EXAMINATIONS IN THE FIELD BY VARIOUS METHODS AND OBTAIN NECESSARY INFORMATION FROM AVAILABLE RECORDS, UTILITY COMPANIES, AND INDIVIDUALS AS TO THE LOCATION OF ALL SUBSURFACE STRUCTURES. REVIEW ALL OF THE DRAWINGS AND SPECIFICATIONS ASSOCIATED WITH THIS PROJECT WORK SCOPE PRIOR TO SUBMITTING A BID. SHOULD THE CONTRACTOR FIND A CONFLICT WITH THE DOCUMENTS RELATIVE TO THE SPECIFICATIONS OR APPLICABLE CODES, IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY THE TOWN ENGINEER IN WRITING PRIOR TO THE BID OPENING. FAILURE BY THE CONTRACTOR TO NOTIFY THE TOWN ENGINEER SHALL CONSTITUTE ACCEPTANCE OF FULL RESPONSIBILITY BY THE CONTRACTOR TO COMPLETE THE SCOPE OF WORK AS DEFINED BY THE DRAWINGS AND IN FULL CONFORMANCE WITH LOCAL REGULATIONS AND CODES.
11. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ALL ELEVATIONS, PROPERTY LINES, LOCATIONS OF UTILITIES AND SITE CONDITIONS IN THE FIELD. IF AN UNFORESEEN INTERFERENCE EXISTS BETWEEN AN EXISTING AND A PROPOSED STRUCTURE, THE CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER SO THAT THE APPROPRIATE REVISIONS CAN BE MADE.

817 CONSTRUCTION NOTES:

1. REMOVAL OF PAVEMENT MARKINGS ALONG STATE ROADWAYS SHALL BE COMPLETED BY A NON-DESTRUCTIVE METHOD IN COMPLIANCE WITH THE STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD, BRIDGES, AND INCIDENTAL CONSTRUCTION FORM 817 SECTION 12.11 AS REVISED.
2. NEW PAVEMENT MARKINGS SHALL BE PAINTED WITH EPOXY RESIN PAINT IN COMPLIANCE WITH THE STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, AND INCIDENTAL CONSTRUCTION FOR 817 SECTION 12.10 AS REVISED.
3. NEW SIGN MATERIAL AND SHEETING SHALL BE MADE OF REFLECTIVE MATERIAL IN COMPLIANCE WITH STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, AND INCIDENTAL CONSTRUCTION FOR 817 SECTION 12.08 AS REVISED. TYPE 1 REFLECTIVE SHEETING SHALL BE USED FOR SIGNS WITH WHITE BACKGROUND, TYPE 3 REFLECTIVE SHEETING SHALL BE USED FOR SIGNS WITH COLORED BACKGROUND EXCEPT FOR SIGNS WITH RED BACKGROUND THAT SHALL BE TYPE 8 OR 9 REFLECTIVE SHEETING.
4. ALL SIGNS AND PAVEMENT MARKINGS INSTALLED ALONG THE STATE ROAD MUST CONFORM TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES," THE LATEST STATE OF CONNECTICUT CATALOG OF SIGNS, AND STANDARDS.
5. ANY DAMAGE TO THE EXISTING CURB, SIDEWALK OR ANY OTHER HIGHWAY APPURTENANCES SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE DISTRICT 3 PERMIT SECTION AT NO COST TO THE STATE.
6. ALL WORK WITHIN THE STATE RIGHT OF WAY WILL COMPLY WITH THE STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES AND INCIDENTAL CONSTRUCTION FORM 817 WITH THE LATEST SPECIAL PROVISIONS AND TYPICAL STATE STANDARD DETAILS.



TYPICAL SECTION 1 - WHITNEY AVENUE
STA: 0+80 TO 1+63
NO SCALE



TYPICAL SECTION 2 - WHITNEY AVENUE
STA: 1+63 TO 2+03
NO SCALE

TYPICAL SECTION LEGEND:

- (A) - 2" HMA S0.5 TRAFFIC LEVEL 2
- (B) - 3" HMA S1.0
- (C) - 5" HMA S1.0 TRAFFIC LEVEL 2 (PLACED IN TWO EQUAL LIFTS)
- (D) - 12" PROCESSED AGGREGATE BASE
- (E) - FORMATION OF SUBGRADE
- (F) - 6" CONCRETE CURBING
- (G) - 4" TOPSOIL & TURF ESTABLISHMENT
- (H) - TACK COAT

STANDARD ABBREVIATIONS

@	AT	CONC	CONCRETE	MISC	MISCELLANEOUS	SAN	SANITARY
&	AND	CO	CLEAN-OUT	MON	MONUMENT	SCH	SCHEDULE
ABDN('D)	ABANDON(ED)	CPP	CORRUGATED POLYETHYLENE PIPE	NIC	NOT IN CONTRACT	SDMH	STORM DRAIN MANHOLE
AC	ACRE	CY	CUBIC YARDS	N	NORTH	SF	SQUARE FEET
AOBE	AS ORDERED BY THE ENGINEER	DIP	DUCTILE IRON PIPE	NTS	NOT TO SCALE	S	SOUTH
BSMT	BASEMENT	DWG	DRAWING	N/A	NOT APPLICABLE	SMH	SANITARY SEWER MANHOLE
BCLC	BITUMINOUS CONCRETE LIP CURBING	E	EAST/ELECTRIC	N/F	NOW OR FORMERLY	STA	STATION
BGS	BELOW GROUND SURFACE	EOP	EDGE OF PAVEMENT	OH	OVERHEAD	STD	STANDARD
BOW	BACK OF WALK	ELEC	ELECTRIC	PC	POINT OF CURVATURE	STRM	STORM
BL	BASELINE	EL/ELEV	ELEVATION	PCC	POINT OF COMPOUND CURVATURE	T	TANGENT LENGTH/ TEL-DATA
BW	BOTTOM OF WALL	EMH	ELECTRIC MANHOLE	PCPP	PERFORATED CORRUGATED POLYETHYLENE	TEL	TEL-DATA
BIT	BITUMINOUS	EX/EXIST	EXISTING	PED	PEDESTRIAN	TF	TOP OF FRAME
BC	BOTTOM OF CURB	EG	EXISTING GRADE	PERF.	PERFORATED	TYP	TYPICAL
BLDG	BUILDING	G	GAS	PI	POINT OF INTERSECTION	TC	TOP OF CURB
BOT	BOTTOM	HC	HANDICAP	PT	POINT OF TANGENCY	TW	TOP OF WALL
CATV	CABLE TELEVISION	HYD	HYDRANT	PRC	POINT OF REVERSE CURVATURE	VSJ	VERTICAL STEM JOINT
CIP	CAST IRON PIPE	IN	INCHES	PS	PARKING SPACES	W	WATER
CB	CATCH BASIN	INC	INCORPORATED	PVC	POLYVINYL CHLORIDE	WG/WV	WATER GATE/VALVE
CJ	CONSTRUCTION JOINT	INV	INVERT	R	RADIUS		
CL	CENTERLINE	L	LENGTH OF CURVE	RCP	REINFORCED CONCRETE PIPE		
CLF	CHAIN LINK FENCE	LT	LEFT	REV	REVISION		
CTDEEP	CONNECTICUT DEPT. OF ENERGY & ENV. PROTECTION	LOC	LOCATION	ROW	RIGHT OF WAY		
CTDOT	CONNECTICUT DEPT. OF TRANSPORTATION	LP	LIGHT POLE	RT	RIGHT		
		MAX	MAXIMUM				
		MIN	MINIMUM				
		MH	MANHOLE				

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Town of Trumbull

Whitney Avenue at Main Street Intersection Improvements

Trumbull, CT

MARK	DATE	DESCRIPTION
4	11/29/2018	TOWN COMMENTS
3	8/30/2018	CTDOT COMMENTS
2	8/15/2018	CTDOT COMMENTS
1	7/2/2018	CTDOT & TOWN COMMENTS

PROJECT NO:	T0196-116
DATE:	08/15/2018
FILE:	T0196-116-2-TS.dwg
DRAWN BY:	ALW
CHECKED:	COG
APPROVED:	DCH

TYPICAL SECTIONS AND GENERAL NOTES

SCALE: NO SCALE

SHEET 2

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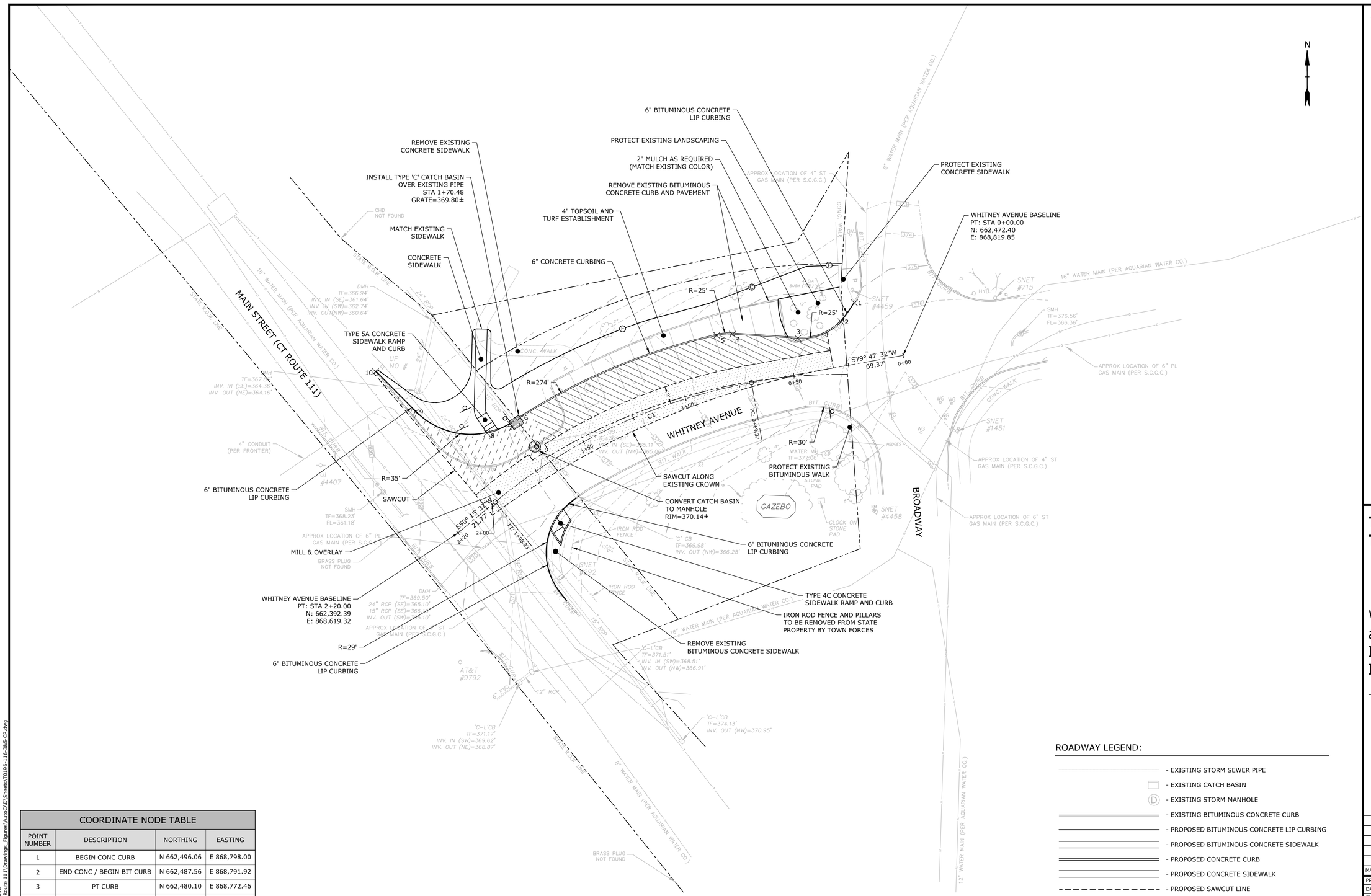

COORDINATE NODE TABLE			
POINT NUMBER	DESCRIPTION	NORTHING	EASTING
1	BEGIN CONC CURB	N 662,496.06	E 868,798.00
2	END CONC / BEGIN BIT CURB	N 662,487.56	E 868,791.92
3	PT CURB	N 662,480.10	E 868,772.46
4	PC CURB	N 662,481.98	E 868,742.99
5	PCC CURB	N 662,481.43	E 868,735.93
6	END BIT CURB	N 662,444.34	E 868,647.38
7	BEGIN CONC CURB	N 662,441.44	E 868,642.90
8	END CONC / BEGIN BIT CURB	N 662,437.32	E 868,632.37
9	PT CURB	N 662,446.90	E 868,600.19
10	END BIT CURB	N 662,465.04	E 868,582.19

CURVE TABLE								
CURVE #		STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C1	P.C.	0+69.37	N: 662,460.11	E: 868,751.58	29° 31' 54.7"	65.89'	128.86'	250.00'
	P.I.	-	N: 662,448.43	E: 868,686.73				
	P.T.	1+98.23	N: 662,406.31	E: 868,636.06				

CONSTRUCTION PLAN NOTES:

1. ALL MANHOLES, CATCH BASINS, AND VALVE BOXES SHALL BE RESET TO FINISHED GRADES.
2. TYPE "A" CATCH BASIN GRATES SHALL BE USED AT ALL PROPOSED CATCH BASINS.

ROADWAY LEGEND:

- EXISTING STORM SEWER PIPE
- EXISTING CATCH BASIN
- EXISTING STORM MANHOLE
- EXISTING BITUMINOUS CONCRETE CURB
- PROPOSED BITUMINOUS CONCRETE LIP CURBING
- PROPOSED BITUMINOUS CONCRETE SIDEWALK
- PROPOSED CONCRETE CURB
- PROPOSED CONCRETE SIDEWALK
- PROPOSED SAWCUT LINE
- PROPOSED STORM MANHOLE
- PROPOSED CATCH BASIN
- FULL DEPTH RECONSTRUCTION (SEE TYPICAL SECTION 1)
- FULL DEPTH RECONSTRUCTION (SEE TYPICAL SECTION 2)
- MILL & OVERLAY
- PROPOSED GRADING CUT LIMIT
- PROPOSED GRADING FILL LIMIT

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SCALE IN FEET
0 20' 40'
GRAPHIC SCALE

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DATE:	08/15/2018
FILE:	T0196-116-3&5-CP.dwg
DRAWN BY:	ALW
CHECKED:	COG
APPROVED:	DCH

CONSTRUCTION PLAN

SCALE: 1" = 20'

SHEET 3

last Saved: 11/29/2018
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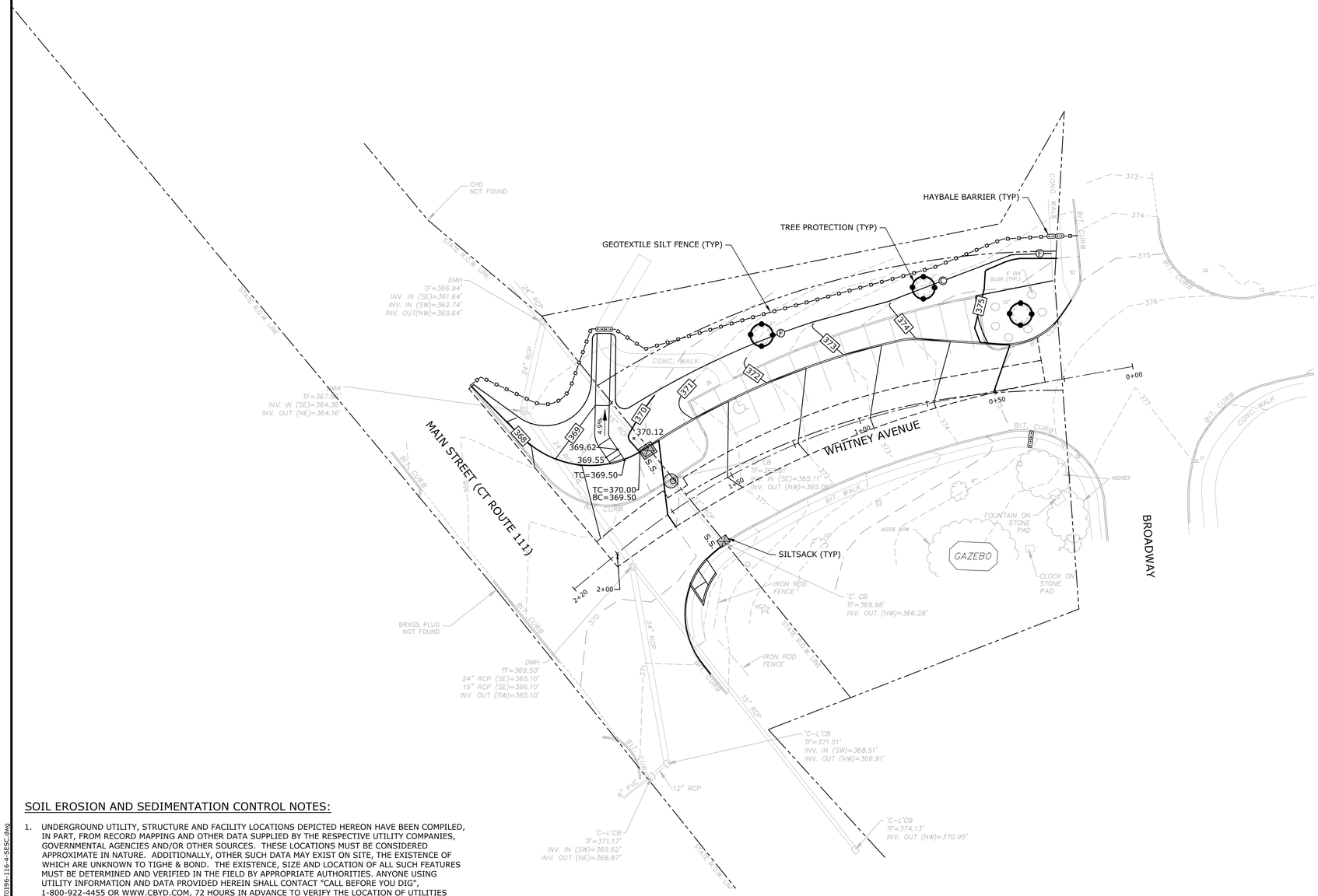

SOIL EROSION AND SEDIMENTATION CONTROL NOTES:

1. UNDERGROUND UTILITY, STRUCTURE AND FACILITY LOCATIONS DEPICTED HEREON HAVE BEEN COMPILED, IN PART, FROM RECORD MAPPING AND OTHER DATA SUPPLIED BY THE RESPECTIVE UTILITY COMPANIES, GOVERNMENTAL AGENCIES AND/OR OTHER SOURCES. THESE LOCATIONS MUST BE CONSIDERED APPROXIMATE IN NATURE. ADDITIONALLY, OTHER SUCH DATA MAY EXIST ON SITE, THE EXISTENCE OF WHICH ARE UNKNOWN TO TIGHE & BOND. THE EXISTENCE, SIZE AND LOCATION OF ALL SUCH FEATURES MUST BE DETERMINED AND VERIFIED IN THE FIELD BY APPROPRIATE AUTHORITIES. ANYONE USING UTILITY INFORMATION AND DATA PROVIDED HEREIN SHALL CONTACT "CALL BEFORE YOU DIG", 1-800-922-4455 OR WWW.CBYD.COM, 72 HOURS IN ADVANCE TO VERIFY THE LOCATION OF UTILITIES PRIOR TO STARTING CONSTRUCTION.
2. ALL SEDIMENTATION AND EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS OF THE "2002 CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL" DEP BULLETIN NO 34, AND ALL AMENDMENTS AND ADDENDA THERETO AS PUBLISHED BY THE CONNECTICUT DEPARTMENT OF ENERGY AND ENVIRONMENTAL PROTECTION.
3. LAND DISTURBANCE SHALL BE KEPT TO THE MINIMUM NECESSARY FOR CONSTRUCTION.
4. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED AS SHOWN ON THE PLANS AND ELSEWHERE AS ORDERED BY THE OWNER'S REPRESENTATIVE, OR THE TOWN OF TRUMBULL.
5. ALL CATCH BASINS SHALL BE PROTECTED WITH SILT SACKS THROUGHOUT THE CONSTRUCTION PERIOD AND UNTIL ALL DISTURBED AREAS ARE THOROUGHLY STABILIZED.
6. WHEREVER POSSIBLE, EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO CONSTRUCTION.
7. ADDITIONAL CONTROL MEASURES SHALL BE INSTALLED DURING CONSTRUCTION PERIOD AS ORDERED BY THE OWNER'S REPRESENTATIVE, OR THE TOWN OF TRUMBULL.
8. ALL SEDIMENTATION AND EROSION CONTROL MEASURES SHALL BE MAINTAINED IN EFFECTIVE CONDITION THROUGHOUT THE CONSTRUCTION PERIOD.
9. SEDIMENT REMOVED SHALL BE DISPOSED OF LEGALLY OFFSITE.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONSTRUCTION AND MAINTENANCE OF ALL EROSION CONTROL MEASURES THROUGHOUT THE CONSTRUCTION PERIOD.
11. THE CONTRACTOR SHALL MAINTAIN A SUPPLY OF SILT FENCE/HAYBALES ON-SITE FOR EMERGENCY REPAIRS.
12. THE CONTRACTOR SHALL UTILIZE APPROVED METHODS/MATERIALS FOR PREVENTING THE BLOWING AND MOVEMENT OF DUST FROM EXPOSED SOIL SURFACES ONTO ADJACENT PROPERTIES AND SITE AREAS.
13. ALL DRAINAGE STRUCTURES SHALL BE INSPECTED WEEKLY BY THE CONTRACTOR AND CLEANED.
14. KEEP ALL PAVED SURFACES CLEAN. SWEEP BEFORE FORECASTED STORMS OR WEEKLY AS NECESSARY. SWEEP IMPACTED PUBLIC ROADS OF ALL DIRT AND DEBRIS AT THE END OF EACH WORK DAY.
15. TREAT ALL UNPAVED SURFACES WITH 4" MINIMUM OF TOPSOIL AND SEEDING PRIOR TO FINAL STABILIZATION.
16. HAYBALE BARRIERS AND SILT FENCING SHALL BE INSTALLED ALONG THE TOE OF CRITICAL CUT AND FILL SLOPES AS SHOWN ON THE PLANS AND AS DIRECTED BY THE OWNER'S REPRESENTATIVE OR THE TOWN OF TRUMBULL.
17. ALL TRUCKS LEAVING THE SITE MUST BE COVERED.
18. ALL SEDIMENTATION AND EROSION CONTROLS SHALL BE CHECKED WEEKLY AND AFTER EACH RAINFALL EVENT. NECESSARY REPAIRS SHALL BE MADE WITHOUT DELAY.
19. PRIOR TO ANY FORECASTED RAINFALL, EROSION AND SEDIMENT CONTROLS SHALL BE INSPECTED AND REPAIRED AS NECESSARY.
20. AFTER ALL DISTURBED AREAS HAVE BEEN STABILIZED, EROSION CONTROLS MAY BE REMOVED ONCE AUTHORIZATION TO DO SO HAS BEEN SECURED FROM THE TOWN OF TRUMBULL. DISTURBED AREAS SHALL BE SEEDED AND MULCHED.

SITE GRADING NOTES:

21. THE GENERAL CONTRACTOR SHALL PROVIDE FIELD ENGINEERING SERVICES TO ESTABLISH AND RECORD GRADES, LINES, AND ELEVATIONS.
22. MAKE SMOOTH TRANSITIONS BETWEEN ALL SLOPE CHANGES AND FEATHER EDGES OF ALL CUTS AND FILLS TO BLEND WITH EXISTING CONDITIONS.
23. ALL EXCAVATED MATERIAL NOT REQUIRED FOR GRADING OR FILLING SHALL BE REMOVED PROMPTLY FROM THE SITE AND DISPOSED OF LEGALLY OFF-SITE PER THE PROJECT SPECIFICATIONS.
24. ANY AREAS DISTURBED BY CONSTRUCTION ACTIVITIES AND IS NOT PROVIDED WITH A SPECIFIC SITE IMPROVEMENT (PAVING, SIDEWALK, LANDSCAPING, ETC.) SHALL HAVE 4" TOPSOIL AND TURF ESTABLISHMENT IN ACCORDANCE WITH THE PROJECT LANDSCAPE SPECIFICATIONS.
25. THE LOCATIONS OF ITEMS NOT DIMENSIONED ON THE DRAWINGS SHALL BE FIELD STAKED BY THE CONTRACTOR AND THEIR LOCATIONS APPROVED BY THE OWNER'S REPRESENTATIVE PRIOR TO INSTALLATION.

SOIL EROSION AND SEDIMENT CONTROL AND GRADING PLAN LEGEND:

- 47 - MINOR CONTOUR
- 50 - MAJOR CONTOUR
- * 370.00 - SPOT ELEVATION
- S.S. - SILT SACK
- -o-o-o-o-o-o- - GEOTEXTILE SILT FENCE
- HAYBALE BARRIER
- TREE PROTECTION
- C - PROPOSED GRADING CUT LIMIT
- F - PROPOSED GRADING FILL LIMIT

N

SCALE IN FEET
0 20' 40'
GRAPHIC SCALE

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Town of Trumbull

Whitney Avenue at Main Street Intersection Improvements

Trumbull, CT

MARK	DATE	DESCRIPTION
4	11/29/2018	TOWN COMMENTS
3	8/30/2018	CTDOT COMMENTS
2	8/15/2018	CTDOT COMMENTS
1	7/2/2018	CTDOT & TOWN COMMENTS

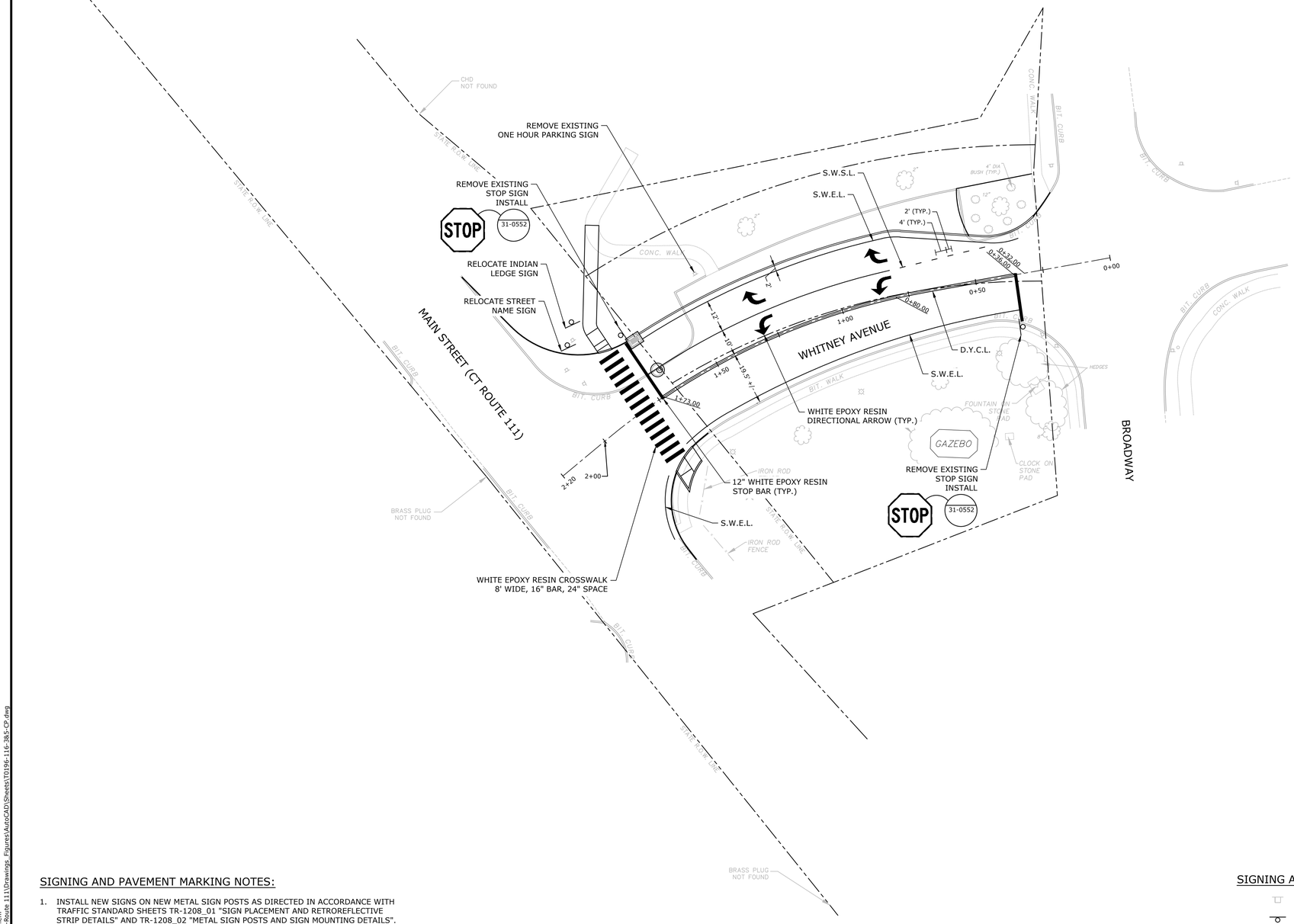
PROJECT NO:	T0196-116
DATE:	08/15/2018
FILE:	T0196-116-4-SESC.dwg
DRAWN BY:	ALW
CHECKED:	COG
APPROVED:	DCH

GRADING & SEDIMENTATION AND EROSION CONTROL PLAN

SCALE: 1" = 20'

SHEET 4

Last Saved: 11/29/2018
Last Plotted: 11/29/2018 2:13pm By: sansonem
Tighe & Bond :\T0196\116 Whitney Ave-Route 111\Drawings Figures\AutoCAD\Sheets\T0196-116-4-SESC.dwg



SIGNING AND PAVEMENT MARKING NOTES:

1. INSTALL NEW SIGNS ON NEW METAL SIGN POSTS AS DIRECTED IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208_01 "SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS" AND TR-1208_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
2. EXACT LOCATIONS OF THE SIGNS TO BE VERIFIED BY THE ENGINEER FOLLOWING LOCATION MARKING BY CONTRACTOR. THE CONTRACTOR SHALL ASSURE ADEQUATE SIGHTLINE IS PROVIDED TO SIGNS. ANY CLEARING OR TRIMMING NEEDED TO PROVIDE SIGHTLINE SHALL BE INCLUDED IN CLEARING AND GRUBBING.
3. RESET EXISTING SIGNS DISTURBED BY CONSTRUCTION.
4. SIGNS SHALL BE AT LEAST 10 FEET FROM ALL UTILITY POLES.
5. ALL PAVEMENT MARKINGS TO BE INSTALLED IN ACCORDANCE WITH STANDARD SHEETS TR-1210_04 "PAVEMENT MARKING LINES AND SYMBOLS" AND TR-1210_08 "PAVEMENT MARKINGS FOR NON FREEWAYS".

SIGNING AND PAVEMENT MARKING LEGEND:

- EXISTING GROUND MOUNTED SIGN
- PROPOSED SIGN (GROUND MOUNTED)
- PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
- XX-XXXX - PROPOSED SIGNS BY CONNDOT SIGN NUMBER
- S.W.E.L. - SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
- D.Y.C.L. - DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- S.W.S.L. - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING

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SCALE IN FEET
0 20' 40'
GRAPHIC SCALE

Town of Trumbull

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Trumbull, CT

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PROJECT NO:	T0196-116
DATE:	08/15/2018
FILE:	T0196-116-3&5-CP.dwg
DRAWN BY:	ALW
CHECKED:	COG
APPROVED:	DCH

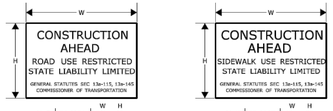
SIGNING AND PAVEMENT MARKING PLAN

SCALE: 1" = 20'

SHEET 5

Last Saved: 11/29/2018
Last Plotted: Nov 29, 2018 - 2:14pm By: sansonem
Tighe & Bond: J:\T\T0196\116 Whitney Ave-Route 111\Drawings, Figures\AutoCAD\Sheets\T0196-116-3&5-CP.dwg

SERIES 16 SIGNS



		W	H
16-E	80-1605	84"	60"
16-H	80-1608	60"	42"
16-M	80-1613	30"	24"

		W	H
16-S	80-1619	48"	30"

THE 16-S SIGN SHALL BE USED ON ALL PROJECTS THAT REQUIRE SIDEWALK RECONSTRUCTION OR RESTRICT PEDESTRIAN TRAVEL ON AN EXISTING SIDEWALK.

SERIES 16 SIGNS SHALL BE INSTALLED IN ADVANCE OF THE TRAFFIC CONTROL PATTERNS TO ALLOW MOTORISTS THE OPPORTUNITY TO AVOID A WORK ZONE. SERIES 16 SIGNS SHALL BE INSTALLED ON ANY MAJOR INTERSECTING ROADWAYS THAT APPROACH THE WORK ZONE. ON LIMITED-ACCESS HIGHWAYS, THESE SIGNS SHALL BE LOCATED IN ADVANCE OF THE NEAREST UPSTREAM EXIT RAMP AND ON ANY ENTRANCE RAMPS PRIOR TO OR WITHIN THE WORK ZONE LIMITS.

THE LOCATION OF SERIES 16 SIGNS CAN BE FOUND ELSEWHERE IN THE PLANS OR INSTALLED AS DIRECTED BY THE ENGINEER.

SIGNS 16-E AND 16-H SHALL BE POST-MOUNTED.

SIGN 16-E SHALL BE USED ON ALL EXPRESSWAYS.

SIGN 16-H SHALL BE USED ON ALL RAMPS, OTHER STATE ROADWAYS, AND MAJOR TOWN/CITY ROADWAYS.

SIGN 16-M SHALL BE USED ON OTHER TOWN ROADWAYS.

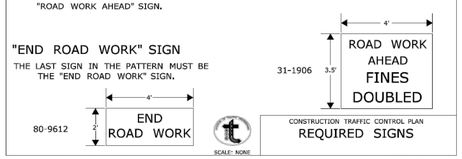
REGULATORY SIGN "ROAD WORK AHEAD, FINES DOUBLED"

THE REGULATORY SIGN "ROAD WORK AHEAD FINES DOUBLED" SHALL BE INSTALLED FOR ALL WORK ZONES THAT OCCUR ON ANY STATE HIGHWAY IN CONNECTICUT WHERE THERE ARE WORKERS ON THE HIGHWAY OR WHEN THERE IS OTHER THAN EXISTING TRAFFIC OPERATIONS.

THE "ROAD WORK AHEAD FINES DOUBLED" REGULATORY SIGN SHALL BE PLACED AFTER THE SERIES 16 SIGN AND IN ADVANCE OF THE "ROAD WORK AHEAD" SIGN.

"END ROAD WORK" SIGN

THE LAST SIGN IN THE PATTERN MUST BE THE "END ROAD WORK" SIGN.



SCALE: NONE

CONSTRUCTION TRAFFIC CONTROL PLAN
REQUIRED SIGNS

CONNECTICUT DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING & CONSTRUCTION

APPROVED Charles S. Harlow 2015.08.05 11:20:43-04'00' Charles S. Harlow
PRINCIPAL ENGINEER

ITEM #971001A

NOTES FOR TRAFFIC CONTROL PLANS

1. IF A TRAFFIC STOPPAGE OCCURS IN ADVANCE OF SIGN (A), THEN AN ADDITIONAL SIGN (A) SHALL BE INSTALLED IN ADVANCE OF THE STOPPAGE.
2. SIGNS (AA), (A), AND (D) SHOULD BE OMITTED WHEN THESE SIGNS HAVE ALREADY BEEN INSTALLED TO DESIGNATE A LARGER WORK ZONE THAN THE WORK ZONE THAT IS ENCOMPASSED ON THIS PLAN.
3. SEE TABLE 1 FOR ADJUSTMENT OF TAPERS IF NECESSARY.
4. IF THIS PLAN REMAINS IN CONTINUOUS OPERATION FOR MORE THAN 36 HOURS, THEN TRAFFIC DRUMS SHALL BE USED IN PLACE OF TRAFFIC CONES.
5. ANY LEGAL SPEED LIMIT SIGNS WITHIN THE LIMITS OF A ROADWAY / LANE CLOSURE AREA SHALL BE COVERED WITH AN OPAQUE MATERIAL WHILE THE CLOSURE IS IN EFFECT, AND UNCOVERED WHEN THE ROADWAY / LANE CLOSURE IS RE-OPENED TO ALL LANES OF TRAFFIC.
6. IF THIS PLAN REMAINS IN CONTINUOUS OPERATION FOR MORE THAN 36 HOURS, THEN ANY EXISTING CONFLICTING PAVEMENT MARKINGS SHALL BE ERADICATED OR COVERED, AND TEMPORARY PAVEMENT MARKINGS THAT DELINEATE THE PROPER TRAVELPATHS SHALL BE INSTALLED.
7. DISTANCES BETWEEN SIGNS IN THE ADVANCE WARNING AREA MAY BE REDUCED TO 100' ON LOW-SPEED URBAN ROADS (SPEED LIMIT < 40 MPH).
8. IF THIS PLAN IS TO REMAIN IN OPERATION DURING THE HOURS OF DARKNESS, INSTALL BARRICADE WARNING LIGHTS - HIGH INTENSITY ON ALL POST-MOUNTED DIAMOND SIGNS IN THE ADVANCE WARNING AREA.
9. A CHANGEABLE MESSAGE SIGN SHALL BE INSTALLED ONE HALF TO ONE MILE IN ADVANCE OF THE LANE CLOSURE TAPER.
10. SIGN (P) SHALL BE MOUNTED A MINIMUM OF 7 FEET FROM THE PAVEMENT SURFACE TO THE BOTTOM OF THE SIGN.

TABLE 1 - MINIMUM TAPER LENGTHS

POSTED SPEED LIMIT (MILES PER HOUR)	MINIMUM TAPER LENGTH FOR A SINGLE LANE CLOSURE
30 OR LESS	180' (55m)
35	250' (75m)
40	320' (100m)
45	540' (165m)
50	600' (180m)
55	660' (200m)
65	780' (240m)

METRIC CONVERSION CHART (1" = 25mm)

ENGLISH	METRIC	ENGLISH	METRIC	ENGLISH	METRIC
12"	300mm	42"	1050mm	72"	1800mm
18"	450mm	48"	1200mm	78"	1950mm
24"	600mm	54"	1350mm	84"	2100mm
30"	750mm	60"	1500mm	90"	2250mm
36"	900mm	66"	1650mm	96"	2400mm

SCALE: NONE

CONSTRUCTION TRAFFIC CONTROL PLAN
NOTES

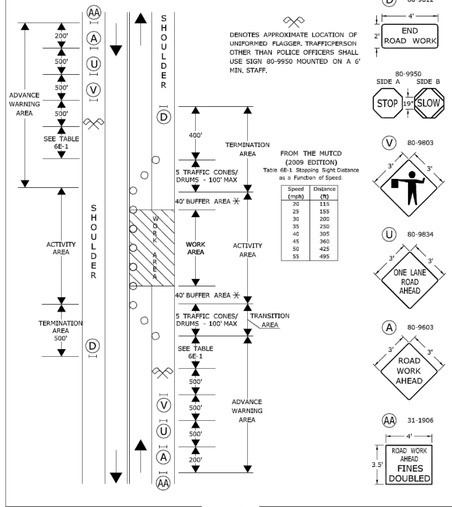
CONNECTICUT DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING & CONSTRUCTION

APPROVED Charles S. Harlow 2015.08.05 15:50:06-04'00' Charles S. Harlow
PRINCIPAL ENGINEER

ITEM #971001A

WORK IN TRAVEL LANE AND SHOULDER TWO LANE HIGHWAY ALTERNATING ONE-WAY TRAFFIC OPERATIONS

SIGN FACE 108 SQ. FT (MIN.)



○ TRAFFIC CONE **OR** TRAFFIC DRUM
✳ OPTIONAL ⊗ TRAFFIC DRUM ⟼ PORTABLE SIGN SUPPORT
⟸ HIGH MOUNTED INTERNALLY ILLUMINATED FLASHING ARROW

SCALE: NONE

CONSTRUCTION TRAFFIC CONTROL PLAN
PLAN 13 - SHEET 1 OF 2
SEE NOTES 1, 2, 4, 6, 7, 8

CONNECTICUT DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING & CONSTRUCTION

APPROVED Charles S. Harlow 2012.08.05 15:05:20-04'00' Charles S. Harlow
PRINCIPAL ENGINEER

ITEM #971001A

WORK IN TRAVEL LANE AND SHOULDER TWO LANE HIGHWAY ALTERNATING ONE-WAY TRAFFIC OPERATIONS

SIGN FACE 108 SQ. FT (MIN.)

HAND SIGNAL METHODS TO BE USED BY UNIFORMED FLAGGERS

THE FOLLOWING METHODS FROM SECTION 6E.07, FLAGGER PROCEDURES, IN THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES," SHALL BE USED BY UNIFORMED FLAGGERS WHEN DIRECTING TRAFFIC THROUGH A WORK AREA. THE STOP/SLOW SIGN PADDLE (SIGN NO. 80-9950) SHOWN ON THE TRAFFIC STANDARD SHEET TR-1220 01 ENTITLED, "SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS" SHALL BE USED.

A. TO STOP TRAFFIC

TO STOP ROAD USERS, THE FLAGGER SHALL FACE ROAD USERS AND AIM THE STOP PADDLE FACE TOWARD ROAD USERS IN A STATIONARY POSITION WITH THE ARM EXTENDED HORIZONTALLY AWAY FROM THE BODY. THE FREE ARM SHALL BE HELD WITH THE PALM OF THE HAND ABOVE SHOULDER LEVEL TOWARD APPROACHING TRAFFIC.

B. TO DIRECT TRAFFIC TO PROCEED

TO DIRECT STOPPED ROAD USERS TO PROCEED, THE FLAGGER SHALL FACE ROAD USERS WITH THE SLOW PADDLE FACE AIMED TOWARD ROAD USERS IN A STATIONARY POSITION WITH THE ARM EXTENDED HORIZONTALLY AWAY FROM THE BODY. THE FLAGGER SHALL MOTION WITH THE FREE HAND FOR ROAD USERS TO PROCEED.

C. TO ALERT OR SLOW TRAFFIC

TO ALERT OR SLOW TRAFFIC, THE FLAGGER SHALL FACE ROAD USERS WITH THE SLOW PADDLE FACE AIMED TOWARD ROAD USERS IN A STATIONARY POSITION WITH THE ARM EXTENDED HORIZONTALLY AWAY FROM THE BODY. TO FURTHER ALERT OR SLOW TRAFFIC, THE FLAGGER HOLDING THE SLOW PADDLE FACE TOWARD ROAD USERS MAY MOTION UP AND DOWN WITH THE FREE HAND, PALM DOWN.

○ TRAFFIC CONE **OR** TRAFFIC DRUM
✳ OPTIONAL ⊗ TRAFFIC DRUM ⟼ PORTABLE SIGN SUPPORT
⟸ HIGH MOUNTED INTERNALLY ILLUMINATED FLASHING ARROW

SCALE: NONE

CONSTRUCTION TRAFFIC CONTROL PLAN
PLAN 13 - SHEET 2 OF 2
SEE NOTES 1, 2, 4, 6, 7, 8

CONNECTICUT DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING & CONSTRUCTION

APPROVED Charles S. Harlow 2012.08.05 15:05:45-04'00' Charles S. Harlow
PRINCIPAL ENGINEER

ITEM #971001A

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Town of Trumbull

Whitney Avenue at Main Street Intersection Improvements

Trumbull, CT

MARK	DATE	DESCRIPTION
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2	8/15/2018	CTDOT COMMENTS
1	7/2/2018	CTDOT & TOWN COMMENTS

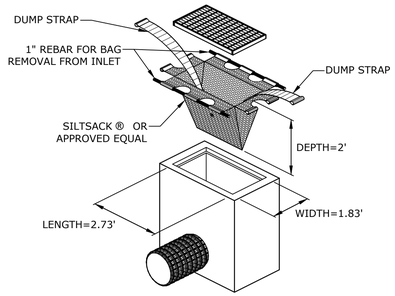
PROJECT NO:	T0196-116
DATE:	08/15/2018
FILE:	T0196-116-6-MPT.dwg
DRAWN BY:	ALW
CHECKED:	COG
APPROVED:	DCH

MAINTENANCE AND PROTECTION OF TRAFFIC DETAILS

SCALE: NO SCALE

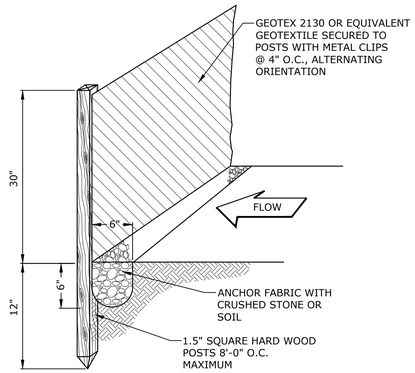
SHEET 6

Last Saved: 4/2/2018
Printed: 08/15/2018, 2:14pm By: sansonem
Tighe & Bond: J:\TT\T0196\116 Whitney Ave-Route 111\Drawings, Figures\AutoCAD\Sheets\T0196-116-6-MPT.dwg

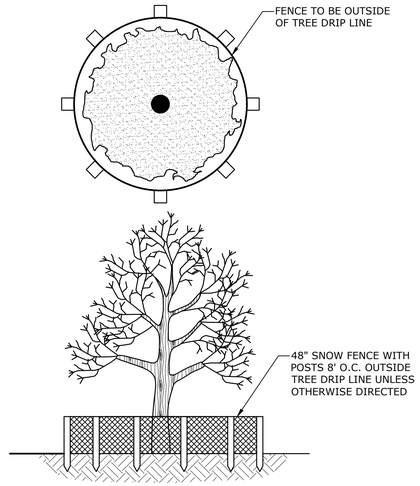


SILTSACK MANUFACTURED BY:
ACF ENFIRONMENTAL
2831 CARDWELL ROAD
RICHMOND, VIRGINIA 23237

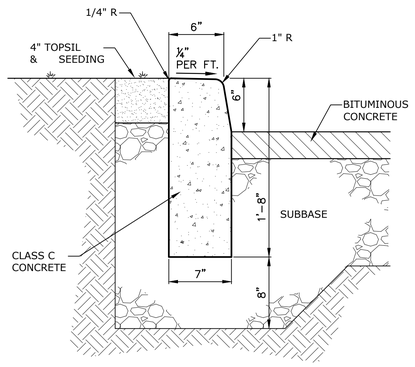
SILTSACK®
NO SCALE



SILT FENCE
NO SCALE



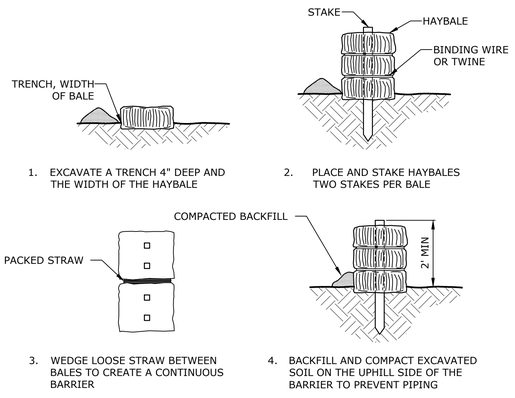
TREE PROTECTION
NO SCALE



NOTES:

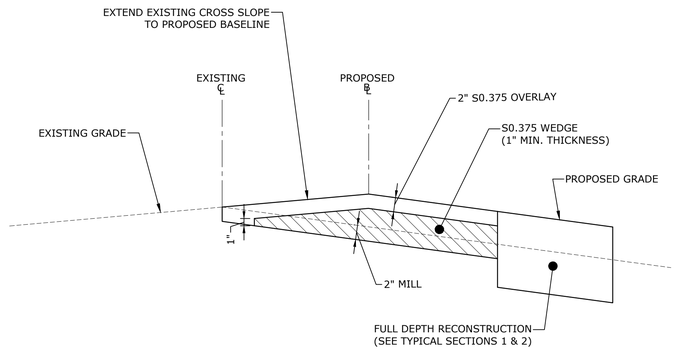
1. CONSTRUCT CURBING IN SECTIONS NOT TO EXCEED 10 FEET IN LENGTH, SUCH THAT THE CURBING JOINTS ALIGN WITH JOINTS IN THE CONCRETE PAVEMENT SLAB. NO SECTION SHALL BE LESS THAN 6 FEET IN LENGTH.

CONCRETE CURB ADJACENT TO GRASS
NO SCALE



1. EXCAVATE A TRENCH 4" DEEP AND THE WIDTH OF THE HAYBALE
2. PLACE AND STAKE HAYBALES TWO STAKES PER BALE
3. WEDGE LOOSE STRAW BETWEEN BALES TO CREATE A CONTINUOUS BARRIER
4. BACKFILL AND COMPACT EXCAVATED SOIL ON THE UPHILL SIDE OF THE BARRIER TO PREVENT PIPING

PLACEMENT AND CONSTRUCTION
OF HAYBALE BARRIER
NO SCALE



PAVEMENT WEDGE
NO SCALE

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Town of Trumbull

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Trumbull, CT

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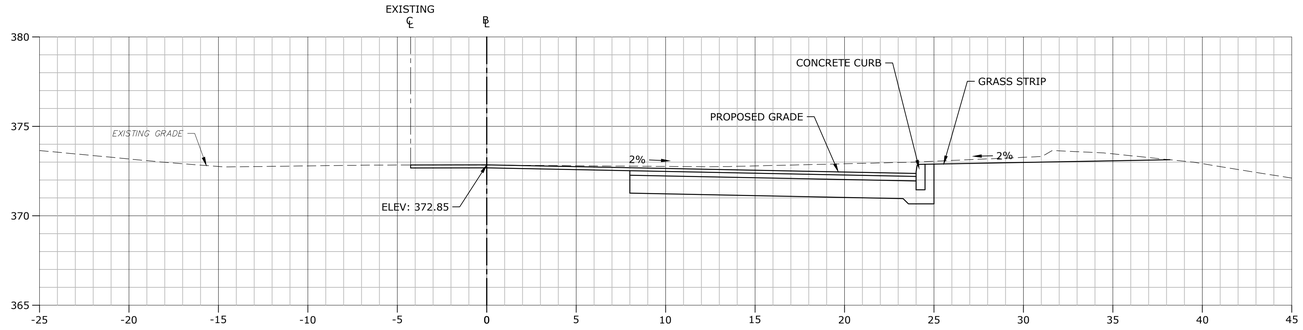
PROJECT NO:	T0196-116
DATE:	08/15/2018
FILE:	T0196-116-7-DS.dwg
DRAWN BY:	ALW
CHECKED:	COG
APPROVED:	DCH

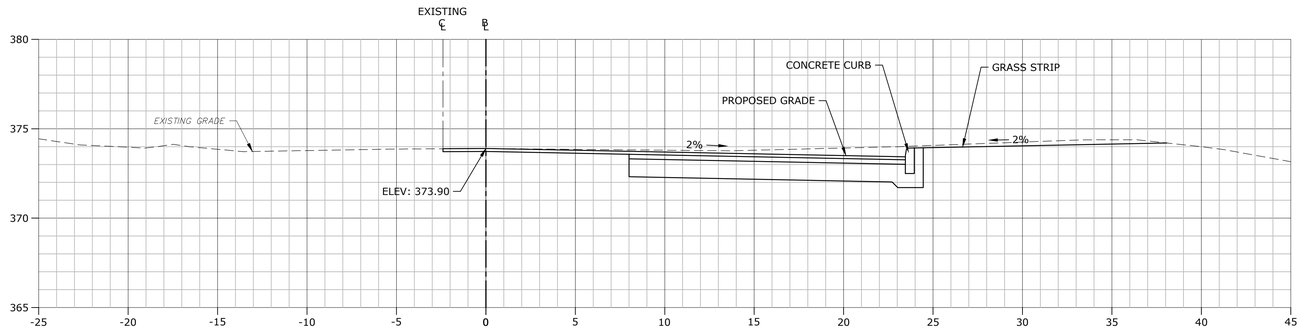
DETAIL SHEET

SCALE: NO SCALE

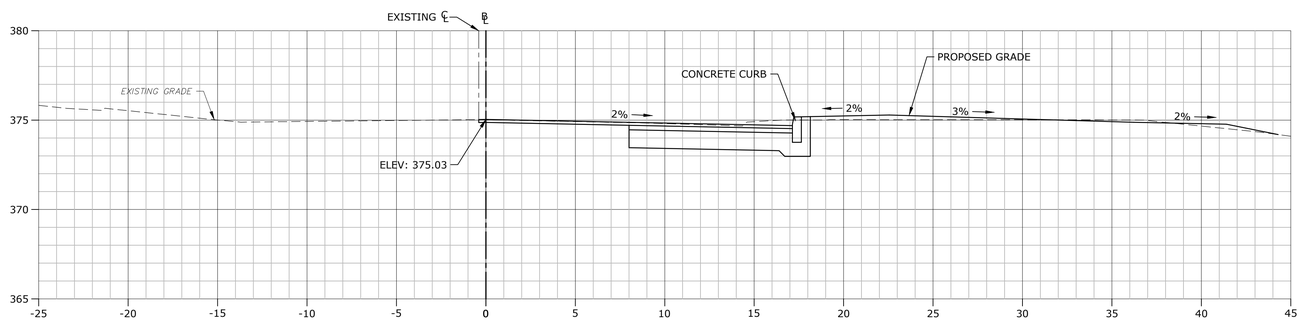
SHEET 7



STATION: 1+00



STATION: 0+75



STATION: 0+50

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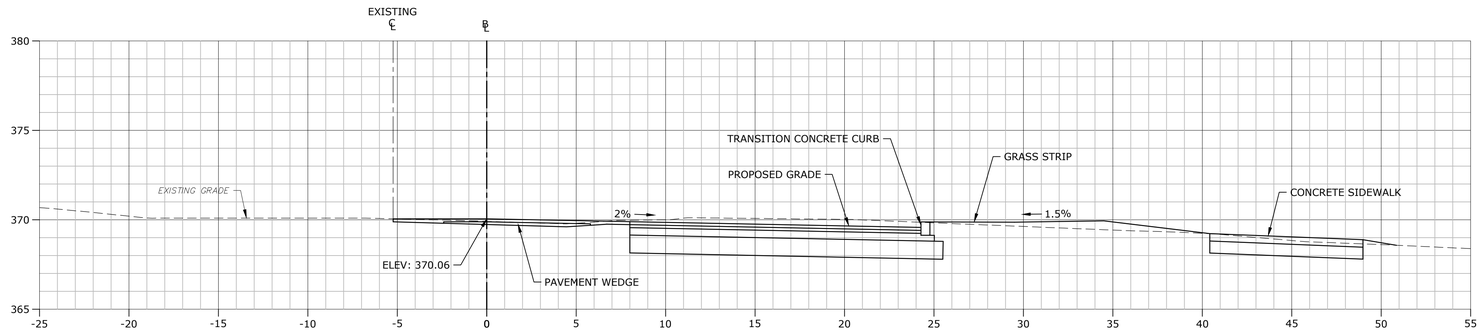
PROJECT NO:	T0196-116
DATE:	08/15/2018
FILE:	T0196-116-8&9-XS.dwg
DRAWN BY:	ALW
CHECKED:	COG
APPROVED:	DCH

CROSS SECTIONS
STA 0+50 TO 1+00

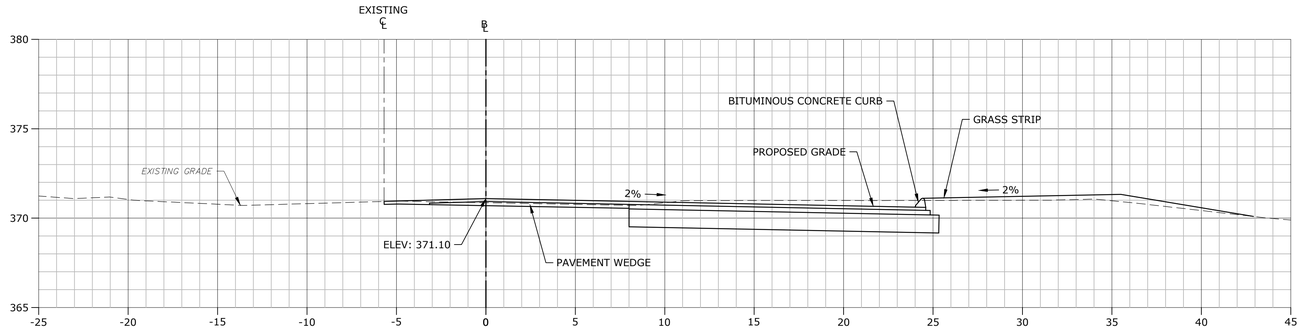
SCALE: 1" = 4'

SHEET 8

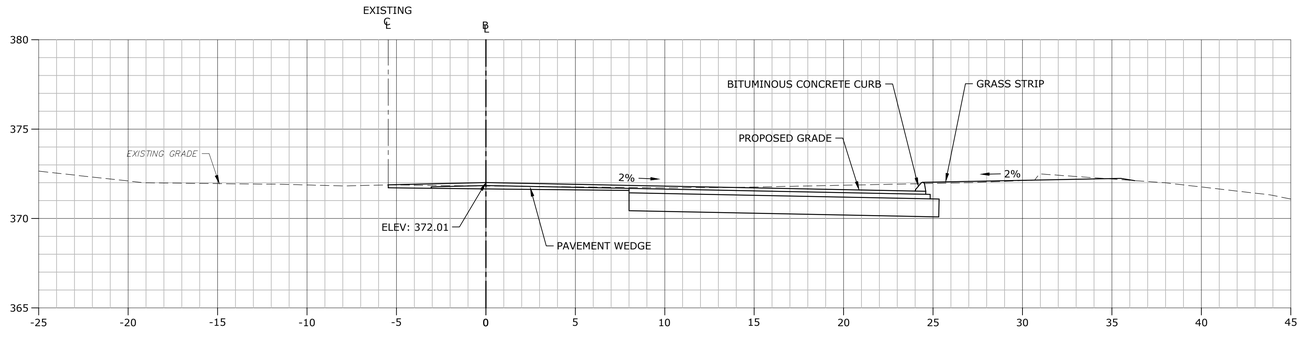
Last Saved: 11/29/2018
Plotted: 11/29/2018 2:14pm By: sansonem
Tighe & Bond: J:\T0196\116 Whitney Ave-Route 111\Drawings, Figures\AutoCAD\Sheets\T0196-116-8&9-XS.dwg



STATION: 1+75



STATION: 1+50



STATION: 1+25

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PROJECT NO:	T0196-116
DATE:	08/15/2018
FILE:	T0196-116-889-XS.dwg
DRAWN BY:	ALW
CHECKED:	COG
APPROVED:	DCH

CROSS SECTIONS
STA 1+25 TO 1+75

SCALE: 1" = 4'

SHEET 9

Last Saved: 11/29/2018
Plotted On: Nov 29, 2018 - 2:14pm By: sansonem
Tighe & Bond: J:\T0196\116_Whitney Ave-Route 111\Drawings_Figures\AutoCADSheets\T0196-116-889-XS.dwg

SU-40 SBL

SU-40 WBL

SU-40 WBR

SU-40 NBR

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SCALE IN FEET
0 20' 40'
GRAPHIC SCALE

Town of Trumbull

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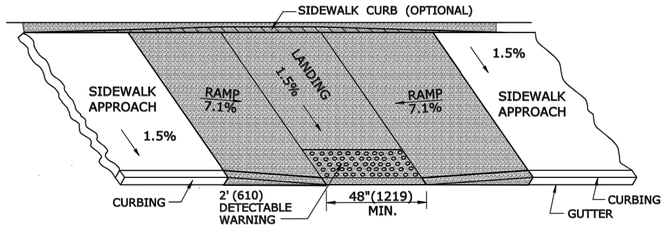
PROJECT NO:	T0196-116
DATE:	08/15/2018
FILE:	T0196-116-10-TUR.dwg
DRAWN BY:	ALW
CHECKED:	COG
APPROVED:	DCH

TRUCK TURN MOVEMENTS

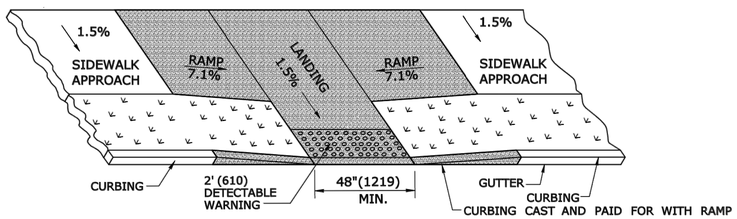
SCALE: 1" = 20'

SHEET 10

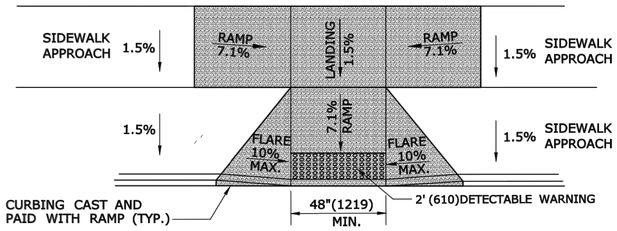
Last Saved: 5/16/2018
Plotted: Nov 29, 2018 - 2:15pm By: sansonem
Tighe & Bond: J:\T\T0196\116 Whitney Ave-Route 111\Drawings, Figures\AutoCAD\Sheets\T0196-116-10-TUR.dwg



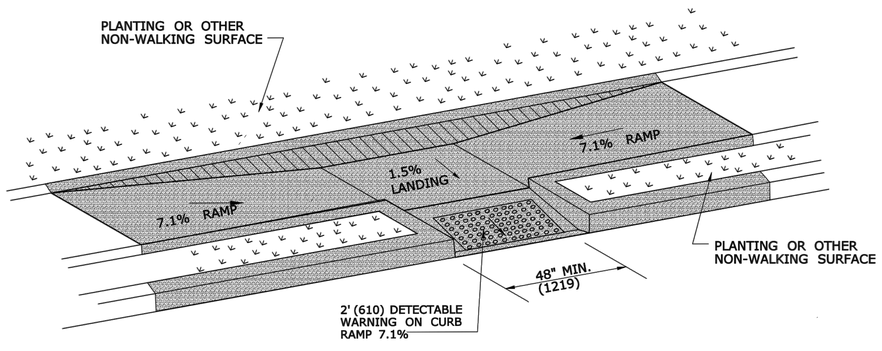
PARALLEL SIDEWALK RAMP (TYPE 1) NO UTILITY STRIP



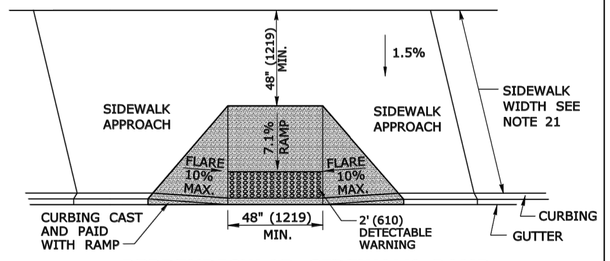
PARALLEL SIDEWALK RAMP (TYPE 1a) WITH UTILITY / GRASS STRIP



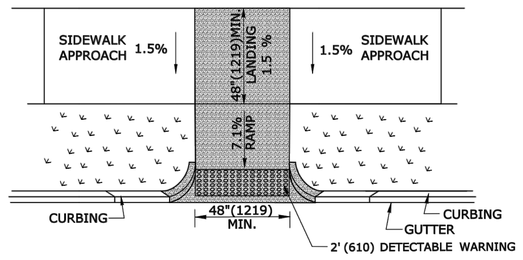
PARALLEL/PERPENDICULAR SIDEWALK RAMP NO UTILITY/GRASS STRIP (TYPE 1b)



PARALLEL SIDEWALK RAMP (TYPE 1c) WITH UTILITY / GRASS STRIP

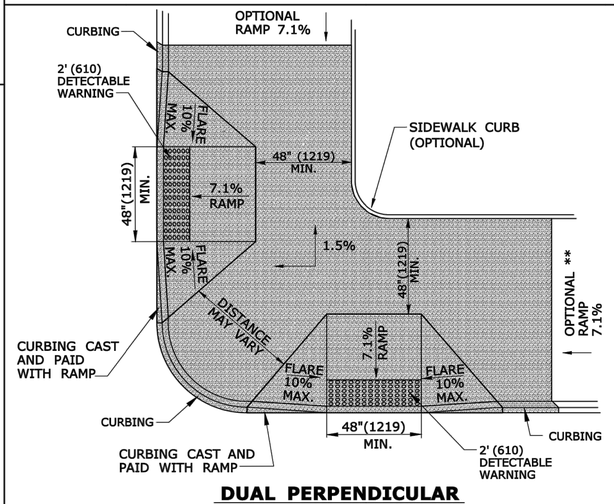


PERPENDICULAR SIDEWALK RAMP W/ 48" (1219) MIN. BY PASS LANDING (TYPE 2)



PERPENDICULAR SIDEWALK RAMP W/CURB RETURNS / UTILITY GRASS STRIP (TYPE 2a)

* OPTIONAL FLARE ONE SIDE OF RAMP

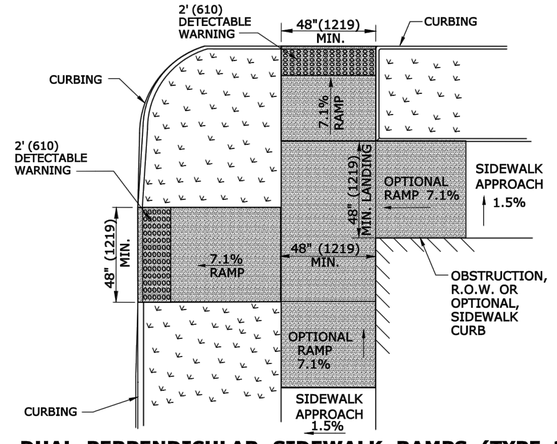


DUAL PERPENDICULAR SIDEWALK RAMPS (TYPE 3)

SEE NOTES 19

* OPTIONAL CURB RETURN ON ONE SIDE OF RAMP

** SEE NOTE 23



DUAL PERPENDICULAR SIDEWALK RAMPS (TYPE 3a) WITH UTILITY / GRASS STRIP

SEE NOTE 20

GENERAL NOTES:

1. MAXIMUM SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE SIDEWALK RAMP SHOULD NOT EXCEED 5%. THE MAXIMUM GRADE DIFFERENCE BETWEEN THE GUTTER AND CURB RAMP SHALL NOT EXCEED 13%. SEE DETAIL 1 ON SHEET 4.
2. RAMP GRADE SHALL BE UNIFORM, FREE OF SAGS AND ABRUPT GRADE CHANGES. RUNNING SLOPES OF RAMPS SHALL NOT EXCEED 8.33% AND SHALL NOT EXCEED 15' (4.5m) WITHOUT PROVIDING A LANDING.
3. ALL RAMPS SHALL BE CONSTRUCTED OF CLASS "F" CONCRETE IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
4. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP. THE SURFACE OF ALL SIDEWALK RAMPS SHALL BE STABLE, FIRM AND SLIP RESISTANT. SURFACE DISCONTINUITIES SHALL NOT EXCEED ½" (13) MAX. VERTICAL DISCONTINUITIES BETWEEN ¼" (6.4) AND ½" (13) MAX. SHALL BE BEVELED 1:2 MINIMUM APPLIED ACROSS THE ENTIRE LEVEL CHANGE.
5. DIAGONAL SIDEWALK RAMPS AT MARKED CROSSINGS SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS, EXCLUDING ANY FLARED SIDES. DIAGONAL AND PERPENDICULAR RAMPS SHALL HAVE THE RAMP CUT PERPENDICULAR TO THE TANGENT OF THE CURB RADIUS FOR THE DESIGNATED ACCESSIBLE ROUTE. BOTH LONGITUDINAL SIDES OF THE RAMP SHOULD BE THE SAME LENGTH. SKEWED RAMPS SHOULD BE AVOIDED. FLARES ARE NOT CONSIDERED PART OF PEDESTRIAN ACCESS ROUTE. DIAGONAL RAMPS SHOULD NOT BE INSTALLED WHERE CURB RADII IS LESS THAN 20'(6096).
6. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT. 8.3% MAXIMUM SLOPE MAY NOT BE ACHIEVABLE DUE TO EXISTING SIDEWALK GRADE. IN RECOGNITION OF THIS, A LIMIT OF 15' (4572) FOR REMOVAL SHALL BE USED UNLESS OTHERWISE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER. SAW CUT REQUIRED FOR DUMMY JOINTS SHALL BE INCLUDED IN THE COST OF "CONCRETE SIDEWALK RAMP" OR "CONCRETE SIDEWALK".
7. EXPANSION JOINTS IN CONCRETE SHALL MATCH THOSE IN ADJACENT SIDEWALKS BUT IN NO CASE SHALL THE SPACING BETWEEN EXPANSION JOINTS EXCEED 12' (3658) UNLESS OTHERWISE NOTED.
8. CONCRETE SIDEWALK RAMPS, SHALL BE PAID FOR UNDER THE ITEM "CONCRETE SIDEWALK RAMP", AS DEFINED BY THE CONSTRUCTION LIMITS ON THE PLANS AND SHALL BE FIELD VERIFIED.
9. SIDEWALK RAMPS SHALL BE CONSTRUCTED WITH THE TOE AT THE GUTTER CAST INTEGRALLY WITH RAMP UNLESS DIRECTED OTHERWISE BY THE ENGINEER (SEE TYPICAL SECTION ON SHEET 3). CURB REMOVAL AND CAST IN PLACE CURBING REQUIRED FOR THE RAMP, SHALL BE INCLUDED WITH PAY ITEM "CONCRETE SIDEWALK RAMP". CURBING OUTSIDE LIMITS OF RAMP OR LANDING SHOWN ON SHEET 3 SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
10. PREFERRED LOCATION TO INSTALL DETECTABLE WARNING STRIP SHALL BE 6" (152) FROM THE EDGE OF ROAD ALONG THE FULL WIDTH OF THE RAMP. FOR ALTERNATE LOCATIONS, REFER TO DETECTABLE WARNING PLACEMENT DETAILS ON SHEET 4.
11. TO PERMIT WHEELCHAIR WHEELS TO ROLL BETWEEN DOMES, ALIGN DOMES ON A SQUARE GRID IN THE DIRECTION OF RUNNING SLOPE (PERPENDICULAR TO CURB OR SLOPE BREAK). THE TRANSITION FROM RAMP TO GUTTER SHALL BE FLUSH WITHOUT A LIP.
12. WHERE COMMERCIAL DRIVEWAYS ARE PROVIDED WITH TRAFFIC SIGNALS AND THE SIDEWALK IS CONTINUOUS THROUGH DRIVEWAY, DETECTABLE WARNINGS ARE REQUIRED AT THE JUNCTION BETWEEN THE PEDESTRIAN ROUTE AND DRIVEWAY.
13. CONSTRUCT A SIDEWALK CURB WHEN THERE IS INSUFFICIENT BUFFER AVAILABLE TO GRADE OR WHEN CALLED FOR IN PLANS. PAID FOR WITH SIDEWALK RAMP WHEN REQUIRED FOR RAMP.
14. THE TOP AND BOTTOM OF RAMPS SHOULD BE PROVIDED WITH A 4'x 4'(1219 x 1219) MINIMUM LEVEL LANDING AREA WITH A CROSS SLOPE LESS THAN OR EQUAL TO 2% IN ANY DIRECTION.
15. UTILITY POLES, LUMINAIRE, PEDESTRIAN OR SIGNAL POLES, GRATES, ACCESS COVERS, AND OTHER APPURTENANCES SHALL NOT BE LOCATED ON RAMPS, LANDINGS, BLENDED TRANSITIONS, AND @ GUTTERS WITHIN THE PEDESTRIAN ACCESS ROUTE.
16. APPROACH SIDEWALK WIDTHS, GRASS STRIP OR UTILITY STRIP WIDTHS MAY VARY.
17. APPROACH SIDEWALK AND LANDING CROSS SLOPE SHALL NOT EXCEED 2%.
18. THE RUNNING OR CROSS SLOPES ON LANDINGS AT MID BLOCK CROSSING MAY BE WARPED TO MEET STREET OR HIGHWAY GRADE.
19. FOR PERPENDICULAR CURB RAMPS A MIN. 4'(1.2m) x 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE TOP OF CURB RAMP. WHERE THE LEVEL LANDING IS RESTRICTED AT THE BACK OF SIDEWALK THE LEVEL LANDING SHALL BE 4'(1.2m) x 5'(1.5m) WITH THE 5'(1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE RAMP RUN.
20. FOR PARALLEL CURB RAMPS, A MIN. 4'(1.2m) x 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE BOTTOM OF CURB RAMP. IF THE LEVEL LANDING IS RESTRICTED ON 2 OR MORE SIDES, THE LEVEL LANDING SHALL BE 4'(1.2m)x 5'(1.5m) WITH THE 5'(1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE PEDESTRIAN STREET CROSSING.
21. WHEN WIDTH OF SIDEWALK IS ≥48" AND A PERPENDICULAR SIDEWALK RAMP IS INSTALLED, THE FLARED SIDES SHALL BE 10% MAX. IF WIDTH OF SIDEWALK IS <48" THE FLARED SIDES MUST NOT EXCEED 8.33% (12:1).
22. SHADED AREAS ARE TYPICAL PAY LIMITS FOR CONCRETE SIDEWALK RAMP BUT, MAY VARY AS DIRECTED BY THE ENGINEER.
23. OPTIONAL RAMP, WHEN REQUIRED, SHALL BE PAID FOR AS PART OF CONCRETE SIDEWALK RAMP.

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
-	-	-	-
-	-	-	-
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-	-	-	-
1	7/13	Created new sheets (4 total).	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 6/17/2014

DESIGNER/DRAFTER: MGB/EMK

CHECKED BY: LLF

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\SIDEWALK RAMP 1_GD.dgn

SIGNATURE/BLOCK: OFFICE OF ENGINEERING

APPROVED BY: Tighe&Bond www.tighebond.com

PROJECT TITLE: WHITNEY AVENUE AT MAIN STREET (ROUTE 111) INTERSECTION IMPROVEMENTS

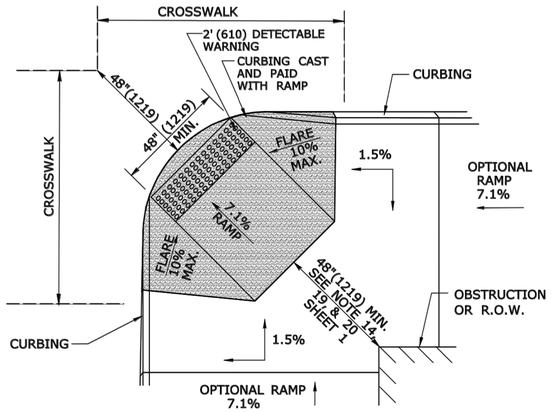
TOWN: TRUMBULL

DRAWING TITLE: SIDEWALK RAMPS SHEET 1

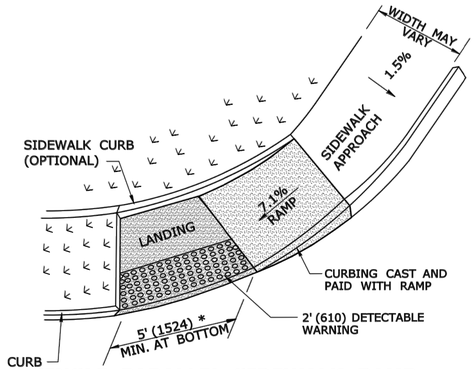
PROJECT NO. T0196-116

DRAWING NO.

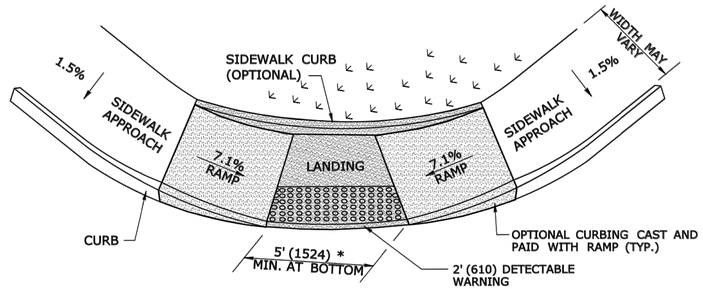
SHEET NO. SHEET 11



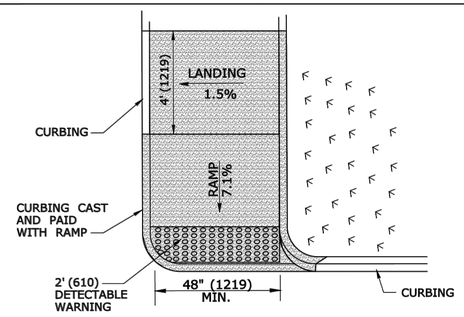
DIAGONAL SIDEWALK RAMP (TYPE 4)
W/LANDING AT TOP



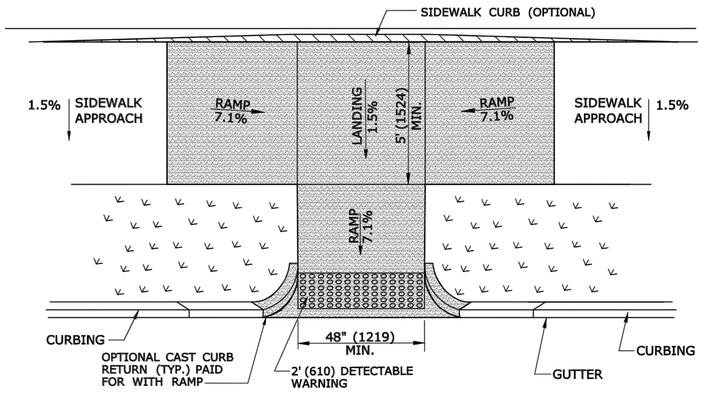
SINGLE PARALLEL SIDEWALK RAMP
W/LANDING AT BOTTOM ON
CORNER (TYPE 4c)
* SEE NOTE 20 SHEET 1



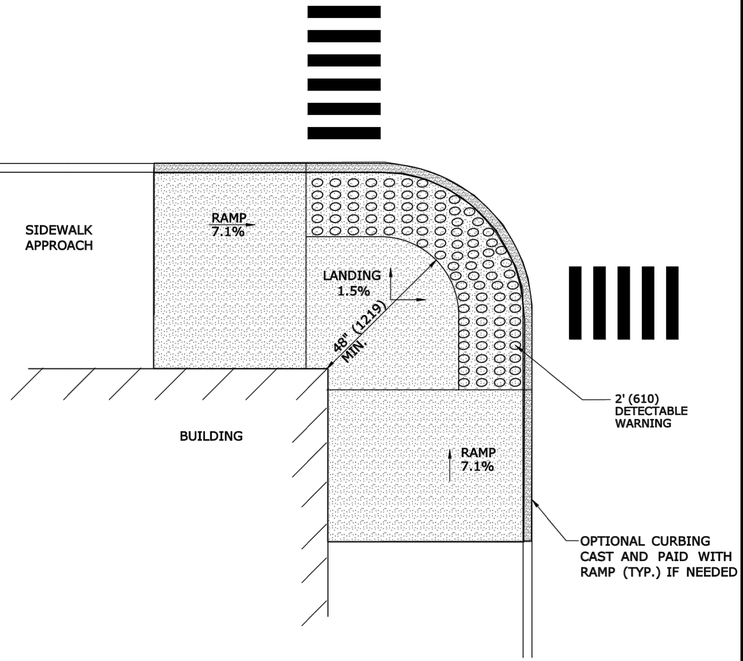
DOUBLE PARALLEL SIDEWALK RAMP
W/LANDING AT BOTTOM ON CORNER (TYPE 4f)
* SEE NOTE 20 SHEET 1



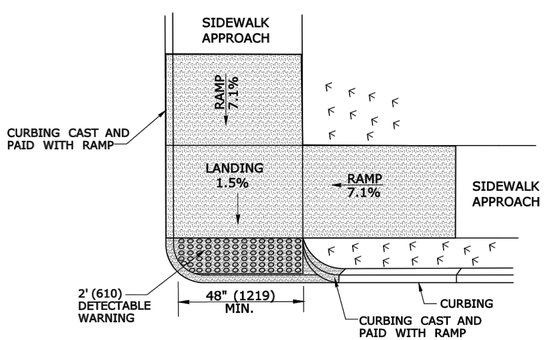
SINGLE DIRECTION
PERPENDICULAR SIDEWALK RAMP
NO / UTILITY GRASS STRIP
(TYPE 4a)



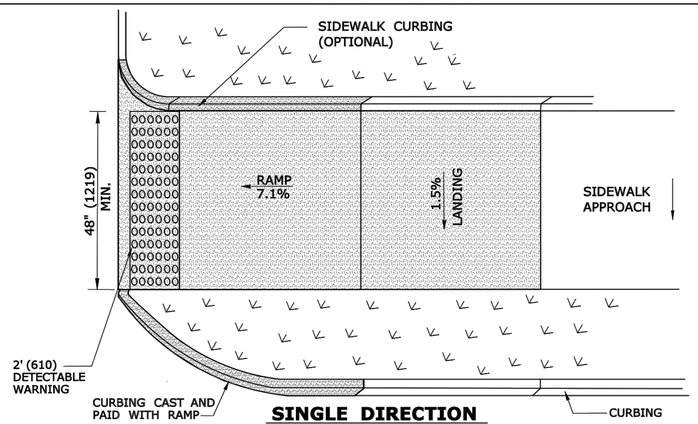
PARALLEL/PERPENDICULAR SIDEWALK RAMP
COMBINATION W/ CURB RETURNS (TYPE 4d)
* OPTIONAL FLARE ONE SIDE



RESTRICTED CONDITION
DIAGONAL SIDEWALK RAMP
(TYPE 4g)



DOUBLE DIRECTION
PARALLEL SIDEWALK RAMP
NO / UTILITY GRASS STRIP
(TYPE 4b)
SEE NOTE 20 SHEET 1



SINGLE DIRECTION
PERPENDICULAR SIDEWALK RAMP
W/ UTILITY GRASS STRIP (TYPE 4e)
REFER TO DETECTABLE WARNING PLACEMENT ON SHEET 4

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

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Plotted Date: 5/27/2014

DESIGNER/DRAFTER: MGB/EMK
CHECKED BY: LLF

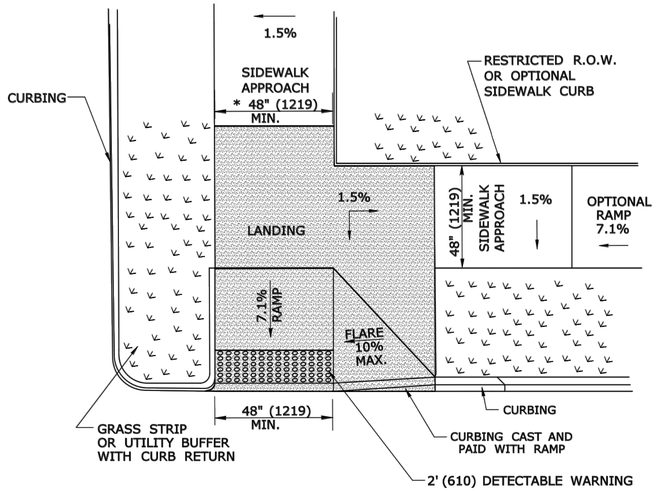
STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\SIDEWALK RAMP 2_GD.dgn

SIGNATURE/BLOCK: OFFICE OF ENGINEERING
APPROVED BY:
Tighe&Bond
www.tighebond.com

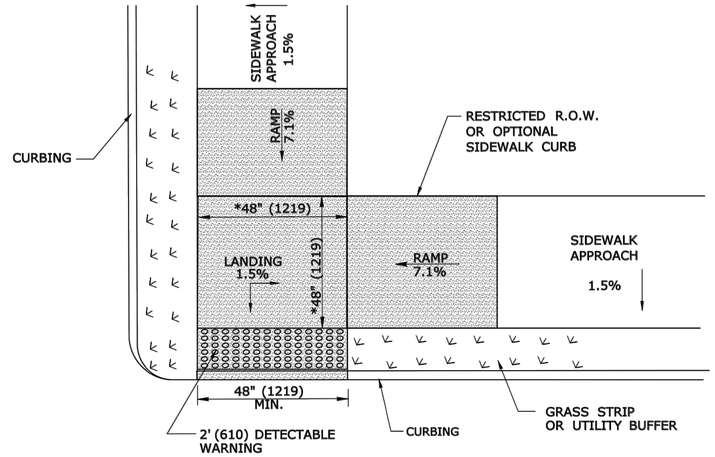
PROJECT TITLE:
WHITNEY AVENUE AT
MAIN STREET (ROUTE 111)
INTERSECTION IMPROVEMENTS

TOWN: TRUMBULL
PROJECT NO. T0196-116
DRAWING NO.
DRAWING TITLE: SIDEWALK RAMP SHEET 2
SHEET NO. SHEET 12



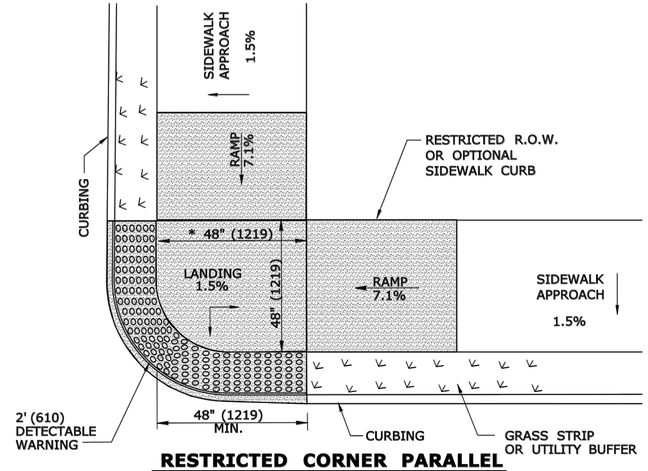
RESTRICTED CORNER PERPENDICULAR SIDEWALK RAMP (TYPE 5)

* SEE NOTE 19 SHEET 1



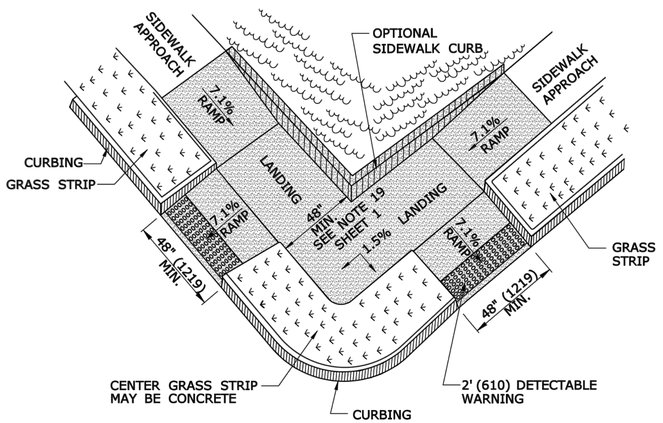
RESTRICTED CORNER PARALLEL DOUBLE SIDEWALK RAMPS W/CENTER LANDING AND UTILITY GRASS STRIP (TYPE 5a)

* SEE NOTE 20 SHEET 1



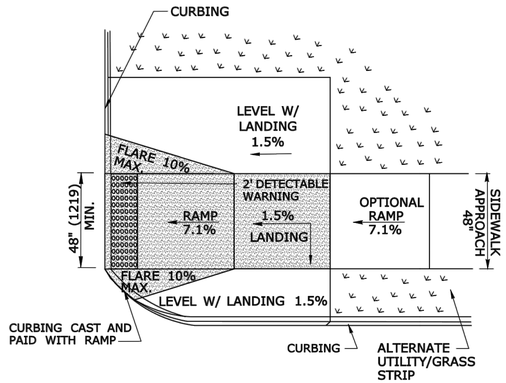
RESTRICTED CORNER PARALLEL SIDEWALK RAMPS W/ CENTER LANDING TWO DIRECTION AND UTILITY GRASS STRIP (TYPE 5b)

* SEE NOTE 20 SHEET 1

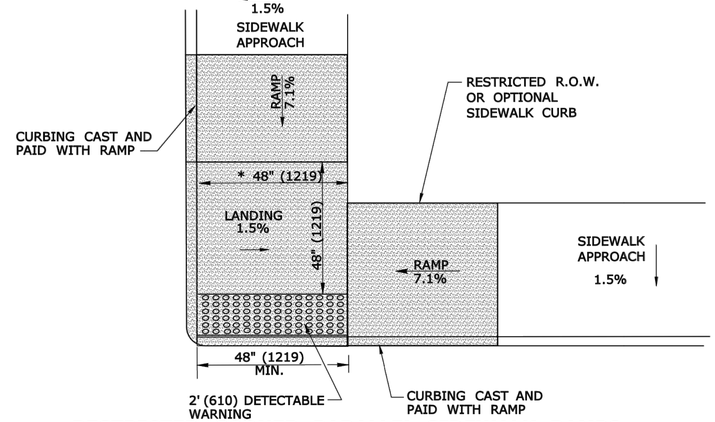


COMBINATION SIDEWALK RAMP (TYPE 6)

* SEE NOTE 19 SHEET 1

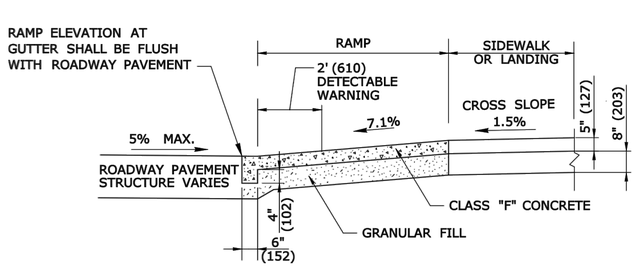


PERPENDICULAR SIDEWALK RAMP ONE DIRECTION ON CORNER (TYPE 6a)



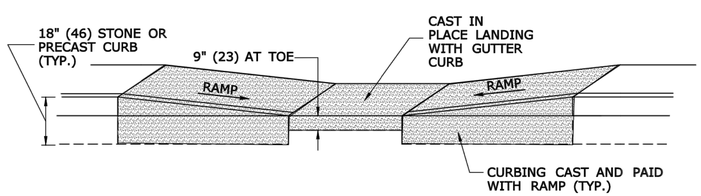
RESTRICTED CORNER PARALLEL SIDEWALK RAMPS W/CENTER LANDING NO GRASS STRIP (TYPE 5c)

* SEE NOTE 20 SHEET 1

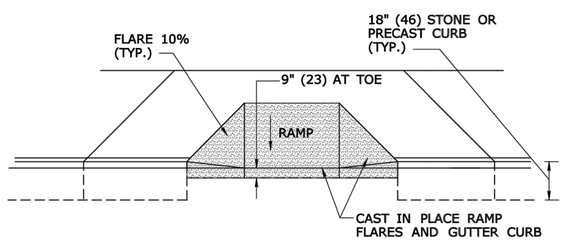


TYPICAL SECTION THRU SIDEWALK RAMP

SEE NOTE 2 AND 17 SHEET 1



TYPICAL ELEVATION PARALLEL SIDEWALK RAMP WITH CAST IN PLACE GUTTER



TYPICAL ELEVATION PERPENDICULAR SIDEWALK RAMP WITH CAST IN PLACE GUTTER

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

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Plotted Date: 5/27/2014

DESIGNER/DRAFTER: MGB/EMK
CHECKED BY: LLF

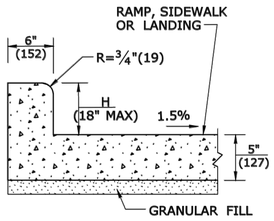
STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\SIDEWALK RAMP 3_GD.dgn

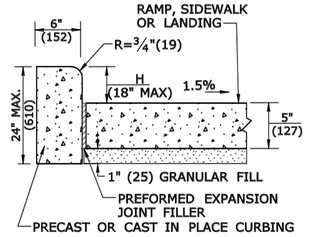
SIGNATURE/BLOCK: OFFICE OF ENGINEERING
APPROVED BY:
Tighe&Bond
www.tighebond.com

PROJECT TITLE: WHITNEY AVENUE AT MAIN STREET (ROUTE 111) INTERSECTION IMPROVEMENTS

TOWN: TRUMBULL
PROJECT NO. T0196-116
DRAWING NO.
DRAWING TITLE: SIDEWALK RAMP SHEET 3
SHEET NO. SHEET 13

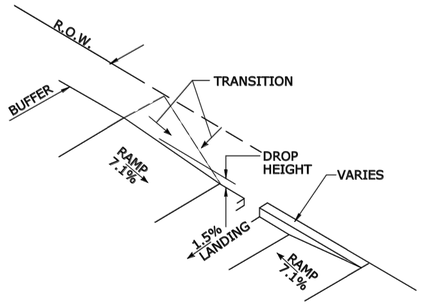


MONOLITHIC CAST CURB

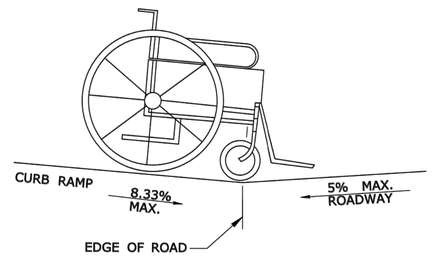


SEPARATELY CAST CURB

SIDEWALK CURB OPTIONS AT BACK OF SIDEWALK



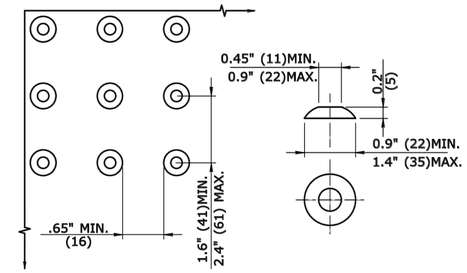
BACK OF SIDEWALK CURB OR BUFFER TRANSITION



DETAIL 1
SEE GRADE CHANGE AT ROADWAY INTERFACE
SEE NOTE 1 SHEET HW-921 02a

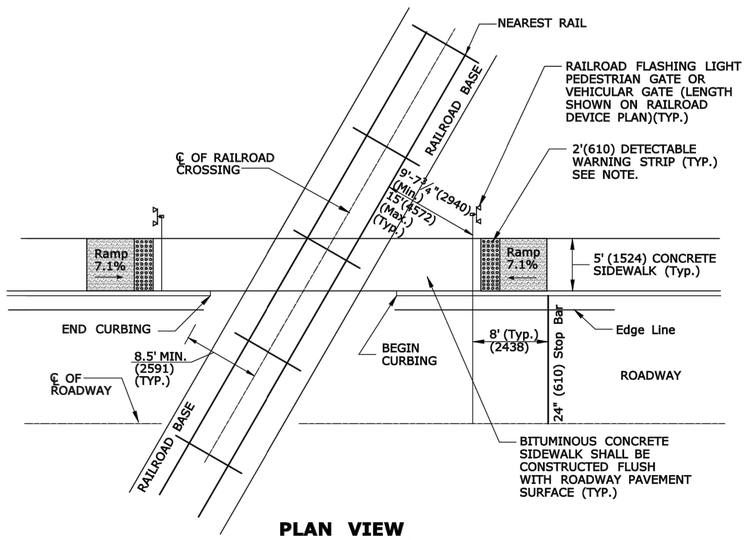
GENERAL NOTES:

1. RAMPED MEDIANS SHALL HAVE A CURB RAMP AT EITHER END AND LEVEL LANDING A MINIMUM OF 5' x 5' (1.5m x 1.5m) IN BETWEEN. CUT-THROUGH MEDIANS SHALL BE A MINIMUM OF 6' (1.8m) LONG AND 5' (1.5m) WIDE. FOR ALL MEDIANS, CUT-THROUGH OR RAMPED, A 2' (610) STRIP OF DETECTABLE WARNINGS SHALL BE INSTALLED AT THE ENTRANCE AND EXIT.
2. SEE GENERAL NOTES ON SHEET 1.

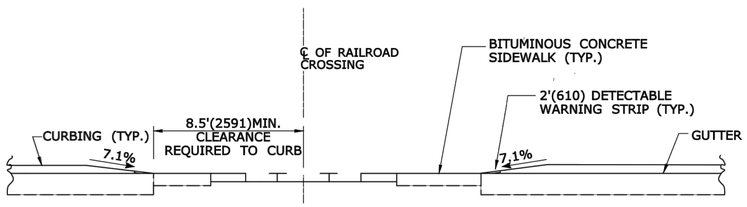


DOME SPACING **DOME SECTION**

STANDARD DOME ON DETECTABLE WARNING TILES



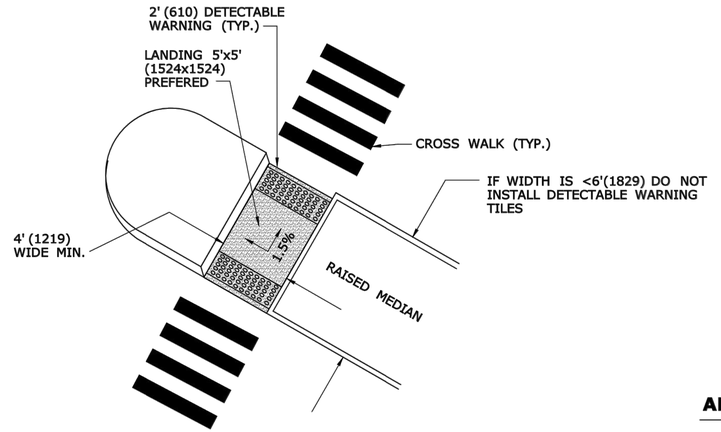
PLAN VIEW



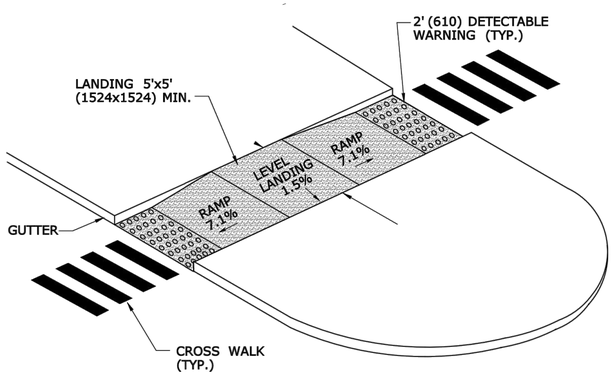
ELEVATION VIEW

DETECTABLE WARNINGS AT RAILROAD CROSSING

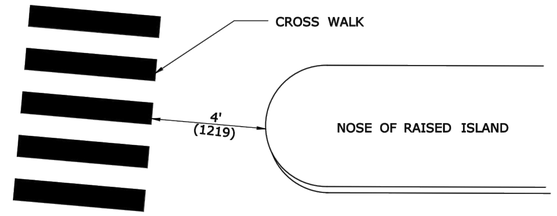
NOTE: WHEN NO GATE IS PRESENT, INSTALL DETECTABLE WARNING SURFACE 12' (3.6m) FROM THE NEAREST RAIL. IF GATE IS PRESENT, INSTALL DETECTABLE WARNING 2' (610) PRIOR TO GATE. THE ROWS OF TRUNCATED DOMES IN A DETECTABLE WARNING SURFACE SHALL BE INSTALLED PARALLEL WITH THE DIRECTION OF PEDESTRIAN TRAVEL.



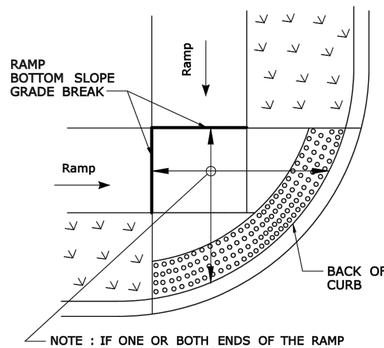
CUT-THROUGH MEDIAN ISLAND



RAISED MEDIAN ISLAND WITH LANDING AND RAMPS

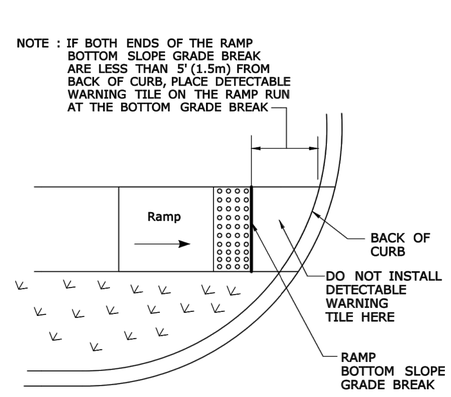


ALTERNATE CROSSWALK WITH MEDIAN ISLAND PULLED BACK



NOTE : IF ONE OR BOTH ENDS OF THE RAMP BOTTOM SLOPE GRADE BREAK ARE GREATER THAN 5' (1.5m) FROM BACK OF CURB, PLACE DETECTABLE WARNING TILE ON THE LOWER LANDING AT BACK OF CURB ALONG FULL WIDTH.

DETECTABLE WARNING PLACEMENT DETAIL 1



DETECTABLE WARNING PLACEMENT DETAIL 2

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 5/27/2014

DESIGNER/DRAFTER: MGB/EMK
CHECKED BY: LLF

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

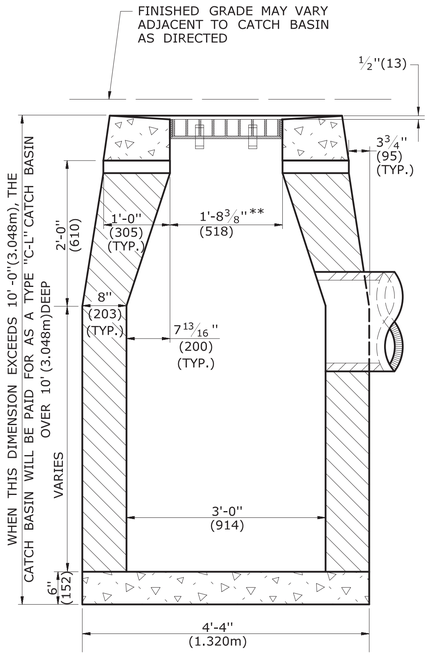
Filename: ...\SIDEWALK RAMP 4_GD.dgn

SIGNATURE/BLOCK: OFFICE OF ENGINEERING
APPROVED BY: Tighe&Bond www.tighebond.com

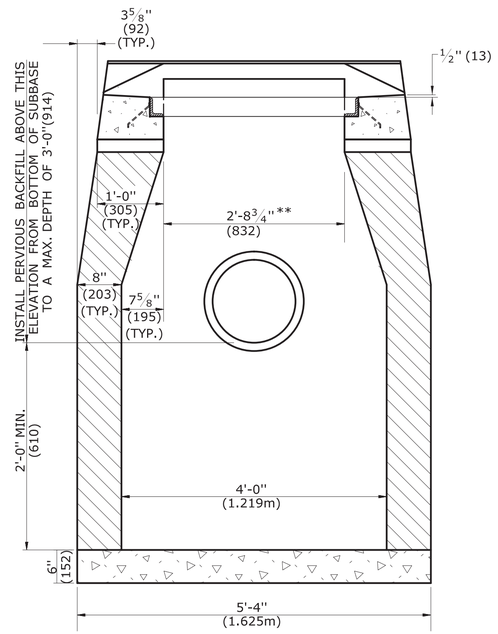
PROJECT TITLE: WHITNEY AVENUE AT MAIN STREET (ROUTE 111) INTERSECTION IMPROVEMENTS

TOWN: TRUMBULL
DRAWING TITLE: SIDEWALK RAMP SHEET 4

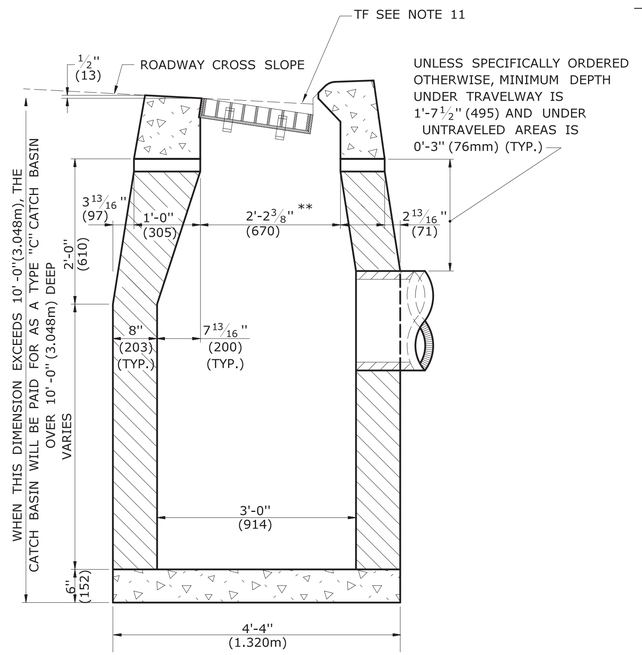
PROJECT NO. T0196-116
DRAWING NO.
SHEET NO. SHEET 14



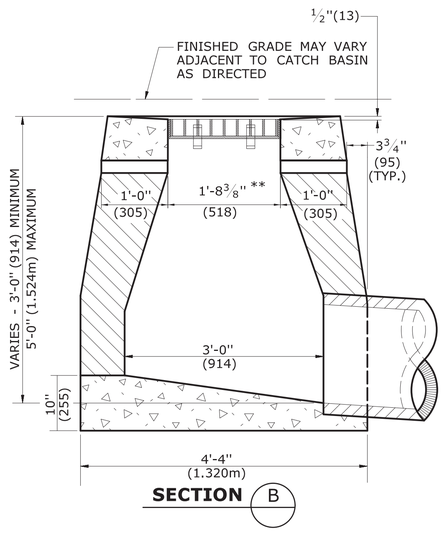
TYPE "C-L" CATCH BASIN



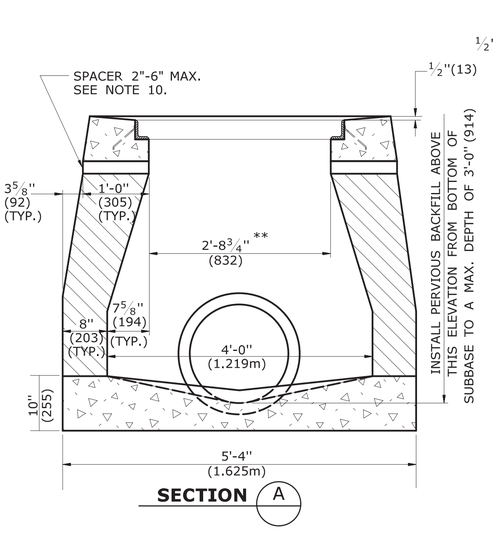
TYPE "C" & "C-L" CATCH BASIN (TYPE "C" TOP SHOWN)



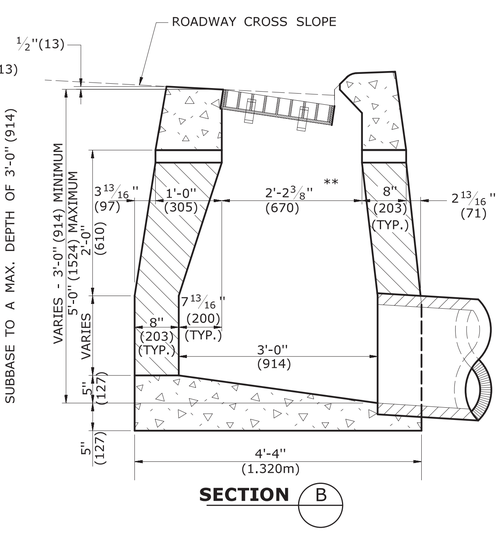
TYPE "C" CATCH BASIN



TYPE "C-L" DROP INLET



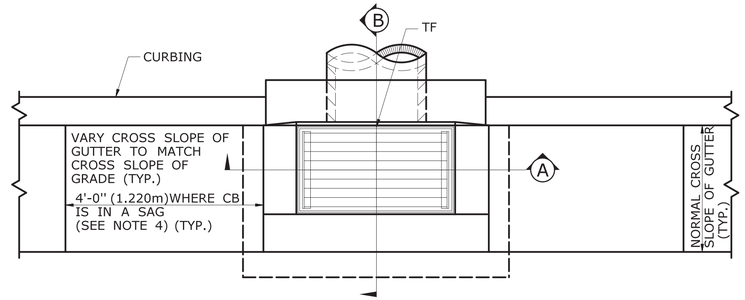
TYPE "C" & "C-L" DROP INLET (TYPE "C-L" TOP SHOWN)



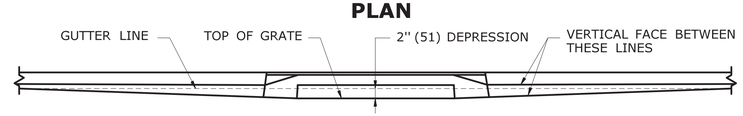
TYPE "C" DROP INLET

GENERAL NOTES:

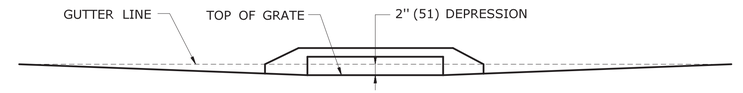
1. FOR DETAILS OF FRAME AND GRATE SEE STANDARD SHEET HW-507_08.
2. USE APPROPRIATE CONCRETE TOP FOR CURBING SHOWN ON PLANS. IF CURBING IS NOT SPECIFIED ON THE PLANS, IT SHALL BE CONSTRUCTED AS DIRECTED BY THE ENGINEER.
3. ALL FACES OF STRUCTURES IN CONTACT WITH CONCRETE PAVEMENT SHALL BE COVERED WITH A LAYER OF TAR PAPER OR APPROVED EQUAL. THE COST FOR THE PAPER SHALL BE INCLUDED IN THE BID PRICE FOR THE TYPE OF CATCH BASIN INSTALLED.
4. USE 6'-0" (1.830m) ON UPGRADE SIDE OF CONTINUOUS GRADE AND 1'-0" (305mm) ON DOWNGRADE SIDE OF CONTINUOUS GRADE OR AS DIRECTED.
5. IF MASONRY UNITS ARE REQUIRED, THE BASIN SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE OVER ALL DIMENSIONS SHOWN HERE AND SECTION 5.07 OF THE STATE OF CONNECTICUT'S STANDARD SPECIFICATIONS. CORBELLING SHALL BE PERMITTED TO A MAXIMUM OF 3" (75mm.) NO PROJECTION SHALL EXTEND INSIDE THE LIMITS NOTED BY **.
6. WALL THICKNESS OF ALL CB'S OVER 10' (3.048m) DEEP SHALL BE INCREASED TO 12" (305mm) THICK. INSIDE DIMENSION SHALL REMAIN THE SAME. 12" (305mm) THICKNESS WILL START AFTER THE FIRST 10' (3.048m).
7. TO CONVEY SUBSURFACE DRAINAGE, OPENINGS SHALL BE FORMED IN THE FOUR WALLS AT OR IMMEDIATELY ABOVE THE BOTTOM OF THE PERVIOUS BACKFILL.
8. MINIMUM CONCRETE COMPRESSIVE STRENGTH OF F'c = 4000 PSI (27,580 kPa) SHALL BE OBTAINED PRIOR TO SHIPPING.
9. LATEST STATE OF CONNECTICUT'S STANDARD SPECIFICATIONS AND SUPPLEMENTALS SHALL GOVERN.
10. SPACER MAY BE CMU OR PRECAST WITH REQUIRED REINFORCING (RECOMMENDED BY THE MANUFACTURER) AS NEEDED TO PROVIDE PROPER GRADE SHOWN ON PLANS.
11. TOP OF FRAME (TF) ELEVATION SHALL BE MEASURED IN THE CENTER OF GRATE @ GUTTER LINE.



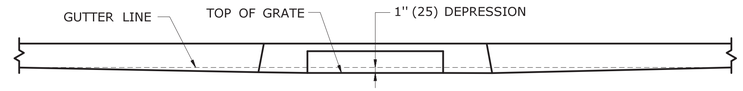
PLAN



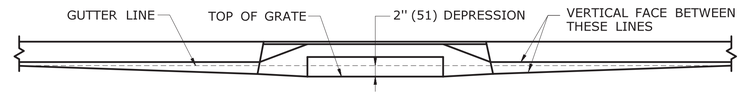
FOR CATCH BASINS IN A LINE OF 4" (102) CONCRETE PARK CURBING OR 4" (102) BITUMINOUS CONCRETE PARK CURBING



FOR CATCH BASINS WHERE NO CURBING OF ANY TYPE EXISTS OR IS PROPOSED



FOR CATCH BASINS IN A LINE OF 6" (152) CONCRETE CURBING OR 6" (152) STONE CURBING



FOR CATCH BASINS IN A LINE OF 6" (152) BITUMINOUS CONCRETE LIP CURBING (MACHINE FORMED)

DETAILS OF DEPRESSED GUTTER STRIP FOR TYPE "C" CATCH BASIN

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
4	7/13	ADD NOTE 11 -
3	9/30/11	ADD SPACERS AND NOTE 10. -
2	9/15/11	MODIFIED DETAILS TO BE CONSISTANT WITH PRECAST -
1	7/28/11	REMOVE MIN. DROP NOTE -
-	-	- -
-	-	- -

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 6/10/2013

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD2013.dgn Model: 6-HW-507_01

SUBMITTED BY: NAME/DATE/TIME:
Digitally signed by Leo L. Fontaine
Date: 2013.07.24 11:02:42-04'00'

APPROVED BY: NAME/DATE/TIME:
James H. Norman
2013.07.24 14:39:55-04'00'

CTDOT STANDARD SHEET

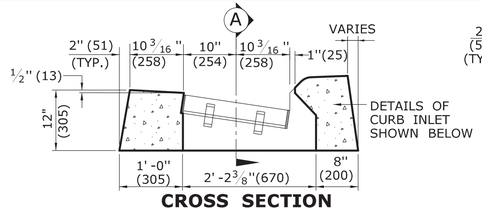
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

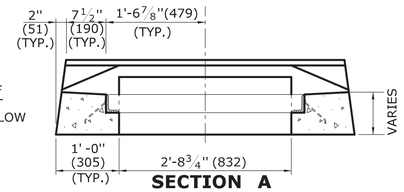
TYPE "C", "C-L" & DROP INLET CATCH BASIN

STANDARD SHEET NO.:

HW-507_01

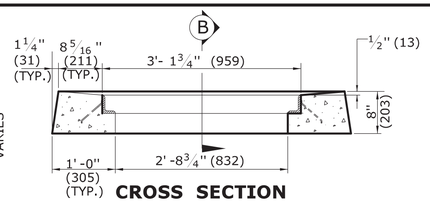


CROSS SECTION

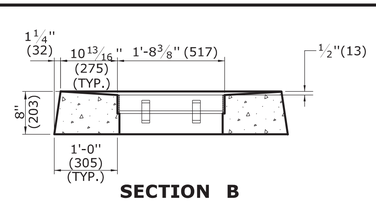


SECTION A

TYPE "C" CATCH BASIN TOP



CROSS SECTION

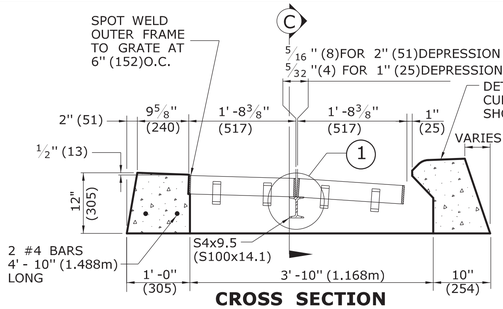


SECTION B

TYPE "C-L" CATCH BASIN TOP

GENERAL NOTES:

1. FOR DETAILS OF FRAME AND GRATE SEE STANDARD SHEET HW-507_08.
2. ALL STEEL, EXCEPT REINFORCING BARS, SHALL BE GALVANIZED IN CONFORMANCE WITH SECTION M06.03 OF CONNECTICUT'S STANDARD SPECIFICATIONS.
3. ALL BARS SHALL HAVE A MINIMUM 2" (51) COVER.



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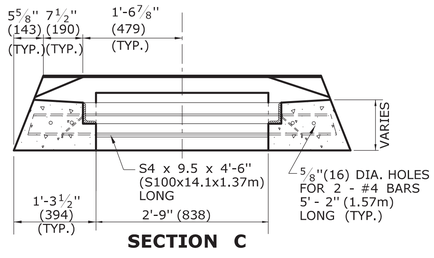
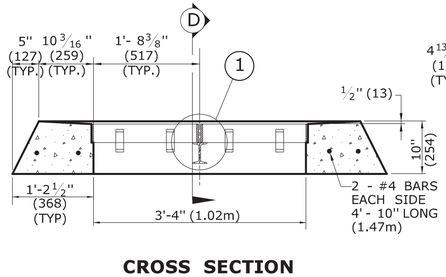
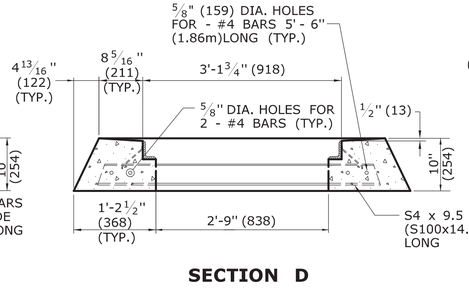


SECTION C

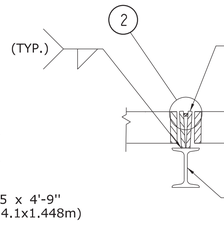
TYPE "C" CATCH BASIN DOUBLE GRATE - TYPE I TOP



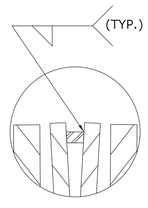
CROSS SECTION



SECTION D

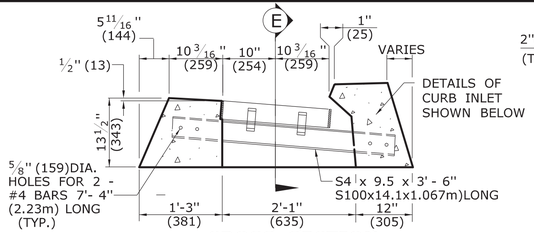


DETAIL "1"

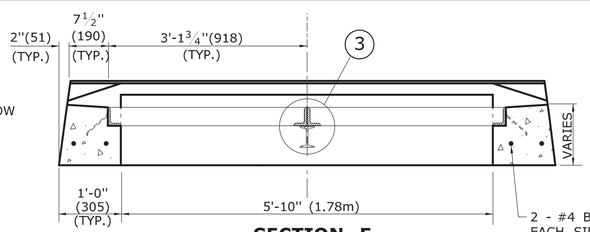


DETAIL "2"

TYPE "C-L" CATCH BASIN DOUBLE GRATE - TYPE I TOP

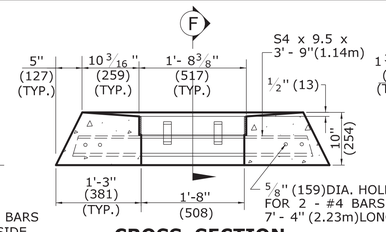


CROSS SECTION

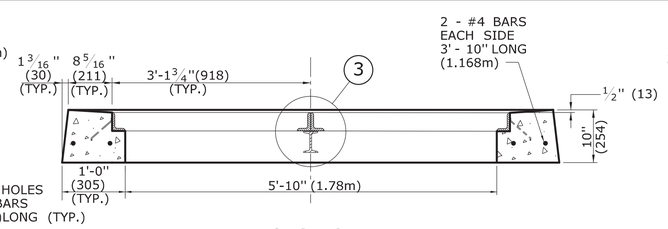


SECTION E

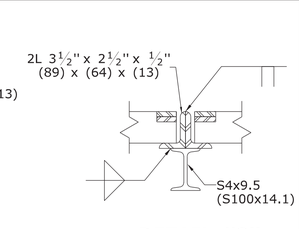
TYPE "C" CATCH BASIN DOUBLE GRATE - TYPE II TOP



CROSS SECTION

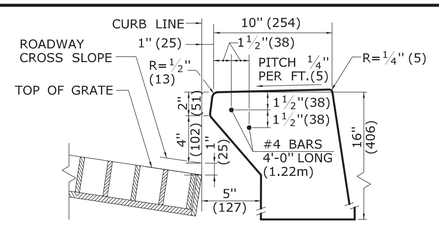


SECTION F

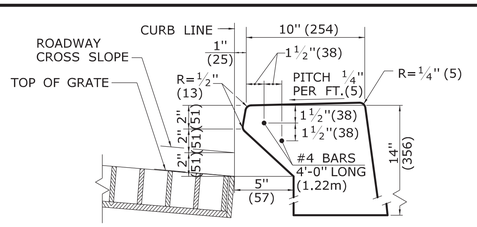


DETAIL "3"

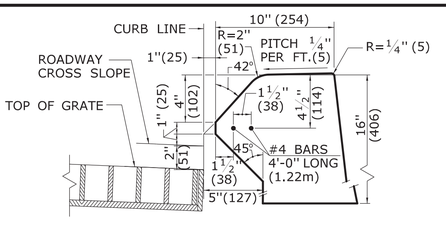
TYPE "C-L" CATCH BASIN DOUBLE GRATE - TYPE II TOP



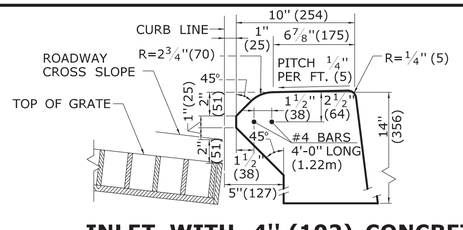
INLET WITH 6" (152) CONCRETE OR STONE CURBING FOR TYPE "C" CB



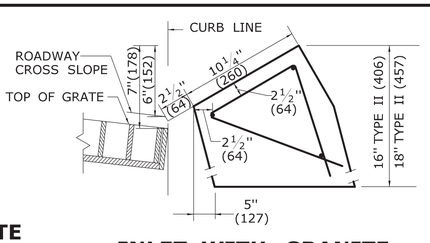
INLET WITH NO CURBING (PLAIN TYPE) FOR TYPE "C" CB



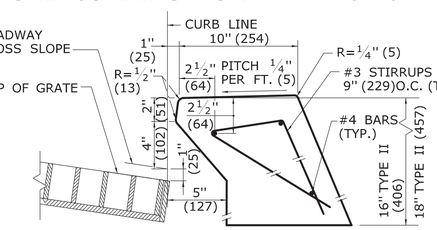
INLET WITH 6" (152) BITUMINOUS CONCRETE LIP CURBING FOR TYPE "C" CB



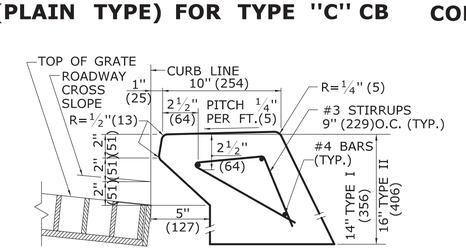
INLET WITH 4" (102) CONCRETE PARK CURBING FOR TYPE "C" CB



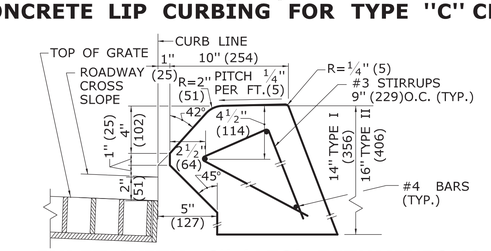
INLET WITH GRANITE SLOPE CURB FOR TYPE "C" CB



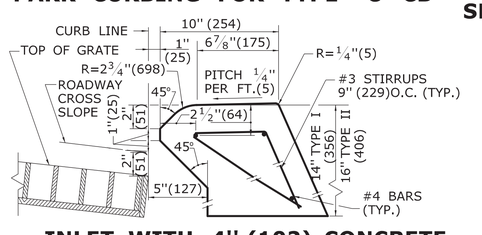
INLET WITH 6" (152) CONCRETE OR STONE CURBING FOR TYPE "C" CB DOUBLE GRATE TYPE I & II



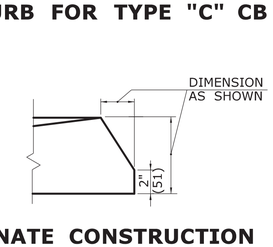
INLET WITH NO CURBING (PLAIN TYPE) FOR TYPE "C" CB DOUBLE GRATE TYPE I & II



INLET WITH 6" (152) BITUMINIOUS CONCRETE LIP CURBING FOR TYPE "C" CB DOUBLE GRATE TYPE I & II



INLET WITH 4" (102) CONCRETE PARK CURBING FOR TYPE "C" CB DOUBLE GRATE TYPE I & II



ALTERNATE CONSTRUCTION OF TYPE II TOP

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
-	-	-
-	-	-
2	7/28/11	REMOVE MIN. DROP NOTE
1	6/01/10	REVISE CALL-OUT

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 11/10/2011

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HWY_STD_Nov2011.dgn Model: 12-HW-507_07

SUBMITTED BY: NAME/DATE/TIME: Leo Fontaine 2011.11.10 10:04:01 -05'00'

APPROVED BY: NAME/DATE/TIME: James H. Norman 2011.11.10 10:19:36 -05'00'

CTDOT STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

TYPE "C" & "C-L" CATCH BASIN TOPS AND CURBS

STANDARD SHEET NO.:

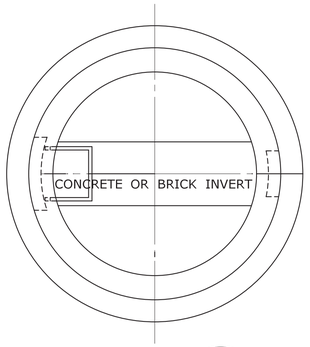
HW-507_07

GENERAL NOTES:

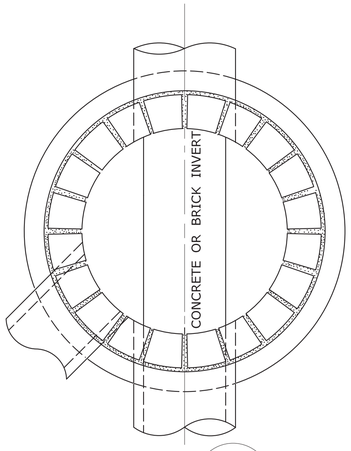
1. CHANNELS MAY BE SHAPED IN CONCRETE BASE OF MANHOLE OR FORMED USING BRICK OR MASONRY.
2. A FRAME DIAMETER OF 3'-3" (991) WITH 4" (102) FLANGE MUST BE USED WHEN THE TOP DIAMETER OF THE PRECAST CONE IS LESS THAN 3'-6" (1067). ALL OTHER FRAME DIMENSIONS SHALL REMAIN THE SAME.
3. COVER:

	CAST IRON	STEEL
MIN. COVER WEIGHT	134LB.(61kg)	134LB.(61kg)

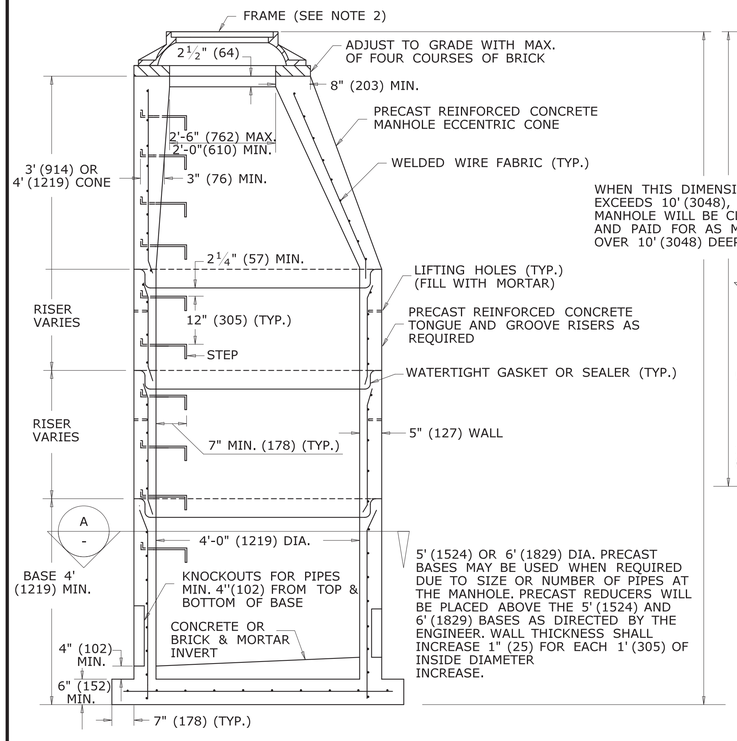
4. ALL DIMENSIONS SUBJECT TO MANUFACTURING TOLERANCES.



SECTION A -



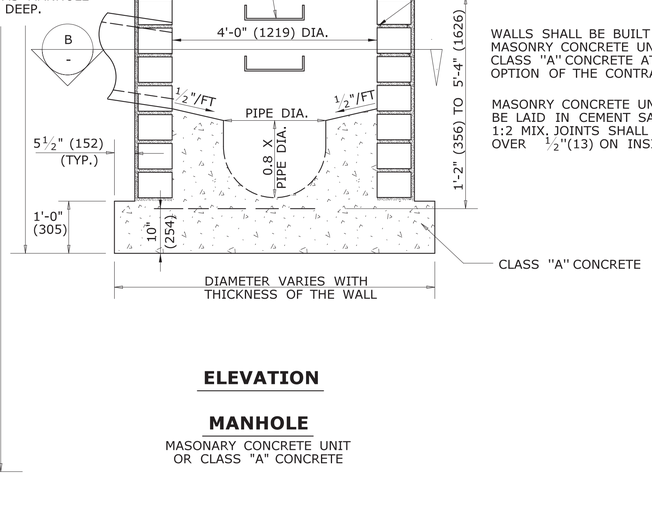
SECTION B -



ELEVATION

MANHOLE

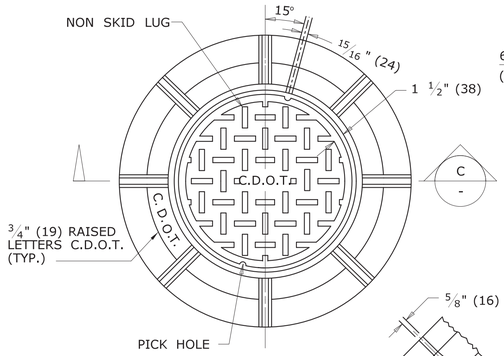
REINFORCED PRECAST CONCRETE UNIT



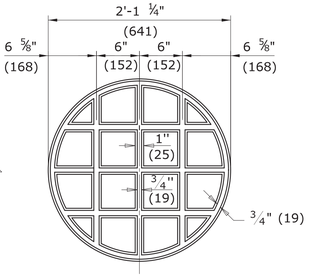
ELEVATION

MANHOLE

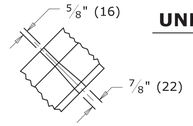
MASONARY CONCRETE UNIT OR CLASS "A" CONCRETE

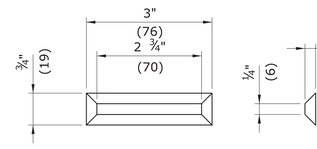


PLAN

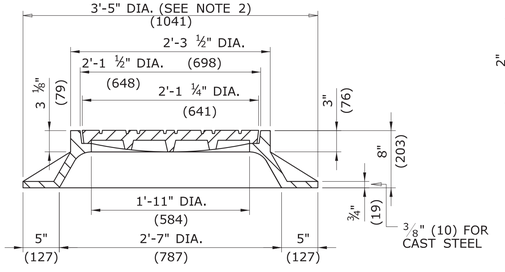


UNDERSIDE OF COVER DETAIL

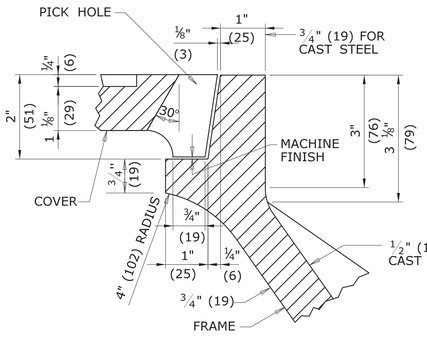




NON SKID LUG DETAIL



SECTION C -



DETAIL OF SEAT

FRAME AND COVER DETAILS

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
1	6/11	REVISE STEP WIDTH PER OSHA
2	7/13	REVISE COVER FRAME WEIGHT
-	-	-
-	-	-
-	-	-
-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 6/13/2013

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD2013.dgn Model: 15-HW-507_10

SUBMITTED BY: NAME/DATE/TIME:
Digitally signed by Leo L. Fontaine
Date: 2013.07.24 11:01:54-04'00'

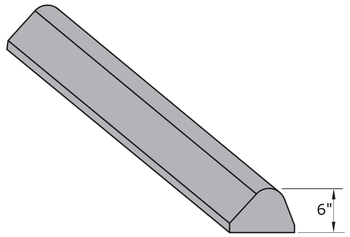
APPROVED BY: NAME/DATE/TIME:
James H. Norman
2013.07.24 14:43:21-04'00'

CTDOT STANDARD SHEET

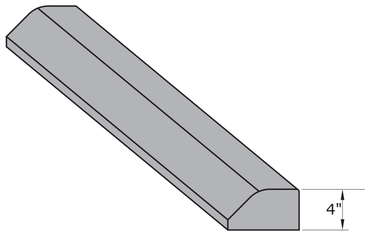
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
MANHOLE - FRAME & COVER

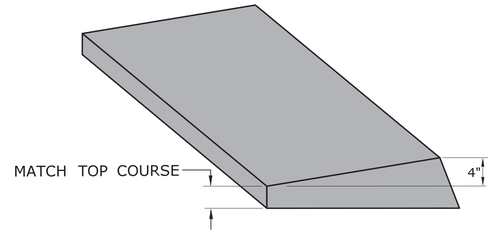
STANDARD SHEET NO.:
HW-507_10



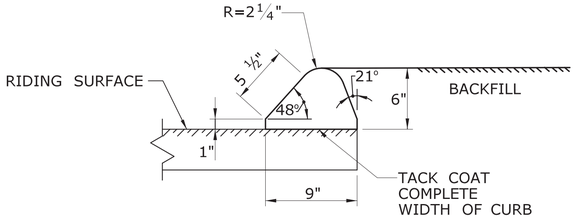
BITUMINOUS CONCRETE LIP CURBING
(6" HIGH)



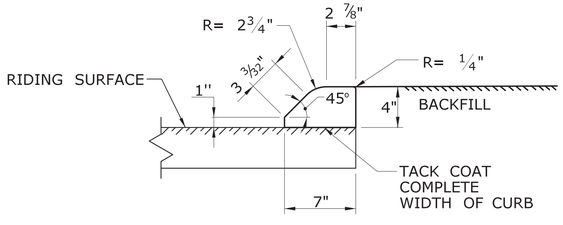
BITUMINOUS CONCRETE PARK CURBING
(4" HIGH)



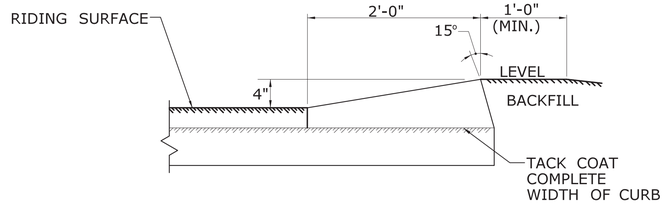
BITUMINOUS CONCRETE BERM CURBING
(4" HIGH)



SECTION



SECTION



SECTION

REV.	DATE	REVISION DESCRIPTION
1	6/17	NEW SHEET

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 6/6/2017

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: HW-815_01.dgn Model: CT_Civil_2D_Sheet

SUBMITTED BY: NAME/DATE/TIME:
Leo Fontaine, P.E.
2017.06.07 07:33:51-04'00'

APPROVED BY: NAME/DATE/TIME:
Gregory M. Dorosh, P.E.
2017.06.07 10:44:27-04'00'

CTDOT STANDARD SHEET

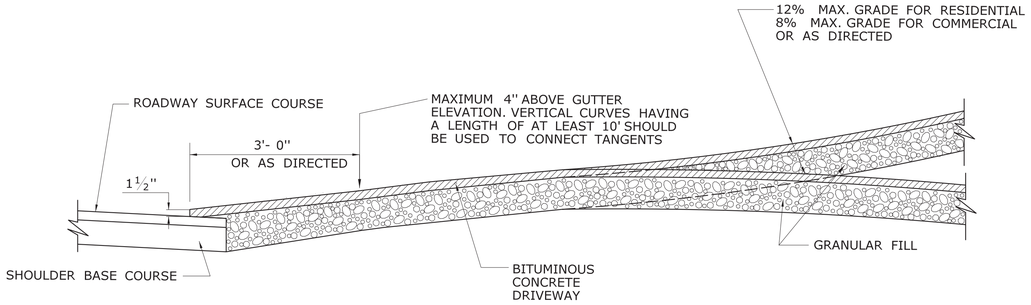
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
BITUMINOUS CONCRETE CURBING

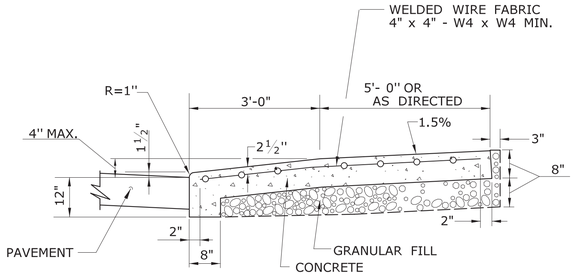
STANDARD SHEET NO.:
HW-815_01

GENERAL NOTES:

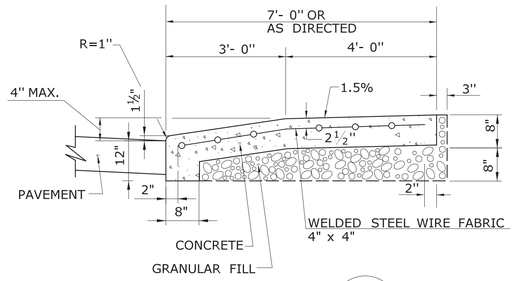
1. DRIVEWAY ENTRANCE SHALL BE A MINIMUM OF 12' WIDE, EXCLUDING CURBING WHEN PRESENT.
2. WELDED WIRE FABRIC MATS WITH REINFORCING AT CLOSER SPACING MAY BE USED.
3. SURFACE HMA S0.375 TO BE PLACED IN TWO EQUAL LIFTS FOR BOTH RESIDENTIAL AND COMMERCIAL DRIVEWAYS.



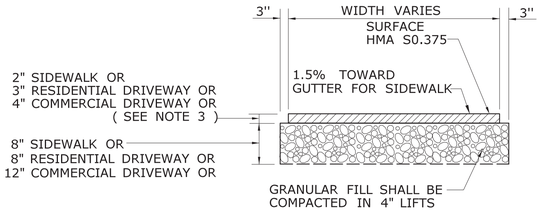
SECTION A



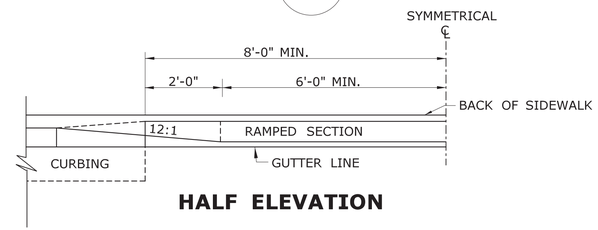
SECTION C



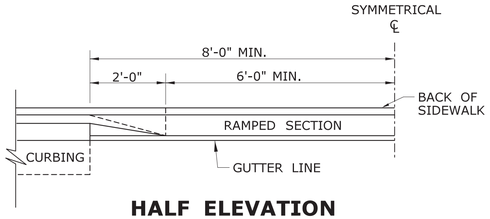
SECTION B



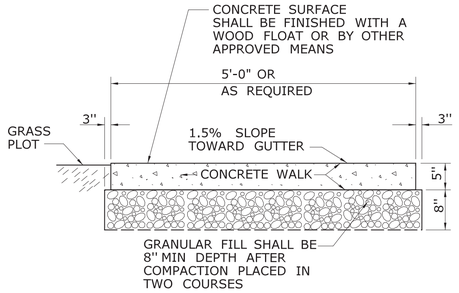
TYPICAL SECTION BITUMINOUS CONCRETE SIDEWALK AND DRIVEWAY



HALF ELEVATION

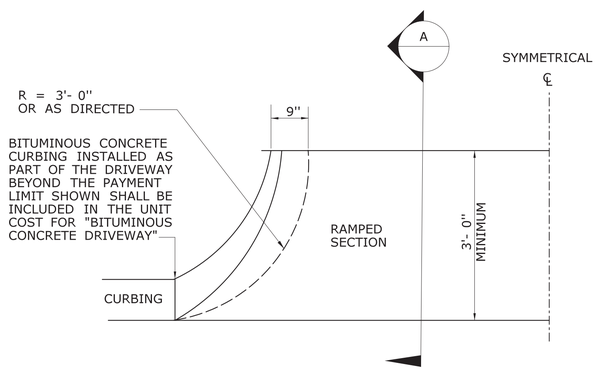


HALF ELEVATION

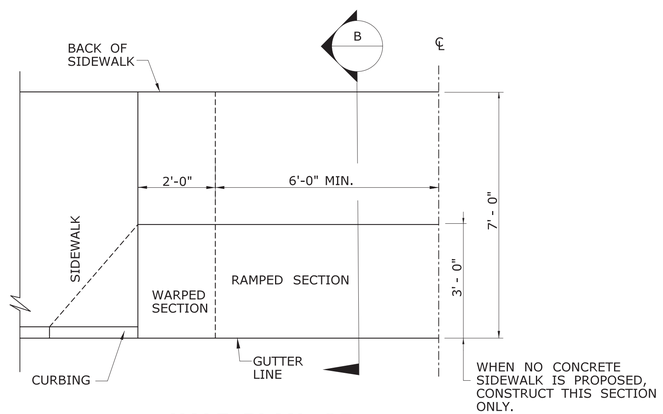


SECTION D

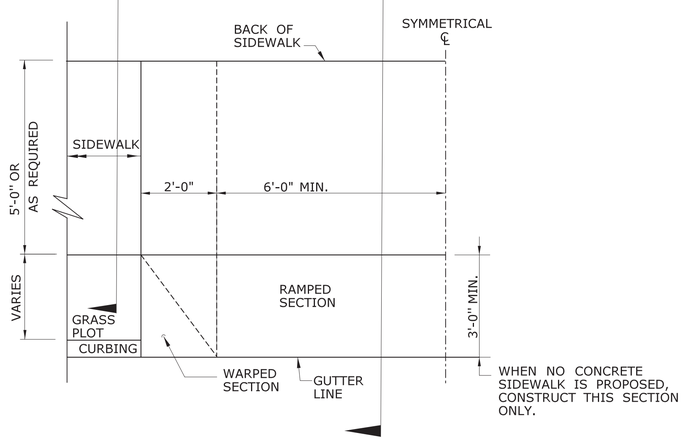
5' WIDE CONCRETE SIDEWALK WITH GRASS PLOT



HALF BITUMINOUS CONCRETE DRIVEWAY PLAN



HALF PLAN OF CONCRETE DRIVEWAY RAMP WHERE SIDEWALK ADJOINS CURBING



HALF PLAN OF CONCRETE DRIVEWAY RAMP WHERE CURB IS SEPARATED FROM SIDEWALK BY GRASS PLOT

REV.	DATE	REVISION DESCRIPTION
1	6/01/10	REVISED BORDER TITLE
2	6/01/10	REVISED HALF ELEVATION DETAILS
3	1/12	REVISE 2% MAX. SLOPE NOTE
4	6/17	REVISED SLOPES & MATERIAL COMPOSITIONS
-	-	INCREASED WALKING WIDTH OF CONCRETE DRIVEWAY RAMP

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Plotted Date: 6/6/2017

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: HW-921_01.dgn Model: CT_Civil_2D_Sheet

SUBMITTED BY:	NAME/DATE/TIME:
	Leo Fontaine, P.E. 2017.06.07 07:34:10-04'00'
APPROVED BY:	NAME/DATE/TIME:
	Gregory M. Dorosh, P.E. 2017.06.07 10:47:32-04'00'

CTDOT STANDARD SHEET

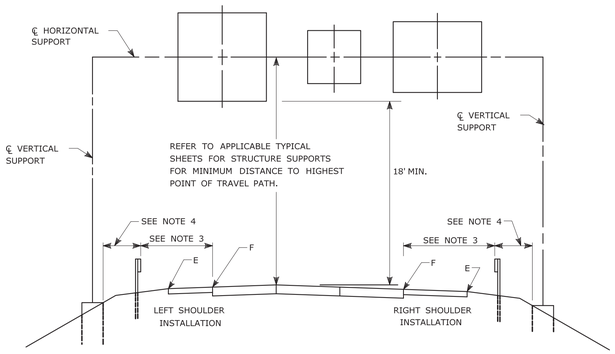
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

DRIVEWAY RAMPS AND SIDEWALKS

STANDARD SHEET NO.:

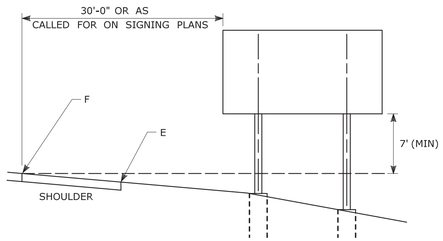
HW-921_01



TYPICAL PLACEMENT OF OVERHEAD SIGNS ON SIGN SUPPORTS

NOTES:

1) FOR PLACEMENT OF CANTILEVER SIGN SUPPORT USE APPLICABLE PORTION OF ABOVE DETAIL.
2) BARRIER SYSTEMS MAY BE REQUIRED FOR BOTH SIDES OF SUPPORTS IN MEDIANS.
3) IMPACT PROTECTION SHALL BE PROVIDED FOR THE SIGN SUPPORTS LOCATED WITHIN CLEAR ZONE.
4) SIGN SUPPORT FOUNDATIONS SHALL BE LOCATED OUTSIDE OF BARRIER SYSTEMS DEFLECTION AREA.
5) ALL SIGNS ARE TO BE LEVEL, REGARDLESS OF CAMBER IN SUPPORT.



TYPICAL PLACEMENT OF SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS

NOTES:

1) MIN. VERTICAL CLEARANCE ABOVE SIDEWALKS SHALL BE 7'.
2) WHERE GUIDE RAIL IS USED, THE OFFSET TO THE NEAR EDGE OF SIGN FACE SHALL BE AS SHOWN ELSEWHERE IN THE CONTRACT PLANS.
3) ON INTERSECTING ROADS AT RAMP TERMINI, THE OFFSET TO THE NEAR EDGE OF OF SIGN FACE SHALL BE 6' MIN. FROM POINT "E".
4) IF 30'-0" MIN. CANNOT BE MET, PLEASE CONTACT THE ENGINEER.

FOR MAXIMUM EFFECTIVENESS, POSITION SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS AS FOLLOWS:

ON A TANGENT SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH THE TRAFFIC LANE WHICH THE SIGN SERVES. SIGNS LOCATED 30 FT OR MORE FROM THE EDGE OF THE ROAD SHALL BE TURNED APPROXIMATELY 3° TOWARD THE ROAD.

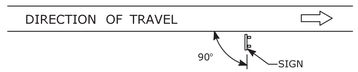


DIAGRAM "A"

ON A HORIZONTAL CURVE SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH A STRAIGHT LINE BETWEEN THE SIGN AND THE POINT AT WHICH THE SIGN SHALL BE READ.

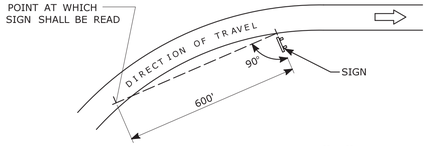
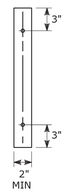


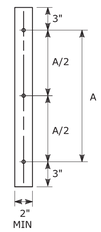
DIAGRAM "B"

SIGN ORIENTATION DETAILS FOR SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS

RETROREFLECTIVE STRIPS 48" LONG OR LESS:



RETROREFLECTIVE STRIPS OVER 48" LONG:

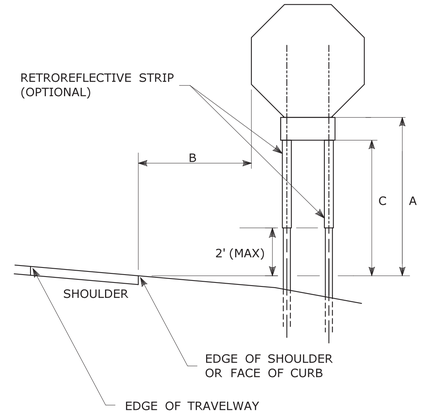


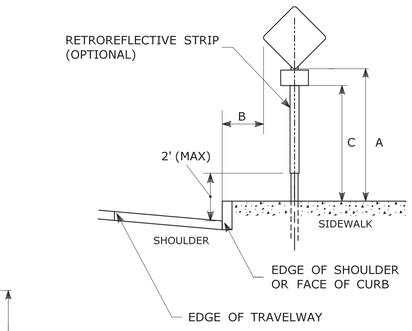
RETROREFLECTIVE STRIP DETAIL

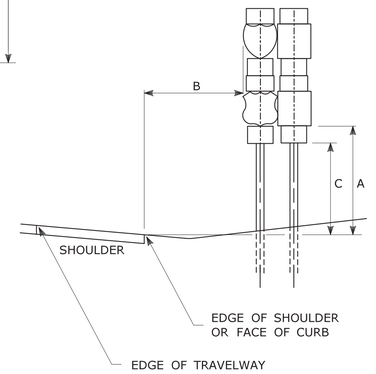
NOTES:

RETROREFLECTIVE STRIPS WHICH ARE 48 IN LONG OR LESS SHALL BE ATTACHED USING 2 BOLTS AND RETROREFLECTIVE STRIPS OVER 48 IN LONG SHALL BE ATTACHED USING 3 BOLTS AS SHOWN ON THE DETAILS ABOVE.
REFER TO STANDARD SHEET No. TR-1208_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR MOUNTING DETAILS.

RETROREFLECTIVE STRIP COLOR SHALL MATCH THE BACKGROUND COLOR OF THE SIGN, EXCEPT THAT THE COLOR OF THE STRIP FOR "YIELD" AND "DO NOT ENTER" SIGNS SHALL BE RED.







TYPICAL SIGN PLACEMENT DETAIL

NOTES:

ALL SIGNS AND SHIELDS ON DIRECTIONAL ASSEMBLIES SHALL ABUT VERTICALLY.

REFER TO STANDARD SHEET No. TR-1208_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR SIGN POSTS AND SIGN MOUNTING.

IF A RETFOREFLECTIVE STRIP IS USED ON SIGN SUPPORT, IT SHALL BE PLACED FOR THE FULL LENGTH OF THE SUPPORT FROM THE BOTTOM OF THE SIGN TO WITHIN 2 FT ABOVE THE EDGE OF THE ROADWAY.

PARKING SIGNS TYPICALLY USE 45° MOUNTING BRACKET.

DIM."A" MIN SIGN HEIGHT	DIM."B" MIN LATERAL OFFSET [1]	DIM."C" MIN PLAQUE HEIGHT [1]	ASSEMBLY LOCATION
7' [2]	6' 12' [3]	5'	SIGNS ON FREEWAYS AND EXPRESSWAYS EXCEPT CHEVRON ALIGNMENT SIGNS, ONE-DIRECTION LARGE ARROW SIGNS, DO NOT ENTER SIGNS, AND WRONG WAY SIGNS
5'	2'	4'	• SIGNS IN RURAL AREAS • DO NOT ENTER AND WRONG WAY SIGNS ALONG EXIT RAMPS • DO NOT ENTER AND WRONG WAY SIGNS ON LIMITED ACCESS HIGHWAYS
5'	2'	N/A	• CHEVRON ALIGNMENT SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMPS, AND IN RURAL AREAS • ONE-DIRECTION LARGE ARROW SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMPS, AND IN RURAL AREAS
4'	2'	4'	CENTRAL ISLANDS OF ROUNDABOUTS
7'	2' [4]	6'	BUSINESS & RESIDENTIAL AREAS WHERE PARKING OR OTHER OBSTRUCTIONS LIMIT VISIBILITY
7'	2' [4]	7'	SIDEWALKS [5]

[1] OR AS DIRECTED BY THE ENGINEER

[2] 8 FT MINIMUM HEIGHT REQUIRED IF A SUPPLEMENTAL PLAQUE IS SUBMOUNTED BELOW THE MAJOR SIGN.

[3] 6 FT FROM EDGE OF SHOULDER, WHEN SHOULDER IS OVER 6 FT WIDE
12 FT FROM EDGE OF TRAVELWAY, WHEN SHOULDER IS LESS THAN 6 FT WIDE.

[4] A LATERAL OFFSET OF AT LEAST 1 FT FROM THE FACE OF THE CURB MAY BE USED WHERE SIDEWALK WIDTH IS LIMITED OR WHERE EXISTING UTILITY POLES ARE CLOSE TO THE CURB.

[5] A CLEAR PATH OF NOT LESS THAN 4 FT SHALL BE PROVIDED IN SIDEWALK AREAS.

REV.	DATE	REVISION DESCRIPTION
2	4-2017	MINOR REVISIONS.
1	2-2011	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 4/3/2017

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: TR_1208_01_LATEST_REVISION.dgn Model: TR-1208_01

SUBMITTED BY: NAME/DATE/TIME:
Mark F. Makuch, P.E. 2017.04.19 11:09:35-04'00'

APPROVED BY: NAME/DATE/TIME:
Gregory M. Dorosh, P.E. 2017.04.20 13:14:38-04'00'

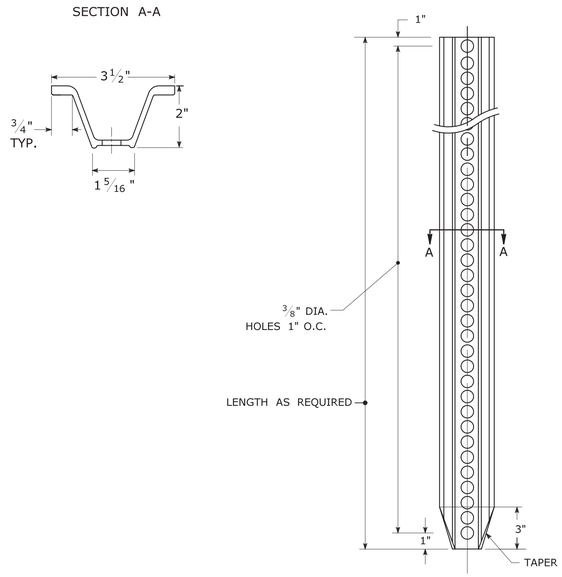
CTDOT STANDARD SHEET

OFFICE OF ENGINEERING

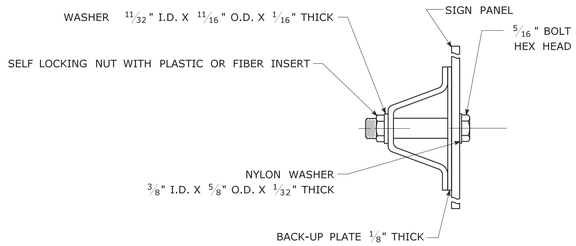
STANDARD SHEET TITLE:
SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS

STANDARD SHEET NO.:
TR-1208_01

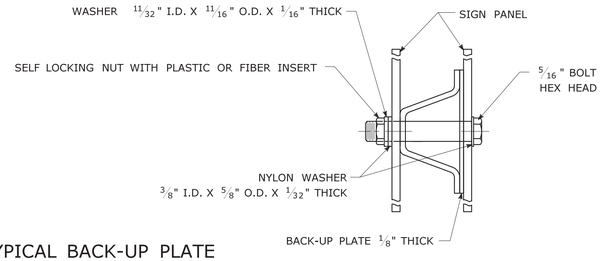
TYPICAL METAL SIGN POSTS



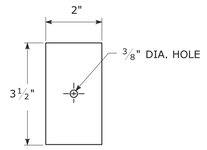
TYPICAL SIGN PANEL ATTACHMENT



TYPICAL BACK TO BACK SIGN PANEL ATTACHMENT



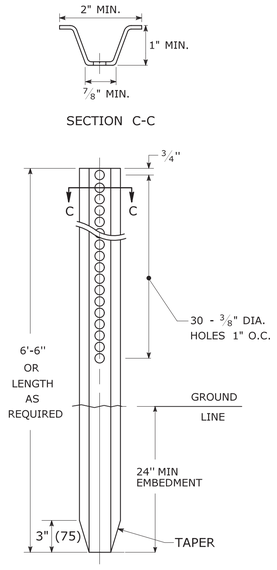
TYPICAL BACK-UP PLATE



BOLTS - STAINLESS STEEL CONFORMING TO ASTM F593, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).
SELF LOCKING NUTS - STAINLESS STEEL CONFORMING TO ASTM F594, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).
WASHERS - STAINLESS STEEL CONFORMING TO ASTM A240, (ALLOY TYPES 304 OR 316).

METAL DELINEATOR POST

WT./FT. = 1.12 LBS./FT. MIN.

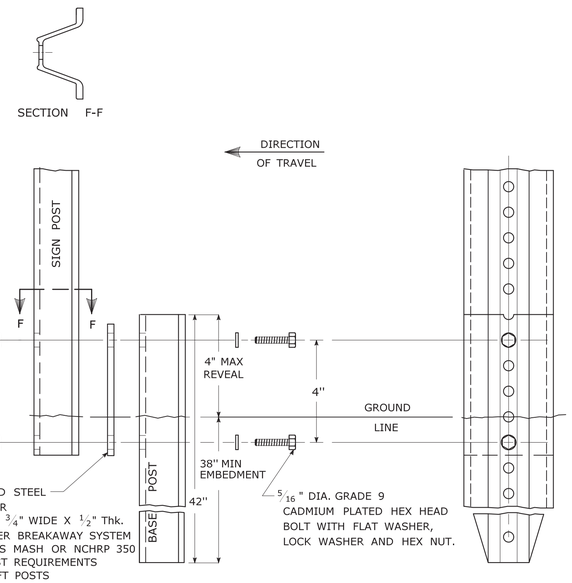


GENERAL NOTES:

1. STEEL FOR DELINEATOR POSTS SHALL BE ASTM A36 STEEL. STEEL FOR ALL OTHER POSTS SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A 499 GRADE 80 AND TO THE CHEMICAL REQUIREMENTS OF ASTM A1 CARBON STEEL TEE RAIL HAVING NOMINAL WEIGHT (MASS) OF 91 LBS. OR GREATER PER LINEAR YARD.
2. AFTER FABRICATION, ALL STEEL POSTS, STRAPS AND PLATES SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A123.
3. WASHERS FOR BREAKAWAY INSTALLATIONS SHALL MEET ASTM F436, TYPE 1.
4. SPACER BAR FOR BREAKAWAY INSTALLATION SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A36.
5. ALL BOLTS, NUTS, AND WASHERS FOR BREAKAWAY INSTALLATIONS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A153.
6. ALL SIGN POSTS SHALL HAVE BREAKAWAY FEATURES THAT MEET AASHTO REQUIREMENTS CONTAINED IN THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS." THE BREAKAWAY FEATURES SHALL BE STRUCTURALLY ADEQUATE TO CARRY THE SIGNS SHOWN IN THE PLANS AT 60 mph WIND LOADINGS. INSTALLATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
7. SIGN POSTS SHALL BE 4 LBS./FT.

BREAKAWAY INSTALLATION

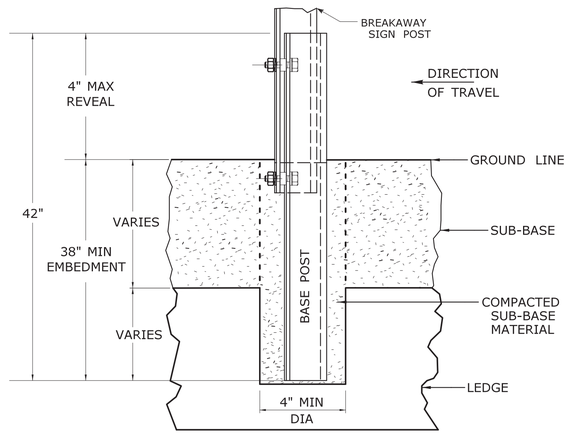
FOR 4 LBS./FT. POSTS



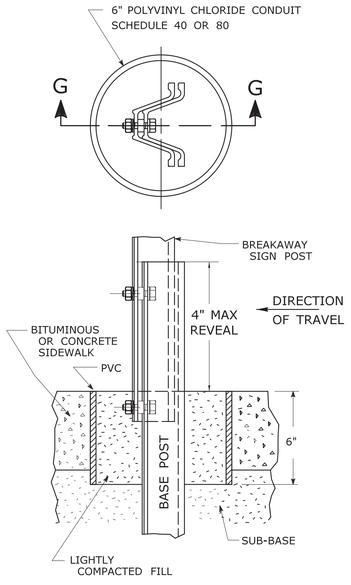
TYPICAL SIGN POST INSTALLATION IN LEDGE

LEDGE SHALL BE REMOVED TO DRIVE THE BASE POST TO A DEPTH OF 38".

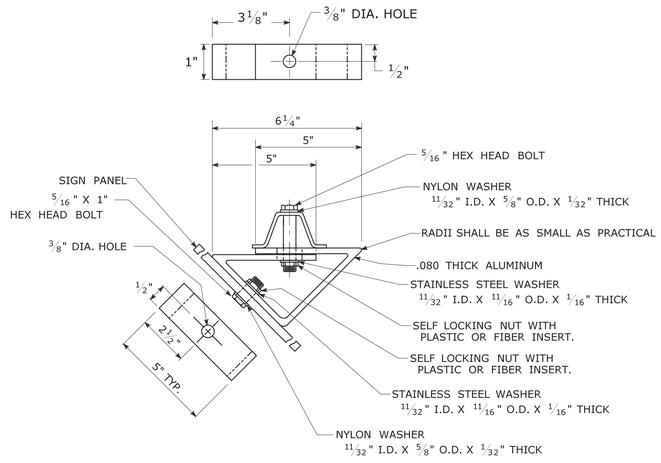
HOLE SHALL BE FILLED WITH SUB-BASE MATERIAL AND COMPACTED WITH A TAMPING BAR, OR TECHNIQUE APPROVED BY THE ENGINEER, PRIOR TO BASE POST INSTALLATION.



TYPICAL SLEEVE FOR PAVED AREAS



45° MOUNTING BRACKET FOR INSTALLATION OF PARKING SIGNS



REV.	DATE	REVISION DESCRIPTION
2	6-2017	SIGN POST REVISIONS.
1	2-2011	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 6/6/2017

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: TR-1208_02_May_2017_Revision.dgn Model: TR-1208_02

SUBMITTED BY:	NAME/DATE/TIME:
Mark Makuch	Mark F. Makuch, P.E. 2017.06.07 07:30:30-04'00'
Mary E. Baker	Mary E. Baker, P.E. 2017.06.13 15:28:14-04'00'
APPROVED BY:	NAME/DATE/TIME:
Gregory M. Dorosh	Gregory M. Dorosh, P.E. 2017.06.15 09:27:29-04'00'

CTDOT STANDARD SHEET

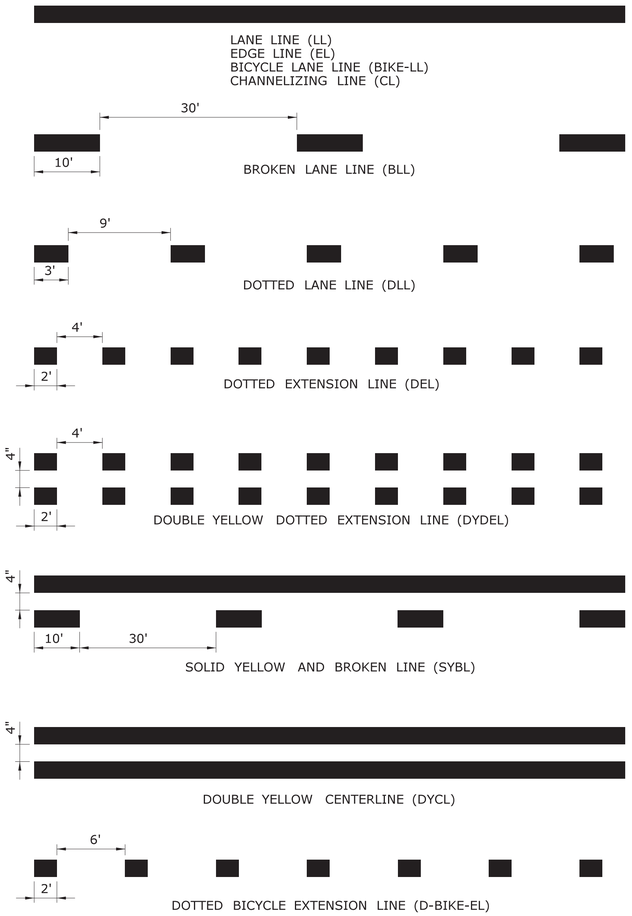
OFFICE OF ENGINEERING

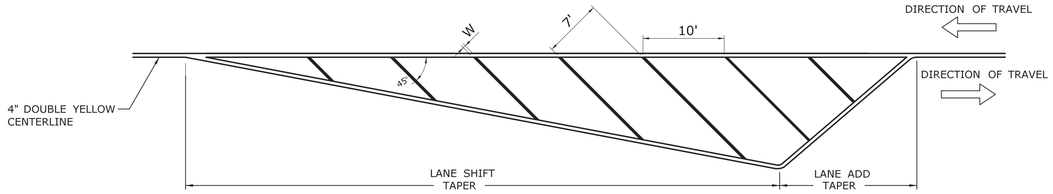
STANDARD SHEET TITLE:

METAL SIGN POSTS AND SIGN MOUNTING DETAILS

GUIDE SHEET NO.:

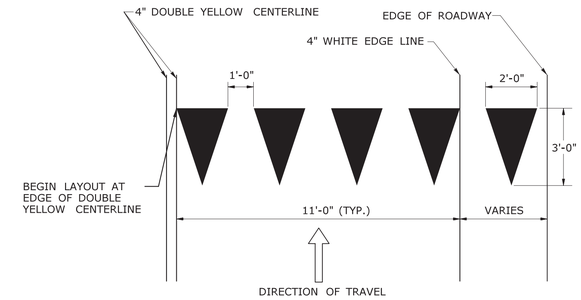
TR-1208_02



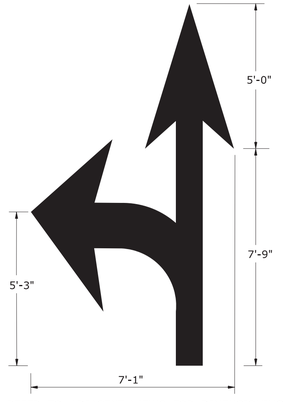


CROSS HATCHED ISLAND DETAIL
(YELLOW)

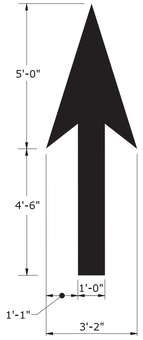
W IS TO BE 6" WHEN POSTED SPEED ≤ 45 MPH
W IS TO BE 12" WHEN POSTED SPEED > 45 MPH
CROSS HATCHED ISLANDS ARE TO BE INSTALLED WHERE CALLED FOR ON THE PLANS



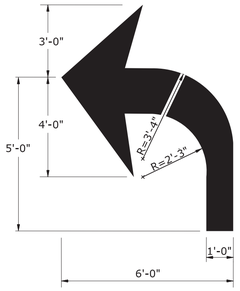
YIELD LINE DETAIL
(WHITE)



REFER TO ADJACENT VIEWS FOR DIMENSIONING
27.0 S.F.



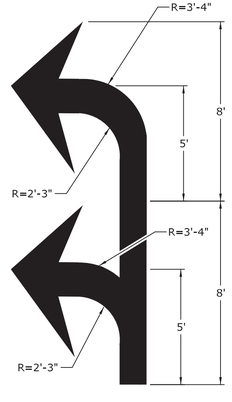
12.5 S.F.



15.5 S.F.

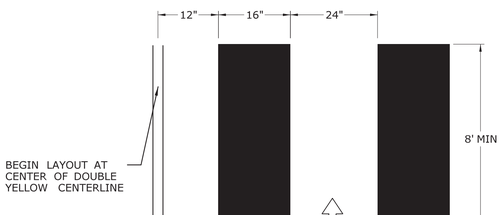
27.0 S.F.
LEFT & RIGHT
ARROW COMBO

38.5 S.F.
3 WAY ARROW
COMBO

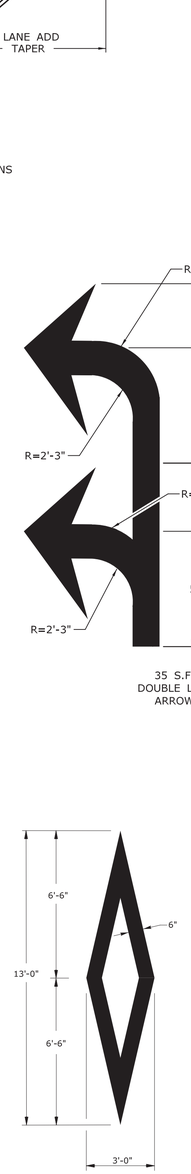


35 S.F.
DOUBLE LEFT
ARROW

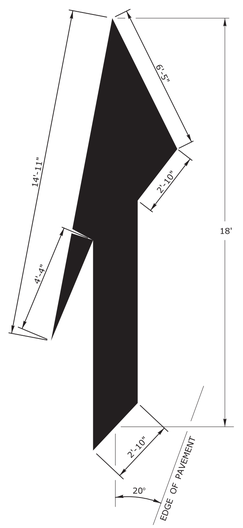
PAVEMENT ARROW DETAILS
(WHITE)
ARROWS SHALL BE CENTERED IN TRAVEL LANE



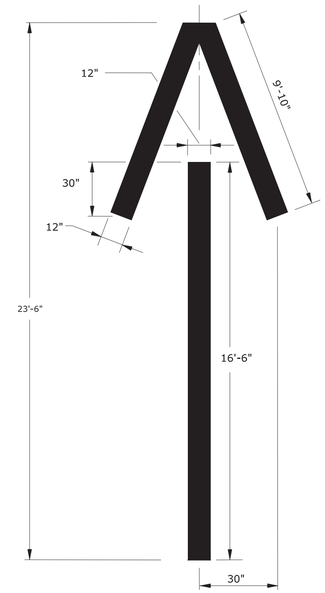
CROSSWALK DETAIL
(WHITE)



WHITE PREFERENTIAL
LANE SYMBOL
13.0 S.F.

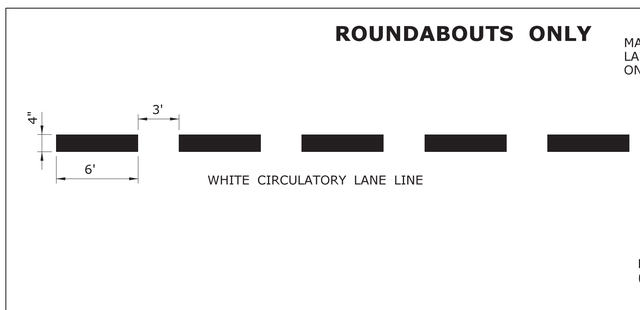


WHITE LANE
REDUCTION ARROW
41.8 S.F.



WHITE WRONG WAY
PAVEMENT ARROW
36.2 S.F.

ROUNDABOUTS ONLY



NOTES :

1. AREA OF PAVEMENT MARKINGS AS INDICATED IS APPROXIMATE.
2. RIGHT TURN PAVEMENT MARKING ARROWS ARE MIRROR IMAGE OF LEFT TURN PAVEMENT MARKING ARROWS.

REV.	DATE	REVISION DESCRIPTION

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 4/3/2017

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: TR-1210_04.dgn Model: CT_Civil_2D_Sheet

SUBMITTED BY: NAME/DATE/TIME:
Mark F. Makuch, P.E.
2017.04.19 11:11:04-04'00'

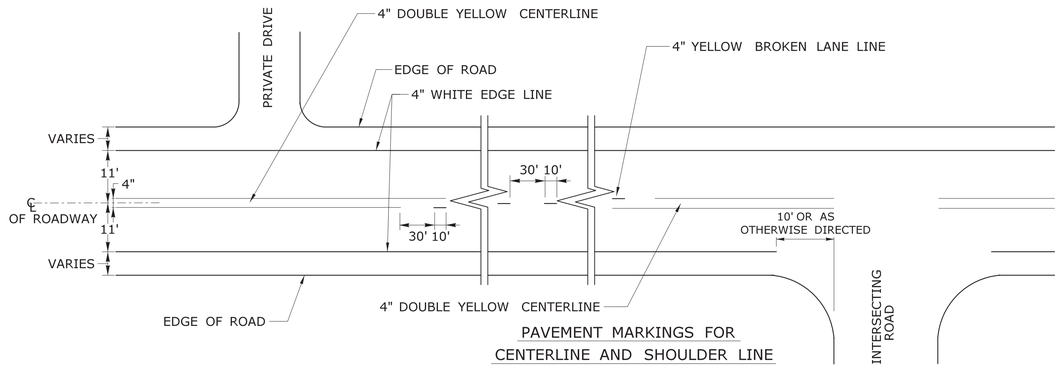
APPROVED BY: NAME/DATE/TIME:
Gregory M. Dorosh, P.E.
2017.04.20 13:16:12-04'00'

CTDOT
STANDARD SHEET

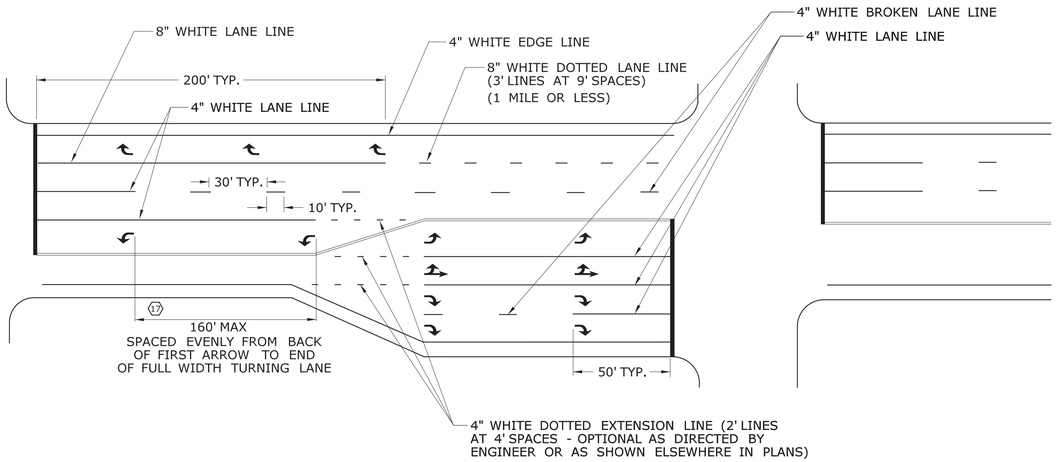
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
PAVEMENT MARKING LINES AND SYMBOLS

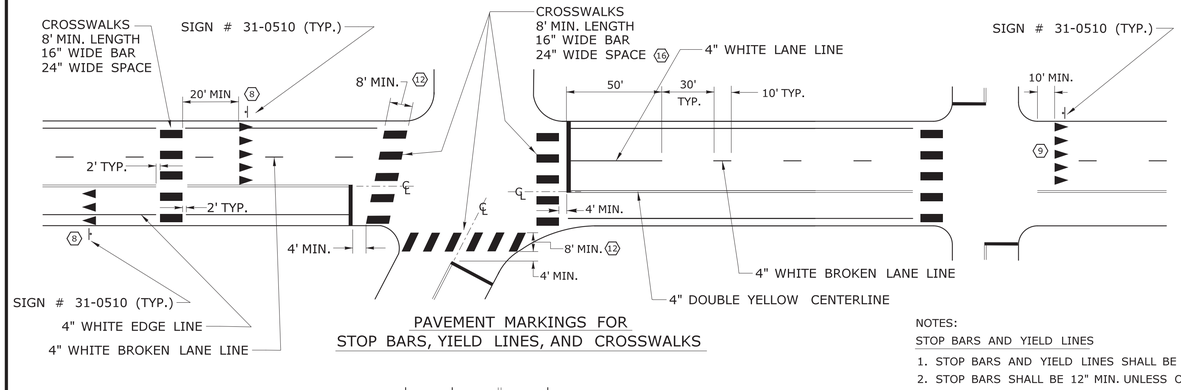
STANDARD SHEET NO.:
TR-1210_04



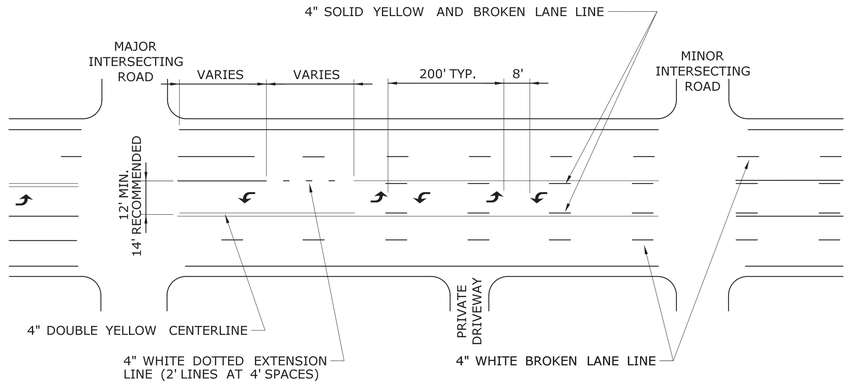
PAVEMENT MARKINGS FOR
CENTERLINE AND SHOULDER LINE



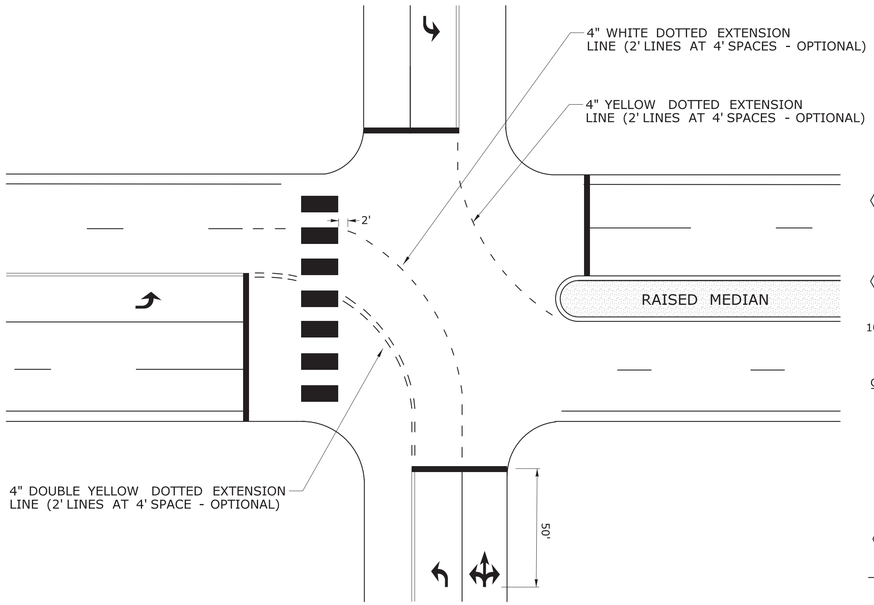
PAVEMENT MARKINGS FOR TURNING LANES
AND ROAD LANE-DROP



PAVEMENT MARKINGS FOR
STOP BARS, YIELD LINES, AND CROSSWALKS



TWO-WAY LEFT-TURN MARKINGS APPLICATIONS



TURNING EXTENSIONS

NOTES:

STOP BARS AND YIELD LINES

1. STOP BARS AND YIELD LINES SHALL BE WHITE.
2. STOP BARS SHALL BE 12" MIN. UNLESS OTHERWISE NOTED ON PLANS.
3. STOP BARS TO BE PLACED A MINIMUM OF 4' IN ADVANCE OF THE NEAREST EDGE OF CROSSWALK AND SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY.
4. IN THE ABSENCE OF A MARKED CROSSWALK THE STOP BAR SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY, AT THE DESIRED STOPPING POINT AT LEAST 5' AND NO MORE THAN 30' FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY.
5. THE STOP SIGN SHOULD BE PLACED IN LINE WITH THE STOP BAR. HOWEVER, IF THE STOP SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO STOP, THE STOP BAR SHOULD BE PLACED AT THE STOPPING POINT.
6. FOR STOP BARS AT RAMPS SEE DETAILS "R", "S", "T", & "U" AND NOTES ON TRAFFIC STANDARD SHEET TR-1210 07 "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS EXIT RAMPS".
7. FOR YIELD LINE INSTALLATIONS, ONLY FULL TRIANGLES ARE TO BE INSTALLED.

⑧ MID-BLOCK CROSSWALKS ARE CROSSWALKS LOCATED MORE THAN 50 FEET FROM A SIGNALIZED OR UNSIGNALIZED INTERSECTION. YIELD LINES ASSOCIATED WITH MIDBLOCK CROSSWALKS SHOULD BE INSTALLED 20 TO 50 FEET IN ADVANCE OF THE NEAREST CROSSWALK LINE OR AS DIRECTED BY THE ENGINEER. SIGN # 31-0510 IS REQUIRED.

⑨ FOR CROSSWALKS AT UNSIGNALIZED INTERSECTIONS WITH MINOR STREET STOP CONTROL, YIELD LINES SHALL BE INSTALLED ON MULTI-LANE APPROACHES, BUT NOT SINGLE LANE APPROACHES.

10. THE YIELD SIGN SHOULD BE PLACED IN LINE WITH A YIELD LINE. HOWEVER, IF THE YIELD SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO YIELD, THE YIELD LINE SHOULD BE PLACED AT THE YIELDING POINT.

CROSSWALKS

11. CROSSWALK MARKINGS SHALL BE WHITE.

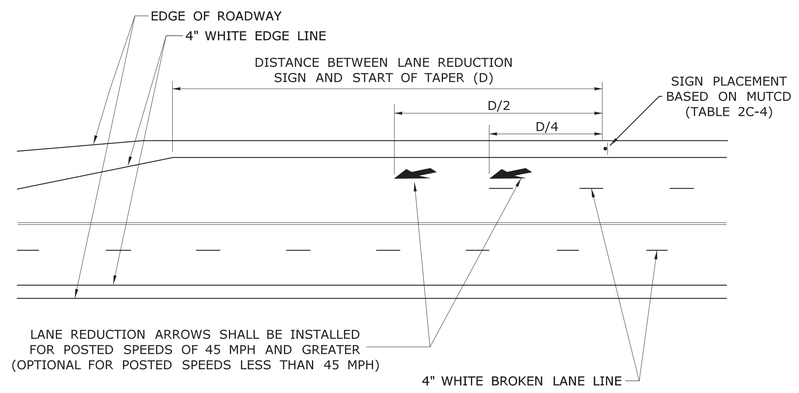
⑫ AT LOCATIONS WHERE THE CROSSWALK IS SKEWED, BARS TO BE PARALLEL TO ℄ AND ENDS OF BARS TO BE PARALLEL. THE LENGTH OF THE BARS WILL VARY DEPENDING ON THE ANGLE OF SKEW.

13. BARS SHOULD BE NO CLOSER THAN 1' FROM EDGE OF ROAD.
14. ONLY FULL LENGTH BARS ARE TO BE INSTALLED.
15. DECORATIVE CROSSWALKS SHALL BE BANDED FROM CURB TO CURB WITH A MINIMUM 12" WIDE WHITE TRANSVERSE LINE ALONG EACH EDGE.

⑯ 24" WIDE SPACE TO BE CENTERED ON YELLOW CENTERLINE.

PAVEMENT MARKINGS FOR TURNING LANES

⑰ INSTALL AT LEAST TWO ARROWS PER LANE WHERE STORAGE LENGTH IS GREATER THAN 150 FEET.



LANE REDUCTION TRANSITIONS

REV.	DATE	REVISION DESCRIPTION

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 4/3/2017

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: TR-1210_08.DGN Model: TR-1210_05

SUBMITTED BY: NAME/DATE/TIME:
Mark F. Makuch, P.E.
2017.04.19 11:14:11-04'00'

APPROVED BY: NAME/DATE/TIME:
Gregory M. Dorosh, P.E.
2017.04.20 13:25:03-04'00'

CTDOT
STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
PAVEMENT MARKINGS
FOR NON FREEWAYS

STANDARD SHEET NO.:
TR-1210_08

E5 - SERIES

E5-1

EXIT

COPY & BORDER - WHITE
BACKGROUND - GREEN

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
16.0	48	51-6147	2

16 - SERIES

CONSTRUCTION AHEAD
ROAD USE RESTRICTED
STATE LIABILITY LIMITED
GENERAL STATUTES SEC 13a-115, 13a-145
COMMISSIONER OF TRANSPORTATION

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
16-M	5.0	30X24	80-1613	1
16-H	17.5	60X42	80-1608	2
16-E	35.0	84X60	80-1605	2B

CONSTRUCTION AHEAD
SIDEWALK USE RESTRICTED
STATE LIABILITY LIMITED
GENERAL STATUTES SEC 13a-115, 13a-145
COMMISSIONER OF TRANSPORTATION

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
16-S	10.0	48X30	80-1619	2

G20 - SERIES

G20-2a

END
ROAD WORK

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
8.0	48X24	80-9612	2

ROAD WORK
NEXT 0 MILE(S)
BE PREPARED
TO STOP

VARIABLE MILEAGE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
90.0	120X108	80-9728	

WORK AREA
BE PREPARED
TO STOP

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9711	1
16.0	48	80-9712	2

NEXT 0 MILES

VARIABLE MILEAGE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
7.0	72X14	80-9720	

BUSINESS ACCESS

VARIABLE ARROW
COPY & BORDER - WHITE
BACKGROUND - BLUE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	50-5934	2

M4 - SERIES

M4-8

DETOUR

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
2.0	24X12	80-9707	1

M4-8a

END
DETOUR

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
3.0	24X18	80-9708	1

M4-10

(R) DETOUR
(L) DETOUR

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
6.0	48X18	80-9701R	2
6.0	48X18	80-9702L	2

DETOUR

VARIABLE ARROW

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
5.0	30X24	80-9710	1

M4-9b

DETOUR

VARIABLE ARROW

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
5.0	30X24	80-9703	1

M4-9b

DETOUR

VARIABLE ARROW

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
6.25	30X30	80-9706	1

R1 - SERIES

R1-1

STOP

*

COPY & BORDER - WHITE
BACKGROUND - RED

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
5.19	30	31-0552	1
13.30	48	31-0557	2

R1-2

YIELD

*

COPY & BORDER - RED
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
3.90	36	31-0523	1
10.83	60	31-0528	2B

R4 - SERIES

R4-7

COPY & BORDER - BLACK
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
5.0	24X30	31-1526	1

R9 & R11 - SERIES

R9-9

SIDEWALK CLOSED

COPY & BORDER - BLACK
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
3.75	30X18	80-9076	1

R9-11

SIDEWALK CLOSED AHEAD
CROSS HERE

VARIABLE ARROW
COPY & BORDER - BLACK
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
3.0	24X18	80-9074	1

R9-11a

SIDEWALK CLOSED
CROSS HERE

VARIABLE ARROW
COPY & BORDER - BLACK
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
2.0	24X12	80-9075	1

R11-2

ROAD CLOSED

COPY & BORDER - BLACK
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
10.0	48X30	80-9080	2

R11-3a

ROAD CLOSED
00 MILES AHEAD
LOCAL TRAFFIC ONLY

VARIABLE MILEAGE
COPY & BORDER - BLACK
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
12.5	60X30	80-9077	2

R11-3b

BRIDGE CLOSED
00 MILES AHEAD
LOCAL TRAFFIC ONLY

VARIABLE MILEAGE
COPY & BORDER - BLACK
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
12.5	60X30	80-9078	2

R11-3b

ROAD CLOSED
TO
THRU TRAFFIC

COPY & BORDER - BLACK
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
12.5	60X30	80-9081	2

ROAD WORK AHEAD
FINES DOUBLED

COPY & BORDER - BLACK
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
14.0	48X42	31-1906	2
22.5	60X54	31-1907	2
30.0	72X60	31-1908	2B

W1 - SERIES

W1-4

(L) (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9432L	1
9.0	36	80-9431R	1
16.0	48	80-9452L	2
16.0	48	80-9451R	2

W1-6

(L or R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
8.0	48X24	80-9424	2
12.5	60X30	80-9423	2

W1-8

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
3.0	18X24	80-9401	1
5.0	24X30	80-9403	1
7.5	30X36	80-9404	1

BOTH LANES SHIFT LEFT
AHEAD (L)

BOTH LANES SHIFT RIGHT
AHEAD (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
16.0	48	80-9433L	2
16.0	48	80-9435R	2

BOTH LANES SHIFT LEFT (L)

BOTH LANES SHIFT RIGHT (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
16.0	48	80-9434L	2
16.0	48	80-9436R	2

AHEAD (L) AHEAD (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
25.0	60	80-9443L	2B
25.0	60	80-9445R	2B

(L) (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
25.0	60	80-9444L	2B
25.0	60	80-9446R	2B

AHEAD (L) AHEAD (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
25.0	60	80-9483L	2B
25.0	60	80-9485R	2B

(L) (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
25.0	60	80-9484L	2B
25.0	60	80-9486R	2B

W3 - SERIES

W3-1

STOP AHEAD

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9808	1

W3-1a

OCTAGON - RED W/ WHITE BORDER
ARROW & BORDER - BLACK
BACKGROUND - FLUORESCENT ORANGE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9050	1
16.0	48	80-9051	2

W3-2a

TRIANGLE - RED W/ WHITE BORDER
ARROW & BORDER - BLACK
BACKGROUND - FLUORESCENT ORANGE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9054	1
16.0	48	80-9055	2

W3-3

TOP CIRCLE - RED
MIDDLE CIRCLE - YELLOW
BOTTOM CIRCLE - GREEN
COPY & BORDER - BLACK
BACKGROUND - FLUORESCENT ORANGE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9052	1
16.0	48	80-9053	2

W4-W6 - SERIES

W4-2

(L) (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
16.0	48	80-9918L	2
16.0	48	80-9917R	2

W6-3

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
16.0	48	80-9945	2

W8-W9 - SERIES

W8-1

BUMP

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9901	1
16.0	48	80-9902	2

BUMP AHEAD

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9801	1
16.0	48	80-9802	2

W9-2

LANE ENDS MERGE LEFT (L)
LANE ENDS MERGE RIGHT (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
16.0	48	80-9910L	2
16.0	48	80-9911R	2

W13 - SERIES

W13-1

00 M.P.H.

SUBPLATE
VARIABLE SPEED

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
4.0	24	80-9569	1
6.25	30	80-9567	1

REDUCE SPEED TO 00 MPH

VARIABLE SPEED

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9506	1
16.0	48	80-9508	2

SPEED LIMIT AHEAD
00 MPH

VARIABLE SPEED

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
12.0	72X24	80-9519	2

W20 - SERIES

W20-1

ROAD WORK AHEAD

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
6.25	30	80-9602	1
9.0	36	80-9603	1
16.0	48	80-9604	2

W20-1

ROAD WORK 0000 FT

VARIABLE DISTANCE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9614	1
16.0	48	80-9615	2

W20-2

DETOUR 1000 FT

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9805	1
16.0	48	80-9806	2

W20-4

ONE LANE ROAD AHEAD

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9834	1
16.0	48	80-9835	2

LEFT LANES CLOSED (L) RIGHT LANES CLOSED (R)

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
(L)	16.0	48	80-9836	2
(R)	16.0	48	80-9839	2

LEFT TWO LANES CLOSED AHEAD (L) RIGHT TWO LANES CLOSED AHEAD (R)

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
(L)	16.0	48	80-9837	2
(R)	16.0	48	80-9838	2

LEFT LANE CLOSED AHEAD (L) RIGHT LANE CLOSED AHEAD (R)

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
(L)	16.0	48	80-9847	2
(R)	16.0	48	80-9848	2

LEFT LANE CLOSED (L) RIGHT LANE CLOSED (R)

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
(L)	16.0	48	80-9846	2
(R)	16.0	48	80-9849	2

W20-7a

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9803	1
16.0	48	80-9804	2

SLOW MOVING TRUCKS AHEAD

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
32.0	96X48	80-9815	2B

AHEAD

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
2.0	24X12	80-9870	
3.75	30X18	80-9871	

1 MILE

VARIABLE MILEAGE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
2.5	30X12	80-9872	
4.5	36X18	80-9873	

W21 - SERIES

W21-6

SURVEY CREW

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9607	1

PEDESTRIAN DETOUR

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
2.0	24X12	80-9912	1

W22 - SERIES

W22-1

BLASTING ZONE 1000 FT

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9620	1
16.0	48	80-9625	2

W22-2

TURN OFF 2-WAY RADIO AND CELL PHONE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
10.5	42X36	80-9623	2

W22-3

END BLASTING ZONE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
7.5	36X30	80-9622	2
10.5	42X36	80-9621	2

(VARIABLE LEGEND)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
4.17	60X10	80-9913	2
12.0	96X18	80-9914	2
3.33	48X10	80-9916	2

BLANK OR VARIABLE LEGEND

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
9.0	36	80-9933	1
16.0	48	80-9934	2

(VARIABLE LEGEND)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
12.5	60X30	80-9928	2
24.0	72X48	80-9929	2

STAY IN LINE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
16.0	48	80-9951	2

USE SHOULDER

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
16.0	48	80-9956	2

SHOULDER CLOSED AHEAD (1) SHOULDER CLOSED (2)

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
(1)	16.0	48	80-9957	2
(2)	9.0	36	80-9958	1
(2)	16.0	48	80-9959	2

STOP-SLOW PADDLE

SIDE A * STOP SIDE B * SLOW

SIDE A
BACKGROUND - RED
COPY & BORDER - WHITE
SIDE B
BACKGROUND - ORANGE
COPY & BORDER - BLACK PLAIN

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
2.51	19	80-9950	PADDLE

NEW

COPY & BORDER - BLACK
BACKGROUND - YELLOW

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
2.0	12X24	41-0815	

NOTES:
1. R1-SERIES SIGN THE LEGEND "O.S.T.A." SHALL APPEAR.
2. POSTS - SEE STANDARD SHEET TR-1208_02 - "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
3. POSTS - TYPE A (EXCEPT WHERE NOTED WITH A "B" FOR TYPE B)
4. ALL POSTS NOTED ARE FOR LONG TERM INSTALLATION. SEE STANDARD SHEET TR-1208_02.
5. FOR TEMPORARY SUPPORTS SEE STANDARD SHEET TR-1220_02 - "CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES".
6. FOR SPECIFIC SIGN DESIGN, CONTACT CONN. D.O.T., DIVISION OF TRAFFIC ENGINEERING. FOR BOLT HOLE PATTERN REFER TO FHWA PUBLICATION "STANDARD HIGHWAY SIGNS". SIGNS OF DIFFERENT DIMENSIONS TO BE ERECTED ON THE SAME POSTS, OR SPAN/MAST ARM MOUNTED, MAY REQUIRE SPECIAL BOLT HOLE PATTERNS.
7. ALL CONSTRUCTION SIGNS TO BE PAID FOR UNDER THE CONSTRUCTION SIGNS ITEM IN THE CONTRACT.
8. MATERIALS & COLORS SHALL CONFORM TO STATE SPECIFICATIONS.

MATERIALS:
SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES. ALUMINUM THICKNESS FOR POST MOUNTED SIGNS SHALL BE .100" EXCEPT SIGN #s. 80-9815, 80-9728, 80-9519, & 51-6147 (L OR R) WHICH SHALL BE .125", PLYWOOD THICKNESS FOR POST MOUNTED SIGNS SHALL BE 1/2" EXTERIOR GRADE A-C OR BETTER. SIGN BLANKS SHALL HAVE ONE COAT OF PRIMER PAINT PRIOR TO APPLICATION OF RETROREFLECTIVE SHEETING & COPY.

COLORS:
BACKGROUND - FLUORESCENT ORANGE - EXCEPT AS NOTED.
LEGEND - BLACK - EXCEPT AS NOTED.
ALL SIGNS WITH FLUORESCENT ORANGE BACKGROUND TO USE TYPE VIII RETROREFLECTIVE SHEETING.
ALL OTHER SIGNS TO USE TYPE IV RETROREFLECTIVE SHEETING EXCEPT AS NOTED BY *.
* SIGNS TO USE TYPE IX RETROREFLECTIVE SHEETING.

REV.	DATE	REVISION DESCRIPTION
5	8-2015	UPDATED PER MUTCD AND FORM 816 JAN 2015 REVISION.
4	6-2012	REVISED NOTE #1 TO REFERENCE "O.S.T.A."
3	4-2012	REVISED NEW SIGNAL SIGN(S) TO CONFORM TO 2009 MUTCD.
2	2-2011	MINOR REVISIONS.
1	3-2010	REMOVED OBSOLETE SIGNS (50-5925, 50-5935).

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 8/25/2015

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.DGN Model: TR-1220_01

SUBMITTED BY: NAME/DATE/TIME:
Mark Makuch
Mark F. Makuch, P.E.
2015.08.26 07:15:03-04'00'

APPROVED BY: NAME/DATE/TIME:
Charles S. Harlow, P.E.
2015.08.28 11:40:30-04'00'

CTDOT STANDARD SHEET

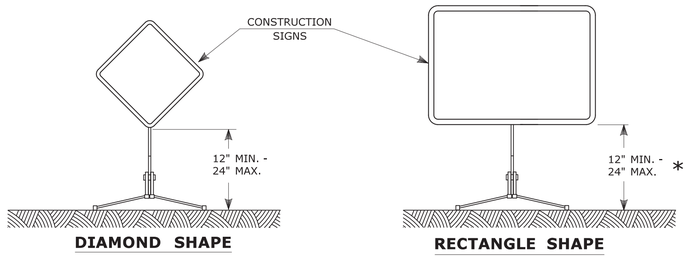
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS

STANDARD SHEET NO.:

TR-1220_01

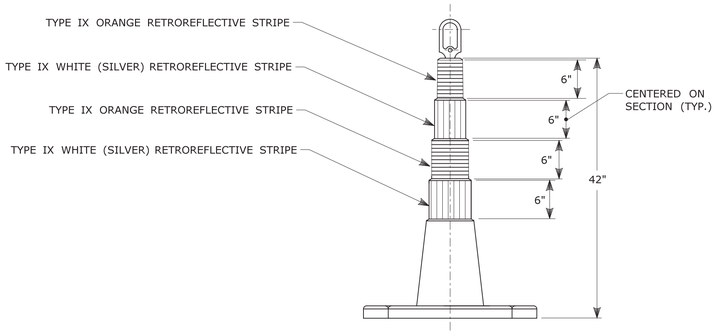


PORTABLE CONSTRUCTION SIGNS

NOTES FOR PORTABLE SIGN SUPPORTS:

1. SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES AND THE LATEST EDITION OF THE MUTCD.
2. MOUNTING HEIGHT OF SIGNS SHALL BE A MINIMUM OF 12" AND A MAXIMUM OF 24". SIGNS SHALL BE MOUNTED HIGHER AS NEEDED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
3. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY SUPPORT DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
4. PORTABLE SIGN SUPPORTS SHALL BE STABILIZED IN A MANNER THAT WILL NOT AFFECT THEIR COMPLIANCE WITH NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES.
5. PORTABLE CONSTRUCTION SIGN SUPPORTS SHOULD NOT BE USED FOR DURATION OF MORE THAN 3 DAYS EXCEPT FOR R9-8 THROUGH R9-11a SERIES, R11 SERIES, W1-6 THROUGH W1-8 SERIES, M4-10, AND E5-1. SEE STANDARD SHEET TR-1220_01 - "SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS" FOR SIGN DETAILS.

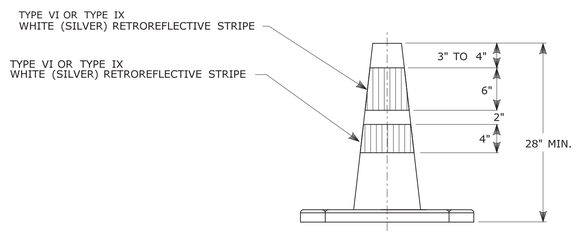
* FOR E5-1 (EXIT SIGNS) USE MIN 48".



42" TRAFFIC CONE

NOTES:

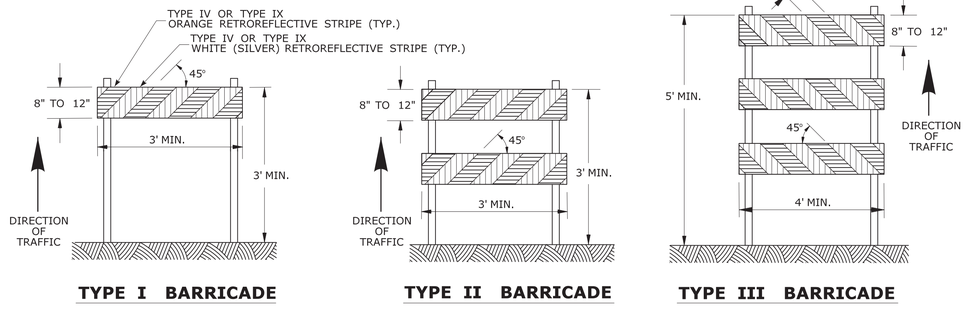
1. TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
2. IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
3. IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
4. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
5. THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
6. THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



TRAFFIC CONE

NOTES:

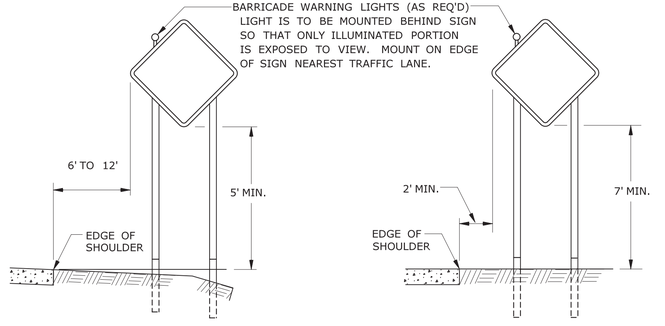
1. TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
2. IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
3. IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
4. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
5. TRAFFIC CONES NOT USED AT NIGHT MAY UTILIZE TYPE III SHEETING.
6. THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



CONSTRUCTION BARRICADES

NOTES:

1. CONSTRUCTION BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH AND THE LATEST EDITION OF THE MUTCD.
2. MARKINGS FOR BARRICADE RAILS SHALL BE ALTERNATE ORANGE AND WHITE STRIPES SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. 6" WIDE STRIPES SHALL BE USED.
3. THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS. THE SIDES OF BARRICADES FACING TRAFFIC SHALL HAVE RETROREFLECTIVE RAIL FACES.
4. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY BARRICADE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
5. CORNERS OF BARRICADE RAILS SHALL BE ROUNDED.
6. SIGNS MAY ONLY BE INSTALLED ON TYPE III BARRICADES AND SHALL BE PLACED SO AS TO COVER NO MORE THAN ONE BARRICADE RAIL.

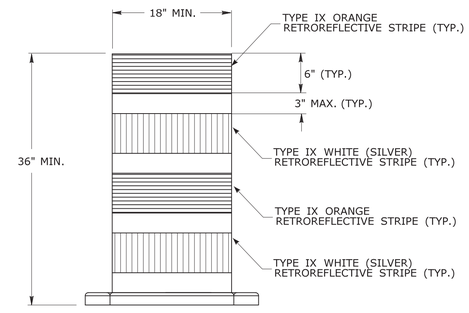


PLACEMENT OF CONSTRUCTION SIGNS TYPICAL LONG TERM INSTALLATION

NOTES:

SUPPORTS SHALL BE METAL SIGN POSTS AND HAVE BREAK-AWAY FEATURES.

SEE TYPICAL SHEETS:
"TYPICAL SIGN SUPPORT AND SIGN PLACEMENT DETAILS-GORE EXIT SIGN"
"TYPICAL METAL SIGN POSTS AND SIGN MOUNTING DETAILS"



TRAFFIC DRUM FRONT VIEW

NOTES:

1. TRAFFIC DRUM SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
2. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY DRUM DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
3. THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
4. THE SECTIONS OF DRUMS NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.

REV.	DATE	REVISION DESCRIPTION
2	8-2015	UPDATED PER MUTCD AND FORM 816 JAN 2015 REVISION.
1	2-2011	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 8/14/2015

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.DGN Model: TR-1220_02

SUBMITTED BY: NAME/DATE/TIME:
Mark F. Makuch, P.E. 2015.08.26 07:15:47-04'00'

APPROVED BY: NAME/DATE/TIME:
Charles S. Harlow, P.E. 2015.08.28 11:40:57-04'00'

CTDOT STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES

STANDARD SHEET NO.:
TR-1220_02