

**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

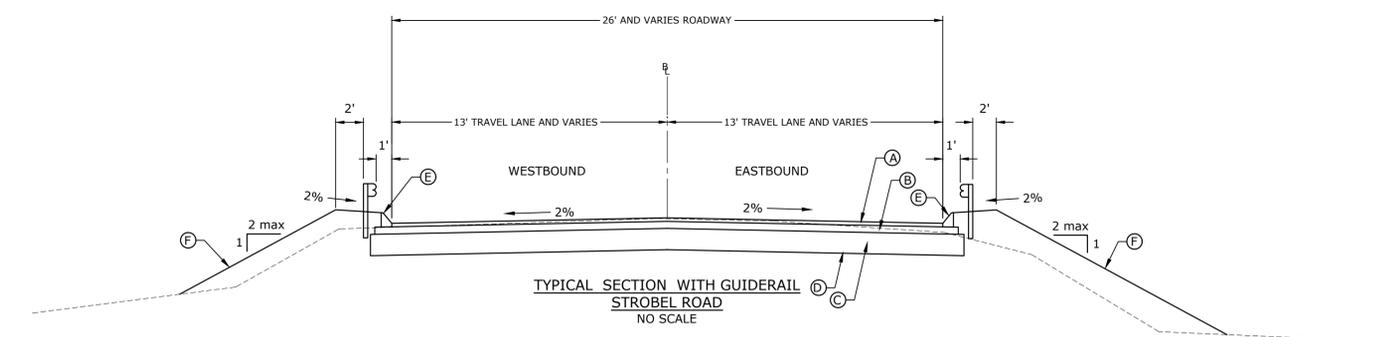
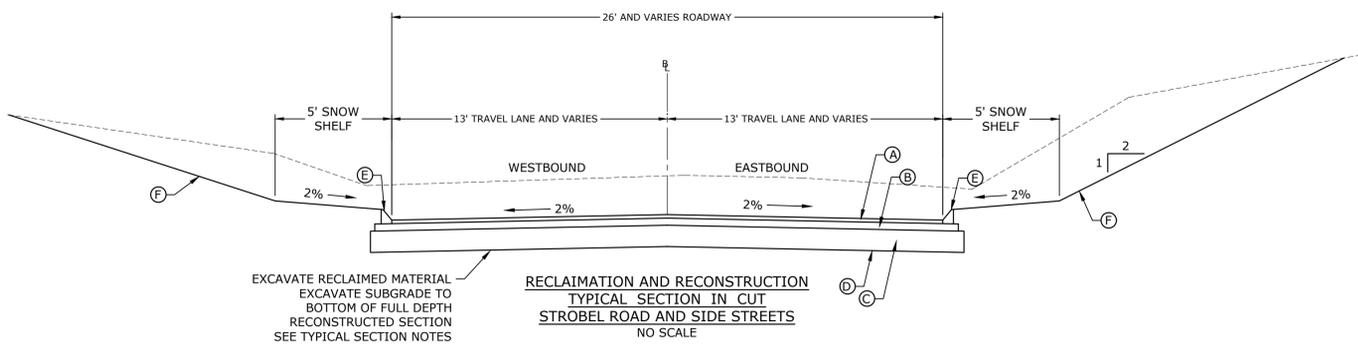
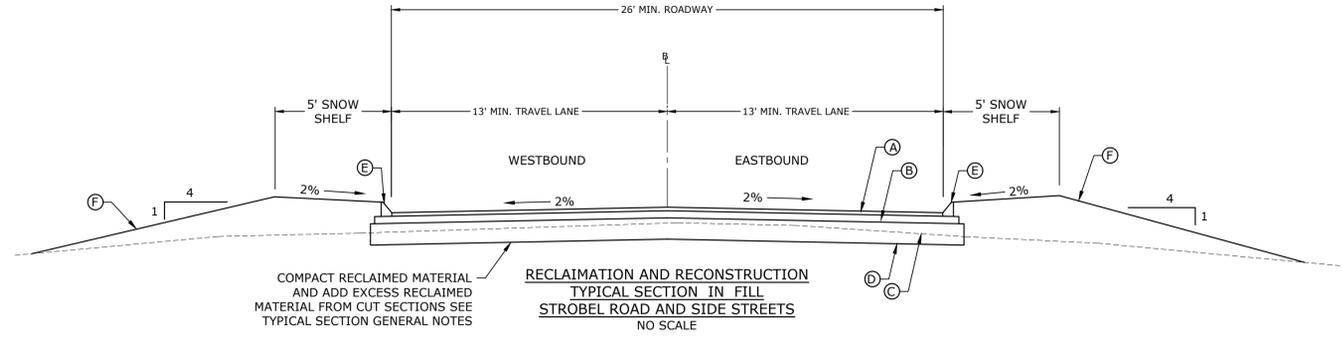
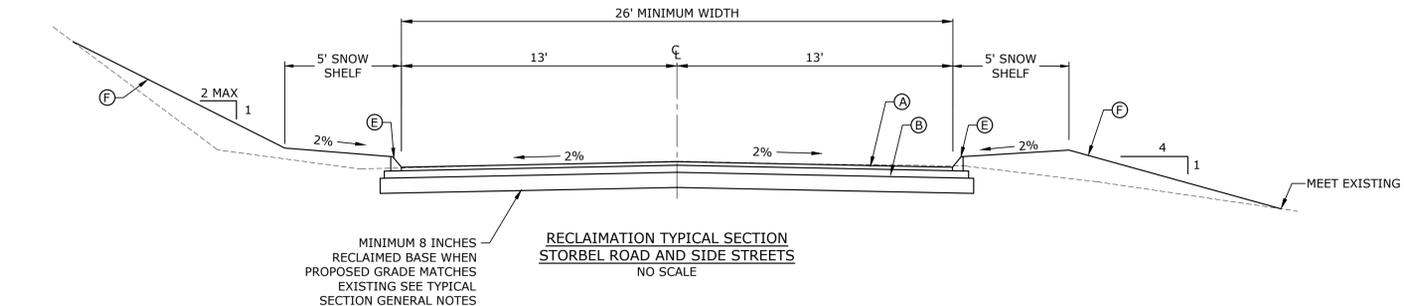
BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-15-C-001-INDEX.dwg	
DRAWN BY:	RAS	
CHECKED:	CRD	
APPROVED:	AJM	

SHEET INDEX PLAN

SCALE: 1" = 100'

C0.00



TYPICAL SECTION GENERAL NOTES:

1. DANIELS FARM ROAD SHALL BE RECLAIMED TO A DEPTH OF 13 INCHES, IN PHASES TO MAINTAIN TRAFFIC, TO THE LIMITS OF CONSTRUCTION.
2. IN AREAS OF CUT THE RECLAIMED MATERIAL IS TO BE STORED WHILE EXCAVATION TO THE BOTTOM OF PROPOSED BASE IS COMPLETED. FORMATION OF THE SUBGRADE CAN THEN BE COMPLETED AND 12 INCHES OF STORED RECLAIM MATERIAL CAN BE PLACED. SEE SPECIAL PROVISIONS, ITEM #0403871A - HANDLING COLD RECLAIMED ASPHALT PAVEMENT.
3. IN AREAS OF FILL THE RECLAIMED MATERIAL IS TO BE COMPACTED AND ADDITIONAL RECLAIM MATERIAL MAY NEED TO BE ADDED TO REACH THE TOP OF PROPOSED BASE MATERIAL.
4. IN AREAS WHERE THE PROPOSED GRADE DOES NOT CHANGE, THE TOP 5 INCHES OF RECLAIMED MATERIAL SHALL BE REMOVED FOR A MINIMUM RECLAIMED BASE THICKNESS OF 8 INCHES. IF A MINIMUM 8 INCHES OF RECLAIMED BASE MATERIAL IS NOT OBTAINED AT THE GUTTER LINE, THE RECLAIMED MATERIAL MUST BE STORED AND SUBBASE MATERIAL EXCAVATED TO THE FULL DEPTH PAVEMENT RECONSTRUCTION.
5. IN AREAS OF WIDENING OR RE-ALIGNMENT, CONTRACTOR SHALL EXCAVATE TO THE FULL DEPTH OF PAVEMENT RECONSTRUCTION BEFORE PLACING SURPLUS RECLAIMED BASE MATERIAL.

TYPICAL SECTION MATERIAL NOTES:

1. FORMATION OF SUBGRADE SHALL BE IN ACCORDANCE WITH SECTION 2.09 OF THE CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 817.
2. TACK COAT MATERIAL SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 4.06 OF THE CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 817.
3. BITUMINOUS CONCRETE SIDEWALK SHALL BE PROVIDED IN ACCORDANCE WITH TOWN OF TRUMBULL STANDARD DETAILS AND SPECIFICATIONS.

TYPICAL SECTION LEGEND:

- (A) - 2" SUPERPAVE HMA S0.375
- (B) - 3" SUPERPAVE HMA S1.0
- (C) - 12" RECLAIMED BASE MATERIAL
- (D) - FORMATION OF SUBGRADE
- (E) - BITUMINOUS CONCRETE LIP CURB
- (F) - TOPSOIL & TURF ESTABLISHMENT - 4" MIN

VERIFY SCALE
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 0 1 INCH
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STROBEL ROAD RECONSTRUCTION PROJECT

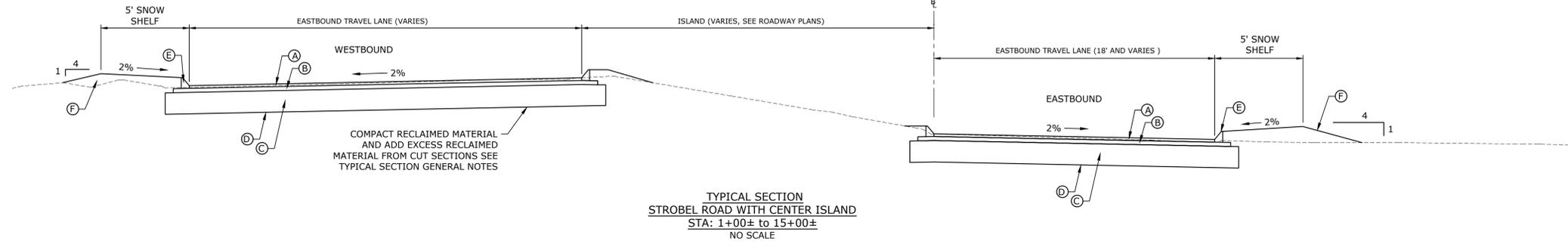
Trumbull, Connecticut

January 18, 2019

MARK	DATE	DESCRIPTION

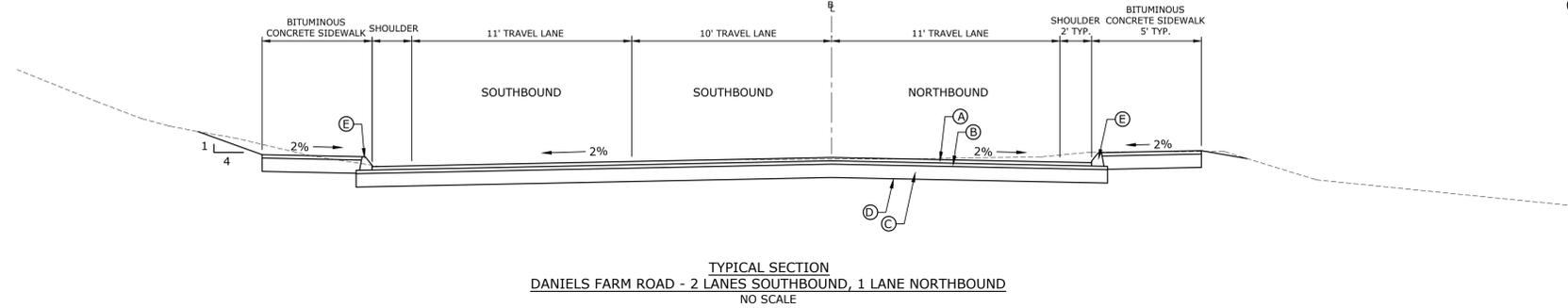
TYPICAL SECTIONS
 SCALE: NO SCALE

C2.0



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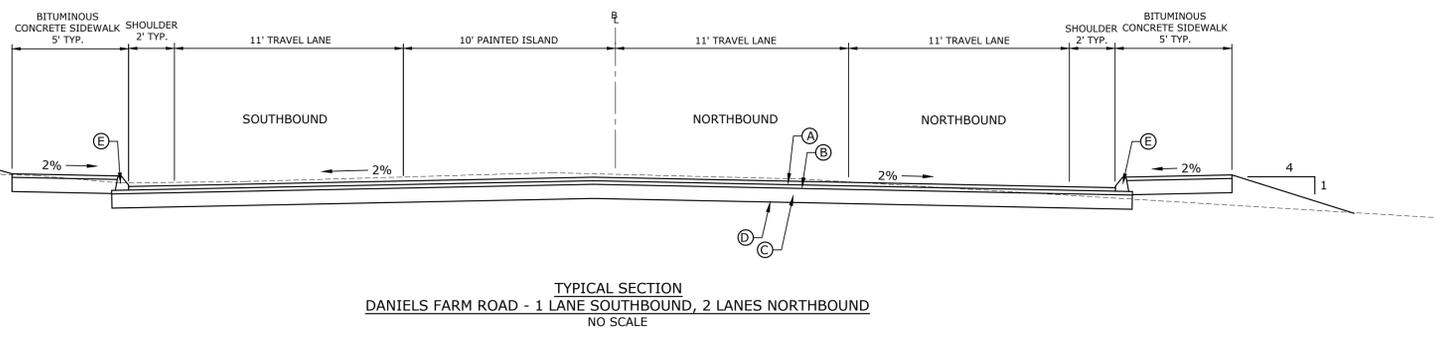
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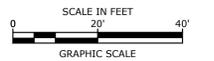
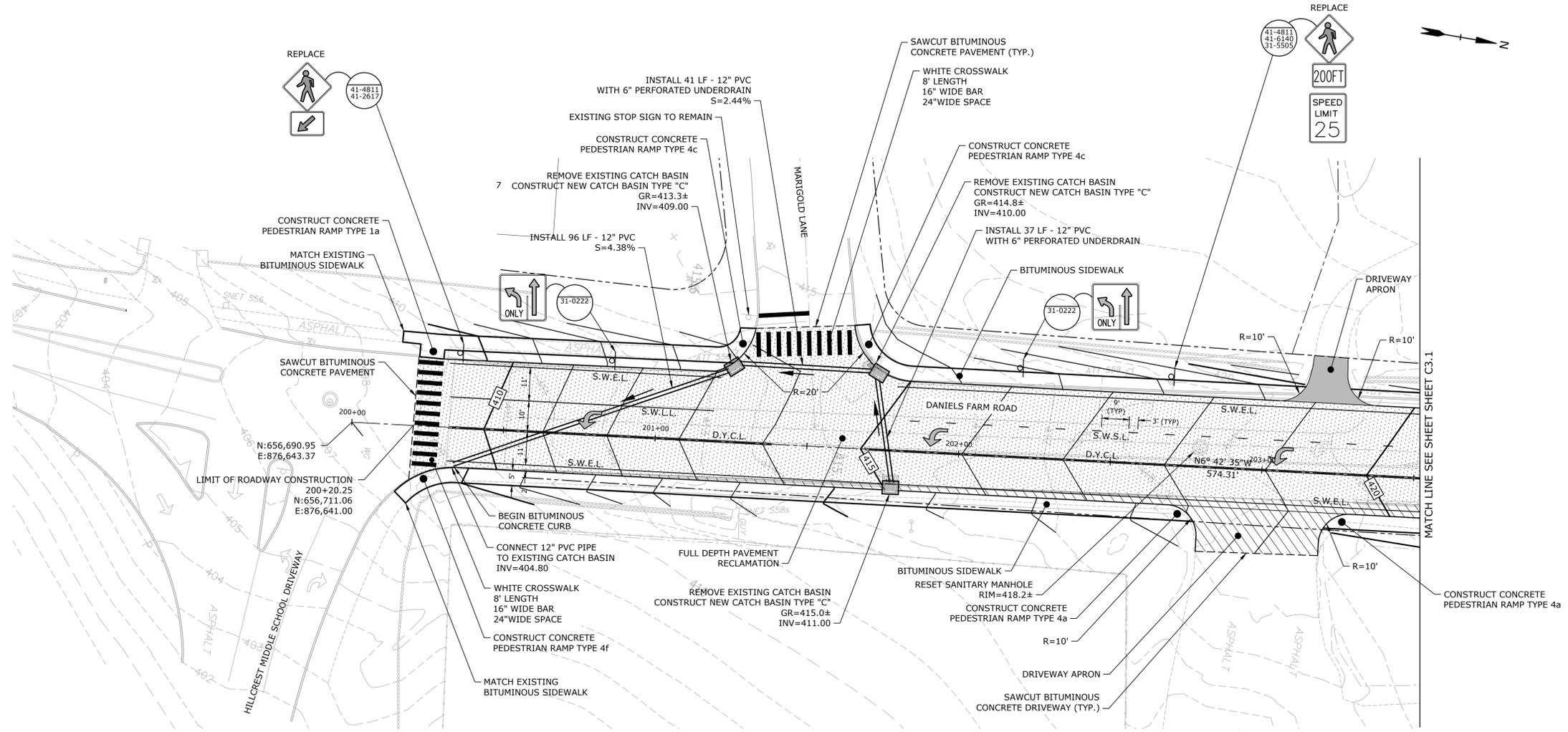
VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
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FILE:	T0196-15-C-200-TYP.dwg	
DRAWN BY:	MDS	
CHECKED:	CRD	
APPROVED:	AJM	

TYPICAL SECTIONS

SCALE: NO SCALE

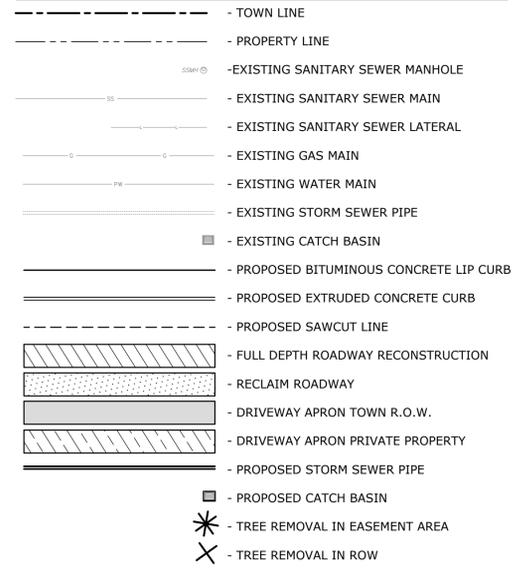
C2.1



ROADWAY PLAN NOTES:

- INFORMATION ON EXISTING CONDITIONS WAS TAKEN FROM GIS INFORMATION PROVIDED BY THE TOWN OF TRUMBULL ENGINEERING DEPARTMENT AND SUPPLEMENTED WITH INFORMATION TAKEN FROM PLAN SET: "TOWN OF TRUMBULL, CONNECTICUT, SANITARY SEWER SYSTEM, PHASE 4, PART B, CONTRACT IV, NORTH NICHOLS AREA, DATED DECEMBER 11, 2008, REVISED 5-17-10, SET 1 OF 2 AND SET 2 OF 2", AS PREPARED BY SPATH-BJORKLUND ASSOCIATES, INC. THE INFORMATION ON EXISTING UTILITIES HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY AND RECORD MAPS AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR AND/OR RESPONSIBLE PARTY IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES INCLUDING SERVICES. PRIOR TO ANY CONSTRUCTION, CALL "CALL BEFORE YOU DIG" AT 1-800-922-4455 AND VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS.
- ALL DRIVEWAY APRONS SHALL BE SAWCUT WHERE PROPOSED PAVEMENT MEETS EXISTING.
- ALL MANHOLE FRAMES, CATCH BASIN TOPS, AND UTILITY ACCESS COVERS SHALL BE SET TO THE TOP OF BINDER COURSE, HMA S 1.0. ALL MANHOLE FRAMES, CATCH BASIN TOPS, AND UTILITY ACCESS COVERS SHALL BE RESET TO FINISHED GRADE BEFORE SURFACE COURSE IS PLACED.
- SURFACE COURSE, HMA S 0.375 TO BE PLACED IN ONE CONTINUOUS PHASE.
- ALL DISTURBED MAIL BOXES SHALL BE ACCESSIBLE FOR MAIL DELIVERY THROUGHOUT THE CONSTRUCTION. THERE SHALL BE NO DIRECT PAYMENT FOR THIS ITEM BUT PAID FOR UNDER CLEARING AND GRUBBING.
- PAVEMENT AT ALL INTERSECTION CROSS-SECTIONS SHALL BE SAWCUT WHERE PROPOSED MEETS EXISTING.
- FINAL CURB LOCATIONS SHALL BE AS DIRECTED BY THE ENGINEER.
- DRIVEWAY APRON ON PRIVATE PROPERTY TO BE CONSTRUCTED WHEN RIGHT OF ENTRY APPROVAL IS OBTAINED FROM HOMEOWNER. IF RIGHT OF ENTRY IS NOT RECEIVED, ALL PROPOSED WORK SHALL STOP AT ROW LINE.

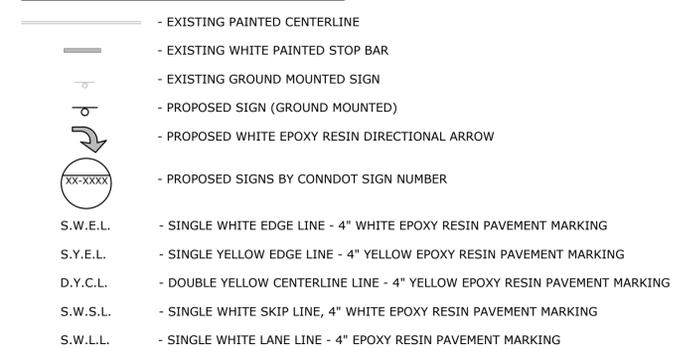
LEGEND



SIGNING AND PAVEMENT MARKING NOTES:

- INSTALL NEW SIGNS ON NEW METAL SIGN POSTS AS DIRECTED IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208_01 "SIGN SUPPORT & SIGN PLACEMENT DETAILS, GORE EXIT SIGN" AND TR-1208_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
- EXACT LOCATIONS OF THE SIGNS TO BE VERIFIED BY THE ENGINEER FOLLOWING LOCATION MARKING BY CONTRACTOR. THE CONTRACTOR SHALL ASSURE ADEQUATE SIGHTLINE IS PROVIDED TO SIGNS. ANY CLEARING OR TRIMMING NEEDED TO PROVIDE SIGHTLINE SHALL BE INCLUDED IN CLEARING AND GRUBBING.
- SIGNS SHALL BE AT LEAST 10- FEET FROM ALL UTILITY POLES.
- ALL PAVEMENT MARKINGS TO BE EPOXY RESIN.
- ALL PAVEMENT MARKINGS TO BE INSTALLED IN ACCORDANCE WITH STANDARD SHEET TR-1210_08 "PAVEMENT MARKINGS FOR NON-FREWAYS".
- CONTRACTOR TO REMOVE ALL CONFLICTING EXISTING PAVEMENT MARKINGS AND SIGNAGE.

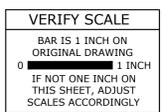
SIGNING AND PAVEMENT MARKING LEGEND:



STROBEL ROAD RECONSTRUCTION PROJECT

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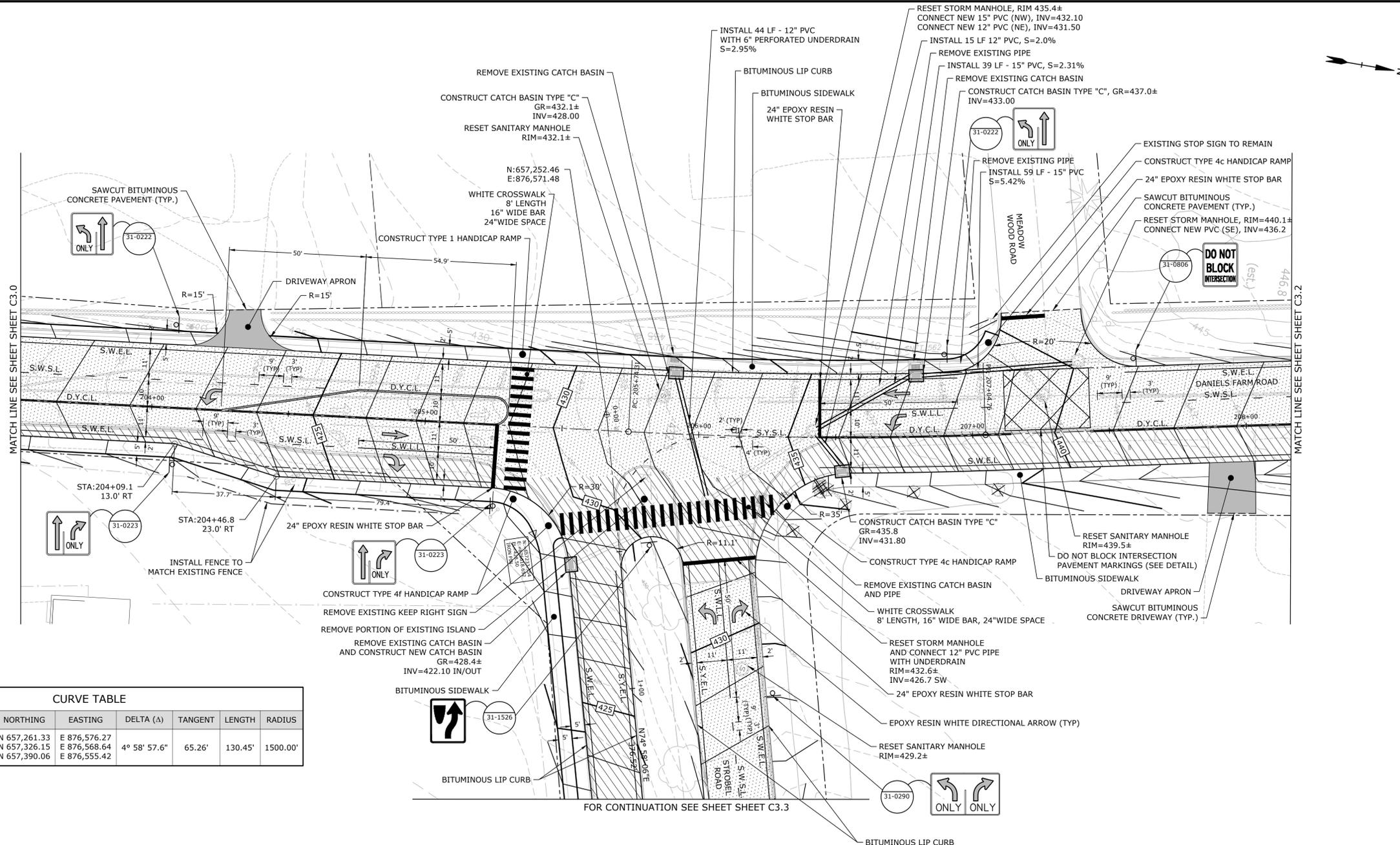
MARK	DATE	DESCRIPTION

ROADWAY PLAN
 STA 200+00 TO 203+25

SCALE: 1" = 20'

C3.0

Plot Sheet: 1/27/2019 9:10:04am By: D:\bubae\c
 Tighe & Bond: J:\1\10196\15_Strobel Rd\Drawing\Sheet\T0196-15-C-300-RDWY.dwg



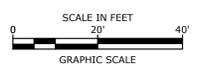
MATCH LINE SEE SHEET C3.0

MATCH LINE SEE SHEET C3.2

FOR CONTINUATION SEE SHEET C3.3



Curve #	STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C1	P.C.	205+74.31	N 657,261.33	E 876,576.27	4° 58' 57.6"	65.26'	130.45'
	P.I.	-	N 657,326.15	E 876,568.64			
	P.T.	207+04.76	N 657,390.06	E 876,555.42			



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LEGEND

- TOWN LINE
- PROPERTY LINE
- EXISTING SANITARY SEWER MANHOLE
- EXISTING SANITARY SEWER MAIN
- EXISTING SANITARY SEWER LATERAL
- EXISTING GAS MAIN
- EXISTING WATER MAIN
- EXISTING STORM SEWER PIPE
- EXISTING CATCH BASIN
- PROPOSED BITUMINOUS CONCRETE LIP CURB
- PROPOSED EXTRUDED CONCRETE CURB
- PROPOSED SAWCUT LINE
- FULL DEPTH ROADWAY RECONSTRUCTION
- RECLAIM ROADWAY
- DRIVEWAY APRON TOWN R.O.W.
- DRIVEWAY APRON PRIVATE PROPERTY
- PROPOSED STORM SEWER PIPE
- PROPOSED CATCH BASIN
- TREE REMOVAL IN EASEMENT AREA
- TREE REMOVAL IN ROW

SIGNING AND PAVEMENT MARKING NOTES:

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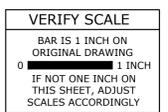
SIGNING AND PAVEMENT MARKING LEGEND:

- EXISTING PAINTED CENTERLINE
- EXISTING WHITE PAINTED STOP BAR
- EXISTING GROUND MOUNTED SIGN
- PROPOSED SIGN (GROUND MOUNTED)
- PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
- PROPOSED SIGNS BY CONDOT SIGN NUMBER
- S.W.E.L. - SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
- S.Y.E.L. - SINGLE YELLOW EDGE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- D.Y.C.L. - DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- S.W.S.L. - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING
- S.Y.S.L. - SINGLE YELLOW SKIP LINE, 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- S.W.L.L. - SINGLE WHITE LANE LINE - 4" EPOXY RESIN PAVEMENT MARKING

STROBELS ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019



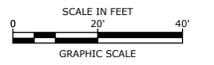
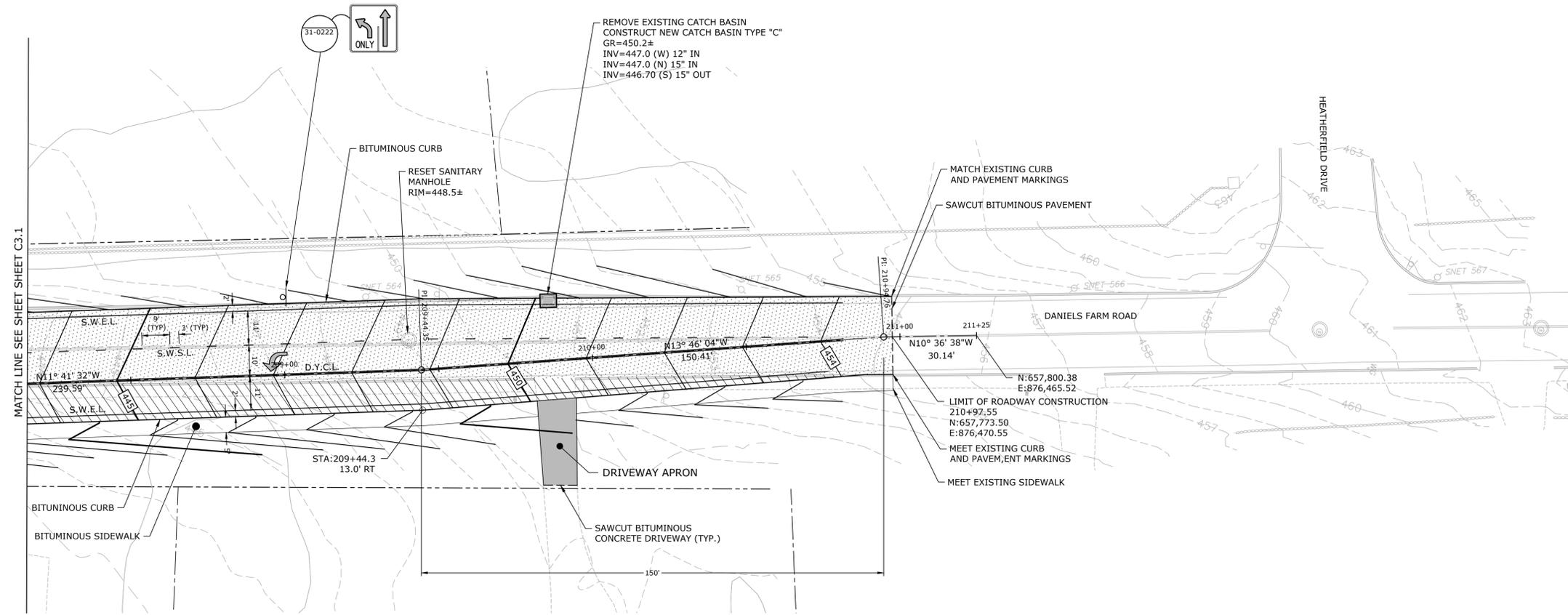
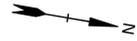
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-300-RDWY.dwg
DRAWN BY:	MDS
CHECKED BY:	AJM
APPROVED BY:	DCH

ROADWAY PLAN
 STA 203+25 TO 207+75
 STA 0+00 TO 1+50
 SCALE: 1" = 20'

C3.1

Plot Sheet: 1/22/2019 9:10:04am By: D:\browne\c
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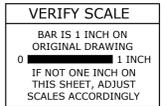
- - - - - TOWN LINE
- - - - - PROPERTY LINE
- - - - - - EXISTING SANITARY SEWER MANHOLE
- — — — — EXISTING SANITARY SEWER MAIN
- — — — — EXISTING SANITARY SEWER LATERAL
- — — — — EXISTING GAS MAIN
- — — — — EXISTING WATER MAIN
- — — — — EXISTING STORM SEWER PIPE
- - - - - - EXISTING CATCH BASIN
- — — — — PROPOSED BITUMINOUS CONCRETE LIP CURB
- — — — — PROPOSED EXTRUDED CONCRETE CURB
- - - - - PROPOSED SAWCUT LINE
- ▨ - - - - - FULL DEPTH ROADWAY RECONSTRUCTION
- ▩ - - - - - RECLAIM ROADWAY
- ▩ - - - - - DRIVEWAY APRON TOWN R.O.W.
- ▩ - - - - - DRIVEWAY APRON PRIVATE PROPERTY
- — — — — PROPOSED STORM SEWER PIPE
- - - - - - PROPOSED CATCH BASIN
- * - - - - - TREE REMOVAL IN EASEMENT AREA
- X - - - - - TREE REMOVAL IN ROW

SIGNING AND PAVEMENT MARKING NOTES:

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SIGNING AND PAVEMENT MARKING LEGEND:

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- — — — — EXISTING WHITE PAINTED STOP BAR
- - - - - - EXISTING GROUND MOUNTED SIGN
- - - - - - PROPOSED SIGN (GROUND MOUNTED)
- - - - - - PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
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- D.Y.C.L. - DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- S.W.S.L. - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING



MARK	DATE	DESCRIPTION

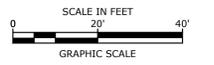
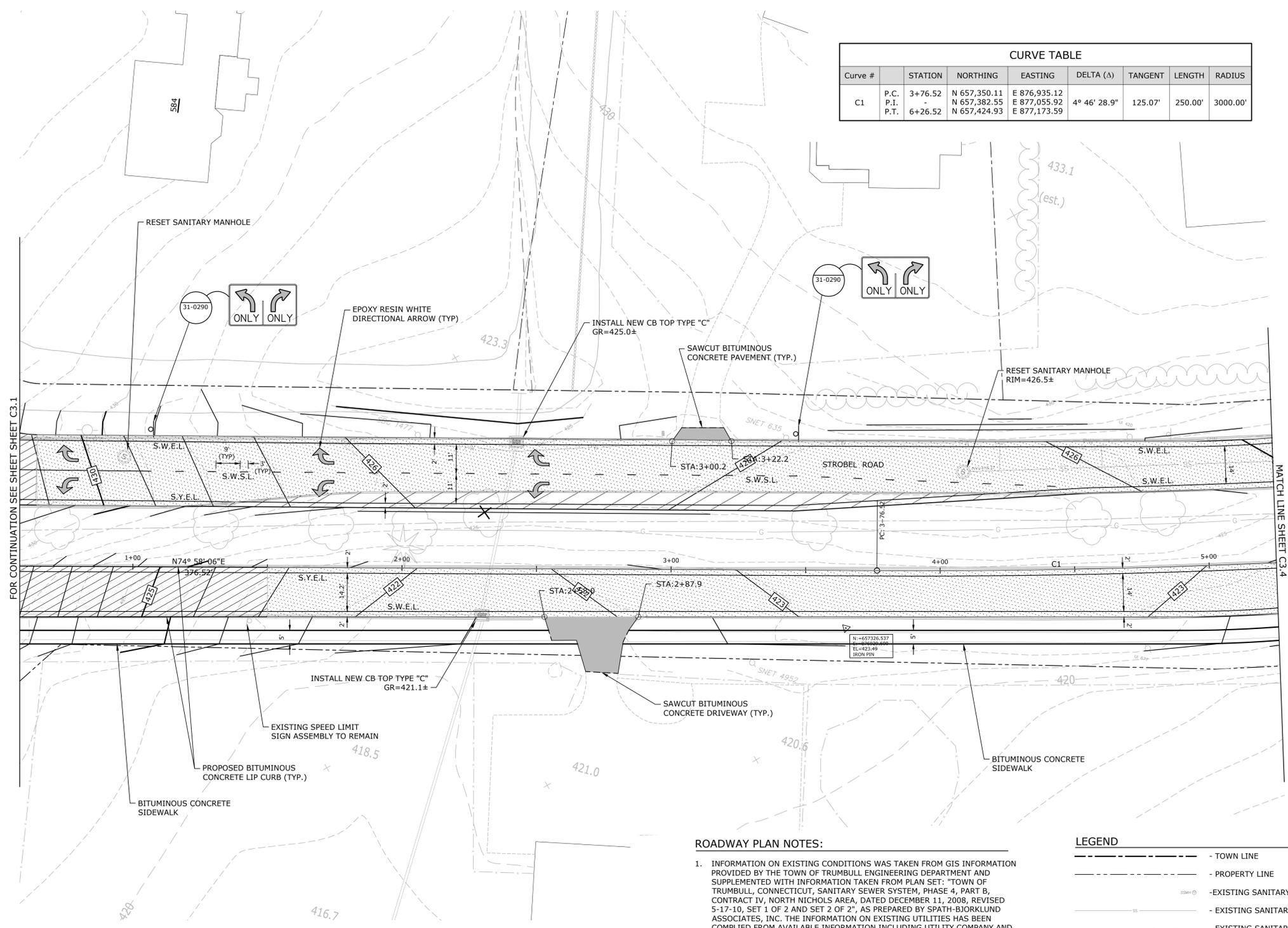
ROADWAY PLAN
 STA 207+75 TO 210+93

SCALE: 1" = 20'

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CURVE TABLE							
Curve #	STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C1	P.C.	3+76.52	N 657,350.11	E 876,935.12	4° 46' 28.9"	125.07'	250.00'
	P.I.	-	N 657,382.55	E 877,055.92			
	P.T.	6+26.52	N 657,424.93	E 877,173.59			



STROBELS ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

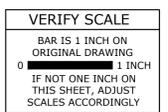
January 18, 2019

- SIGNING AND PAVEMENT MARKING LEGEND:**
- EXISTING PAINTED CENTERLINE
 - EXISTING WHITE PAINTED STOP BAR
 - EXISTING GROUND MOUNTED SIGN
 - PROPOSED SIGN (GROUND MOUNTED)
 - PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
 - PROPOSED SIGNS BY CONNDOT SIGN NUMBER
 - S.W.E.L. - SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
 - S.Y.E.L. - SINGLE YELLOW EDGE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
 - D.Y.C.L. - DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
 - S.W.S.L. - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING

- SIGNING AND PAVEMENT MARKING NOTES:**
1. INSTALL NEW SIGNS ON NEW METAL SIGN POSTS AS DIRECTED IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208_01 "SIGN SUPPORT & SIGN PLACEMENT DETAILS, GORE EXIT SIGN" AND TR-1208_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
 2. EXACT LOCATIONS OF THE SIGNS TO BE VERIFIED BY THE ENGINEER FOLLOWING LOCATION MARKING BY CONTRACTOR. THE CONTRACTOR SHALL ASSURE ADEQUATE SIGHTLINE IS PROVIDED TO SIGNS. ANY CLEARING OR TRIMMING NEEDED TO PROVIDE SIGHTLINE SHALL BE INCLUDED IN CLEARING AND GRUBBING.
 3. SIGNS SHALL BE AT LEAST 10-FEET FROM ALL UTILITY POLES.
 4. ALL PAVEMENT MARKINGS TO BE EPOXY RESIN.
 5. ALL PAVEMENT MARKINGS TO BE INSTALLED IN ACCORDANCE WITH STANDARD SHEET TR-1210_08 "PAVEMENT MARKINGS FOR NON-FREEWAYS".
 6. CONTRACTOR TO REMOVE ALL CONFLICTING EXISTING PAVEMENT MARKINGS AND SIGNAGE.

- ROADWAY PLAN NOTES:**
1. INFORMATION ON EXISTING CONDITIONS WAS TAKEN FROM GIS INFORMATION PROVIDED BY THE TOWN OF TRUMBULL ENGINEERING DEPARTMENT AND SUPPLEMENTED WITH INFORMATION TAKEN FROM PLAN SET: "TOWN OF TRUMBULL, CONNECTICUT, SANITARY SEWER SYSTEM, PHASE 4, PART B, CONTRACT IV, NORTH NICHOLS AREA, DATED DECEMBER 11, 2008, REVISED 5-17-10, SET 1 OF 2 AND SET 2 OF 2", AS PREPARED BY SPATH-BJORKLUND ASSOCIATES, INC. THE INFORMATION ON EXISTING UTILITIES HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY AND RECORD MAPS AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR AND/OR RESPONSIBLE PARTY IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES INCLUDING SERVICES. PRIOR TO ANY CONSTRUCTION, CALL "CALL BEFORE YOU DIG" AT 1-800-922-4455 AND VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS.
 2. LIMITS OF REINFORCED CONCRETE PAVEMENT SHOWN ARE APPROXIMATE.
 3. ALL DRIVEWAY APRONS SHALL BE SAWCUT WHERE PROPOSED PAVEMENT MEETS EXISTING.
 4. ALL MANHOLE FRAMES, CATCH BASIN TOPS, AND UTILITY ACCESS COVERS SHALL BE RESET.
 5. ALL DISTURBED MAIL BOXES SHALL BE ACCESSIBLE FOR MAIL DELIVERY THROUGHOUT THE CONSTRUCTION. THERE SHALL BE NO DIRECT PAYMENT FOR THIS ITEM BUT PAID FOR UNDER CLEARING AND GRUBBING.
 6. PAVEMENT AT ALL INTERSECTION CROSS-SECTIONS SHALL BE SAWCUT WHERE PROPOSED MEETS EXISTING.
 7. FINAL CURB LOCATIONS SHALL BE AS DIRECTED BY THE ENGINEER.
 8. DRIVEWAY APRON ON PRIVATE PROPERTY TO BE CONSTRUCTED WHEN RIGHT OF ENTRY APPROVAL IS OBTAINED FROM HOMEOWNER. IF RIGHT OF ENTRY IS NOT RECEIVED, ALL PROPOSED WORK SHALL STOP AT ROW LINE.

- LEGEND**
- TOWN LINE
 - PROPERTY LINE
 - SMW - EXISTING SANITARY SEWER MANHOLE
 - SS - EXISTING SANITARY SEWER MAIN
 - SL - EXISTING SANITARY SEWER LATERAL
 - GM - EXISTING GAS MAIN
 - WM - EXISTING WATER MAIN
 - SSP - EXISTING STORM SEWER PIPE
 - CB - EXISTING CATCH BASIN
 - BC - PROPOSED BITUMINOUS CONCRETE LIP CURB
 - EC - PROPOSED EXTRUDED CONCRETE CURB
 - SL - PROPOSED SAWCUT LINE
 - FR - FULL DEPTH ROADWAY RECONSTRUCTION
 - RR - RECLAIM ROADWAY
 - DA - DRIVEWAY APRON TOWN R.O.W.
 - DA - DRIVEWAY APRON PRIVATE PROPERTY
 - SSP - PROPOSED STORM SEWER PIPE
 - CB - PROPOSED CATCH BASIN
 - * - TREE REMOVAL IN EASEMENT AREA
 - X - TREE REMOVAL IN ROW



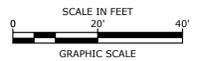
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-300-RDWY.dwg
 DRAWN BY: MDS
 CHECKED BY: AJM
 APPROVED BY: DCH

ROADWAY PLAN
 STA 0+50 TO 5+25
 SCALE: 1" = 20'

C3.3

Plot Sheet: 1/27/2019 9:10:04am By: D:\browne\Tighe & Bond\15\T0196\15_Strobel RD\Drawings\Sheet\T0196-15-C-300-RDWY.dwg



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

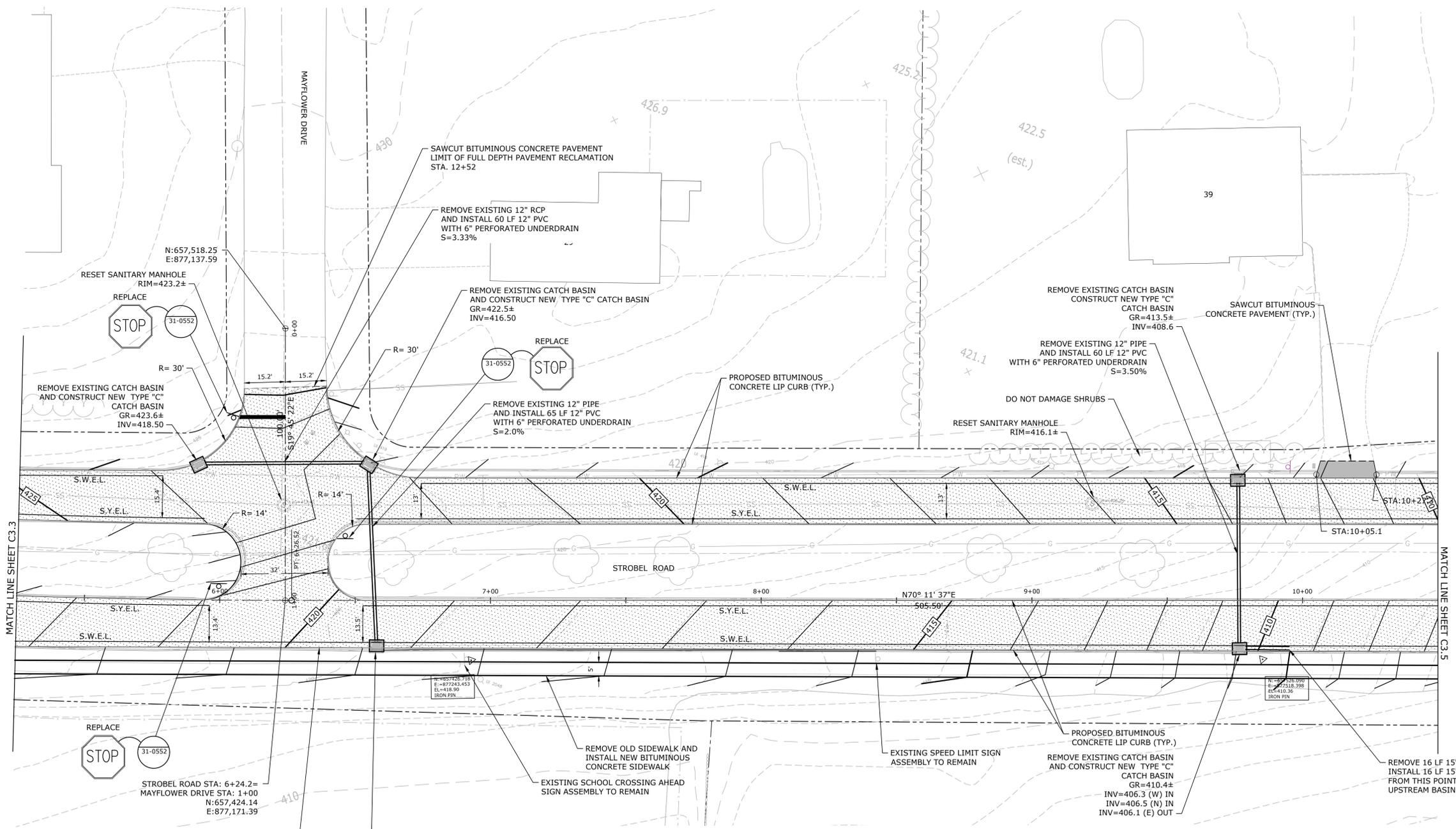
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 DRAWN BY: MDS
 CHECKED BY: AJM
 APPROVED BY: DCH

SCALE: 1" = 20'

ROADWAY PLAN STA 5+25 TO 10+50

C3.4



ROADWAY PLAN NOTES:

- INFORMATION ON EXISTING CONDITIONS WAS TAKEN FROM GIS INFORMATION PROVIDED BY THE TOWN OF TRUMBULL ENGINEERING DEPARTMENT AND SUPPLEMENTED WITH INFORMATION TAKEN FROM PLAN SET: "TOWN OF TRUMBULL, CONNECTICUT, SANITARY SEWER SYSTEM, PHASE 4, PART B, CONTRACT IV, NORTH NICHOLS AREA, DATED DECEMBER 11, 2008, REVISED 5-17-10, SET 1 OF 2 AND SET 2 OF 2", AS PREPARED BY SPATH-BJORKLUND ASSOCIATES, INC. THE INFORMATION ON EXISTING UTILITIES HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY AND RECORD MAPS AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR AND/OR RESPONSIBLE PARTY IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES INCLUDING SERVICES. PRIOR TO ANY CONSTRUCTION, CALL "CALL BEFORE YOU DIG" AT 1-800-922-4455 AND VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS.
- LIMITS OF REINFORCED CONCRETE PAVEMENT SHOWN ARE APPROXIMATE.
- ALL DRIVEWAY APRONS SHALL BE SAWCUT WHERE PROPOSED PAVEMENT MEETS EXISTING.
- ALL MANHOLE FRAMES, CATCH BASIN TOPS, AND UTILITY ACCESS COVERS SHALL BE RESET.
- ALL DISTURBED MAIL BOXES SHALL BE ACCESSIBLE FOR MAIL DELIVERY THROUGHOUT THE CONSTRUCTION. THERE SHALL BE NO DIRECT PAYMENT FOR THIS ITEM BUT PAID FOR UNDER CLEARING AND GRUBBING.
- PAVEMENT AT ALL INTERSECTION CROSS-SECTIONS SHALL BE SAWCUT WHERE PROPOSED MEETS EXISTING.
- FINAL CURB LOCATIONS SHALL BE AS DIRECTED BY THE ENGINEER.
- DRIVEWAY APRON ON PRIVATE PROPERTY TO BE CONSTRUCTED WHEN RIGHT OF ENTRY APPROVAL IS OBTAINED FROM HOMEOWNER. IF RIGHT OF ENTRY IS NOT RECEIVED, ALL PROPOSED WORK SHALL STOP AT ROW LINE.

SIGNING AND PAVEMENT MARKING NOTES:

- INSTALL NEW SIGNS ON NEW METAL SIGN POSTS AS DIRECTED IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208_01 "SIGN SUPPORT & SIGN PLACEMENT DETAILS, GORE EXIT SIGN" AND TR-1208_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
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- SIGNS SHALL BE AT LEAST 10-FEET FROM ALL UTILITY POLES.
- ALL PAVEMENT MARKINGS TO BE EPOXY RESIN.
- ALL PAVEMENT MARKINGS TO BE INSTALLED IN ACCORDANCE WITH STANDARD SHEET TR-1210_08 "PAVEMENT MARKINGS FOR NON-FREWAYS".
- CONTRACTOR TO REMOVE ALL CONFLICTING EXISTING PAVEMENT MARKINGS AND SIGNAGE.

SIGNING AND PAVEMENT MARKING LEGEND:

- EXISTING PAINTED CENTERLINE
- EXISTING WHITE PAINTED STOP BAR
- EXISTING GROUND MOUNTED SIGN
- PROPOSED SIGN (GROUND MOUNTED)
- PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
- PROPOSED SIGNS BY CONDOT SIGN NUMBER
- S.W.E.L. - SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
- S.Y.E.L. - SINGLE YELLOW EDGE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- D.Y.C.L. - DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- S.W.S.L. - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING

Job Sheet: 1/22/2019
 Job No: 10196-15-C-300-RDWAY.dwg
 Drawn By: MDS
 Checked By: AJM
 Approved By: DCH
 Tighe & Bond: J:\1\10196\15-Strobel Rd\Drawings\Sheet\T0196-15-C-300-RDWAY.dwg

**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

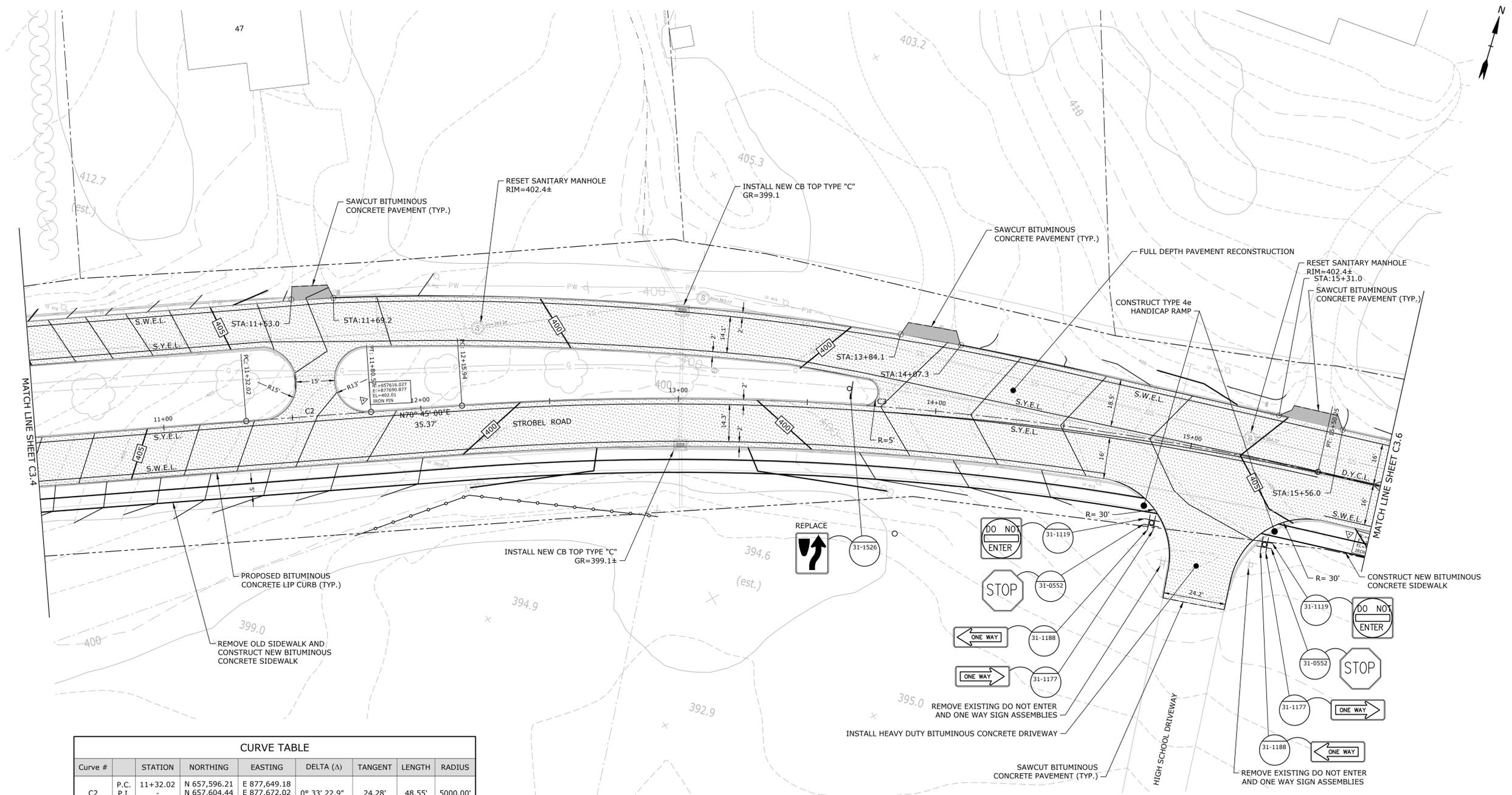
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 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

**ROADWAY PLAN
 STA 10+50 TO 15+75**

SCALE: 1" = 20'

C3.5



CURVE TABLE

Curve #	STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C2	P.C.	11+32.02	N 657,596.21	E 877,649.18	0° 33' 22.9"	24.28'	48.55'
	P.I.	-	N 657,604.44	E 877,672.02			
	P.T.	11+80.57	N 657,612.44	E 877,694.94			
C3	P.C.	12+15.94	N 657,624.10	E 877,728.33	16° 39' 21.8"	168.34'	334.31'
	P.I.	-	N 657,679.60	E 877,887.26			
	P.T.	15+50.25	N 657,687.22	E 878,055.43			

SIGNING AND PAVEMENT MARKING LEGEND:

- EXISTING PAINTED CENTERLINE
- EXISTING WHITE PAVED STOP BAR
- EXISTING GROUND MOUNTED SIGN
- PROPOSED SIGN (GROUND MOUNTED)
- PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
- PROPOSED SIGNS BY CONNDOT SIGN NUMBER
- S.W.E.L. - SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
- S.Y.E.L. - SINGLE YELLOW EDGE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- D.Y.C.L. - DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- S.W.S.L. - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING

SIGNING AND PAVEMENT MARKING NOTES:

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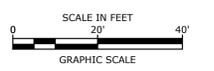
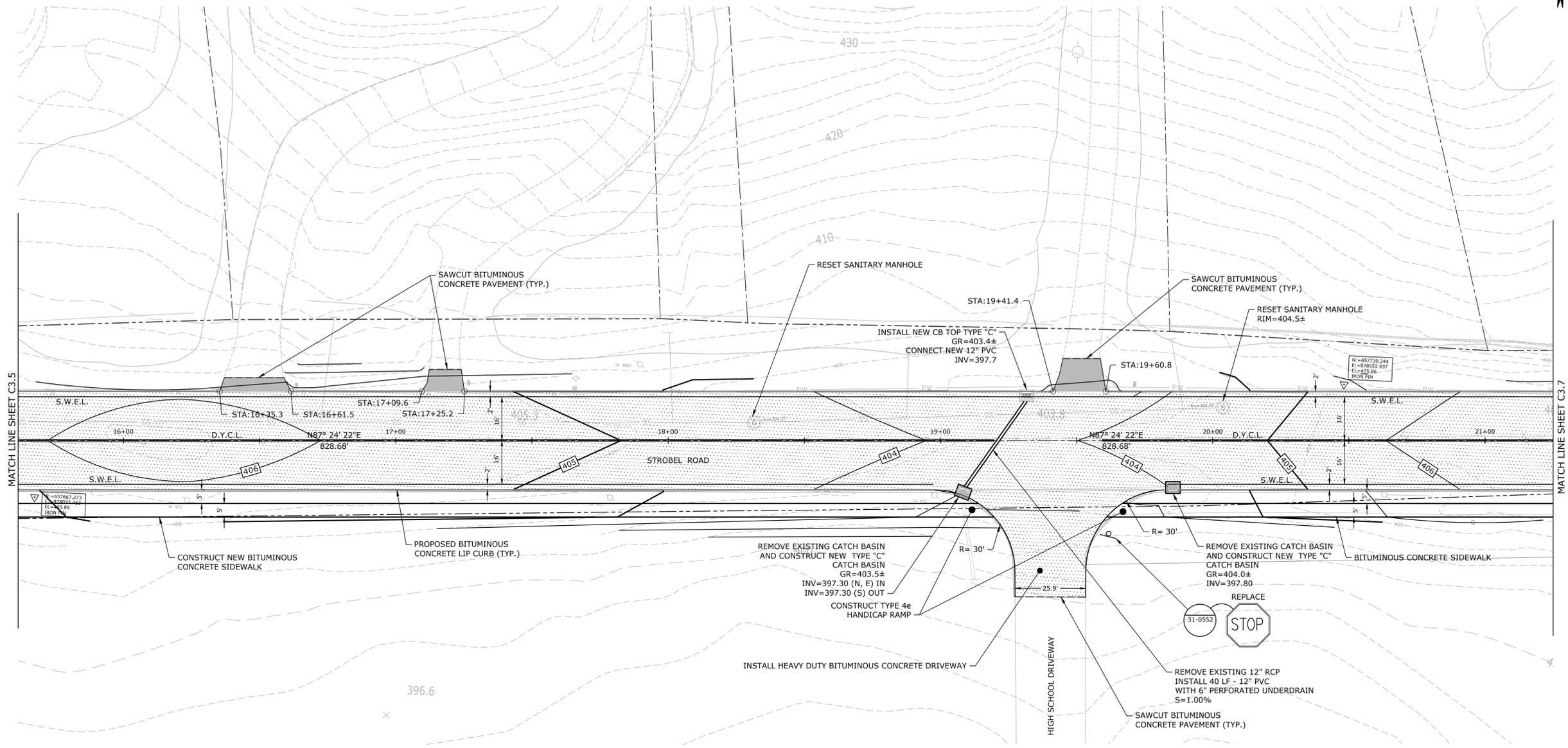
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LEGEND

- - - - - TOWN LINE
- - - - - PROPERTY LINE
- SSW-Ø - EXISTING SANITARY SEWER MANHOLE
- SS - EXISTING SANITARY SEWER MAIN
- SS-L - EXISTING SANITARY SEWER LATERAL
- Ø - EXISTING GAS MAIN
- Ø - EXISTING WATER MAIN
- Ø - EXISTING STORM SEWER PIPE
- - EXISTING CATCH BASIN
- - - - - PROPOSED BITUMINOUS CONCRETE LIP CURB
- - - - - PROPOSED EXTRUDED CONCRETE CURB
- - - - - PROPOSED SAWCUT LINE
- ▨ - FULL DEPTH ROADWAY RECONSTRUCTION
- ▨ - RECLAIM ROADWAY
- ▨ - DRIVEWAY APRON TOWN R.O.W.
- ▨ - DRIVEWAY APRON PRIVATE PROPERTY
- - - - - PROPOSED STORM SEWER PIPE
- - PROPOSED CATCH BASIN
- * - TREE REMOVAL IN EASEMENT AREA
- X - TREE REMOVAL IN ROW

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STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

SIGNING AND PAVEMENT MARKING LEGEND:

	- EXISTING PAINTED CENTERLINE
	- EXISTING WHITE PAINTED STOP BAR
	- EXISTING GROUND MOUNTED SIGN
	- PROPOSED SIGN (GROUND MOUNTED)
	- PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
	- PROPOSED SIGNS BY CONNDOT SIGN NUMBER
S.W.E.L.	- SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
S.Y.E.L.	- SINGLE YELLOW EDGE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
D.Y.C.L.	- DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
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LEGEND

	- TOWN LINE
	- PROPERTY LINE
	- EXISTING SANITARY SEWER MANHOLE
	- EXISTING SANITARY SEWER MAIN
	- EXISTING SANITARY SEWER LATERAL
	- EXISTING GAS MAIN
	- EXISTING WATER MAIN
	- EXISTING STORM SEWER PIPE
	- EXISTING CATCH BASIN
	- PROPOSED BITUMINOUS CONCRETE LIP CURB
	- PROPOSED EXTRUDED CONCRETE CURB
	- PROPOSED SAWCUT LINE
	- FULL DEPTH ROADWAY RECONSTRUCTION
	- RECLAIM ROADWAY
	- DRIVEWAY APRON TOWN R.O.W.
	- DRIVEWAY APRON PRIVATE PROPERTY
	- PROPOSED STORM SEWER PIPE
	- PROPOSED CATCH BASIN
	- TREE REMOVAL IN EASEMENT AREA
	- TREE REMOVAL IN ROW

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-300-RDWY.dwg
DRAWN BY:	MDS
CHECKED BY:	AJM
APPROVED:	DCH

ROADWAY PLAN STA 15+75 TO 21+25

SCALE: 1" = 20'

C3.6

Plot: Sheet: 5/26/2019 10:55:15 from Br: D:\bunac\Tighe & Bond\1\T0196\15-Strobel Rd\Drawing\Sheet\T0196-15-C-300-RDWY.dwg

ROADWAY PLAN NOTES:

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LEGEND

- - - - - TOWN LINE
- - - - - PROPERTY LINE
- SSW 6 - EXISTING SANITARY SEWER MANHOLE
- SS - EXISTING SANITARY SEWER MAIN
- SSL - EXISTING SANITARY SEWER LATERAL
- G - EXISTING GAS MAIN
- W - EXISTING WATER MAIN
- SSP - EXISTING STORM SEWER PIPE
- - EXISTING CATCH BASIN
- - - - - PROPOSED BITUMINOUS CONCRETE LIP CURB
- - - - - PROPOSED EXTRUDED CONCRETE CURB
- - - - - PROPOSED SAWCUT LINE
- ▨ - FULL DEPTH ROADWAY RECONSTRUCTION
- ▨ - RECLAIM ROADWAY
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- * - TREE REMOVAL IN EASEMENT AREA
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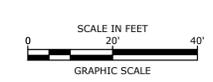
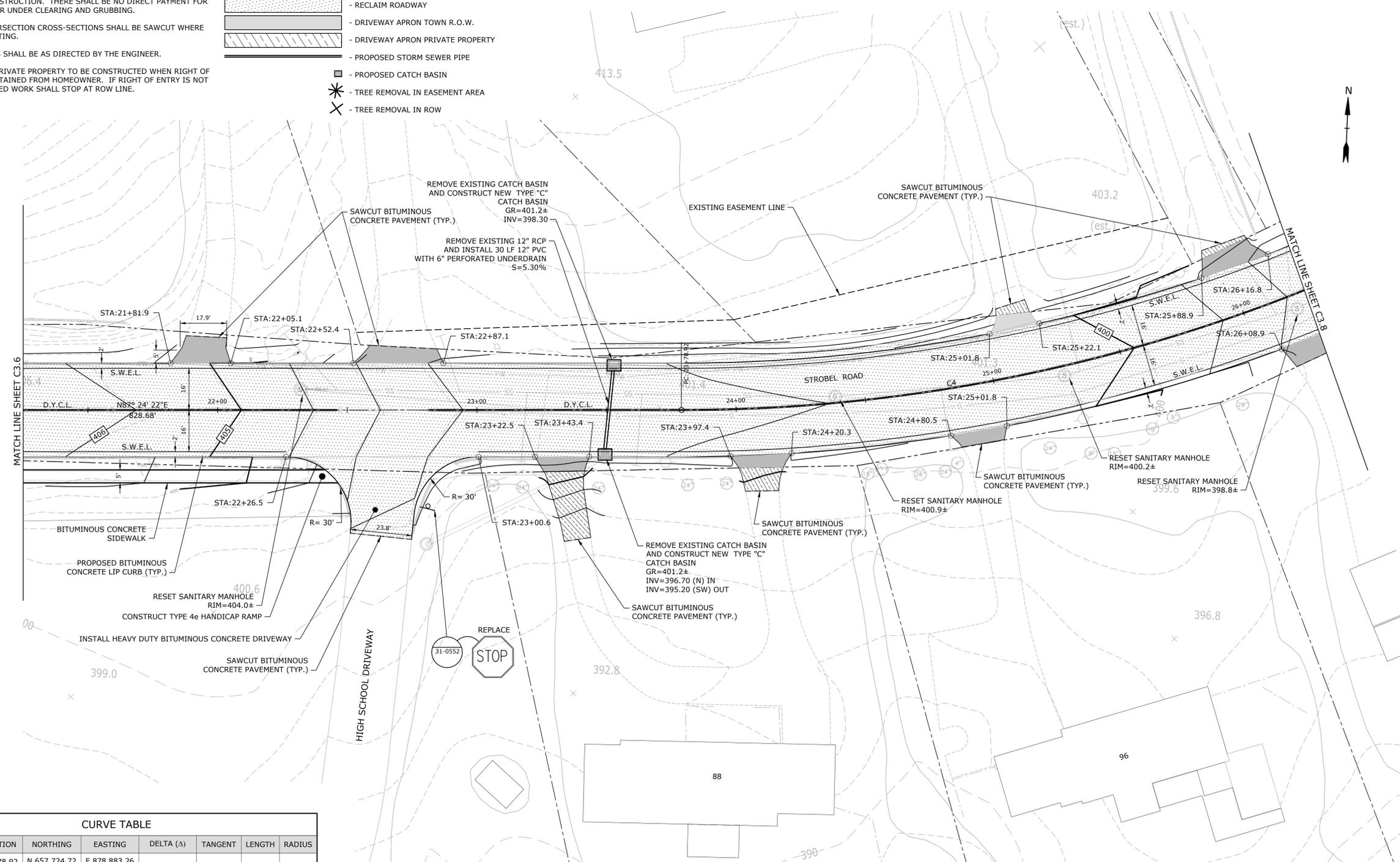
SIGNING AND PAVEMENT MARKING LEGEND:

- - - - - EXISTING PAINTED CENTERLINE
- - - - - EXISTING WHITE PAINTED STOP BAR
- - - - - EXISTING GROUND MOUNTED SIGN
- ⊙ - PROPOSED SIGN (GROUND MOUNTED)
- ↩ - PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
- ⊙ - PROPOSED SIGNS BY CONNDOT SIGN NUMBER
- S.W.E.L. - SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
- S.Y.E.L. - SINGLE YELLOW EDGE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- D.Y.C.L. - DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- S.W.S.L. - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING

SIGNING AND PAVEMENT MARKING NOTES:

1. INSTALL NEW SIGNS ON NEW METAL SIGN POSTS AS DIRECTED IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208_01 "SIGN SUPPORT & SIGN PLACEMENT DETAILS", GORE EXIT SIGN" AND TR-1208_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
2. EXACT LOCATIONS OF THE SIGNS TO BE VERIFIED BY THE ENGINEER FOLLOWING LOCATION MARKING BY CONTRACTOR. THE CONTRACTOR SHALL ASSURE ADEQUATE SIGHTLINE IS PROVIDED TO SIGNS. ANY CLEARING OR TRIMMING NEEDED TO PROVIDE SIGHTLINE SHALL BE INCLUDED IN CLEARING AND GRUBBING.
3. SIGNS SHALL BE AT LEAST 10- FEET FROM ALL UTILITY POLES.
4. ALL PAVEMENT MARKINGS TO BE EPOXY RESIN.
5. ALL PAVEMENT MARKINGS TO BE INSTALLED IN ACCORDANCE WITH STANDARD SHEET TR-1210_08 "PAVEMENT MARKINGS FOR NON-FREEWAYS".
6. CONTRACTOR TO REMOVE ALL CONFLICTING EXISTING PAVEMENT MARKINGS AND SIGNAGE.

Tighe & Bond
 www.tighebond.com
 1000 Bridgeport Avenue
 Suite 320
 Shelton, CT 06484
 (203) 712-1100



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

CURVE TABLE							
Curve #	STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C4	P.C.	23+78.92	N 657,724.72	E 878,883.26	25° 23' 00.7"	146.39'	287.97'
	P.I.	-	N 657,731.35	E 879,029.49			
	P.T.	26+66.89	N 657,800.02	E 879,158.77			

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-15-C-300-RDWAY.dwg	
DRAWN BY:	MDS	
CHECKED BY:	AJM	
APPROVED:	DCH	

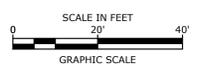
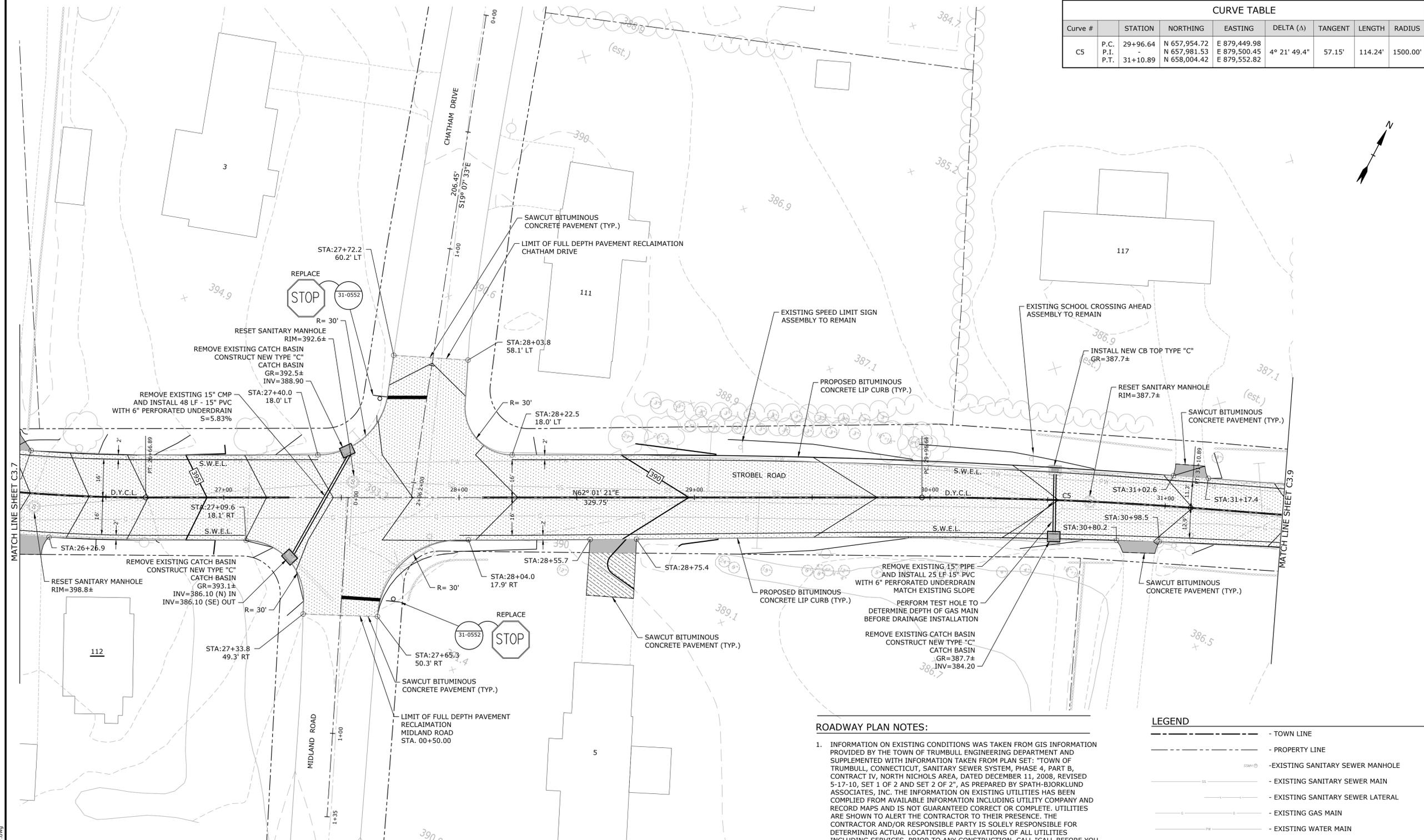
ROADWAY PLAN
 STA 21+25 TO 26+25

SCALE: 1" = 20'

C3.7

Plot: Sheet: 5/18/2019 10:09:55:3pm By: Duhonuec
 Tighe & Bond: \\T0196\15_Strobel Rd\Drawings\Sheet\T0196-15-C-300-RDWAY.dwg

CURVE TABLE							
Curve #	STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C5	P.C.	29+96.64	N 657,954.72	E 879,449.98	4° 21' 49.4"	57.15'	114.24'
	P.I.	-	N 657,981.53	E 879,500.45			
	P.T.	31+10.89	N 658,004.42	E 879,552.82			



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

ROADWAY PLAN NOTES:

- INFORMATION ON EXISTING CONDITIONS WAS TAKEN FROM GIS INFORMATION PROVIDED BY THE TOWN OF TRUMBULL ENGINEERING DEPARTMENT AND SUPPLEMENTED WITH INFORMATION TAKEN FROM PLAN SET: "TOWN OF TRUMBULL, CONNECTICUT, SANITARY SEWER SYSTEM, PHASE 4, PART B, CONTRACT IV, NORTH NICHOLS AREA, DATED DECEMBER 11, 2008, REVISED 5-17-10, SET 1 OF 2 AND SET 2 OF 2", AS PREPARED BY SPATH-BJORKLUND ASSOCIATES, INC. THE INFORMATION ON EXISTING UTILITIES HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY AND RECORD MAPS AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR AND/OR RESPONSIBLE PARTY IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES INCLUDING SERVICES. PRIOR TO ANY CONSTRUCTION, CALL "CALL BEFORE YOU DIG" AT 1-800-922-4455 AND VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS.
- ALL DRIVEWAY APRONS SHALL BE SAWCUT WHERE PROPOSED PAVEMENT MEETS EXISTING.
- ALL MANHOLE FRAMES, CATCH BASIN TOPS, AND UTILITY ACCESS COVERS SHALL BE SET TO THE TOP OF BINDER COURSE, HMA S 1.0. ALL MANHOLE FRAMES, CATCH BASIN TOPS, AND UTILITY ACCESS COVERS SHALL BE RESET TO FINISHED GRADE BEFORE SURFACE COURSE IS PLACED.
- SURFACE COURSE, HMA S 0.375 TO BE PLACED IN ONE CONTINUOUS PHASE.
- ALL DISTURBED MAIL BOXES SHALL BE ACCESSIBLE FOR MAIL DELIVERY THROUGHOUT THE CONSTRUCTION. THERE SHALL BE NO DIRECT PAYMENT FOR THIS ITEM BUT PAID FOR UNDER CLEARING AND GRUBBING.
- PAVEMENT AT ALL INTERSECTION CROSS-SECTIONS SHALL BE SAWCUT WHERE PROPOSED MEETS EXISTING.
- FINAL CURB LOCATIONS SHALL BE AS DIRECTED BY THE ENGINEER.
- DRIVEWAY APRON ON PRIVATE PROPERTY TO BE CONSTRUCTED WHEN RIGHT OF ENTRY APPROVAL IS OBTAINED FROM HOMEOWNER. IF RIGHT OF ENTRY IS NOT RECEIVED, ALL PROPOSED WORK SHALL STOP AT ROW LINE.

LEGEND

- - - TOWN LINE
- - - PROPERTY LINE
- ⊙ EXISTING SANITARY SEWER MANHOLE
- EXISTING SANITARY SEWER MAIN
- EXISTING SANITARY SEWER LATERAL
- EXISTING GAS MAIN
- EXISTING WATER MAIN
- EXISTING STORM SEWER PIPE
- EXISTING CATCH BASIN
- PROPOSED BITUMINOUS CONCRETE LIP CURB
- PROPOSED EXTRUDED CONCRETE CURB
- PROPOSED SAWCUT LINE
- ▨ FULL DEPTH ROADWAY RECONSTRUCTION
- ▨ RECLAIM ROADWAY
- ▨ DRIVEWAY APRON TOWN R.O.W.
- ▨ DRIVEWAY APRON PRIVATE PROPERTY
- PROPOSED STORM SEWER PIPE
- PROPOSED CATCH BASIN
- * TREE REMOVAL IN EASEMENT AREA
- X TREE REMOVAL IN ROW

SIGNING AND PAVEMENT MARKING LEGEND:

- EXISTING PAINTED CENTERLINE
- EXISTING WHITE PAINTED STOP BAR
- EXISTING GROUND MOUNTED SIGN
- PROPOSED SIGN (GROUND MOUNTED)
- PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
- ⊙ PROPOSED SIGNS BY CONNDOT SIGN NUMBER
- S.W.E.L. - SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
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- D.Y.C.L. - DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- S.W.S.L. - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING

SIGNING AND PAVEMENT MARKING NOTES:

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VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-300-RDWY.dwg
 DRAWN BY: MDS
 CHECKED BY: AJM
 APPROVED BY: DCH

ROADWAY PLAN STA 26+25 TO 31+50
 SCALE: 1" = 20'
C3.8

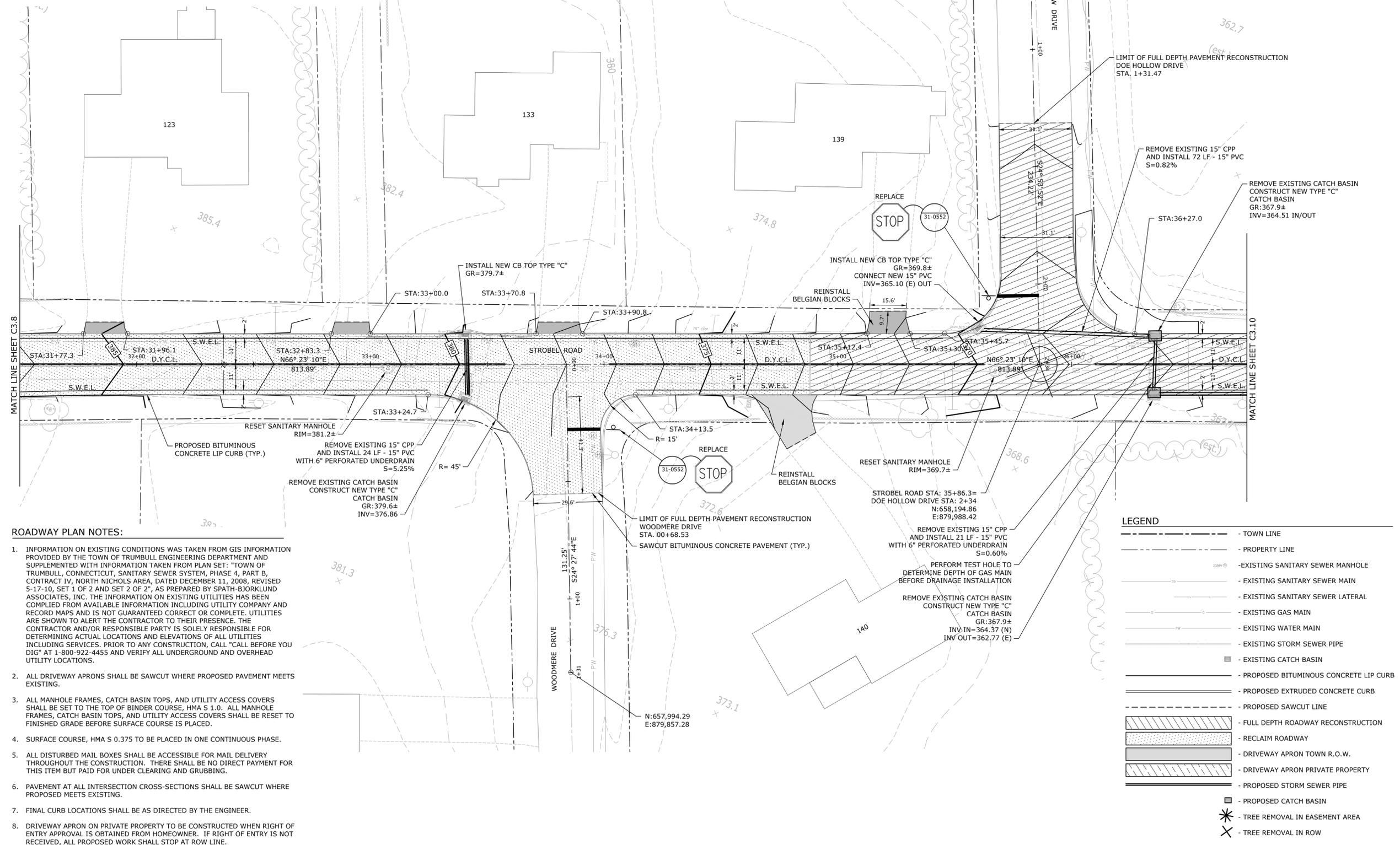
Plot: Sheet: 1/27/2019 9:10:05am By: D:\browne\Tighe & Bond\T0196\15_C-300-RDWY.dwg

SIGNING AND PAVEMENT MARKING LEGEND:

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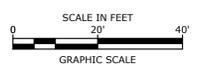
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LEGEND

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- PROPERTY LINE
- EXISTING SANITARY SEWER MANHOLE
- EXISTING SANITARY SEWER MAIN
- EXISTING SANITARY SEWER LATERAL
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- EXISTING WATER MAIN
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- PROPOSED EXTRUDED CONCRETE CURB
- PROPOSED SAWCUT LINE
- FULL DEPTH ROADWAY RECONSTRUCTION
- RECLAIM ROADWAY
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- * - TREE REMOVAL IN EASEMENT AREA
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STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

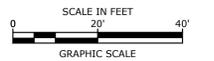
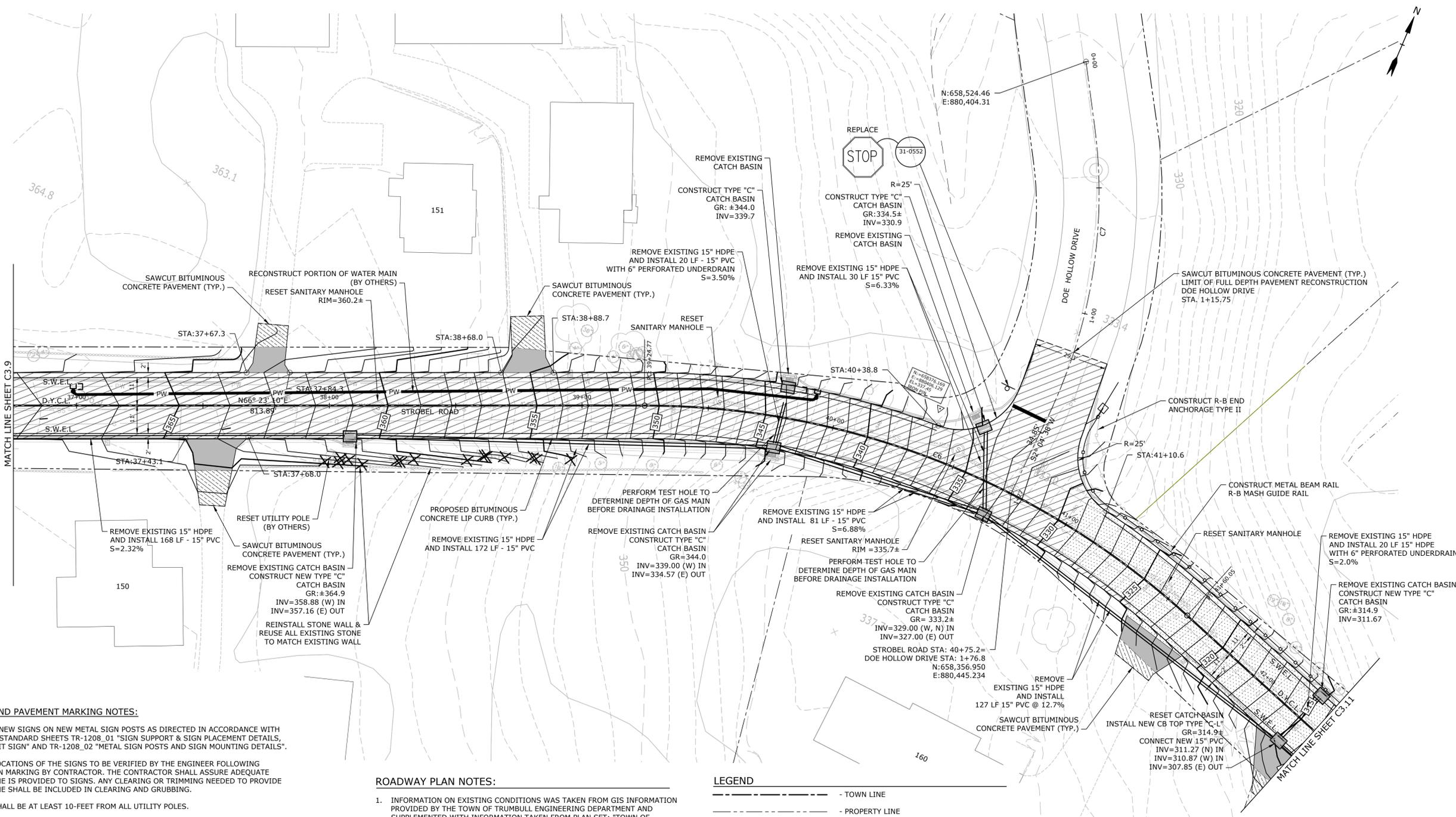
PROJECT NO: T0196
 FILE: T0196-15-C-300-RDWY.dwg
 DRAWN BY: MDS
 CHECKED: AJM
 APPROVED: DCH

ROADWAY PLAN
 STA 31+50 TO 36+75

SCALE: 1" = 20'

C3.9

Plot Sheet: 1/27/2019 9:10:06am By: DurbroweC
 Tighe & Bond: J:\T0196\15_C-300-RDWY.dwg



SIGNING AND PAVEMENT MARKING NOTES:

- INSTALL NEW SIGNS ON NEW METAL SIGN POSTS AS DIRECTED IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208_01 "SIGN SUPPORT & SIGN PLACEMENT DETAILS, GORE EXIT SIGN" AND TR-1208_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
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SIGNING AND PAVEMENT MARKING LEGEND:

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- EXISTING WHITE PAINTED STOP BAR
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LEGEND

- - - TOWN LINE
- - - PROPERTY LINE
- SS - EXISTING SANITARY SEWER MANHOLE
- SS - EXISTING SANITARY SEWER MAIN
- SS - EXISTING SANITARY SEWER LATERAL
- GW - EXISTING GAS MAIN
- WM - EXISTING WATER MAIN
- SS - EXISTING STORM SEWER PIPE
- - EXISTING CATCH BASIN
- ▬ - PROPOSED BITUMINOUS CONCRETE LIP CURB
- ▬ - PROPOSED EXTRUDED CONCRETE CURB
- - - PROPOSED SAWCUT LINE
- ▨ - FULL DEPTH ROADWAY RECONSTRUCTION
- ▨ - RECLAIM ROADWAY
- ▬ - DRIVEWAY APRON TOWN R.O.W.
- ▬ - DRIVEWAY APRON PRIVATE PROPERTY
- ▬ - PROPOSED STORM SEWER PIPE
- - PROPOSED CATCH BASIN
- * - TREE REMOVAL IN EASEMENT AREA
- X - TREE REMOVAL IN ROW

CURVE TABLE							
Curve #	STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C6	P.C.	39+24.77	N 658,330.44	E 880,298.56	42° 07' 35.1"	123.24'	320.00'
	P.T.	41+60.05	N 658,379.81	E 880,411.48			
C7	P.C.	0+25.59	N 658,502.59	E 880,417.59	33° 20' 46.2"	59.90'	200.00'
	P.T.	1+41.99	N 658,451.39	E 880,448.68			

STROBOL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

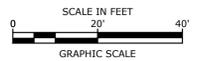
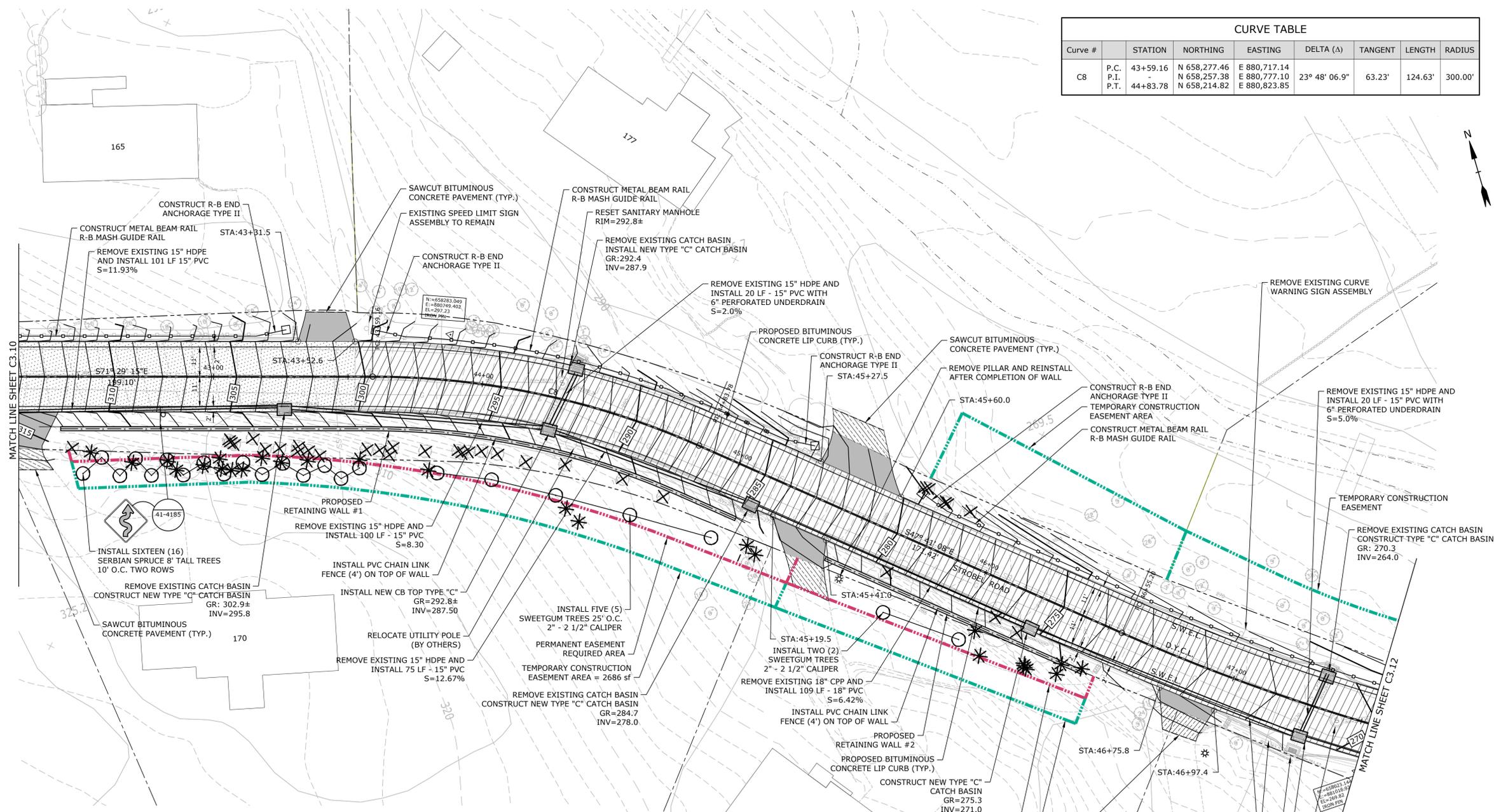
PROJECT NO: T0196
 FILE: T0196-15-C-300-RDWAY.dwg
 DRAWN BY: MDS
 CHECKED: AJM
 APPROVED: DCH

ROADWAY PLAN STA 36+75 TO 42+25

SCALE: 1" = 20'

C3.10

CURVE TABLE							
Curve #	STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C8	P.C.	43+59.16	N 658,277.46	E 880,717.14	23° 48' 06.9"	63.23'	124.63'
	P.I.		N 658,257.38	E 880,777.10			
	P.T.	44+83.78	N 658,214.82	E 880,823.85			



ROADWAY PLAN NOTES:

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- TREE REMOVAL IN EASEMENT AREA
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- SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING

SIGNING AND PAVEMENT MARKING NOTES:

- INSTALL NEW SIGNS ON NEW METAL SIGN POSTS AS DIRECTED IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208.01 "SIGN SUPPORT & SIGN PLACEMENT DETAILS", GORE EXIT SIGN" AND TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
- EXACT LOCATIONS OF THE SIGNS TO BE VERIFIED BY THE ENGINEER FOLLOWING LOCATION MARKING BY CONTRACTOR. THE CONTRACTOR SHALL ASSURE ADEQUATE SIGHTLINE IS PROVIDED TO SIGNS. ANY CLEARING OR TRIMMING NEEDED TO PROVIDE SIGHTLINE SHALL BE INCLUDED IN CLEARING AND GRUBBING.
- SIGNS SHALL BE AT LEAST 10-FEET FROM ALL UTILITY POLES.
- ALL PAVEMENT MARKINGS TO BE EPOXY RESIN.
- ALL PAVEMENT MARKINGS TO BE INSTALLED IN ACCORDANCE WITH STANDARD SHEET TR-1210.08 "PAVEMENT MARKINGS FOR NON-FREEWAYS".
- CONTRACTOR TO REMOVE ALL CONFLICTING EXISTING PAVEMENT MARKINGS AND SIGNAGE.

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
BAR IS 1 INCH ON ORIGINAL DRAWING
IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

PROJECT NO: T0196
FILE: T0196-15-C-300-RDWY.dwg
DRAWN BY: MDS
CHECKED: AJM
APPROVED: DCH

ROADWAY PLAN
STA 42+25 TO 47+50
SCALE: 1" = 20'
C3.11

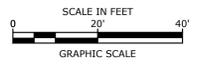
Job: Sheet: 5/18/2019 10:54:41 am By: Duhonuec
 Tighe & Bond: T:\0196\15_C-300-RDWY.dwg

ROADWAY PLAN NOTES:

1. INFORMATION ON EXISTING CONDITIONS WAS TAKEN FROM GIS INFORMATION PROVIDED BY THE TOWN OF TRUMBULL ENGINEERING DEPARTMENT AND SUPPLEMENTED WITH INFORMATION TAKEN FROM PLAN SET: "TOWN OF TRUMBULL, CONNECTICUT, SANITARY SEWER SYSTEM, PHASE 4, PART B, CONTRACT IV, NORTH NICHOLS AREA, DATED DECEMBER 11, 2008, REVISED 5-17-10, SET 1 OF 2 AND SET 2 OF 2", AS PREPARED BY SPATH-BJORKLUND ASSOCIATES, INC. THE INFORMATION ON EXISTING UTILITIES HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY AND RECORD MAPS AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR AND/OR RESPONSIBLE PARTY IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES INCLUDING SERVICES. PRIOR TO ANY CONSTRUCTION, CALL "CALL BEFORE YOU DIG" AT 1-800-922-4455 AND VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS.
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4. SURFACE COURSE, HMA S 0.375 TO BE PLACED IN ONE CONTINUOUS PHASE.
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8. DRIVEWAY APRON ON PRIVATE PROPERTY TO BE CONSTRUCTED WHEN RIGHT OF ENTRY APPROVAL IS OBTAINED FROM HOMEOWNER. IF RIGHT OF ENTRY IS NOT RECEIVED, ALL PROPOSED WORK SHALL STOP AT ROW LINE.

LEGEND

- - - - - TOWN LINE
- - - - - PROPERTY LINE
- ⊙ - - - - - EXISTING SANITARY SEWER MANHOLE
- - - - - EXISTING SANITARY SEWER MAIN
- - - - - EXISTING SANITARY SEWER LATERAL
- - - - - EXISTING GAS MAIN
- - - - - EXISTING WATER MAIN
- - - - - EXISTING STORM SEWER PIPE
- - - - - - EXISTING CATCH BASIN
- - - - - PROPOSED BITUMINOUS CONCRETE LIP CURB
- - - - - PROPOSED EXTRUDED CONCRETE CURB
- - - - - PROPOSED SAWCUT LINE
- ▨ - - - - - FULL DEPTH ROADWAY RECONSTRUCTION
- ▩ - - - - - RECLAIM ROADWAY
- ▧ - - - - - DRIVEWAY APRON TOWN R.O.W.
- ▦ - - - - - DRIVEWAY APRON PRIVATE PROPERTY
- - - - - PROPOSED STORM SEWER PIPE
- - - - - - PROPOSED CATCH BASIN
- * - - - - - TREE REMOVAL IN EASEMENT AREA
- X - - - - - TREE REMOVAL IN ROW



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
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PROJECT NO: T0196
FILE: T0196-15-C-300-RDWAY.dwg
DRAWN BY: MDS
CHECKED BY: AJM
APPROVED BY: DCH

ROADWAY PLAN
STA 47+50 TO 52+50

SCALE: 1" = 20'

C3.12

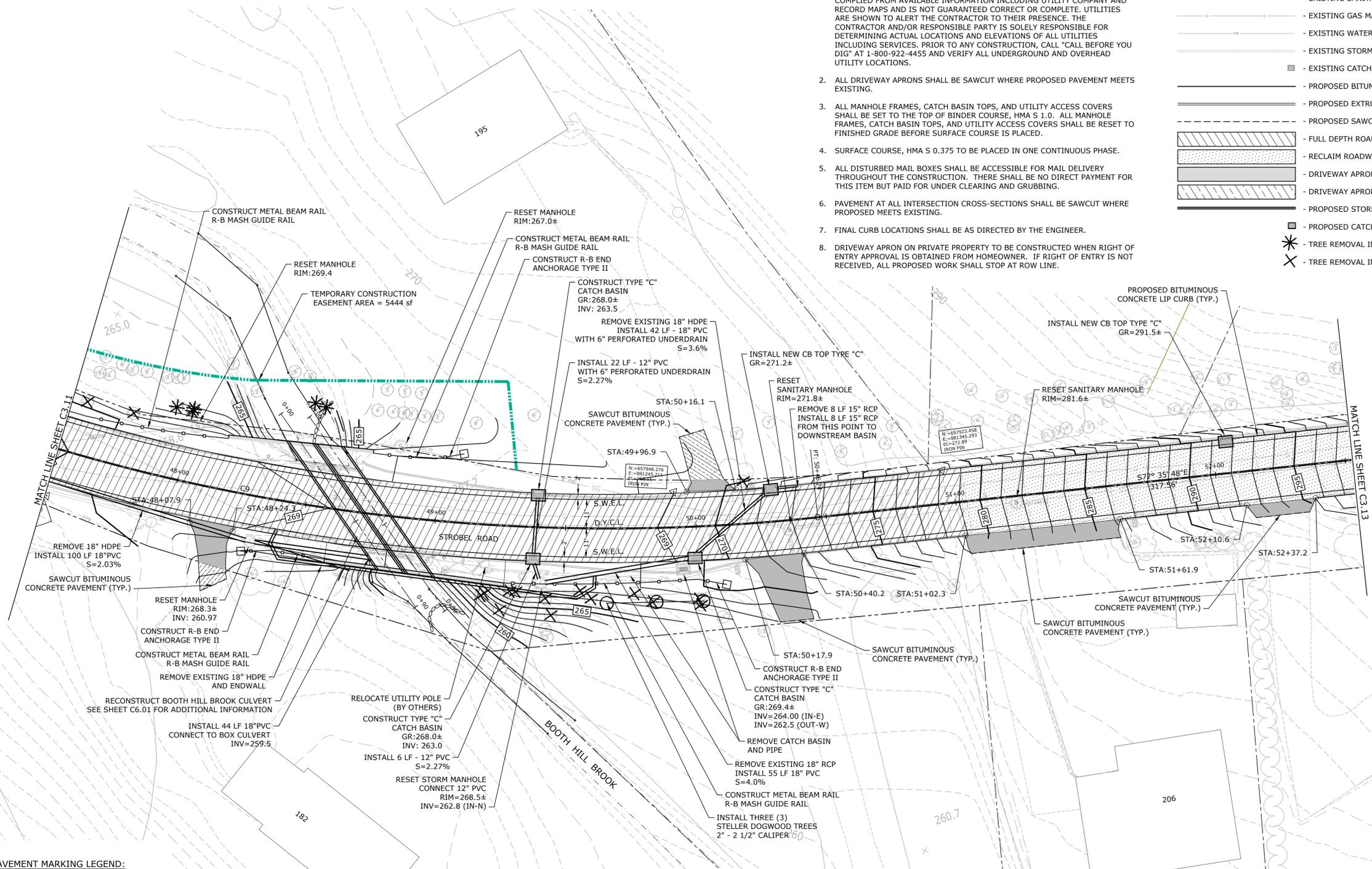
CURVE TABLE

Curve #		STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C9	P.C.	46+55.20	N 658,099.42	E 880,950.61	29° 54' 40.0"	200.34'	391.54'	750.00'
	P.I.	50+46.74	N 657,964.55	E 881,098.75				
	P.T.	50+46.74	N 657,921.52	E 881,294.41				

- SIGNING AND PAVEMENT MARKING NOTES:**
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- SIGNING AND PAVEMENT MARKING LEGEND:**
- - - - - EXISTING PAINTED CENTERLINE
 - ▬ - - - - - EXISTING WHITE PAINTED STOP BAR
 - ⊙ - - - - - EXISTING GROUND MOUNTED SIGN
 - ⊙ - - - - - PROPOSED SIGN (GROUND MOUNTED)
 - ➔ - - - - - PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
 - ⊙ - - - - - PROPOSED SIGNS BY CONDOT SIGN NUMBER
 - S.W.E.L. - - - - - SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
 - S.Y.E.L. - - - - - SINGLE YELLOW EDGE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
 - D.Y.C.L. - - - - - DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
 - S.W.S.L. - - - - - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING

Job: Sheet: 1/22/2019 9:10:06am By: D:\browne\Tighe & Bond\15\T0196\15_Strobel Rd\Drawing\Sheet\T0196-15-C-300-RDWAY.dwg



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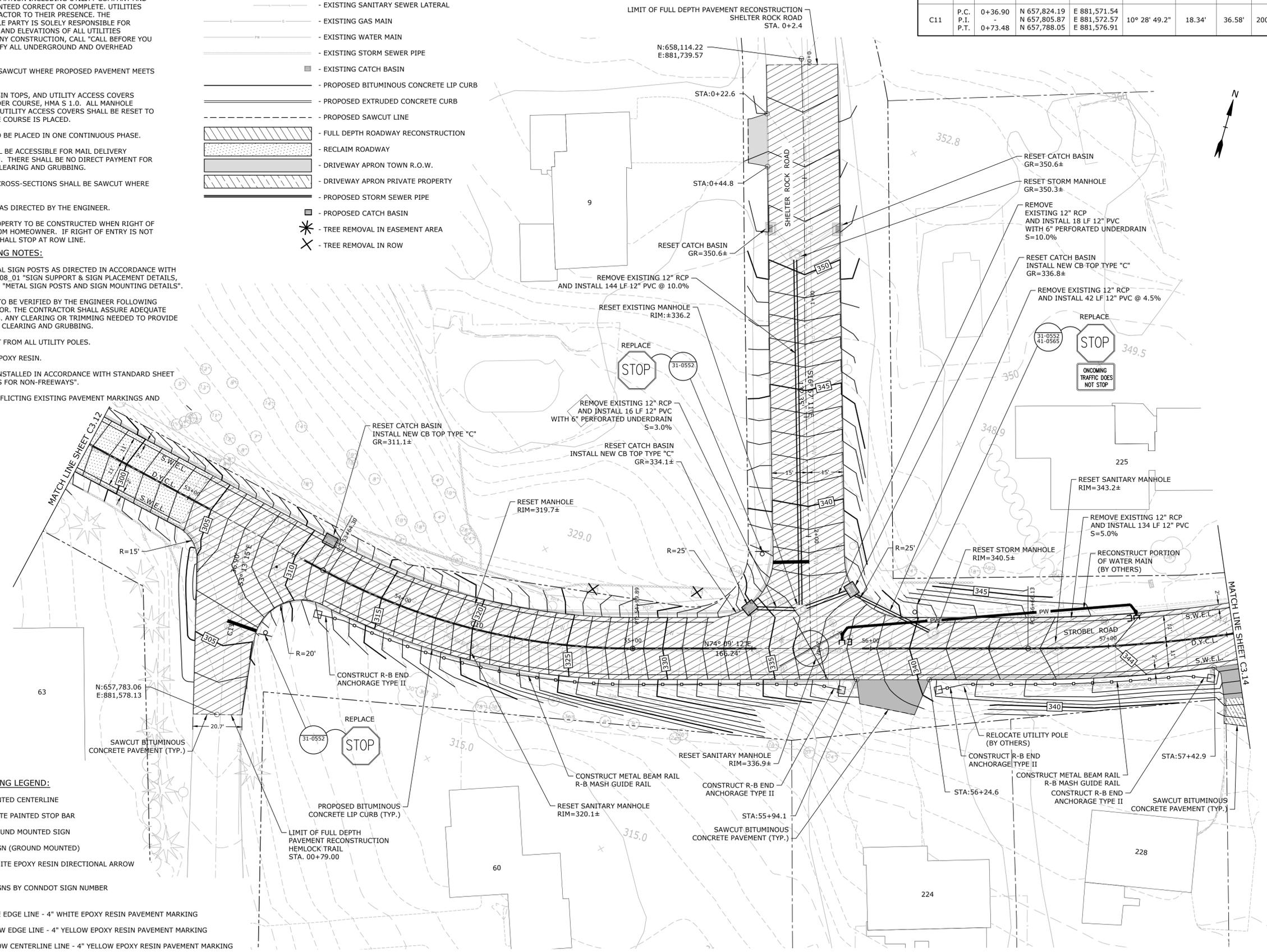
LEGEND

- - - - - TOWN LINE
- - - - - PROPERTY LINE
- - EXISTING SANITARY SEWER MANHOLE
- - - - - EXISTING SANITARY SEWER MAIN
- - - - - EXISTING SANITARY SEWER LATERAL
- - - - - EXISTING GAS MAIN
- - - - - EXISTING WATER MAIN
- - - - - EXISTING STORM SEWER PIPE
- - EXISTING CATCH BASIN
- - - - - PROPOSED BITUMINOUS CONCRETE LIP CURB
- - - - - PROPOSED EXTRUDED CONCRETE CURB
- - - - - PROPOSED SAWCUT LINE
- ▨ - FULL DEPTH ROADWAY RECONSTRUCTION
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- - - - - PROPOSED STORM SEWER PIPE
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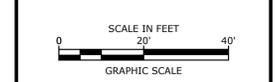
SIGNING AND PAVEMENT MARKING LEGEND:

- - - - - EXISTING PAINTED CENTERLINE
- - - - - EXISTING WHITE PAINTED STOP BAR
- - - - - EXISTING GROUND MOUNTED SIGN
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- S.W.E.L. - SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
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- S.W.S.L. - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING

CURVE TABLE							
Curve #	STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C10	P.C.	53+64.30	N 657,853.31	E 881,604.56	28° 15' 00.6"	69.20'	135.59'
	P.I.	54+99.89	N 657,838.45	E 881,672.14			
	P.T.	54+99.89	N 657,857.34	E 881,738.72			
C11	P.C.	0+36.90	N 657,824.19	E 881,571.54	10° 28' 49.2"	18.34'	200.00'
	P.I.	-	N 657,805.87	E 881,572.57			
	P.T.	0+73.48	N 657,788.05	E 881,576.91			



Tighe & Bond
 www.tighebond.com
 1000 Bridgeport Avenue
 Suite 320
 Shelton, CT 06484
 (203) 712-1100



STROBOL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-15-C-300-RDWY.dwg	
DRAWN BY:	MDS	
CHECKED:	AJM	
APPROVED:	DCH	

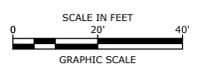
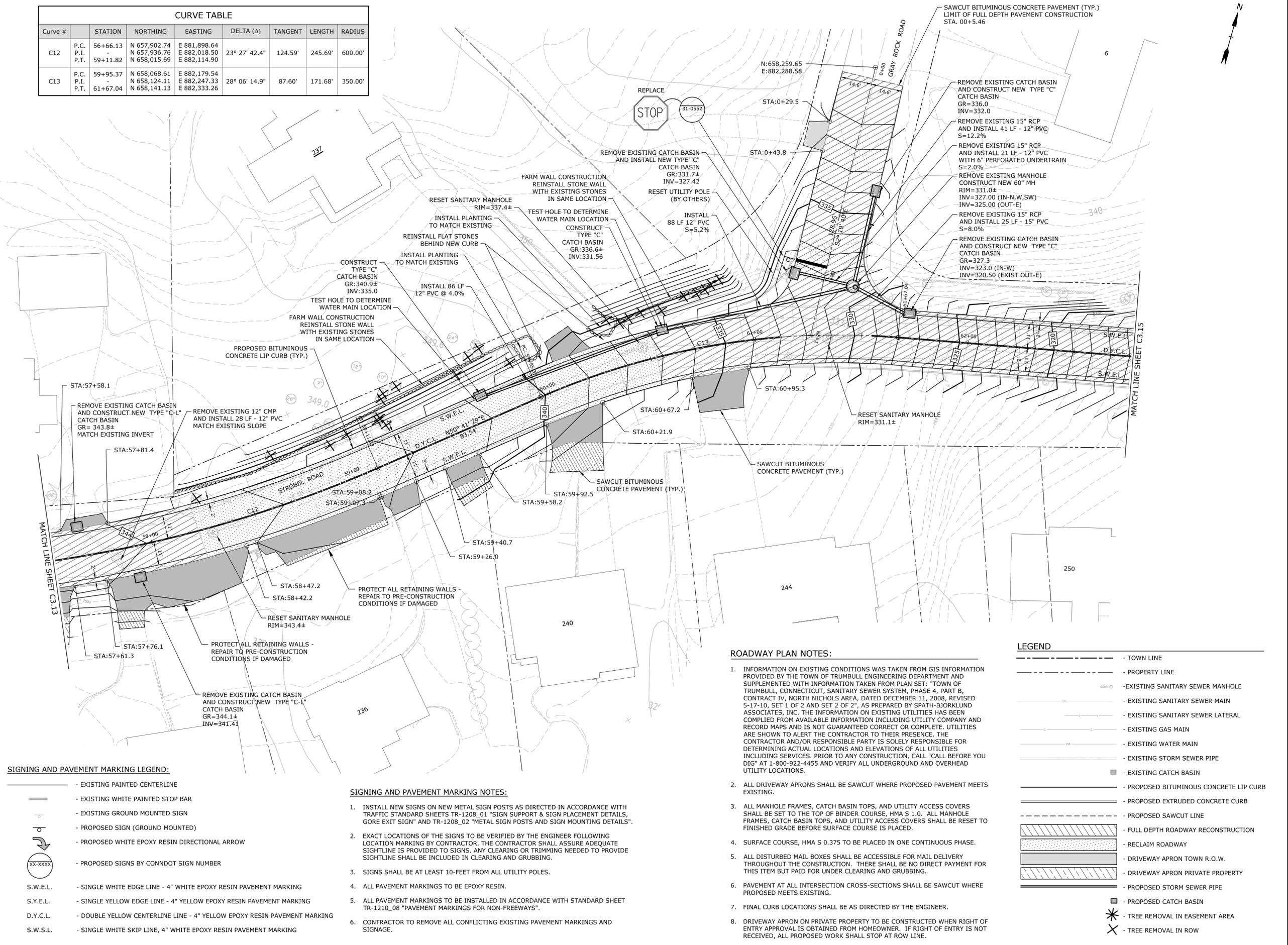
ROADWAY PLAN
 STA 52+50 TO 57+50

SCALE: 1" = 20'

C3.13

Plot Sheet: 1/22/2019 9:40:07am By: Duhrauec
 Tighe & Bond: J:\T0196\15 Strobol Rd\Drawing\Sheet\T0196-15-C-300-RDWY.dwg

CURVE TABLE							
Curve #	STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C12	P.C.	56+66.13	N 657,902.74	E 881,898.64	23° 27' 42.4"	124.59'	245.69'
	P.I.	-	N 657,936.76	E 882,018.50			
	P.T.	59+11.82	N 658,015.69	E 882,114.90			
C13	P.C.	59+95.37	N 658,068.61	E 882,179.54	28° 06' 14.9"	87.60'	171.68'
	P.I.	-	N 658,124.11	E 882,247.33			
	P.T.	61+67.04	N 658,141.13	E 882,333.26			



STROBELS ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

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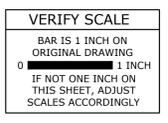
- - - - - TOWN LINE
- - - - - PROPERTY LINE
- SMH ○ - EXISTING SANITARY SEWER MANHOLE
- SS - - - - - EXISTING SANITARY SEWER MAIN
- SSL - - - - - EXISTING SANITARY SEWER LATERAL
- G - - - - - EXISTING GAS MAIN
- W - - - - - EXISTING WATER MAIN
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FILE: T0196-15-C-300-RDWAY.dwg
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CHECKED: AJM
APPROVED: DCH

ROADWAY PLAN
STA 57+50 TO 62+75
SCALE: 1" = 20'
C3.14

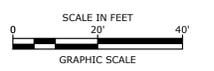
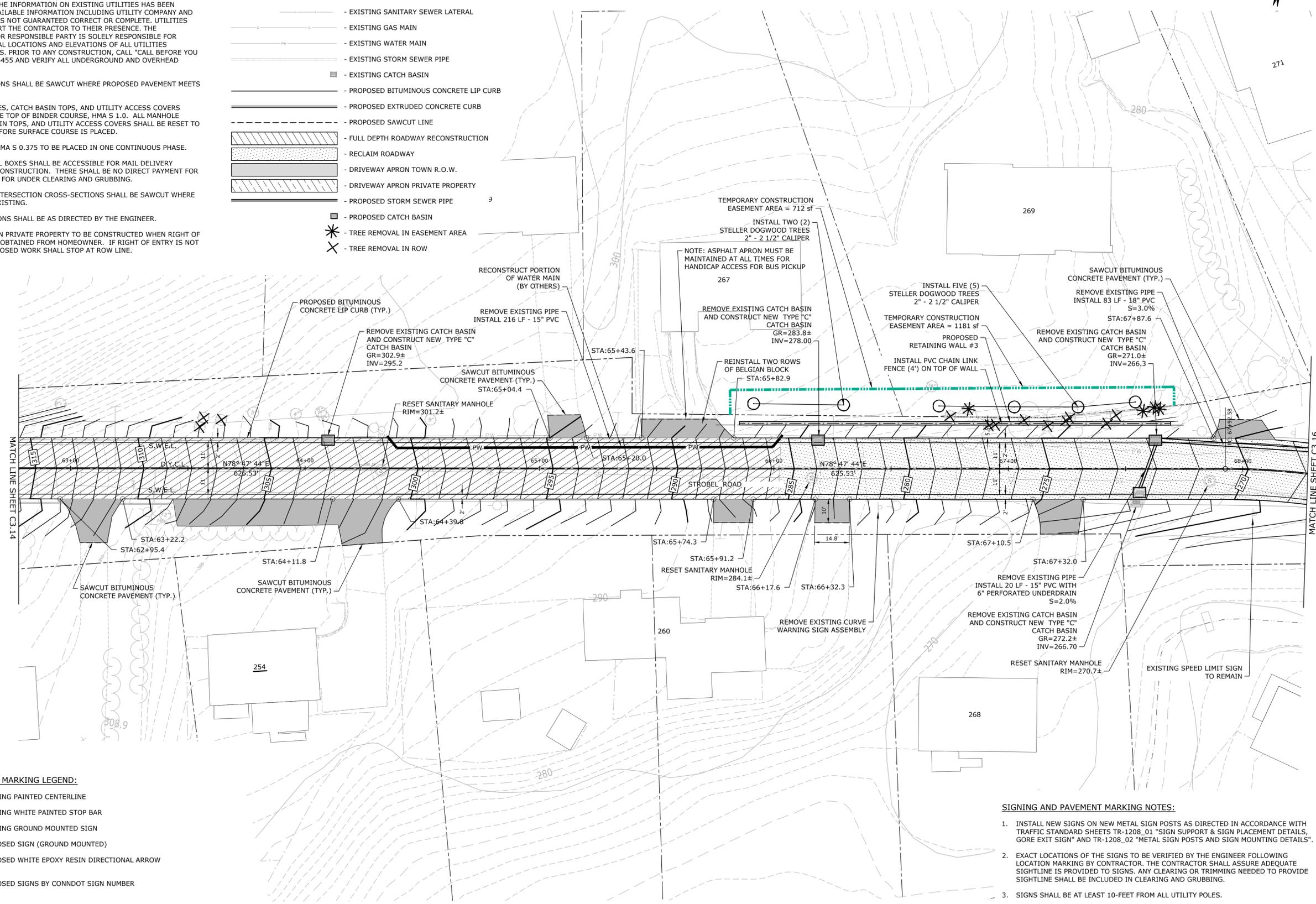
Plot Sheet: 1/22/2019 9:40:07am By: D:\bubae\c
 Tighe & Bond: \A\T0196\15-Strobel RD\Drawing\Sheet\T0196-15-C-300-RDWAY.dwg

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- — — — — PROPOSED STORM SEWER PIPE
- - - - - - PROPOSED CATCH BASIN
- * - - - - - TREE REMOVAL IN EASEMENT AREA
- X - - - - - TREE REMOVAL IN ROW



STROBOL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
BAR IS 1 INCH ON ORIGINAL DRAWING
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SIGNING AND PAVEMENT MARKING LEGEND:

- - - - - EXISTING PAINTED CENTERLINE
- — — — — EXISTING WHITE PAINTED STOP BAR
- - - - - - EXISTING GROUND MOUNTED SIGN
- - - - - - PROPOSED SIGN (GROUND MOUNTED)
- - - - - - PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
- - - - - - PROPOSED SIGNS BY CONDOT SIGN NUMBER
- S.W.E.L. - SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
- S.Y.E.L. - SINGLE YELLOW EDGE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- D.Y.C.L. - DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- S.W.S.L. - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING

SIGNING AND PAVEMENT MARKING NOTES:

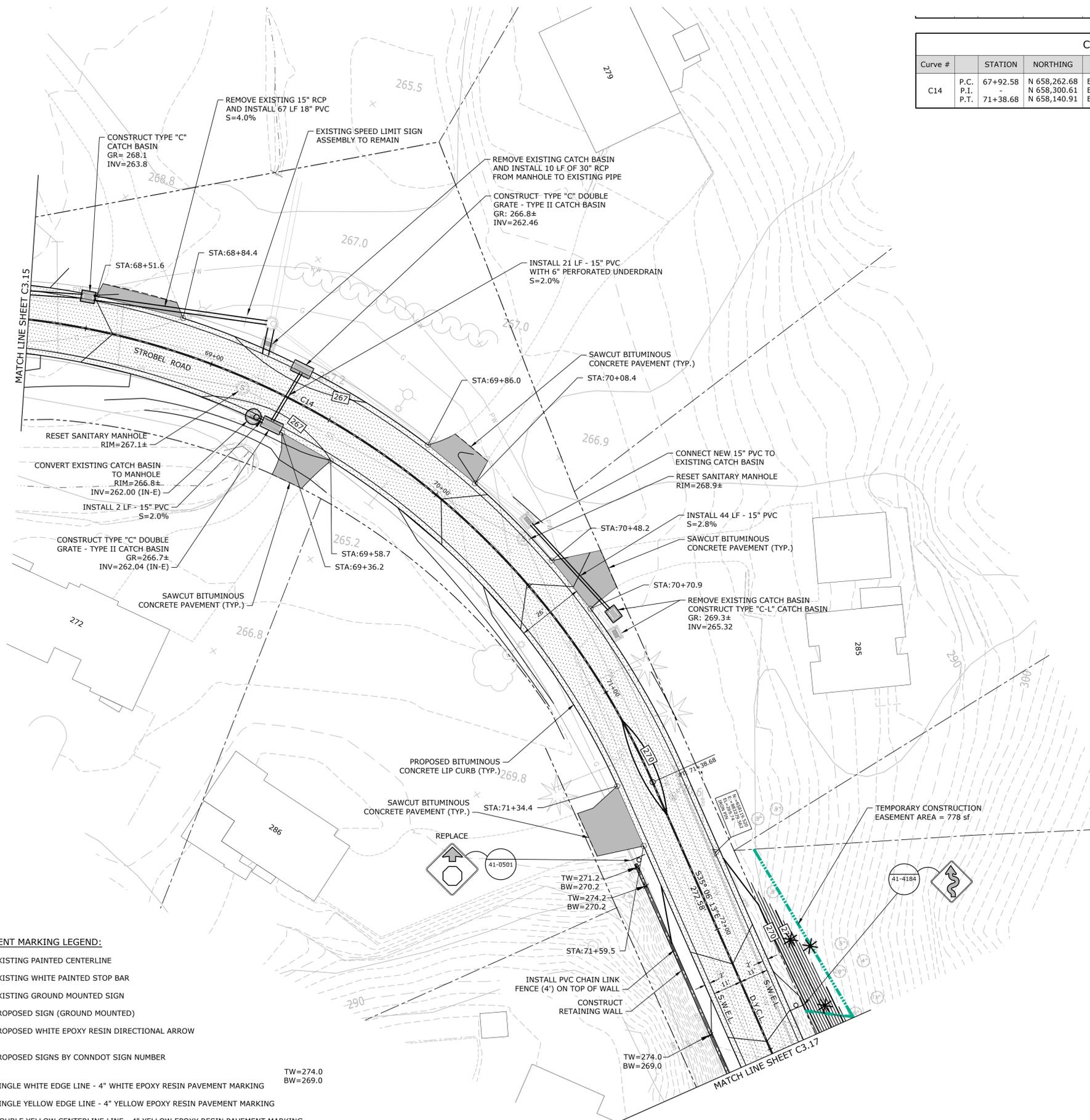
1. INSTALL NEW SIGNS ON NEW METAL SIGN POSTS AS DIRECTED IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208.01 "SIGN SUPPORT & SIGN PLACEMENT DETAILS", GORE EXIT SIGN" AND TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
2. EXACT LOCATIONS OF THE SIGNS TO BE VERIFIED BY THE ENGINEER FOLLOWING LOCATION MARKING BY CONTRACTOR. THE CONTRACTOR SHALL ASSURE ADEQUATE SIGHTLINE IS PROVIDED TO SIGNS. ANY CLEARING OR TRIMMING NEEDED TO PROVIDE SIGHTLINE SHALL BE INCLUDED IN CLEARING AND GRUBBING.
3. SIGNS SHALL BE AT LEAST 10- FEET FROM ALL UTILITY POLES.
4. ALL PAVEMENT MARKINGS TO BE EPOXY RESIN.
5. ALL PAVEMENT MARKINGS TO BE INSTALLED IN ACCORDANCE WITH STANDARD SHEET TR-1210.08 "PAVEMENT MARKINGS FOR NON-FREEWAYS".
6. CONTRACTOR TO REMOVE ALL CONFLICTING EXISTING PAVEMENT MARKINGS AND SIGNAGE.

MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-300-RDWY.dwg
DRAWN BY:	MDS
CHECKED BY:	AJM
APPROVED:	DCH
ROADWAY PLAN	
STA 62+75 TO 68+25	
SCALE:	1" = 20'
C3.15	

Plot: Sheet: 1/27/2019 9:10:07am By: D:\brouse\Tighe & Bond\15\T0196\15_Strobol Rd\Drawing\Sheet\T0196-15-C-300-RDWY.dwg

CURVE TABLE							
Curve #	STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C14	P.C.	67+92.58	N 658,262.68	E 882,946.87	66° 06' 03.1"	195.20'	346.10'
	P.I.	-	N 658,300.61	E 883,138.35			
	P.T.	71+38.68	N 658,140.91	E 883,250.60			

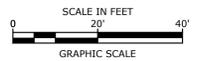


ROADWAY PLAN NOTES:

1. INFORMATION ON EXISTING CONDITIONS WAS TAKEN FROM GIS INFORMATION PROVIDED BY THE TOWN OF TRUMBULL ENGINEERING DEPARTMENT AND SUPPLEMENTED WITH INFORMATION TAKEN FROM PLAN SET: "TOWN OF TRUMBULL, CONNECTICUT, SANITARY SEWER SYSTEM, PHASE 4, PART B, CONTRACT IV, NORTH NICHOLS AREA, DATED DECEMBER 11, 2008, REVISED 5-17-10, SET 1 OF 2 AND SET 2 OF 2", AS PREPARED BY SPATH-BJORKLUND ASSOCIATES, INC. THE INFORMATION ON EXISTING UTILITIES HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY AND RECORD MAPS AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR AND/OR RESPONSIBLE PARTY IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES INCLUDING SERVICES. PRIOR TO ANY CONSTRUCTION, CALL "CALL BEFORE YOU DIG" AT 1-800-922-4455 AND VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS.
2. ALL DRIVEWAY APRONS SHALL BE SAWCUT WHERE PROPOSED PAVEMENT MEETS EXISTING.
3. ALL MANHOLE FRAMES, CATCH BASIN TOPS, AND UTILITY ACCESS COVERS SHALL BE SET TO THE TOP OF BINDER COURSE, HMA S 1.0. ALL MANHOLE FRAMES, CATCH BASIN TOPS, AND UTILITY ACCESS COVERS SHALL BE RESET TO FINISHED GRADE BEFORE SURFACE COURSE IS PLACED.
4. SURFACE COURSE, HMA S 0.375 TO BE PLACED IN ONE CONTINUOUS PHASE.
5. ALL DISTURBED MAIL BOXES SHALL BE ACCESSIBLE FOR MAIL DELIVERY THROUGHOUT THE CONSTRUCTION. THERE SHALL BE NO DIRECT PAYMENT FOR THIS ITEM BUT PAID FOR UNDER CLEARING AND GRUBBING.
6. PAVEMENT AT ALL INTERSECTION CROSS-SECTIONS SHALL BE SAWCUT WHERE PROPOSED MEETS EXISTING.
7. FINAL CURB LOCATIONS SHALL BE AS DIRECTED BY THE ENGINEER.
8. DRIVEWAY APRON ON PRIVATE PROPERTY TO BE CONSTRUCTED WHEN RIGHT OF ENTRY APPROVAL IS OBTAINED FROM HOMEOWNER. IF RIGHT OF ENTRY IS NOT RECEIVED, ALL PROPOSED WORK SHALL STOP AT ROW LINE.

SIGNING AND PAVEMENT MARKING NOTES:

1. INSTALL NEW SIGNS ON NEW METAL SIGN POSTS AS DIRECTED IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208_01 "SIGN SUPPORT & SIGN PLACEMENT DETAILS, GORE EXIT SIGN" AND TR-1208_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
2. EXACT LOCATIONS OF THE SIGNS TO BE VERIFIED BY THE ENGINEER FOLLOWING LOCATION MARKING BY CONTRACTOR. THE CONTRACTOR SHALL ASSURE ADEQUATE SIGHTLINE IS PROVIDED TO SIGNS. ANY CLEARING OR TRIMMING NEEDED TO PROVIDE SIGHTLINE SHALL BE INCLUDED IN CLEARING AND GRUBBING.
3. SIGNS SHALL BE AT LEAST 10-FEET FROM ALL UTILITY POLES.
4. ALL PAVEMENT MARKINGS TO BE EPOXY RESIN.
5. ALL PAVEMENT MARKINGS TO BE INSTALLED IN ACCORDANCE WITH STANDARD SHEET TR-1210_08 "PAVEMENT MARKINGS FOR NON-FREEWAYS".
6. CONTRACTOR TO REMOVE ALL CONFLICTING EXISTING PAVEMENT MARKINGS AND SIGNAGE.



LEGEND

- - - TOWN LINE
- - - PROPERTY LINE
- ⊙ EXISTING SANITARY SEWER MANHOLE
- - - EXISTING SANITARY SEWER MAIN
- - - EXISTING SANITARY SEWER LATERAL
- - - EXISTING GAS MAIN
- - - EXISTING WATER MAIN
- - - EXISTING STORM SEWER PIPE
- EXISTING CATCH BASIN
- - - PROPOSED BITUMINOUS CONCRETE LIP CURB
- - - PROPOSED EXTRUDED CONCRETE CURB
- - - PROPOSED SAWCUT LINE
- ▨ FULL DEPTH ROADWAY RECONSTRUCTION
- ▨ RECLAIM ROADWAY
- ▨ DRIVEWAY APRON TOWN R.O.W.
- ▨ DRIVEWAY APRON PRIVATE PROPERTY
- - - PROPOSED STORM SEWER PIPE
- PROPOSED CATCH BASIN
- * TREE REMOVAL IN EASEMENT AREA
- X TREE REMOVAL IN ROW
- TW=XXX.X TOP OF WALL ELEVATION
- BW=XXX.X BOTTOM OF WALL ELEVATION

SIGNING AND PAVEMENT MARKING LEGEND:

- - - EXISTING PAINTED CENTERLINE
- - - EXISTING WHITE PAINTED STOP BAR
- - - EXISTING GROUND MOUNTED SIGN
- ⊙ PROPOSED SIGN (GROUND MOUNTED)
- PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
- ⊙-XXX PROPOSED SIGNS BY CONNDOT SIGN NUMBER
- S.W.E.L. - SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
- S.Y.E.L. - SINGLE YELLOW EDGE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- D.Y.C.L. - DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- S.W.S.L. - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING

Plot Sheet: 1/27/2019 9:10:07am By: Duhrauec
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawing\Sheet\T0196-15-C-300-RD.WY.dwg

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-15-C-300-RD.WY.dwg	
DRAWN BY:	MDS	
CHECKED:	AJM	
APPROVED:	DCH	

ROADWAY PLAN
 STA 68+25 TO 72+50

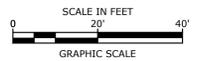
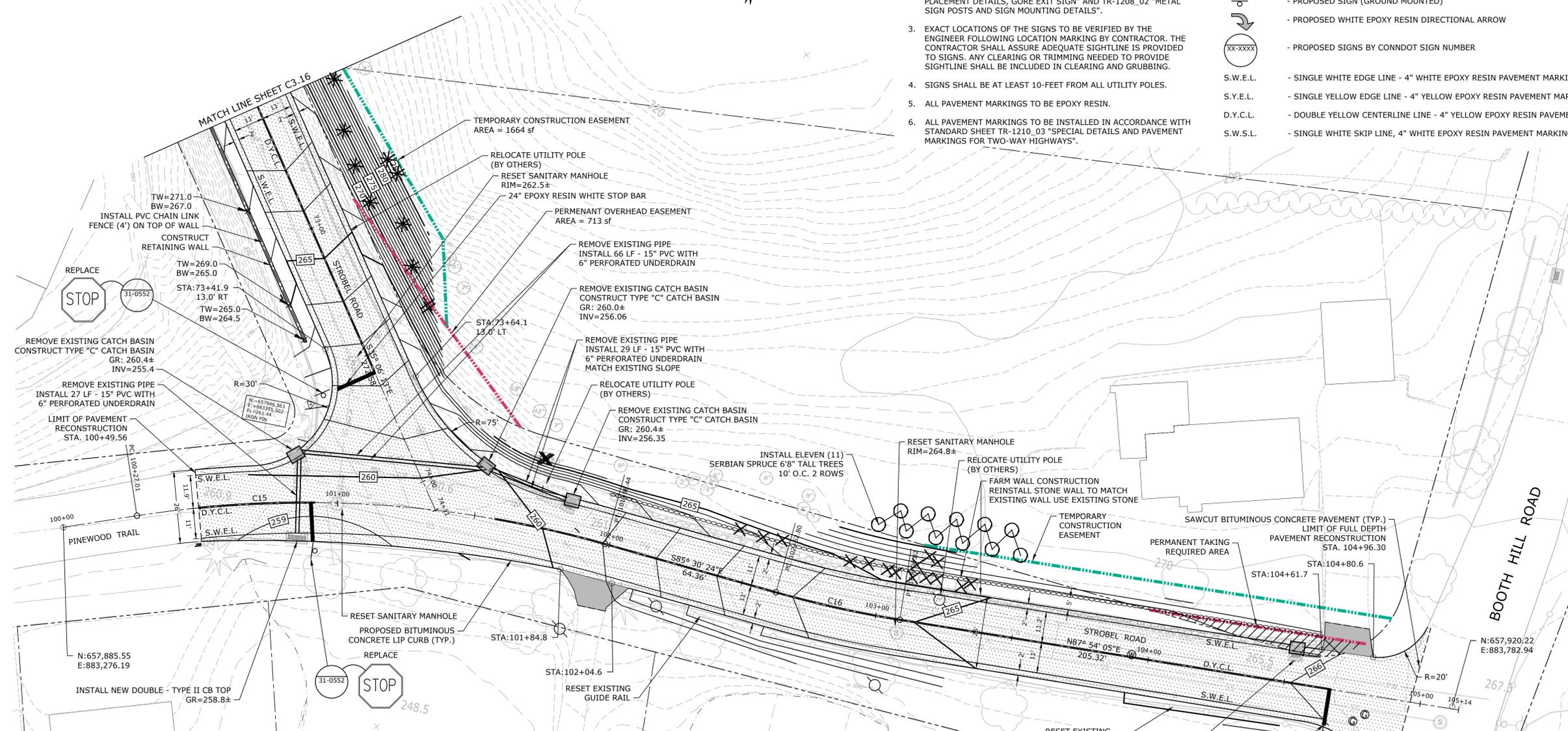
SCALE: 1" = 20'

SIGNING AND PAVEMENT MARKING NOTES:

- ALL EXISTING SIGNS WITHIN THE PROJECT LIMITS SHALL BE REMOVED, EXCEPT FOR STREET NAME SIGNS, WHICH SHALL BE RELOCATED.
- INSTALL SIGNS AND METAL SIGN POSTS IN ACCORDANCE WITH TRAFFIC STANDARD SHEETS TR-1208.01 "SIGN SUPPORT & SIGN PLACEMENT DETAILS, GORE EXIT SIGN" AND TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
- EXACT LOCATIONS OF THE SIGNS TO BE VERIFIED BY THE ENGINEER FOLLOWING LOCATION MARKING BY CONTRACTOR. THE CONTRACTOR SHALL ASSURE ADEQUATE SIGHTLINE IS PROVIDED TO SIGNS. ANY CLEARING OR TRIMMING NEEDED TO PROVIDE SIGHTLINE SHALL BE INCLUDED IN CLEARING AND GRUBBING.
- SIGNS SHALL BE AT LEAST 10-FEET FROM ALL UTILITY POLES.
- ALL PAVEMENT MARKINGS TO BE EPOXY RESIN.
- ALL PAVEMENT MARKINGS TO BE INSTALLED IN ACCORDANCE WITH STANDARD SHEET TR-1210.03 "SPECIAL DETAILS AND PAVEMENT MARKINGS FOR TWO-WAY HIGHWAYS".

SIGNING AND PAVEMENT MARKING LEGEND:

- EXISTING PAINTED CENTERLINE
- EXISTING WHITE PAINTED STOP BAR
- EXISTING GROUND MOUNTED SIGN
- PROPOSED SIGN (GROUND MOUNTED)
- PROPOSED WHITE EPOXY RESIN DIRECTIONAL ARROW
- PROPOSED SIGNS BY CONDOT SIGN NUMBER
- S.W.E.L. - SINGLE WHITE EDGE LINE - 4" WHITE EPOXY RESIN PAVEMENT MARKING
- S.Y.E.L. - SINGLE YELLOW EDGE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- D.Y.C.L. - DOUBLE YELLOW CENTERLINE LINE - 4" YELLOW EPOXY RESIN PAVEMENT MARKING
- S.W.S.L. - SINGLE WHITE SKIP LINE, 4" WHITE EPOXY RESIN PAVEMENT MARKING



ROADWAY PLAN NOTES:

- INFORMATION ON EXISTING CONDITIONS WAS TAKEN FROM GIS INFORMATION PROVIDED BY THE TOWN OF TRUMBULL ENGINEERING DEPARTMENT AND SUPPLEMENTED WITH INFORMATION TAKEN FROM PLAN SET, "TOWN OF TRUMBULL, CONNECTICUT, SANITARY SEWER SYSTEM, PHASE 4, PART B, CONTRACT IV, NORTH NICHOLS AREA, DATED DECEMBER 11, 2008, REVISED 5-17-10, SET 1 OF 2 AND SET 2 OF 2", AS PREPARED BY SPATH-BJORKLUND ASSOCIATES, INC. THE INFORMATION ON EXISTING UTILITIES HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY AND RECORD MAPS AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR AND/OR RESPONSIBLE PARTY IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES INCLUDING SERVICES. PRIOR TO ANY CONSTRUCTION, CALL "CALL BEFORE YOU DIG" AT 1-800-922-4455 AND VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS.
- ALL DRIVEWAY APRONS SHALL BE SAWCUT WHERE PROPOSED PAVEMENT MEETS EXISTING.
- ALL MANHOLE FRAMES, CATCH BASIN TOPS, AND UTILITY ACCESS COVERS SHALL BE SET TO THE TOP OF BINDER COURSE, HMA S 1.0. ALL MANHOLE FRAMES, CATCH BASIN TOPS, AND UTILITY ACCESS COVERS SHALL BE RESET TO FINISHED GRADE BEFORE SURFACE COURSE IS PLACED.
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- PAVEMENT AT ALL INTERSECTION CROSS-SECTIONS SHALL BE SAWCUT WHERE PROPOSED MEETS EXISTING.
- FINAL CURB LOCATIONS SHALL BE AS DIRECTED BY THE ENGINEER.
- DRIVEWAY APRON ON PRIVATE PROPERTY TO BE CONSTRUCTED WHEN RIGHT OF ENTRY APPROVAL IS OBTAINED FROM HOMEOWNER. IF RIGHT OF ENTRY IS NOT RECEIVED, ALL PROPOSED WORK SHALL STOP AT ROW LINE.

LEGEND

- TOWN LINE
- PROPERTY LINE
- EXISTING SANITARY SEWER MANHOLE
- EXISTING SANITARY SEWER MAIN
- EXISTING SANITARY SEWER LATERAL
- EXISTING GAS MAIN
- EXISTING WATER MAIN
- EXISTING STORM SEWER PIPE
- EXISTING CATCH BASIN
- PROPOSED BITUMINOUS CONCRETE LIP CURB
- PROPOSED EXTRUDED CONCRETE CURB
- PROPOSED SAWCUT LINE
- FULL DEPTH ROADWAY RECONSTRUCTION
- RECLAIM ROADWAY
- DRIVEWAY APRON TOWN R.O.W.
- DRIVEWAY APRON PRIVATE PROPERTY
- PROPOSED STORM SEWER PIPE
- PROPOSED CATCH BASIN
- TREE REMOVAL IN EASEMENT AREA
- TREE REMOVAL IN ROW
- TW=XXX.X - TOP OF WALL ELEVATION
- BW=XXX.X - BOTTOM OF WALL ELEVATION

Curve #		STATION	NORTHING	EASTING	DELTA (Δ)	TANGENT	LENGTH	RADIUS
C15	P.C.	100+27.01	N 657,894.88	E 883,301.54	24° 33' 17.4"	87.05'	171.43'	400.00'
	P.I.	-	N 657,924.74	E 883,383.31				
	P.T.	101+98.44	N 657,917.92	E 883,470.09				
C16	P.C.	102+62.80	N 657,912.87	E 883,534.25	6° 35' 30.5"	23.04'	46.02'	400.00'
	P.I.	-	N 657,911.07	E 883,557.22				
	P.T.	103+08.82	N 657,911.91	E 883,580.24				

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

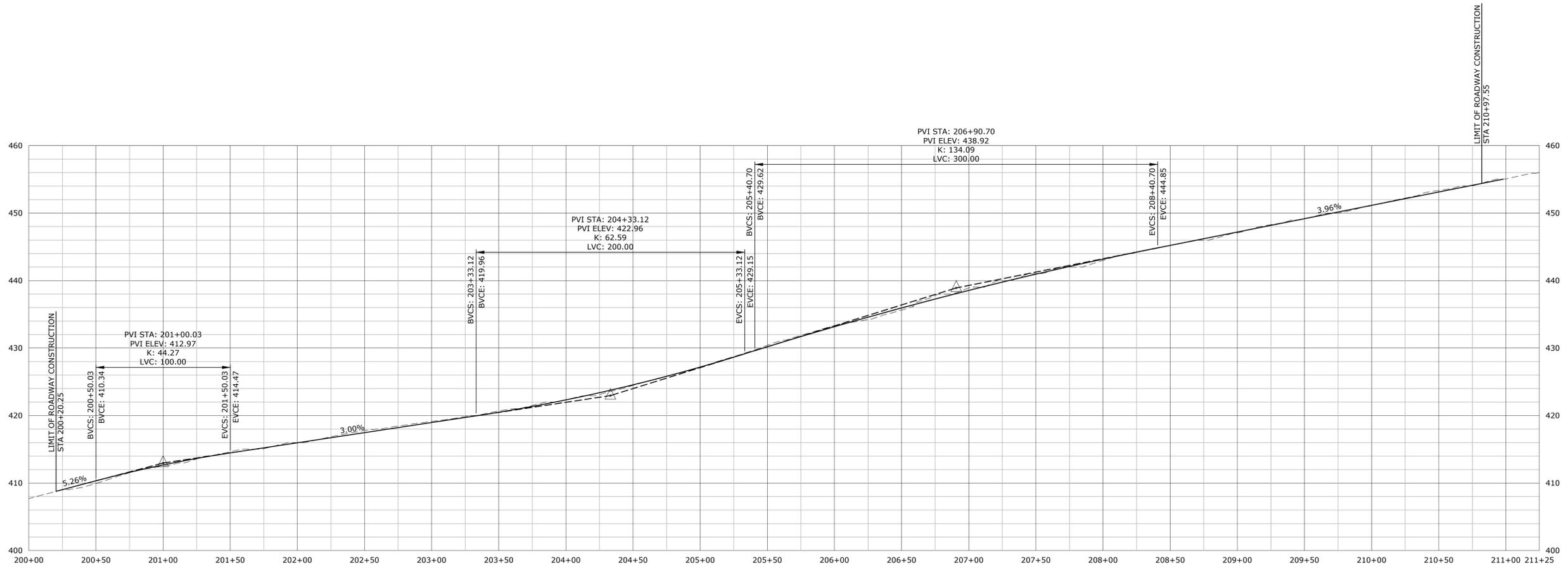
VERIFY SCALE
BAR IS 1 INCH ON ORIGINAL DRAWING
IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
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FILE:	T0196-15-C-300-RDWY.dwg	
DRAWN BY:	MDS	
CHECKED:	AJM	
APPROVED:	DCH	

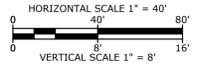
ROADWAY PLAN STA 72+50 TO 105+12

SCALE: 1" = 20'

Job: Sheet: 1/22/2019 9:10:08am By: D:\brouse\Tighe & Bond\T0196\15_C-300-RDWY.dwg



DANIELS FARM ROAD
 HOR: 1" = 40' VERT: 1" = 8'



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
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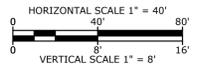
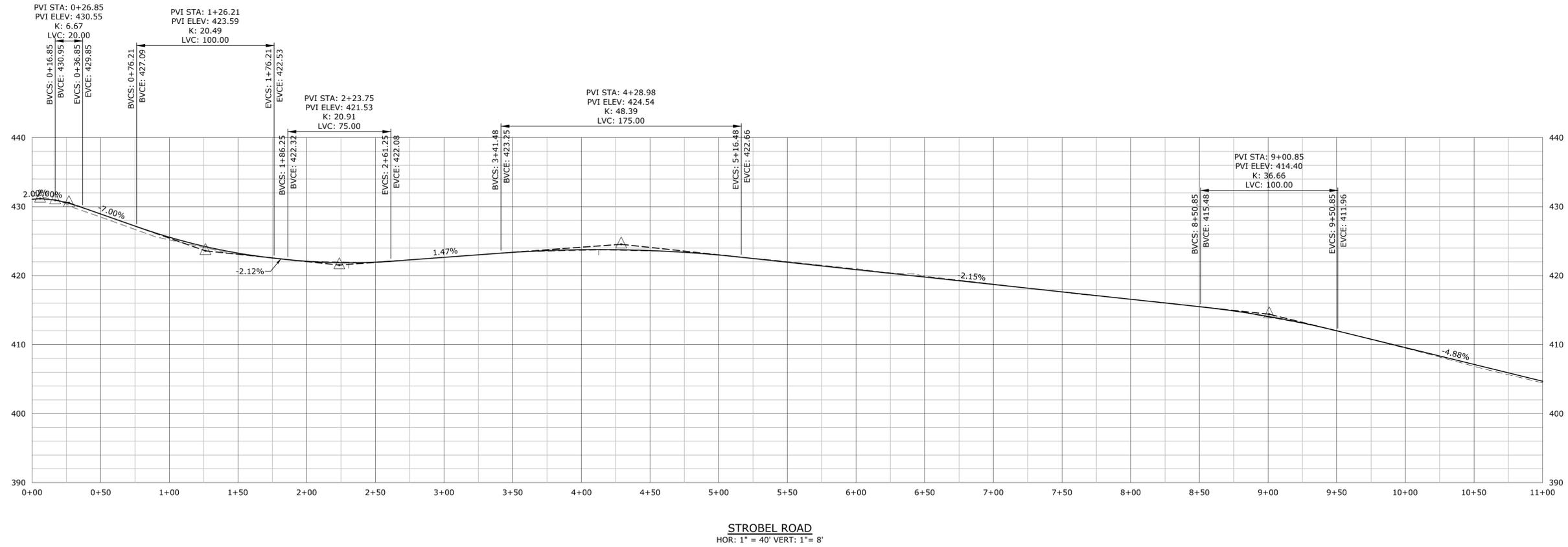
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-400-PROFILE.dwg
 DRAWN BY: APW
 CHECKED: AJM
 APPROVED: DCH

ROADWAY PROFILE
 STA: 0+00 TO 11+00
 SCALE: HORZ: 1" = 40' VERT: 1" = 8'

C4.0

Plot Sheet: 6/29/2018 9:27am By: DuhaineC
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawings\Sheet\T0196-15-C-400-PROFILE.dwg



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

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MARK	DATE	DESCRIPTION

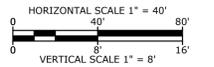
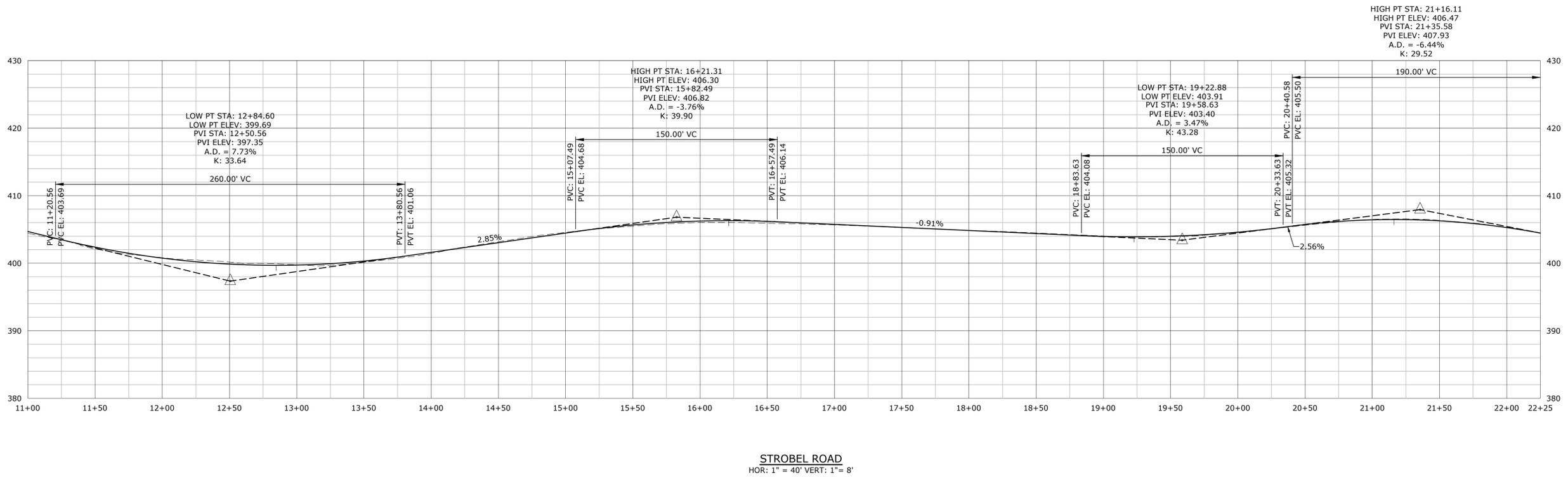
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FILE:	T0196-15-C-400-PROFILE.dwg
DRAWN BY:	APW
CHECKED BY:	AJM
APPROVED BY:	DCH

**ROADWAY PROFILE
 STA: 0+00 TO 11+00**

SCALE: HORZ: 1" = 40' VERT: 1" = 8'

C4.1

Plot Sheet: 6/29/2018 9:23:46am By: DuhaineC
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawings\Sheet\T0196-15-C-400-PROFILE.dwg



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
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MARK	DATE	DESCRIPTION

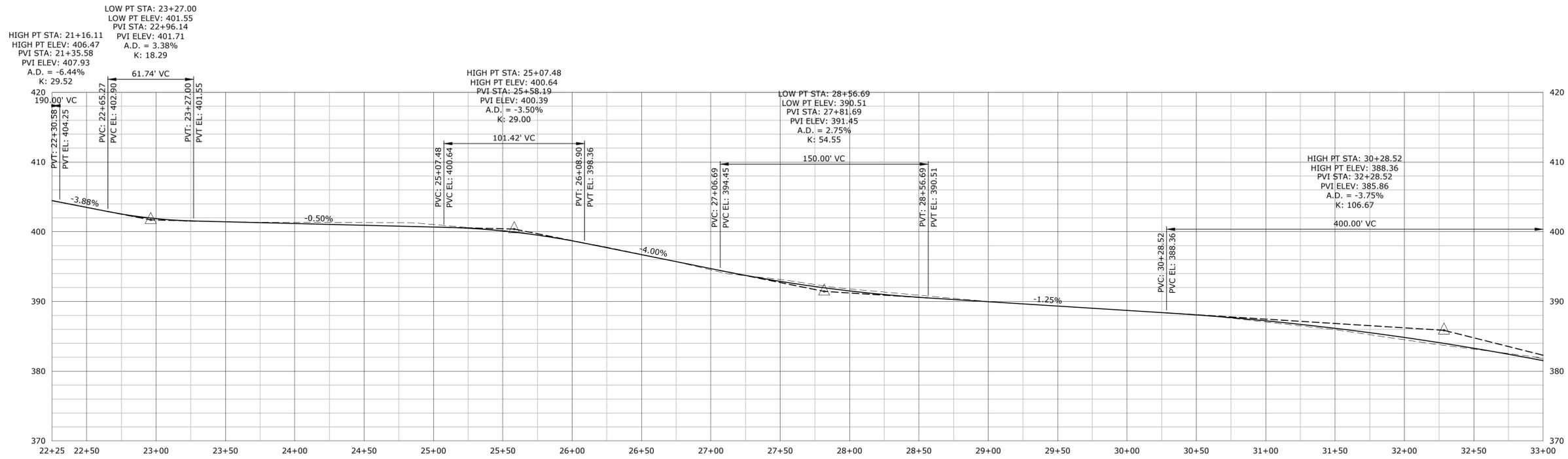
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FILE:	T0196-15-C-400-PROFILE.dwg
DRAWN BY:	APW
CHECKED:	AJM
APPROVED:	DCH

ROADWAY PROFILE
 STA: 11+00 TO 22+50

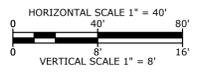
SCALE: HORZ: 1" = 40' VERT: 1" = 8'

C4.2

Plot Sheet: 6/29/2018 9:27am By: DuhannaC
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawing\Sheet\T0196-15-C-400-PROFILE.dwg



STROBEL ROAD
 HOR: 1" = 40' VERT: 1" = 8'



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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MARK	DATE	DESCRIPTION

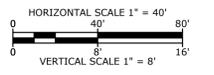
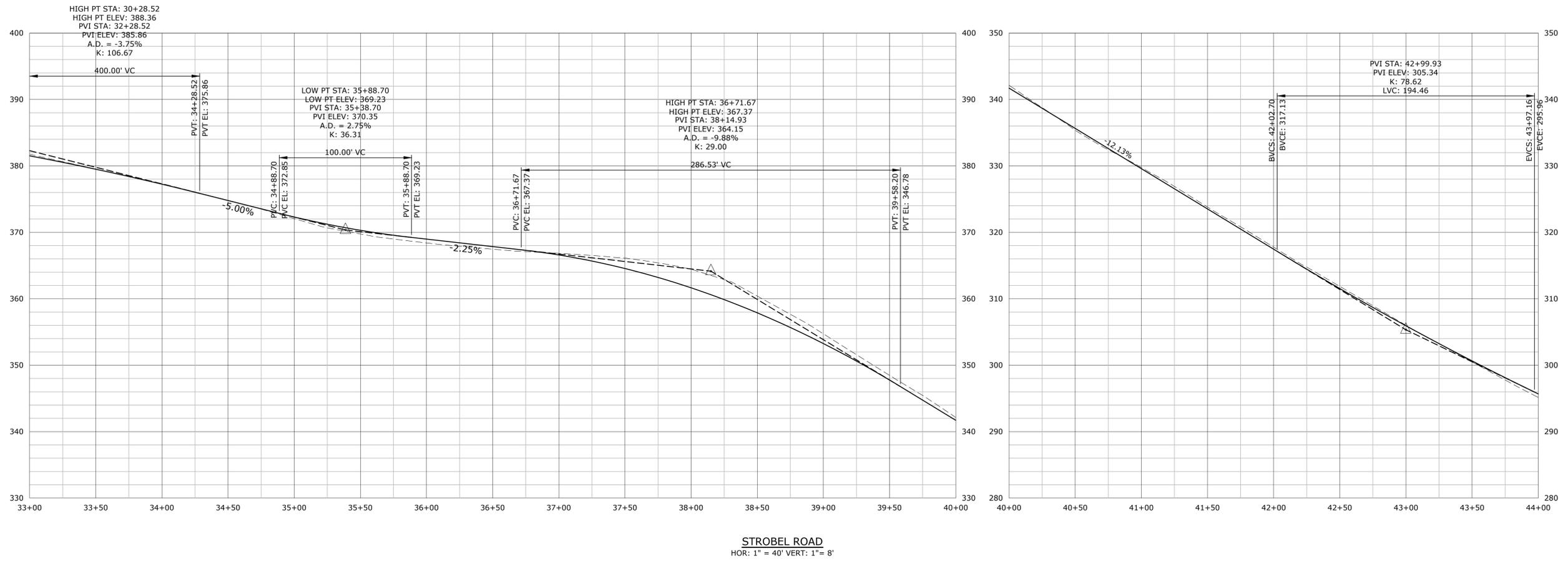
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 FILE: T0196-15-C-400-PROFILE.dwg
 DRAWN BY: APW
 CHECKED: AJM
 APPROVED: DCH

ROADWAY PROFILE
 STA: 22+25 TO 33+00

SCALE: HORZ: 1" = 40' VERT: 1" = 8'

C4.3

Plot Sheet: 6/29/2018 9:23:46am By: DuhaneC
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawing\Sheet\T0196-15-C-400-PROFILE.dwg



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
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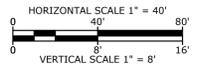
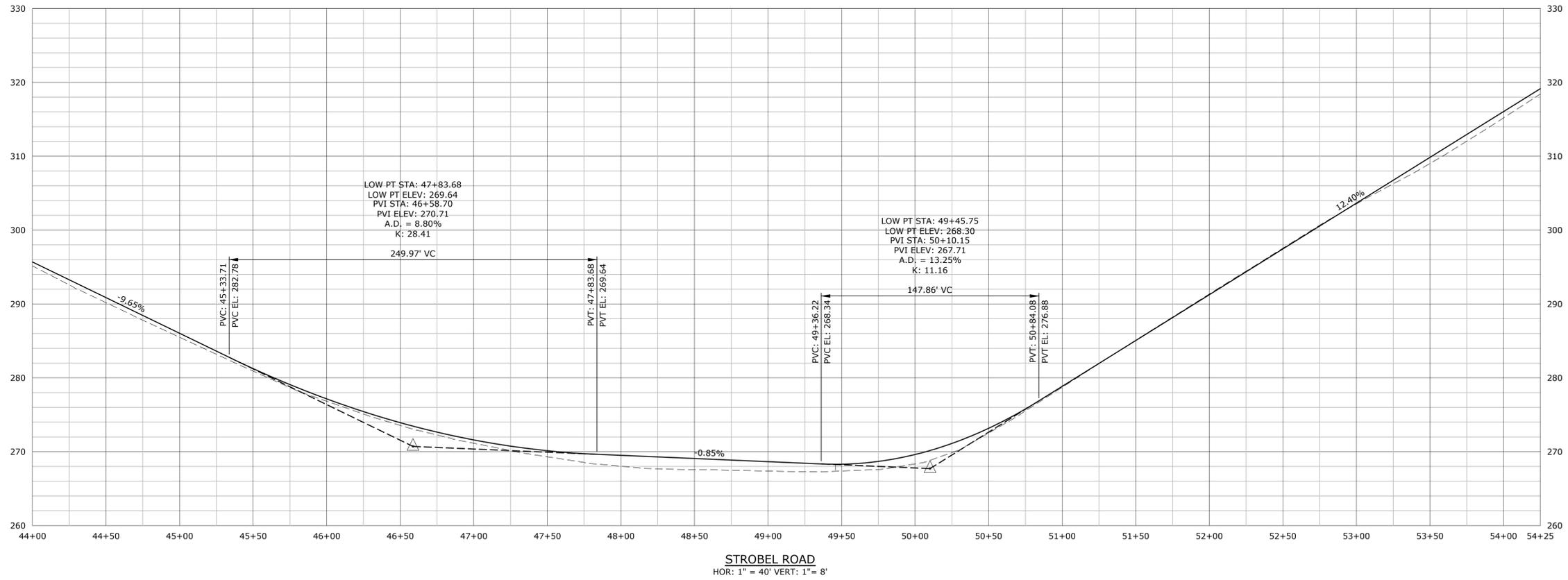
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-400-PROFILE.dwg
 DRAWN BY: APW
 CHECKED: AJM
 APPROVED: DCH

ROADWAY PROFILE
 STA 33+00 TO 44+00
 SCALE: HORZ: 1" = 40' VERT: 1" = 8'

C4.4

Last Saved: 6/26/2018 9:23am By: DubuqueC
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawing\Sheet\T0196-15-C-400-PROFILE.dwg



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON
 ORIGINAL DRAWING
 IF NOT ONE INCH ON
 THIS SHEET, ADJUST
 SCALES ACCORDINGLY

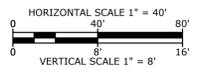
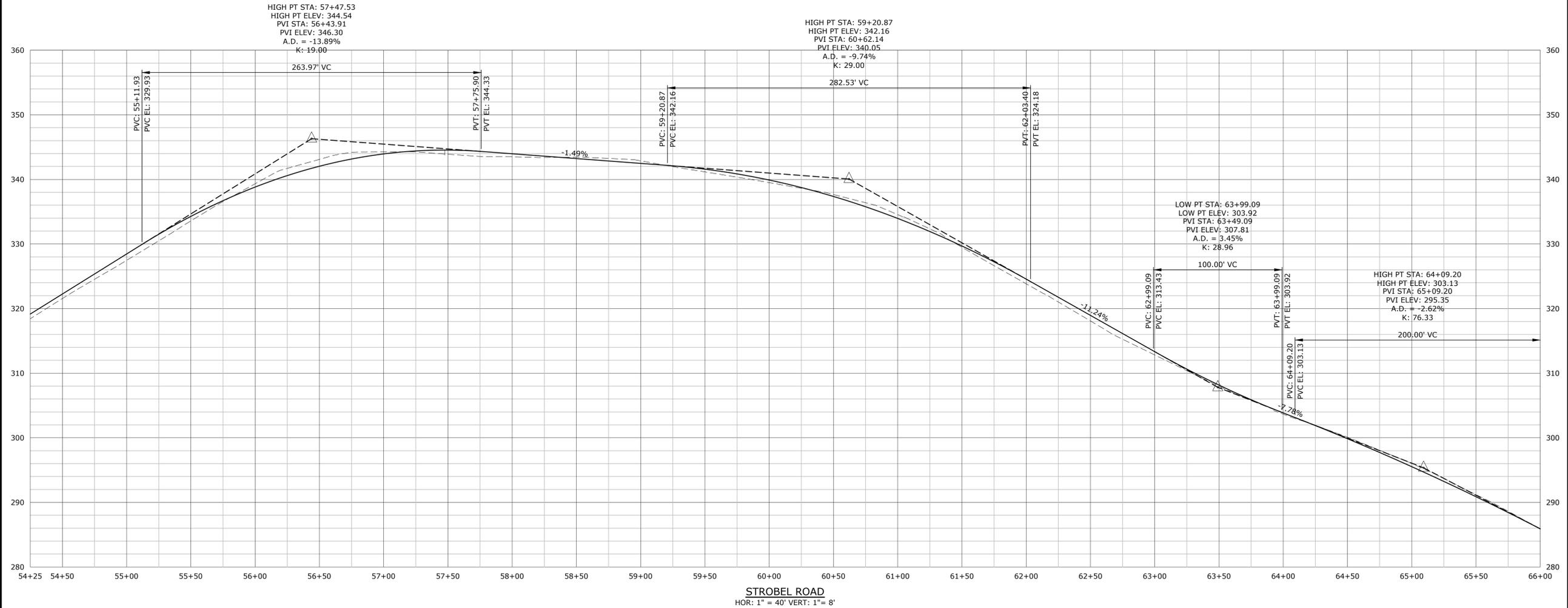
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PROJECT NO:	T0196
FILE:	T0196-15-C-400-PROFILE.dwg
DRAWN BY:	APW
CHECKED:	AJM
APPROVED:	DCH

**ROADWAY PROFILE
 STA 44+00 TO 54+25**

SCALE: HORZ: 1" = 40' VERT: 1" = 8'

C4.5



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

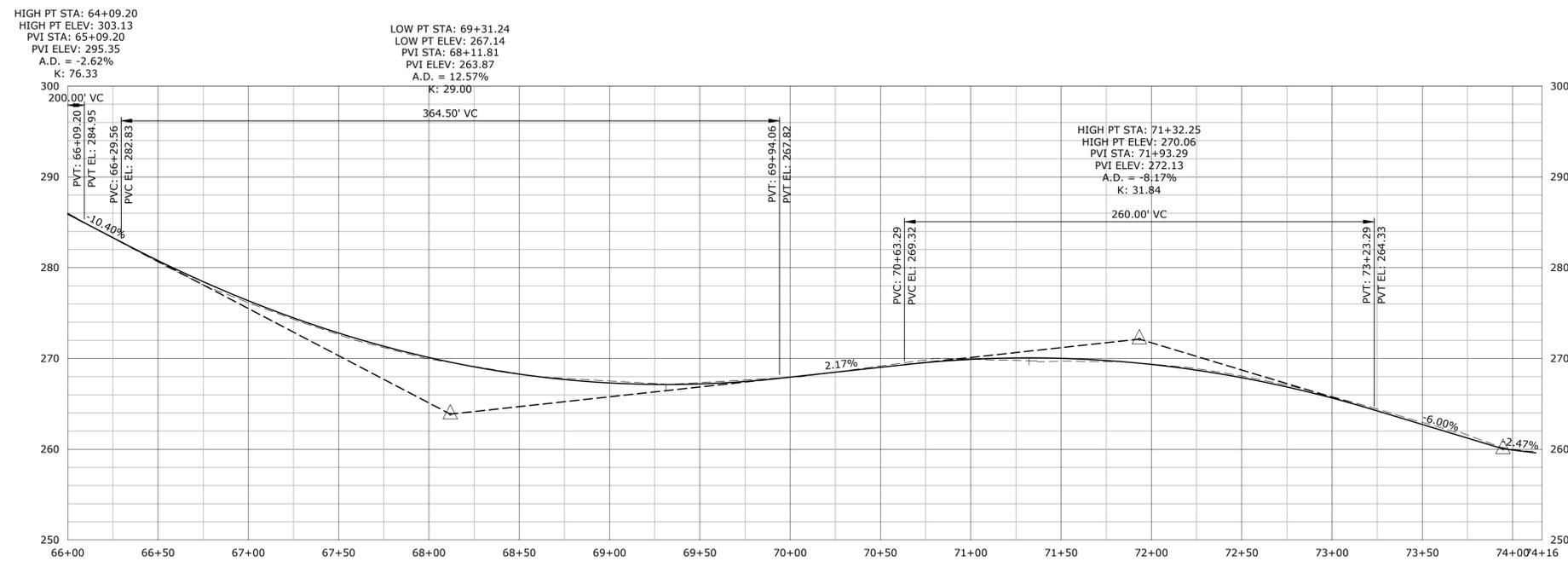
MARK	DATE	DESCRIPTION

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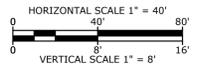
ROADWAY PROFILE
 STA 54+25 TO 66+00
 SCALE: HORZ: 1" = 40' VERT: 1" = 8'

C4.6

Plot Sheet: 6/26/2019 9:27:59am By: DubuqueC
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawings\Sheet\T0196-15-C-400-PROFILE.dwg



STROBEL ROAD
 HOR: 1" = 40' VERT: 1" = 8'



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

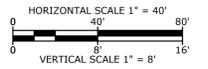
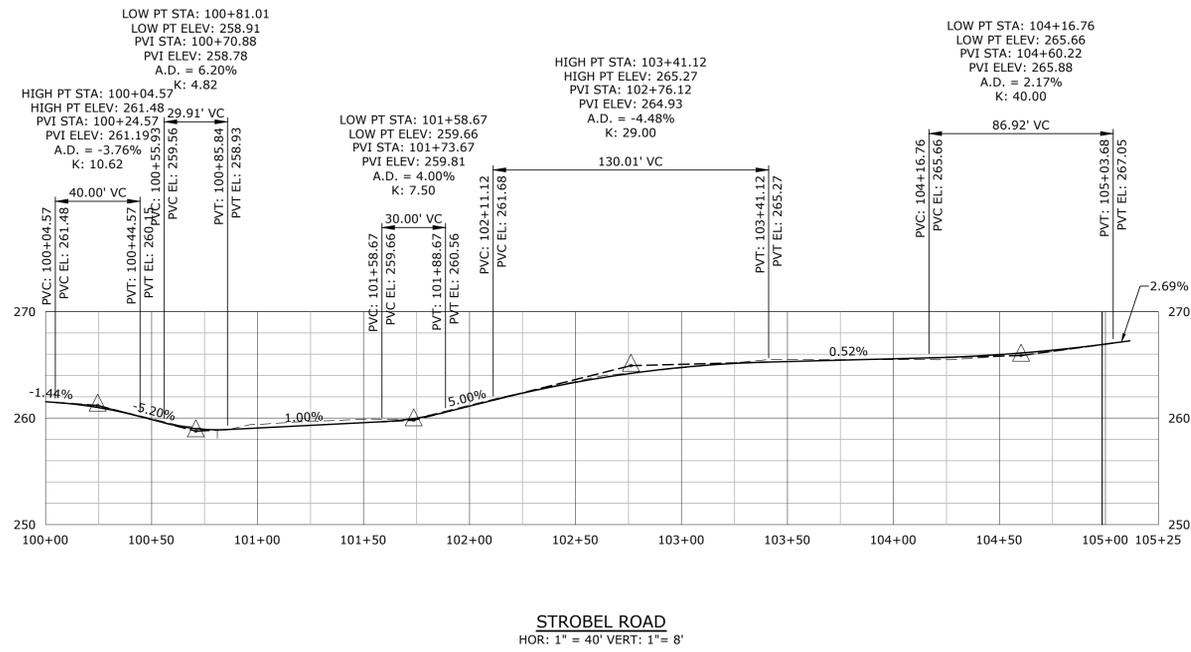
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FILE:	T0196-15-C-400-PROFILE.dwg
DRAWN BY:	APW
CHECKED:	AJM
APPROVED:	DCH

**ROADWAY PROFILE
 STA 66+00 TO 74+17**

SCALE: HORZ: 1" = 40' VERT: 1" = 8'

C4.7

Plot Sheet: 6/26/2018 9:27:59am By: DatabaseC Tighe & Bond: J:\T0196\15_Strobel Rd\Drawings\Sheet\T0196-15-C-400-PROFILE.dwg



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-400-PROFILE.dwg
DRAWN BY:	APW
CHECKED:	AJM
APPROVED:	DCH

**ROADWAY PROFILE
 STA 100+00 TO 105+11
 AND SIDE ROADS**

SCALE: HORZ: 1" = 40' VERT: 1" = 8'

C4.8

Plot Sheet: 6/29/2018 9:23am By: DuhaneC
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GENERAL

- G1 STRUCTURAL WORK SHALL CONFORM TO CONNECTICUT DEPARTMENT OF TRANSPORTATION LRFD BRIDGE MANUAL, LATEST EDITION, INCLUDING MOST RECENT ADDENDA, AND CONTRACT DOCUMENTS. IN CASE OF CONFLICT, MOST STRINGENT REQUIREMENT SHALL GOVERN.
- G2 CONTRACTOR SHALL VERIFY AND COORDINATE DIMENSIONS RELATED TO THIS PROJECT PRIOR TO STARTING CONSTRUCTION.
- G3 PREFORMED BITUMINOUS JOINT FILLER SHALL BE NON-EXTRUDING AND RESILIENT BITUMINOUS TYPE PREFORMED EXPANSION JOINT FILLER. IT SHALL MEET THE REQUIREMENTS OF CTDOT FORM 816.
- G4 CONSTRUCTION DETAILS AND MATERIALS SHALL CONFORM TO CT DOT FORM 817, 2016 AS REVISED BY THE SUPPLEMENTAL SPECIFICATIONS DATED JANUARY 2018.

REINFORCEMENT

- R1 DETAILING, FABRICATION, AND ERECTION OF REINFORCEMENT, UNLESS OTHERWISE NOTED, SHALL CONFORM TO ACI "BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE (ACI 318)" AND ACI "MANUAL OF STANDARD PRACTICE FOR DETAILING REINFORCED CONCRETE STRUCTURES (ACI 315)", LATEST EDITION.
- R2 STEEL REINFORCEMENT UNLESS OTHERWISE SHOWN SHALL CONFORM TO ASTM A615 GRADE 60 MINIMUM (YIELD STRENGTH - 60,000 PSI).
- R3 EPOXY COATED BAR REINFORCEMENT SHALL CONFORM TO THE REQUIREMENTS OF ASTM A615 GRADE 60 AND BE COATED TO THE REQUIREMENTS OF ASTM D3963.
- R4 PROVIDE AND SCHEDULE ON SHOP DRAWINGS, ALL NECESSARY ACCESSORIES TO HOLD REINFORCEMENT SECURELY IN POSITION: MINIMUM REQUIREMENTS SHALL BE: HIGH CHAIRS, 4'-0" ON CENTER, #5 SUPPORT BAR FOR HIGH CHAIRS, SLAB BOLSTERS, 3'-6" ON CENTER, ALL WIRE CHAIRS AND BOLSTERS TO BE PLASTIC TIPPED.
- R5 WHERE CONTINUOUS BARS ARE CALLED FOR THEY SHALL BE RUN CONTINUOUSLY AROUND CORNERS AND LAPPED AT NECESSARY SPLICES OR HOOKED AT DISCONTINUOUS ENDS.
- R6 WHERE REINFORCEMENT IS CALLED FOR IN SECTION, REINFORCEMENT IS CONSIDERED TYPICAL WHEREVER THE SECTION APPLIES.
- R7 REINFORCEMENT SHALL BE CONTINUOUS THROUGH ALL CONSTRUCTION JOINTS UNLESS OTHERWISE INDICATED ON THE DRAWINGS.
- R8 WELDED WIRE FABRIC SHALL LAP 12" OR TWO SPACES, WHICHEVER IS LARGER, AND SHALL BE WIRED TOGETHER.
- R9 REINFORCEMENT COUPLER SPLICES SHALL BE MECHANICAL DEVICES CAPABLE OF TRANSMITTING THE ULTIMATE TENSILE AND COMPRESSIVE STRENGTH OF THE BAR.
INSTALLATION OF REINFORCEMENT SHALL BE COMPLETED AT LEAST 24 HOURS PRIOR TO SCHEDULED CONCRETE PLACEMENT. NOTIFY ENGINEER OF COMPLETION AT LEAST 24 HOURS PRIOR TO SCHEDULED COMPLETION OF PLACEMENT OF REINFORCEMENT.
- R11 REINFORCEMENT SHALL BE SET BEFORE PLACING CONCRETE. SETTING ANY REINFORCEMENT INTO WET CONCRETE IS PROHIBITED.

CONCRETE

- C1 CONCRETE WORK SHALL CONFORM TO THE LATEST EDITIONS OF THE BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE (ACI 318), AND SPECIFICATIONS FOR STRUCTURAL CONCRETE FOR BUILDING (ACI 301).
- C2 CONCRETE SHALL BE CONTROLLED CONCRETE, PROPORTIONED, MIXED AND PLACED UNDER THE SUPERVISION OF AN APPROVED CONCRETE TESTING AGENCY OR THE ENGINEER.
- C3 CAST-IN-PLACE FOOTINGS AND STEMS SHALL BE CT DOT STANDARD "CLASS A" CONCRETE. TOP SLAB SHALL BE CT DOT STANDARD "CLASS F" CONCRETE.
- C4 THE USE OF CONSTRUCTION JOINTS WHERE SHOWN ON THE DRAWINGS IS MANDATORY. OMISSIONS, ADDITIONS OR CHANGES SHALL NOT BE MADE EXCEPT WITH THE SUBMISSION OF A WRITTEN REQUEST TOGETHER WITH DRAWINGS OF THE PROPOSED JOINT LOCATIONS FOR APPROVAL OF THE STRUCTURAL ENGINEER.
- C5 WHERE CONSTRUCTION JOINTS ARE NOT SHOWN, DRAWINGS SHOWING LOCATION OF CONSTRUCTION JOINTS AND CONCRETE PLACING SEQUENCE SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO PREPARATION OF THE REINFORCEMENT SHOP DRAWINGS.
- C5 CONCRETE SLABS SHALL BE CAST SO THAT THE SLAB THICKNESS IS AT NO POINT LESS THAN THAT INDICATED ON THE DRAWINGS.
- C7 CONCRETE SHALL BE PLACED WITHOUT HORIZONTAL CONSTRUCTION JOINTS EXCEPT WHERE SHOWN OR NOTED.
- C8 EXPOSED EDGES OF CONCRETE ELEMENTS SHALL HAVE 1"x1" CHAMFERED CORNERS.

FOUNDATIONS

- F1 NO CONCRETE SHALL BE PLACED IN WATER OR ON FROZEN GROUND.
- F2 BOTTOM OF FOUNDATION ELEVATIONS GIVEN ON DRAWINGS ARE TO BE CONSIDERED MINIMUM DEPTHS. CONTRACTOR SHALL HAVE FURTHER EXCAVATION AS REQUIRED TO REACH GOOD BEARING.
- F3 ALL FINISHED EXCAVATIONS SHALL BE INSPECTED BY THE ENGINEER BEFORE ANY CONCRETE IS PLACED.
- F4 ALL BACKFILL SHALL BE COMPACTED IN MAXIMUM 12" LIFTS. SEE SPECIFICATIONS.
- F5 BACKFILL WITH CT DOT SPECIFIED PERVIOUS STRUCTURE BACKFILL.
- F6 GROUP 2 NON-WOVEN GEOTEXTILE SHALL BE AMOCO 4506, CARTHAGE MILLS FX-60HS, MIRAFT 160N, OR APPROVED EQUAL.
- F7 EXCAVATED SOIL SUBGRADES SHOULD BE PROOF COMPACTED WITH EITHER 10 PASSES OF A 10-TON VIBRATORY DRUM ROLLER FOR OPEN EXCAVATIONS OR 6 PASSES OF A LARGE, REVERSIBLE, WALK BEHIND VIBRATORY COMPACTOR CAPABLE OF EXERTING A MINIMUM FORCE OF 2,000 LBS IN TRENCH OR FIT EXCAVATIONS.
- F8 OVER-EXCAVATE ANY SOFT OR WEAK SPOTS AND REPLACE WITH COMPACTED GRANULAR FILL OR COMPACTED CRUSHED STONE WRAPPED IN A NON-WOVEN GEOTEXTILE.

BAR SIZE DESIGNATION		DEVELOPMENT LENGTH (INCHES)	SPLICE LENGTH (INCHES)	
ENGLISH	METRIC	Ld	CLASS B	CLASS B TOP BARS
#3	#10	15	19	25
#4	#13	19	25	33
#5	#16	24	31	40
#6	#19	29	37	48
#7	#22	42	54	70
#8	#25	48	62	81
#9	#29	54	70	91
#10	#32	61	79	103

REBAR SPLICE LENGTH SCHEDULE

NOTES:

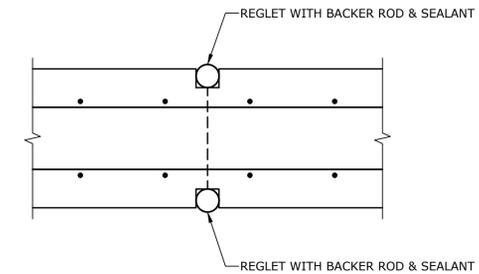
- IF CLEAR SPACING BETWEEN THE REBARS IS LESS THAN THREE BAR DIAMETERS, OR IF COVER IS LESS THAN TWO BAR DIAMETERS, INCREASE THE SPLICE LENGTH BY AN ADDITIONAL 50%.
- IF EPOXY COATED REBAR IS USED, INCREASE THE SPLICE LENGTH BY AN ADDITIONAL 50%.
- IF LIGHTWEIGHT CONCRETE IS USED, INCREASE THE SPLICE LENGTH BY AN ADDITIONAL 30%.
- THE MINIMUM REBAR SPLICE LENGTH SCHEDULE IS BASED ON F'c= 4,000 PSI AND Fy= 60,000 PSI. ADJUST FOR OTHER STRENGTHS USING ACI-318.
- FOR HORIZONTAL REINFORCEMENT SO PLACED THAT MORE THAN 12 INCHES OF FRESH CONCRETE IS CAST IN THE MEMBER BELOW, INCREASE THE DEVELOPMENT LENGTH BY AN ADDITIONAL 30%.
- WHEN BARS OF DIFFERENT SIZE ARE LAP SPLICED, THE SPLICE LENGTH SHALL BE THE LARGER OF EITHER THE DEVELOPMENT LENGTH OF THE LARGER BAR OR THE SPLICE LENGTH OF THE SMALLER BAR.

QUANTITIES

ITEM	UNIT	QUANTITY
EXCAVATION AND REUSE OF EXISTING CHANNEL BOTTOM MATERIAL	CY	75
STRUCTURE EXCAVATION-EARTH (COMPLETE)	CY	600
HANDLING WATER	LS	LS
PERVIOUS STRUCTURE BACKFILL	CY	250
REMOVAL OF SUBSTRUCTURE	LS	LS
CLASS "A" CONCRETE	CY	150
PRECAST CONCRETE BOX STRUCTURE	LF	120
DRILLING AND GROUTING REINFORCING BARS	LF	200
MODIFIED RIPRAP	CY	50
MEMBRANE WATERPROOFING (COLD LIQUID ELASTOMERIC)	SY	50
DAMPPOOFING	SY	50
TEMPORARY EARTH RETAINING SYSTEM	SF	700
TEMPORARY SUPPORT OF UTILITIES	LS	LS

NOTES:

- FOR REBAR DETAILS, TYPICAL WALL SECTIONS, REFER TO SHEET C5.5
- FOR WALL TO SLAB CONNECTION DETAILS, REFER TO SHEET C5.5
- ALL REINFORCING STEEL IN SLABS TO BE #5 BARS @ 12" O.C. U.N.O.
- JOINTS SHOWN IN PLAN VIEW ARE FOR SLABS ONLY.
- VERTICAL CONSTRUCTION JOINTS FOR WALLS ONLY.
- FOR SUPPORT WALL REINFORCING SEE SHEET C5.5



NOTE: CONCRETE IS PLACED MONOLITHICALLY ON BOTH SIDES OF ALL CONSTRUCTION JOINTS.

CONCRETE SLAB CONSTRUCTION JOINT
NO SCALE

HYDRAULIC DATA TABLE

	SQUARE MILES	
DRAINAGE AREA	1.33	
DESIGN FREQUENCY	YEAR	100
DESIGN DISCHARGE	CFS	785
AVERAGE DAILY FLOW ELEVATION	FT ESTIMATED	260.0
UPSTREAM DESIGN WATER SURFACE ELEVATION	FT	267.26
DOWNSTREAM DESIGN WATER SURFACE ELEVATION	FT	262.47
TEMPORARY DESIGN FREQUENCY	YEAR	2
TEMPORARY DESIGN FLOW	CFS	219
TEMP UPSTREAM DESIGN WATER SURFACE ELEV.	FT	263.1
TEMP DOWNSTREAM DESIGN WATER SURFACE ELEV.	FT	263.0

Strobel Road

Culvert Rehabilitation Project

Trumbull, Connecticut

March, 2016

VERIFY SCALE
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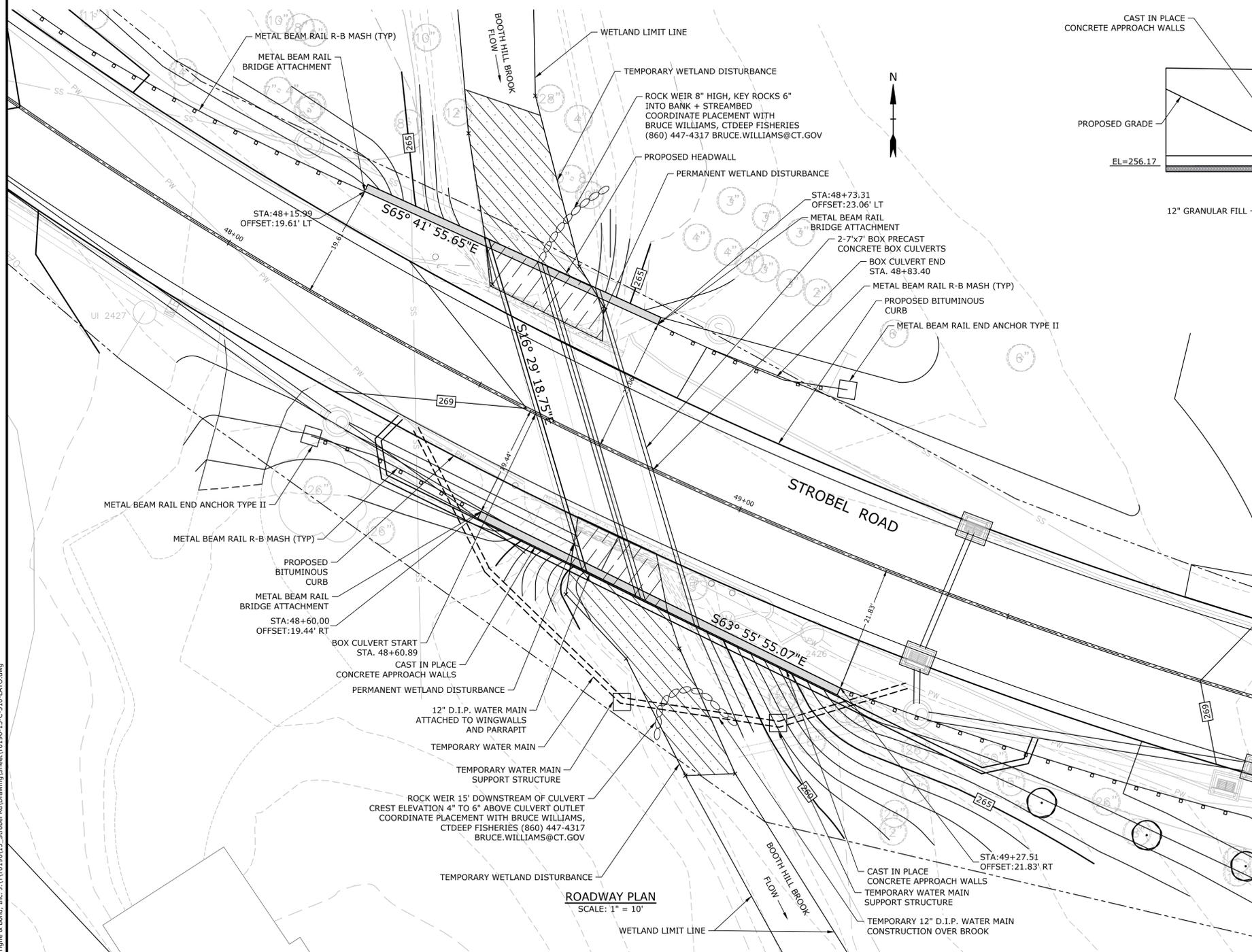
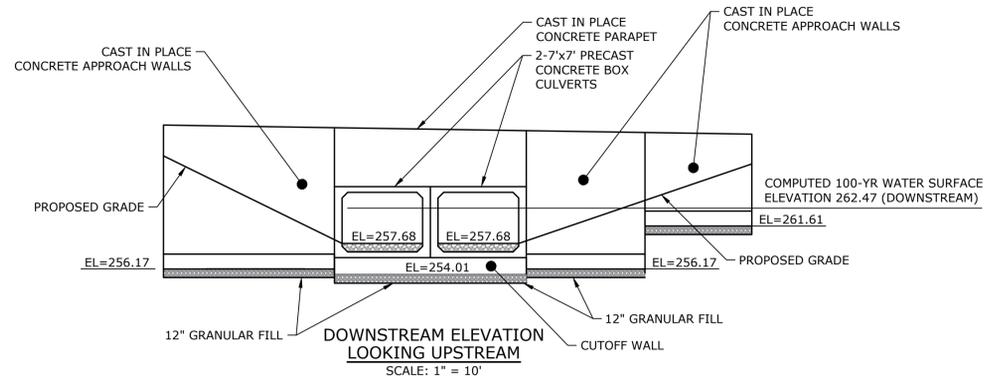
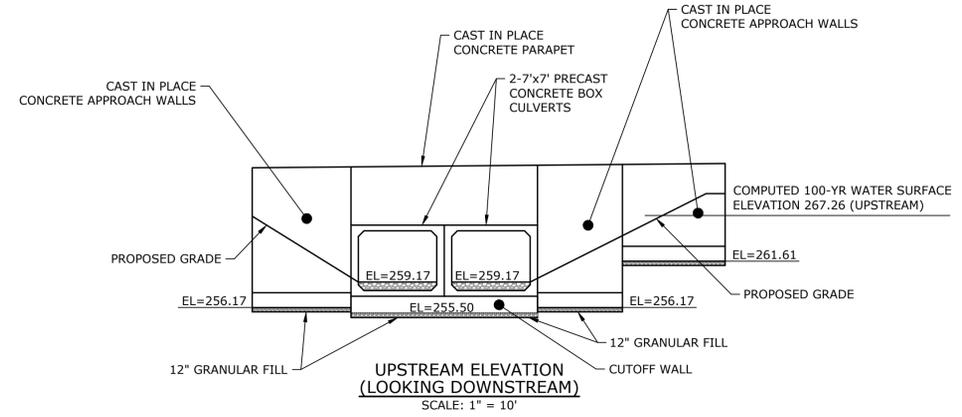
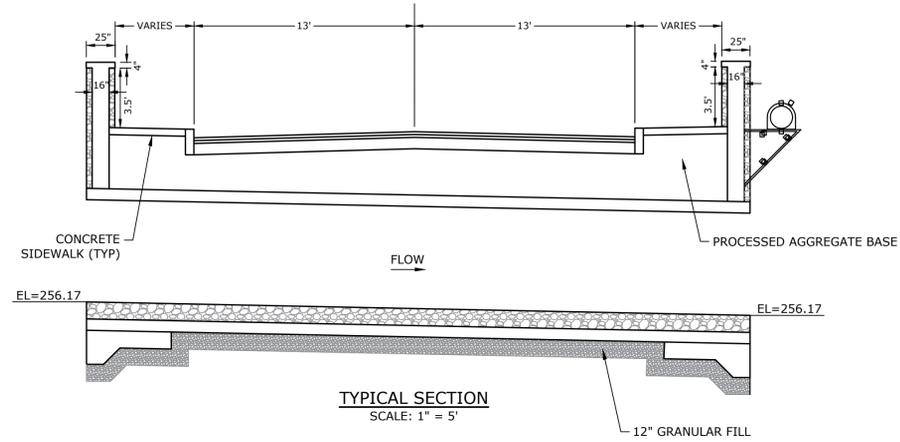
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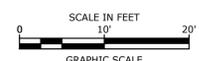
STRUCTURAL NOTES AND DETAILS

SCALE: AS NOTED

C5.1



- TEMPORARY WETLAND DISTURBANCE AREA = 743 sf
- PERMANENT WETLAND DISTURBANCE AREA = 298 sf



Strobel Road

Culvert Rehabilitation Project

Trumbull, Connecticut

March, 2016

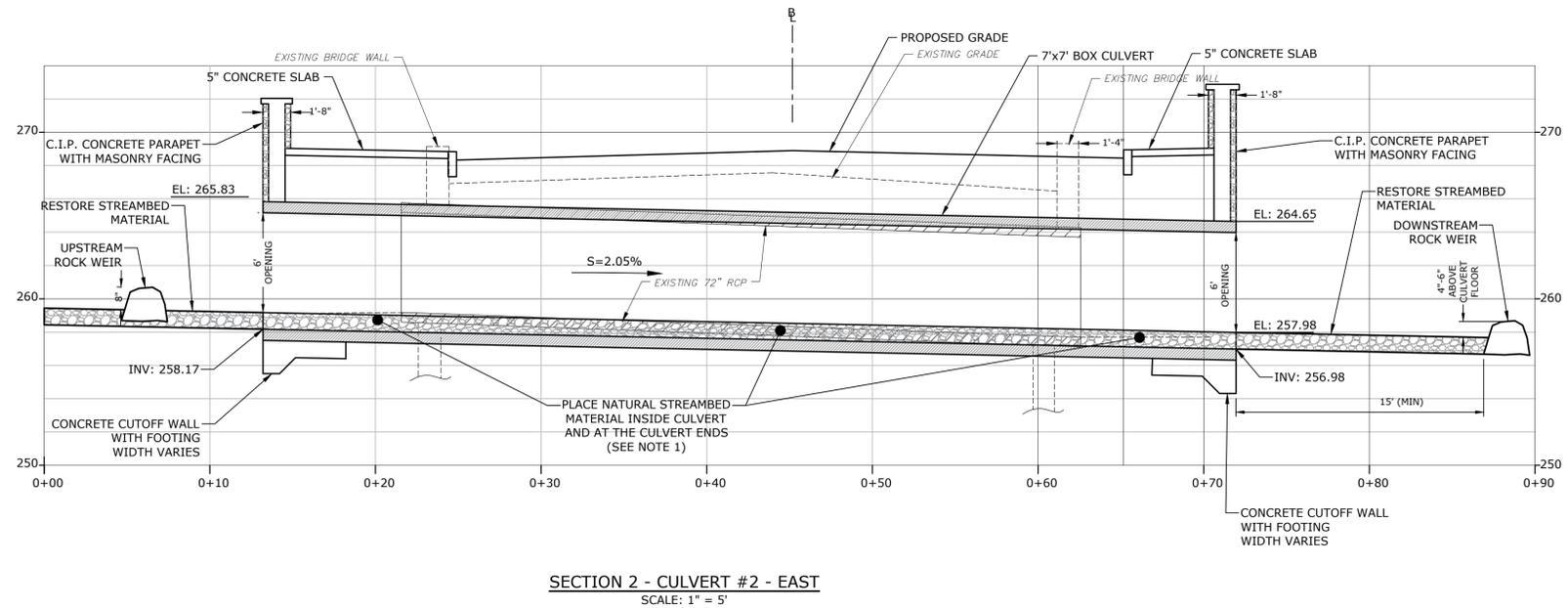
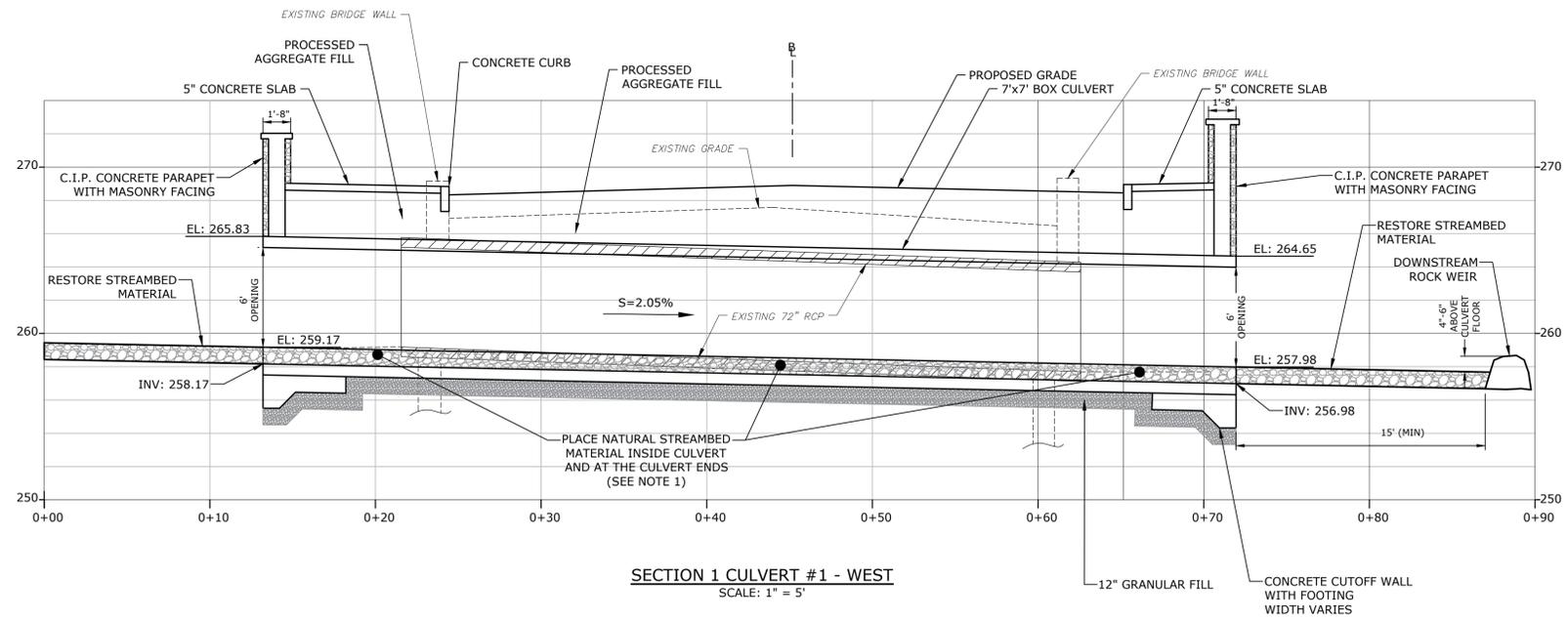
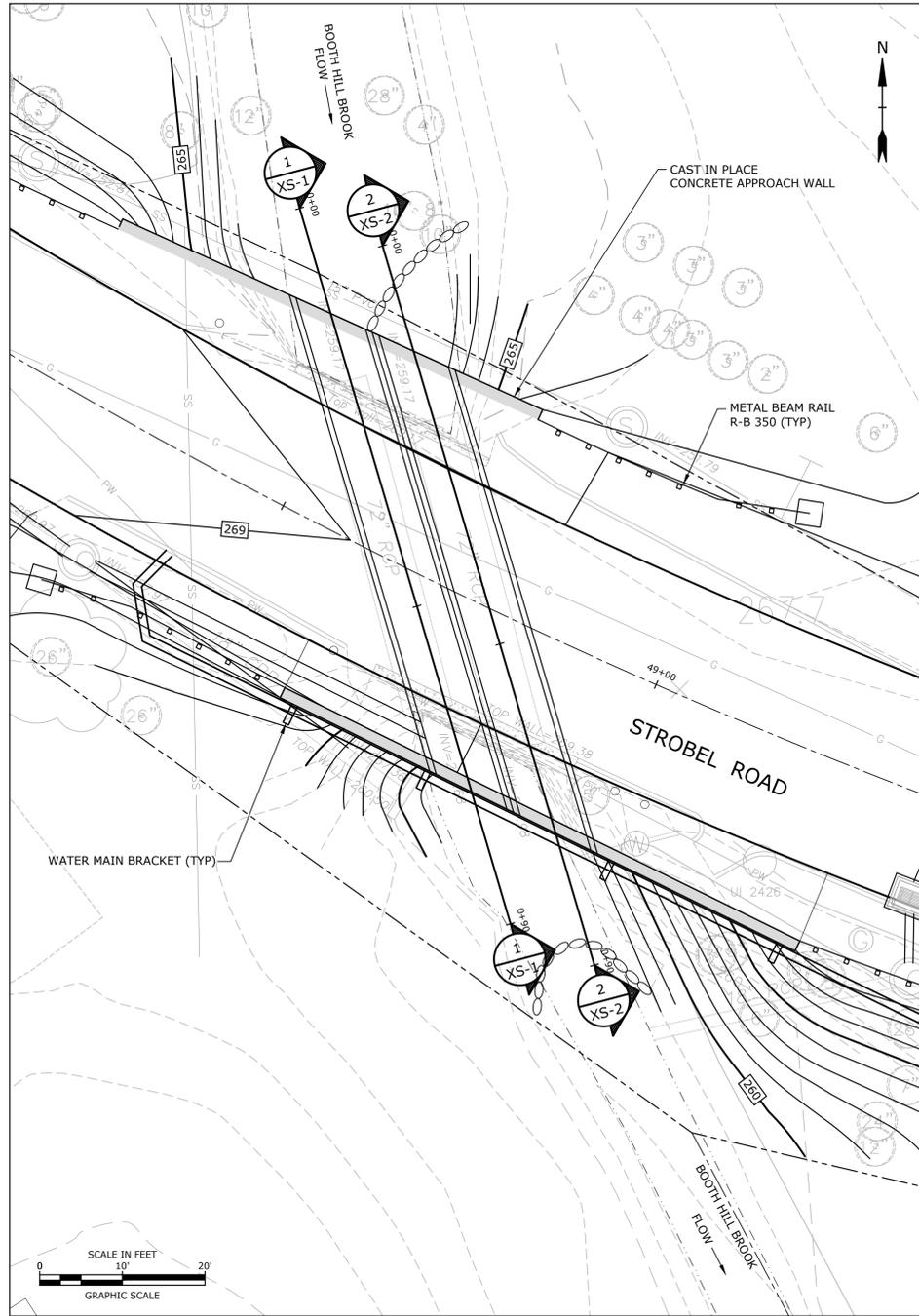
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MARK	DATE	DESCRIPTION

CULVERT LAYOUT AND ELEVATION PLAN

SCALE: AS NOTED

C5.2



NOTES

- NATURAL STREAMBED SUBSTRATE SHALL BE A HETEROGENEOUS MIXTURE OF GRAVEL, COBBLES AND FINES WITH 40% OF MATERIAL BY WEIGHT TO BE 8"-12" COBBLES. REMAINING 60% OF MATERIAL SHALL BE SMALL COBBLES AND GRAVEL. REFER TO SPECIAL PROVISIONS.
- PROPER SEDIMENT AND EROSION CONTROLS ARE CRITICAL TO PROTECT FISH HABITAT. PROVIDE ALL SEDIMENT AND EROSION CONTROLS SHOWN ON THESE PLANS AND SUPPLEMENTAL CONTROLS AS ORDERED OR REQUIRED TO NOT INCREASE TURBIDITY LEVELS. STABILIZE DISTURBED AREAS AS SOON AS PRACTICABLE.
- UNCONFIRMED IN-STREAM WORK IS ALLOWED ONLY BETWEEN JUNE 1 AND SEPTEMBER 30.

Strobel Road

Culvert Rehabilitation Project

Trumbull, Connecticut

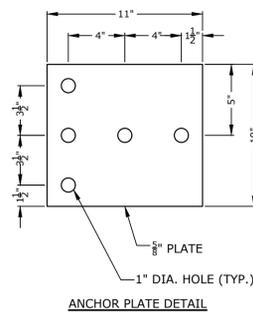
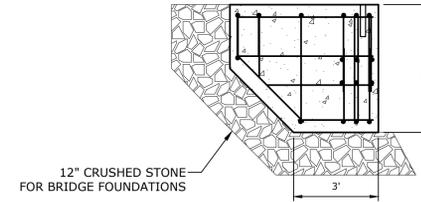
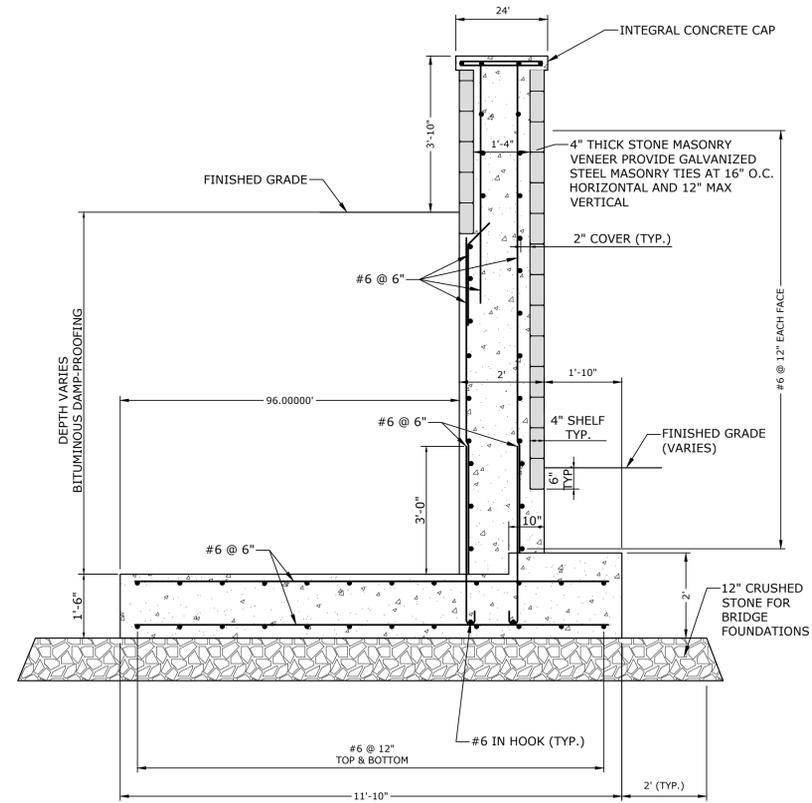
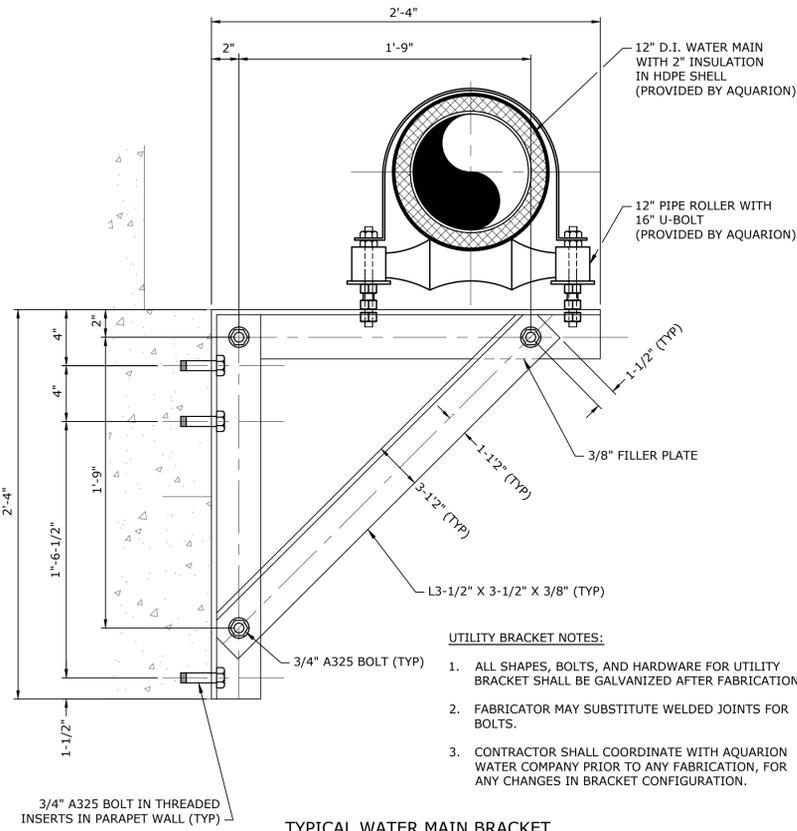
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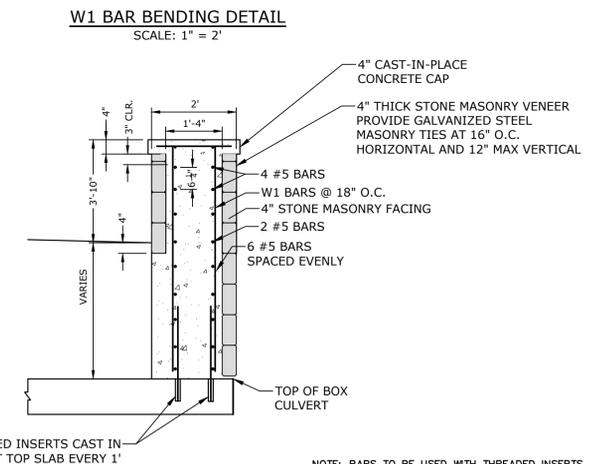
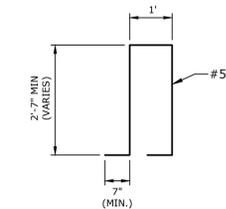
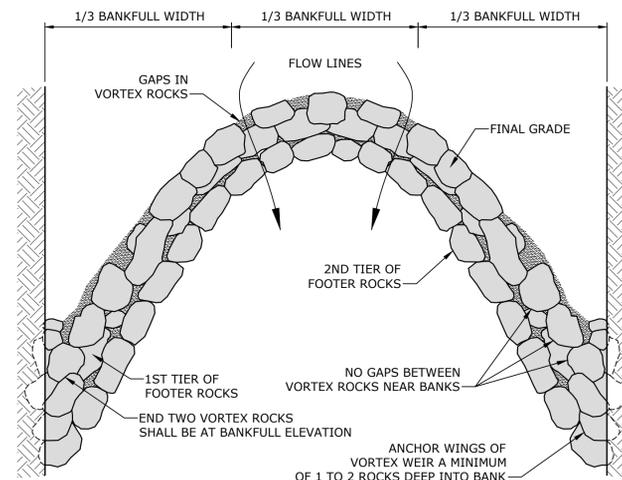
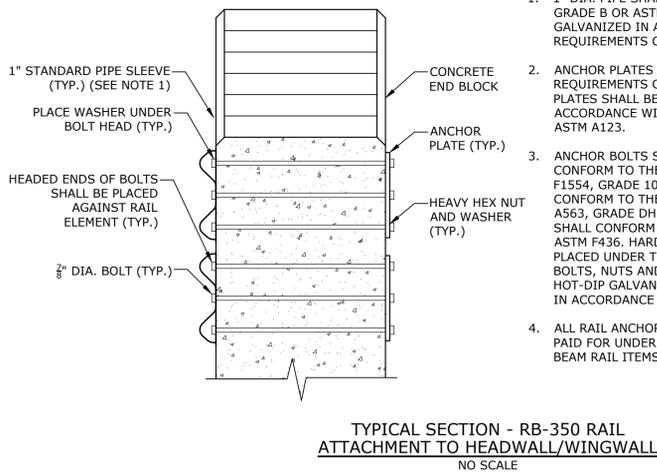
CULVERT SECTIONS
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C5.3



NOTES:

1. 1" DIA. PIPE SHALL CONFORM TO ASTM A53 GRADE B OR ASTM A501 AND SHALL BE GALVANIZED IN ACCORDANCE WITH THE REQUIREMENTS OF ASTM A123.
2. ANCHOR PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A36. THE STEEL PLATES SHALL BE HOT-DIP GALVANIZED IN ACCORDANCE WITH THE REQUIREMENTS OF ASTM A123.
3. ANCHOR BOLTS SHALL BE HEX-HEADED AND CONFORM TO THE REQUIREMENTS OF ASTM F1554, GRADE 105. THE NUTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A563, GRADE DH. THE HARDENED WASHERS SHALL CONFORM TO THE REQUIREMENTS OF ASTM F436. HARDENED WASHERS SHALL BE PLACED UNDER THE TURNED ELEMENTS. THE BOLTS, NUTS AND WASHERS SHALL BE HOT-DIP GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH ASTM A153.
4. ALL RAIL ANCHORAGE MATERIAL WILL BE PAID FOR UNDER THE APPLICABLE METAL BEAM RAIL ITEMS.



NOTE: BARS TO BE USED WITH THREADED INSERTS CAST INTO THE TOP OF THE PRECAST CONCRETE BOX. THREADED INSERTS SHALL BE PAID FOR UNDER THE ITEM "PRECAST CONCRETE BOX CULVERT".

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

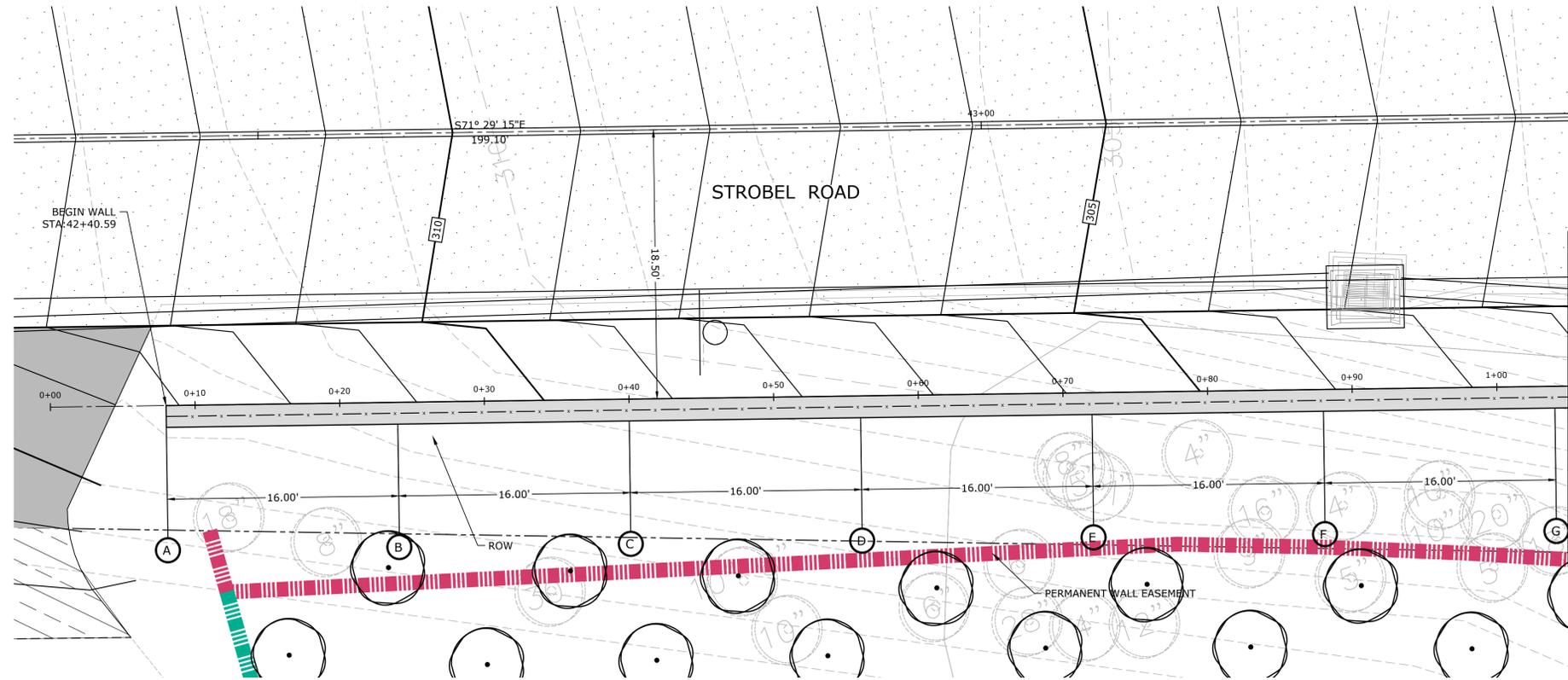
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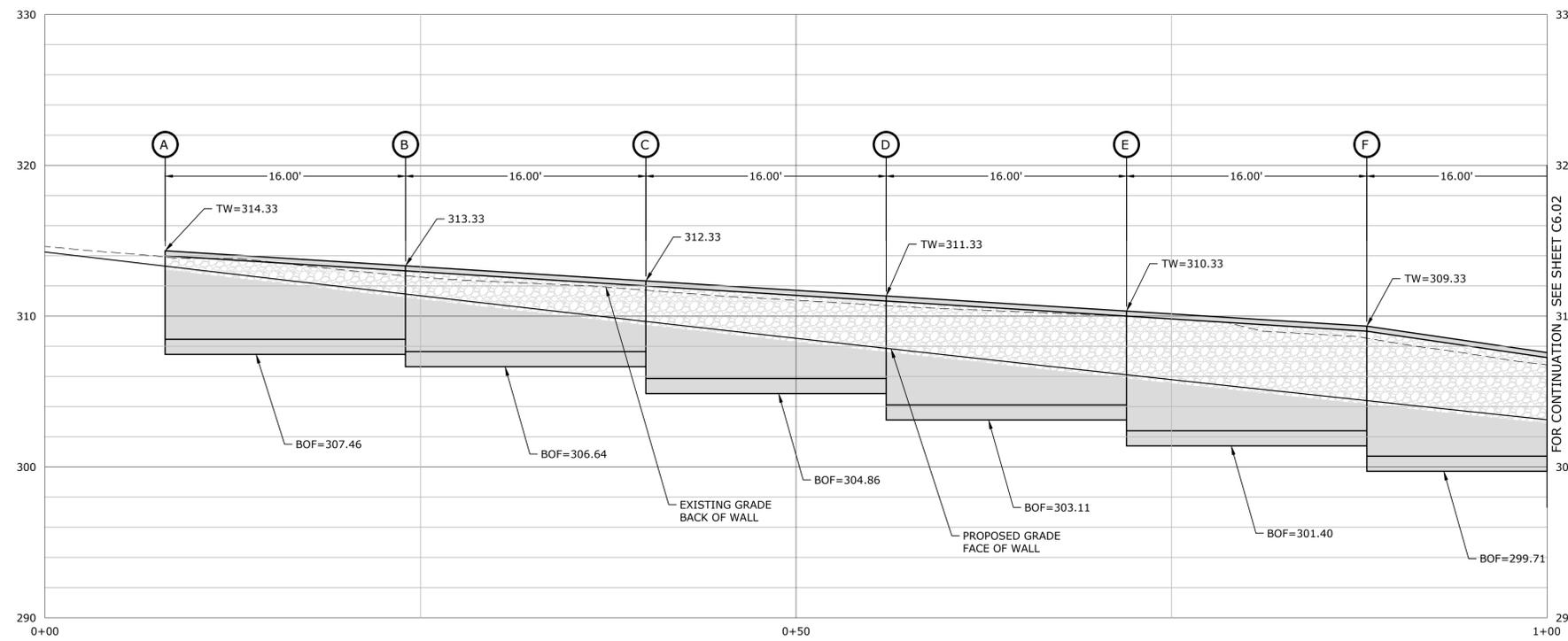
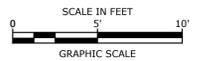
DETAILS

SCALE: AS NOTED

C5.5



FOR CONTINUATION SEE SHEET C6.02



FOR CONTINUATION SEE SHEET C6.02

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

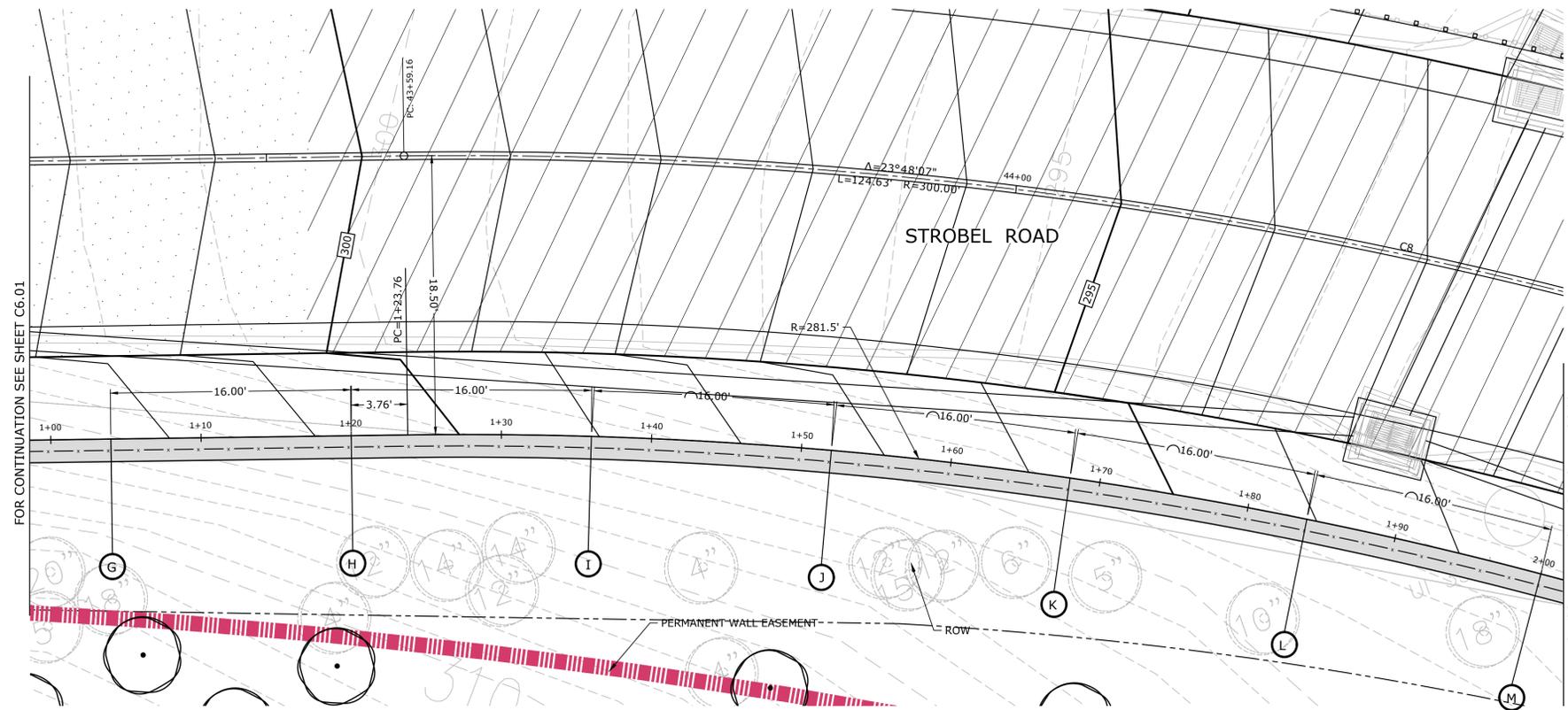
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APPROVED:	DCH	

RETAINING WALL PLAN WALL #1

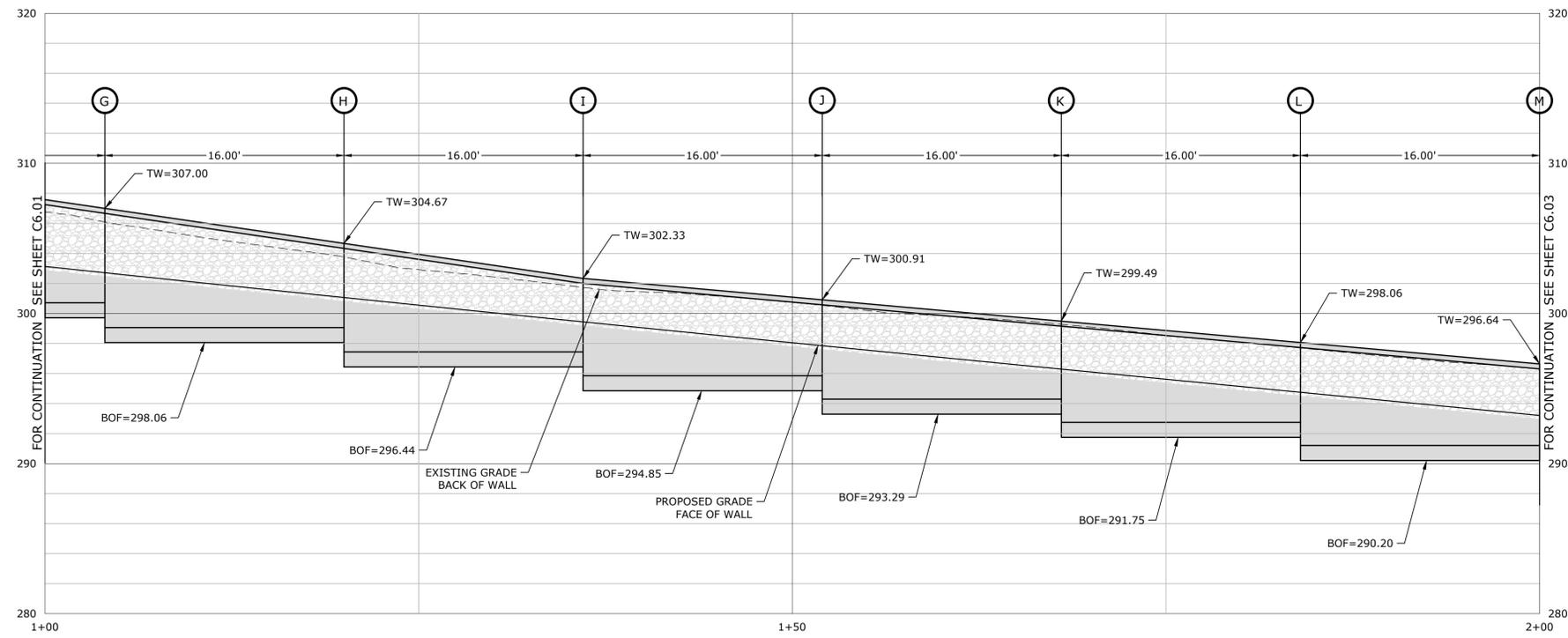
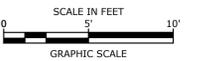
SCALE: 1" = 5'

C6.0



FOR CONTINUATION SEE SHEET C6.01

FOR CONTINUATION SEE SHEET C6.03



FOR CONTINUATION SEE SHEET C6.01

FOR CONTINUATION SEE SHEET C6.03

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
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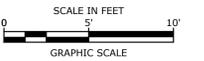
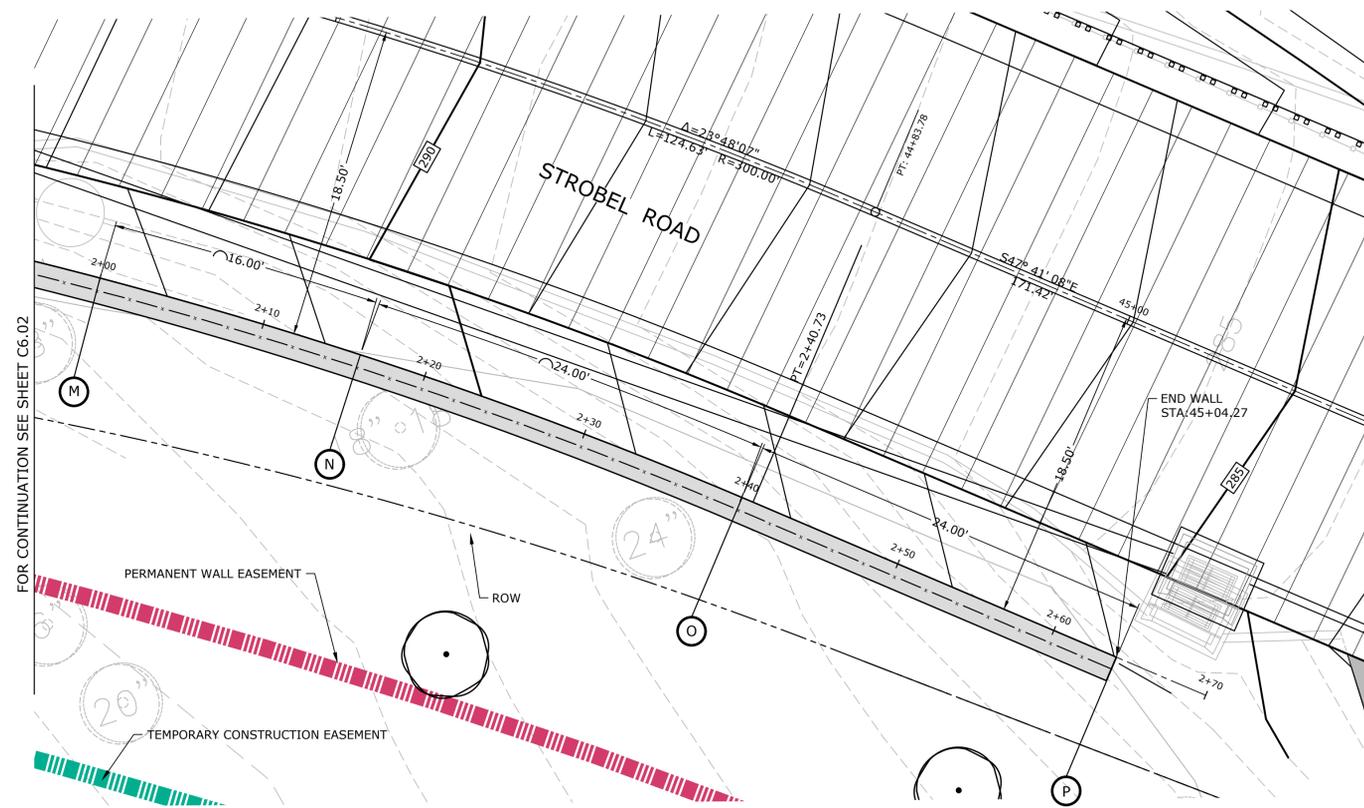
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RETAINING WALL PLAN WALL #1

SCALE: 1" = 5'

C6.1



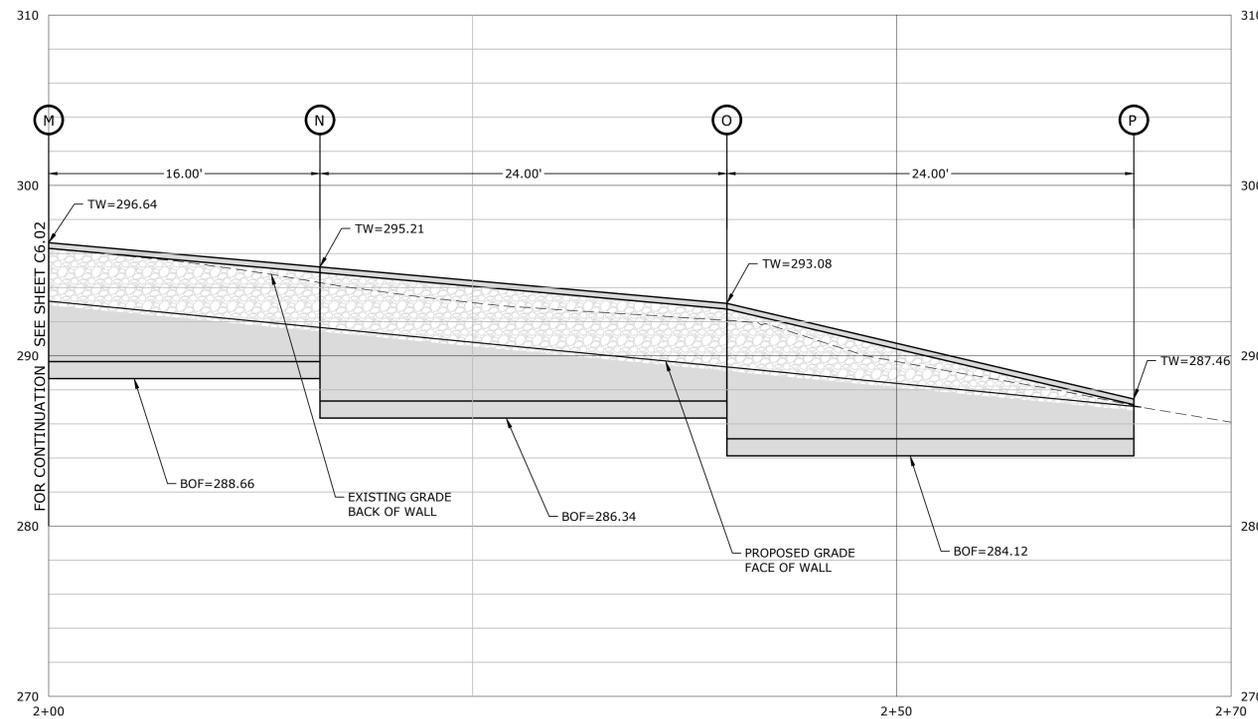
STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

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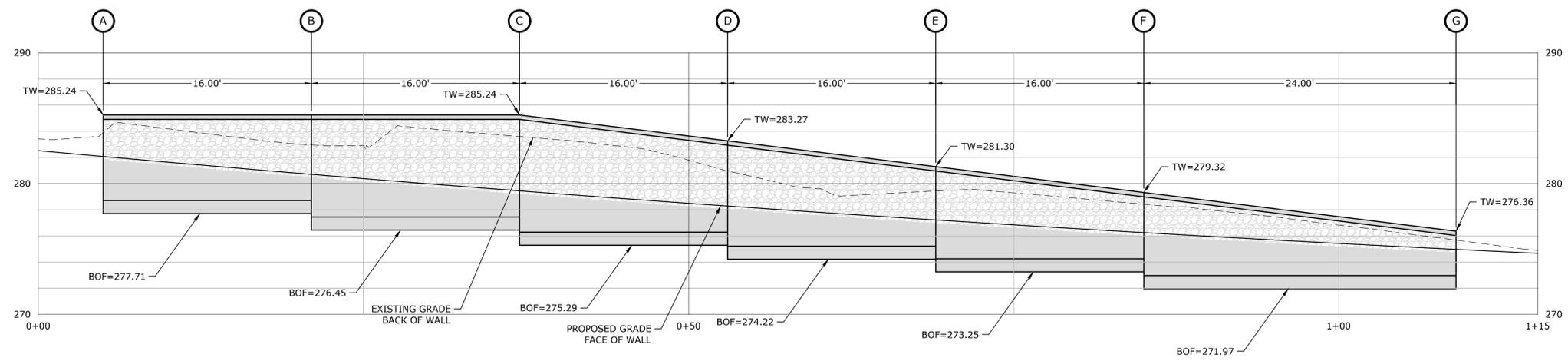
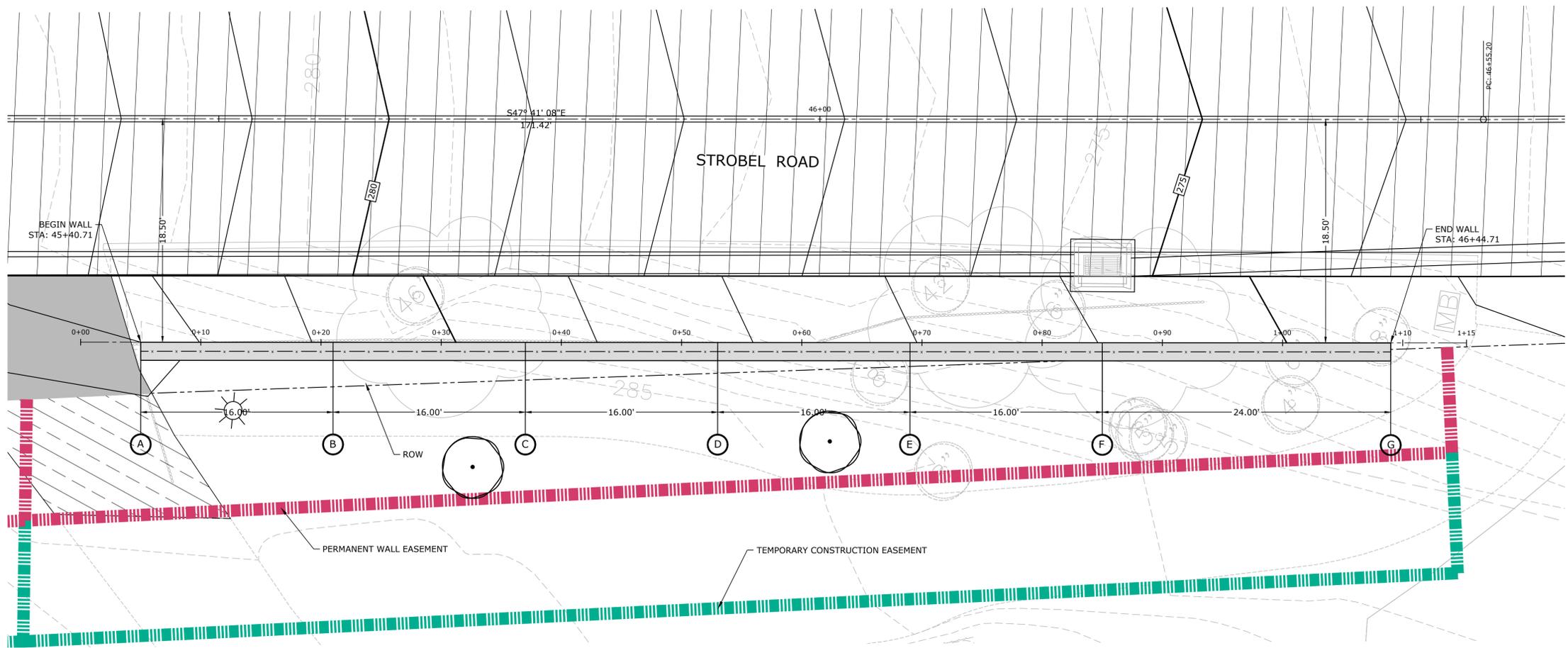
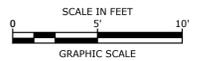


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CHECKED BY:	AJM	
APPROVED BY:	DCH	

RETAINING WALL PLAN WALL #1

SCALE: 1" = 5'

C6.2



WALL #2

**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

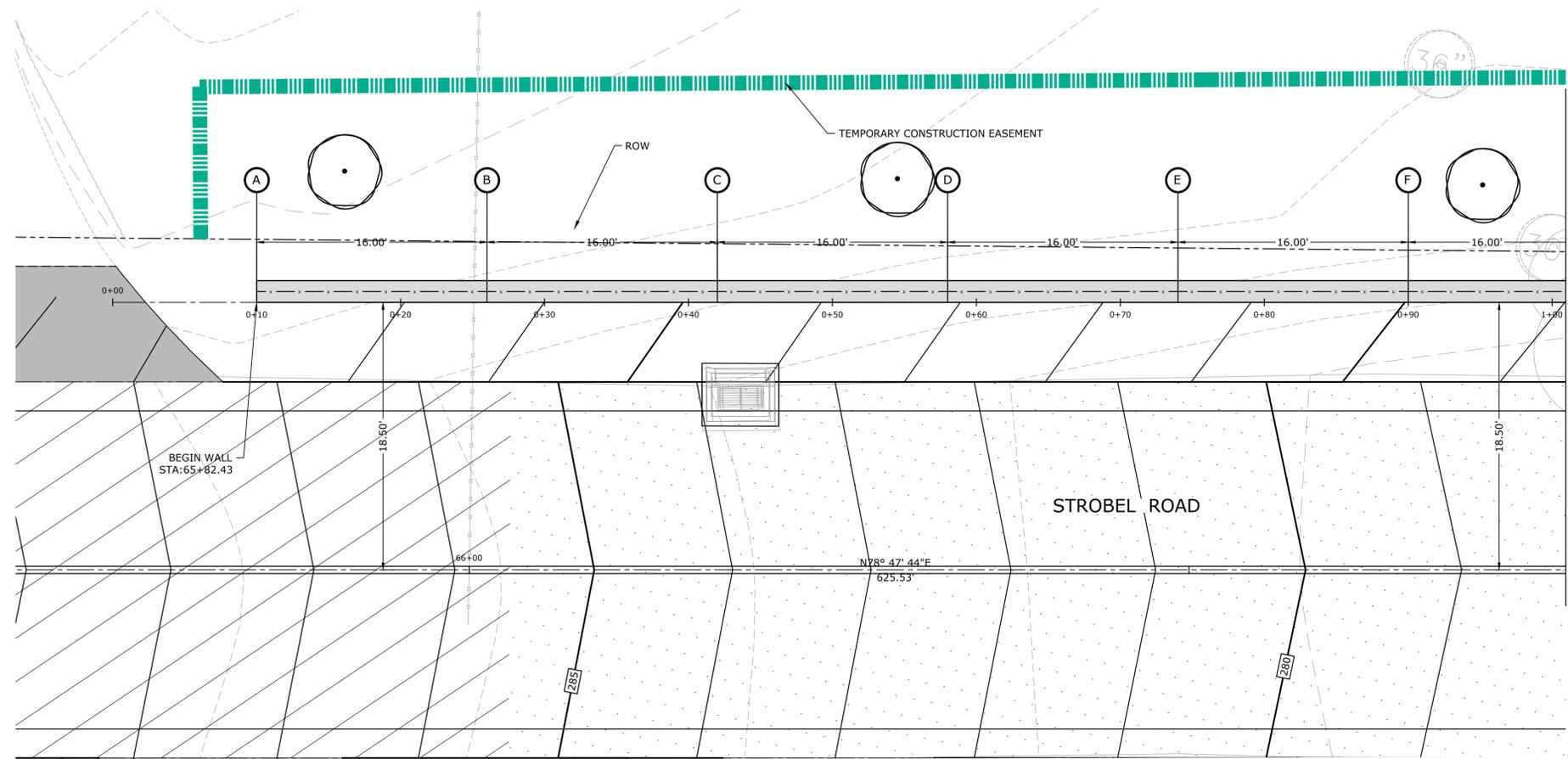
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APPROVED:	DCH	

**RETAINING WALL PLAN
 WALL #2**

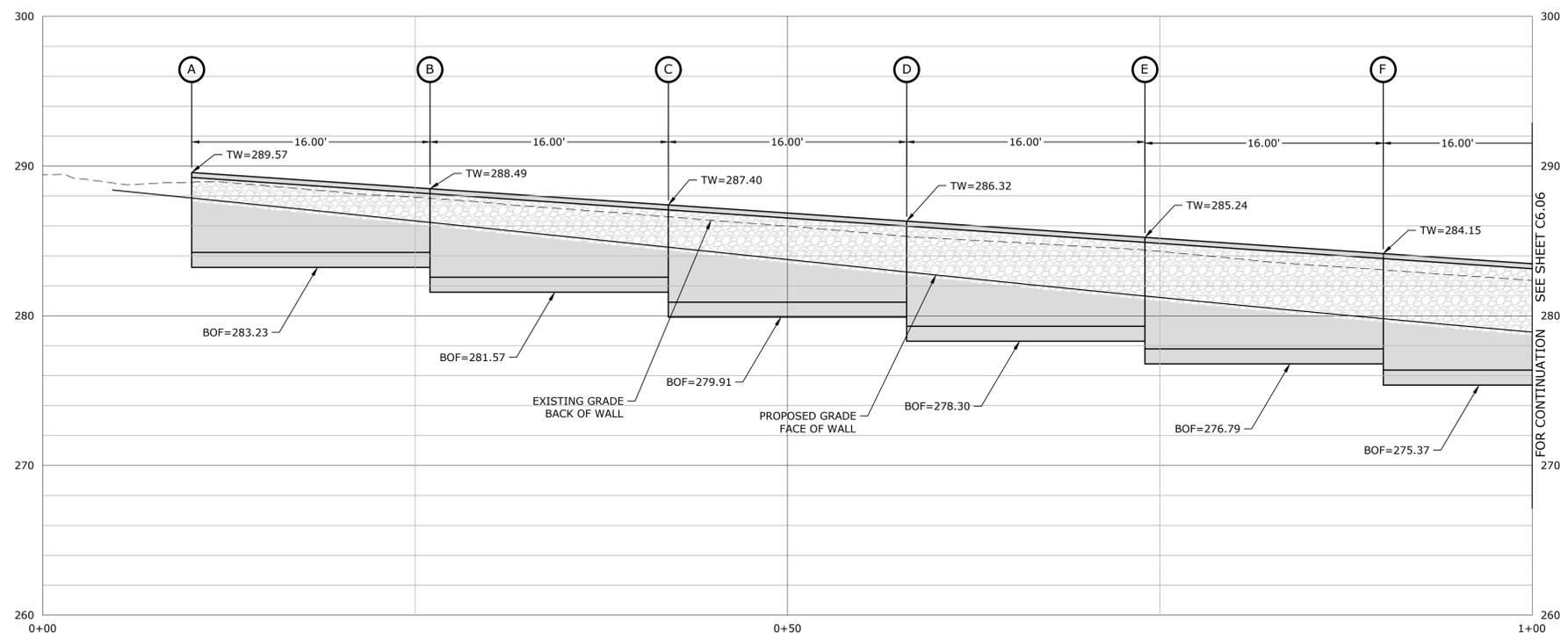
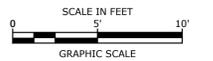
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C6.3

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 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawing\Sheet\T0196-15-C-601-WALL DETAIL.dwg



FOR CONTINUATION SEE SHEET C6.06



FOR CONTINUATION SEE SHEET C6.06

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

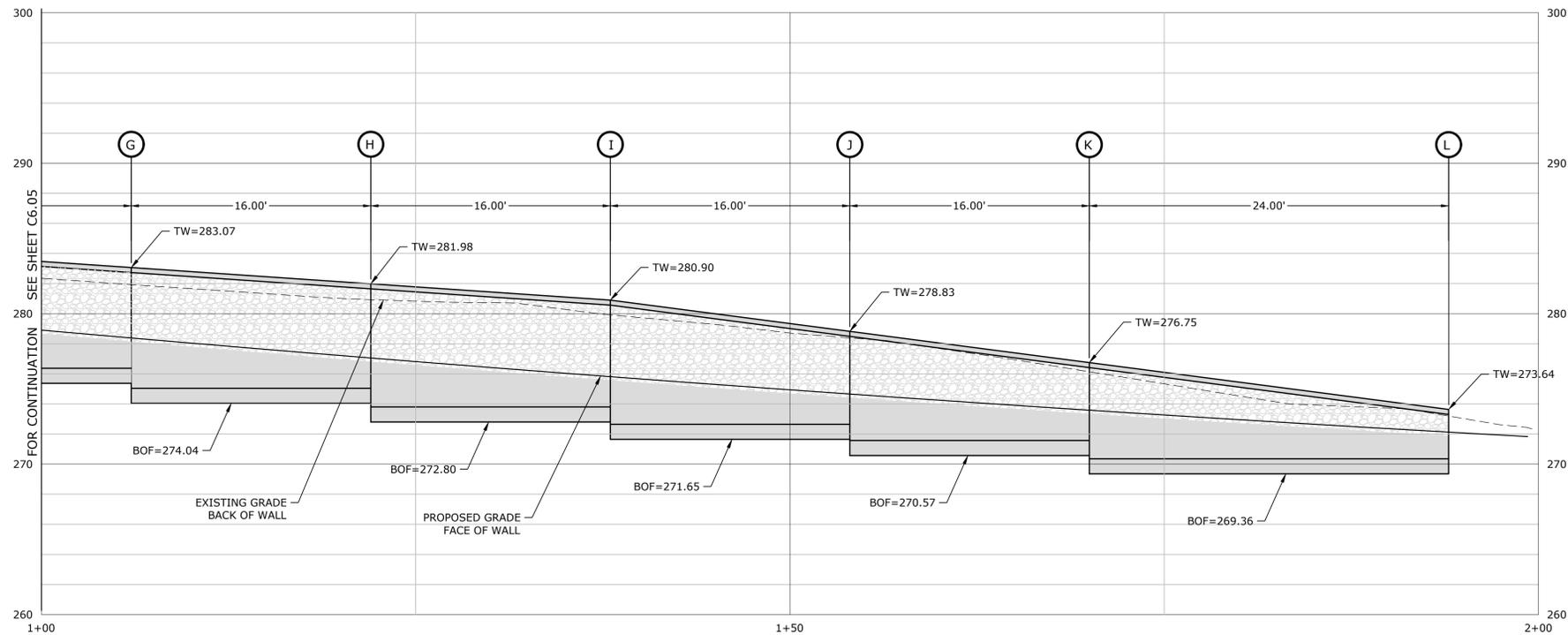
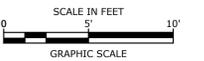
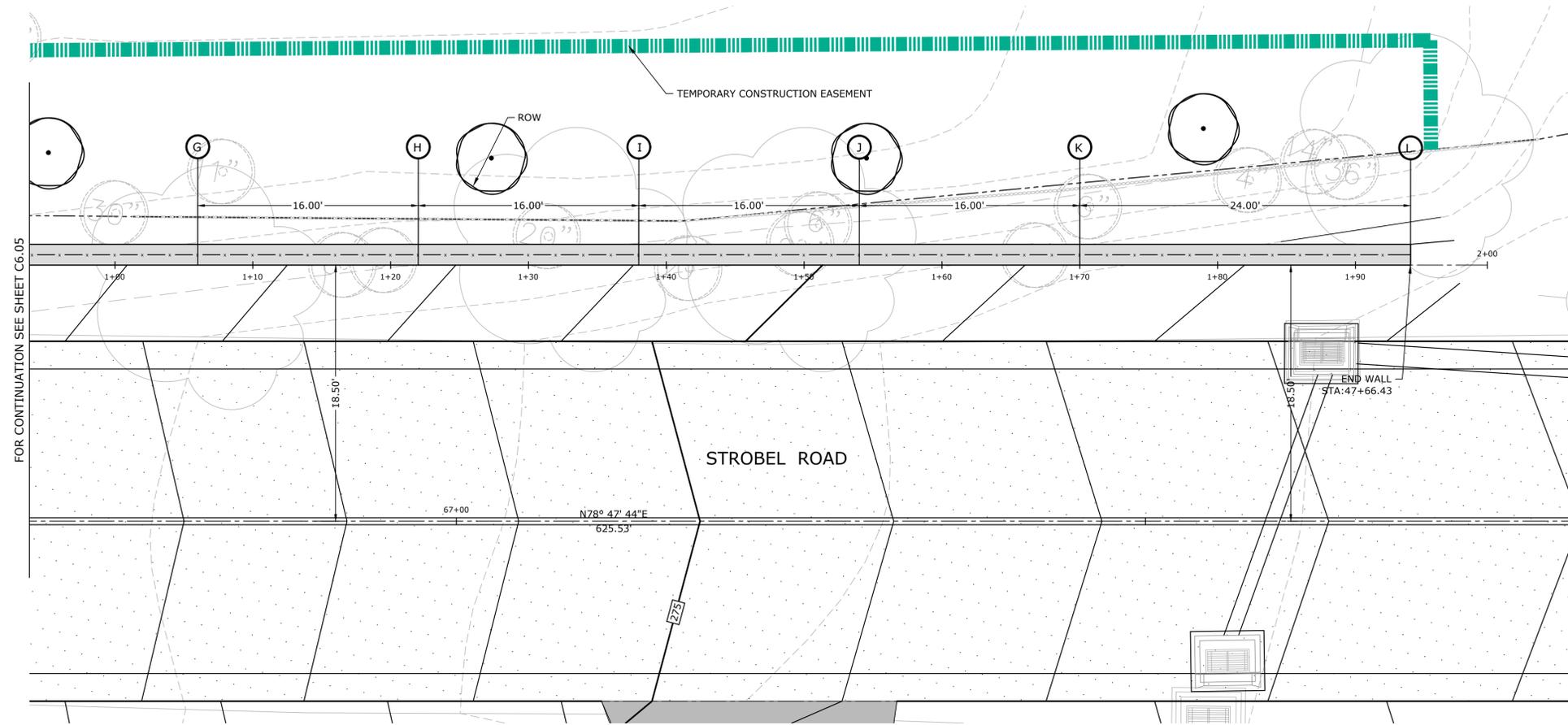
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-601-WALL DETAIL.dwg
 DRAWN BY: CRD
 CHECKED: AJM
 APPROVED: DCH

RETAINING WALL PLAN WALL #3

SCALE: 1" = 5'

C6.4



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON
 ORIGINAL DRAWING
 IF NOT ONE INCH ON
 THIS SHEET, ADJUST
 SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-15-C-601-WALL DETAIL.dwg	
DRAWN BY:	CRD	
CHECKED:	AJM	
APPROVED:	DCH	

**RETAINING WALL PLAN
 WALL #3**

SCALE: 1" = 5'

C6.5

OFFICE RECORD			
REV #	TIR # 144-1705-02	SM # N/A	SIGNAL REVISED:
NEW SIGNAL			

CONSTRUCTION NOTES

ALL MATERIAL AND CONSTRUCTION METHODS SHALL CONFORM TO THE FOLLOWING CURRENT D.O.T. DOCUMENTS WHICH CAN BE ACCESSED ON THE D.O.T. WEBSITE:
 * STANDARD SPECIFICATIONS FOR ROADS, BRIDGES AND INCIDENTAL CONSTRUCTION,
 * SUPPLEMENTAL SPECIFICATIONS TO THE STANDARD SPECIFICATIONS,
 * SPECIAL PROVISIONS TO THE STANDARD SPECIFICATIONS,
 * STANDARD INSTALLATION AND GUIDE DETAIL SHEETS.

ALL TRAFFIC SIGNAL EQUIPMENT IS NEW.
 CONTRACTOR SHALL STAKE ALL R.O.W. PRIOR TO EXCAVATION. ALL WORK, INCLUDING ALL FOUNDATIONS SHALL BE WITHIN THE R.O.W.

CONTRACTOR SHALL TELEPHONE "CALL BEFORE YOU DIG" AT 1-800-922-4455 48 HOURS PRIOR TO ANY EXCAVATION. CONTRACTOR SHALL CONTACT UTILITY REPRESENTATIVES AND TOWN AGENCIES FOUR WEEKS PRIOR TO EXCAVATION.

COORDINATE WITH UTILITY COMPANY REPRESENTATIVES LISTED IN THE SPECIAL PROVISION, 1.07 - LEGAL RELATIONS AND RESPONSIBILITIES.

ANY PROPOSED REVISIONS TO THE LOCATION OF THE APPURTENANCES SHOWN ON THE PLAN WHEN IN OR ADJACENT TO SIDEWALKS SHALL BE VERIFIED PRIOR TO INSTALLATION TO PROVIDE A FREE PATH OF NOT LESS THAN 4 FEET.

THE LOCATION OF TRAFFIC SIGNAL APPURTENANCES (MAST ARMS, PEDESTALS, AND HAND HOLES) WHEN IN OR ADJACENT TO SIDEWALKS SHALL BE VERIFIED PRIOR TO INSTALLATION TO PROVIDE A FREE PATH OF NOT LESS THAN 4 FEET.

THE CONTRACTOR SHALL OBTAIN ALL NECESSARY TOWN PERMITS TO CONDUCT THE WORK. CABINET DOOR TO OPEN FIELD SIDE.

ALL SIGNS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED WITH NEW SIGNS AND POSTS, IF NEEDED, IN KIND BY THE CONTRACTOR AT THEIR EXPENSE.

CONTRACTOR SHALL REMOVE ALL EXISTING SIGNS THAT CONFLICT WITH THE PROPOSED SIGNS.

CONTRACTOR SHALL REPLACE IN KIND ALL DISTURBED AREAS (CURBING, SIDEWALK, ETC.) ASSOCIATED WITH THE CONSTRUCTION OF SIGNAL EQUIPMENT. THE LIMIT OF WORK ASSOCIATED WITH THE CONSTRUCTION OF SIGNAL EQUIPMENT SHALL BE A MINIMUM OF TEN (10) FEET UNLESS OTHERWISE SPECIFIED. THE SIDEWALK SHALL BE RESTORED WITHIN 48 HOURS OF DISTURBANCE.

INSTALL 30" X 30" HANDHOLE. ALL OTHERS TYPE II.
 INSTALL HANDHOLES APPROX. 1' BEHIND CURB OR SIDEWALK. IF NO CURB OR SIDEWALK, 2' BEHIND EDGE OF ROAD UNLESS OTHERWISE SPECIFIED.

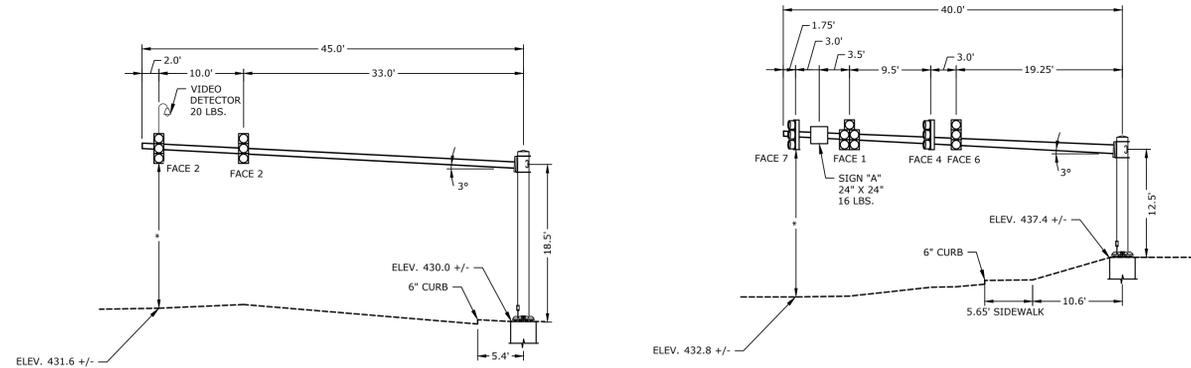
HANDHOLES INSTALLED IN SIDEWALK SHALL HAVE CAST IRON HANDHOLE COVERS.

VIDEO DETECTOR LOCATIONS ARE FOR ILLUSTRATION ONLY. EXACT LOCATIONS SHALL BE DETERMINED BY THE MANUFACTURER OR THEIR DESIGNATED REPRESENTATIVE. DETECTOR CABLES ARE TO BE INSTALLED CONTINUOUS BETWEEN EACH DETECTOR AND THE TRAFFIC SIGNAL CONTROLLER.

INSTALL PEDESTAL FOUNDATIONS ADJACENT TO LANDING AREA.
 ALL MAST ARM MOUNTED TRAFFIC SIGNALS ARE FIXED MOUNTED TO THE ARM BY USE OF ADJUSTABLE BRACKETS.

CONTRACTOR TO CONDUCT TEST PITS AT PROPOSED MAST ARM LOCATIONS PRIOR TO DRILLING FOUNDATION.

INSTALL SIGN 41-0815 "NEW" AND 41-0836 (SIGNAL AHEAD SYMBOL) ON ALL APPROACHES APPROXIMATELY 500 FEET IN ADVANCE OF THE INTERSECTION. REMOVE SIGN ASSEMBLY AFTER 14 CALENDAR DAYS FROM THE DATE THE SIGNAL IS PLACED IN OPERATION.

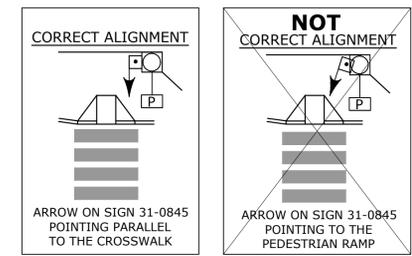


DANIELS FARM ROAD AT STROBEL ROAD
 144-XXX-M
 NO SCALE

DANIELS FARM ROAD AT STROBEL ROAD
 144-XXX-N
 NO SCALE

NOTES:
 *DISTANCE BETWEEN PAVEMENT AND SIGNAL HEAD SHALL BE BETWEEN 16' AND 18'. CONTRACTOR TO VERIFY MAST ARM INFORMATION INCLUDING CROSS-SECTIONS AND ELEVATIONS PRIOR TO SUBMITTING WORKING DRAWINGS.

PEDESTRIAN PUSHBUTTON ALIGNMENT



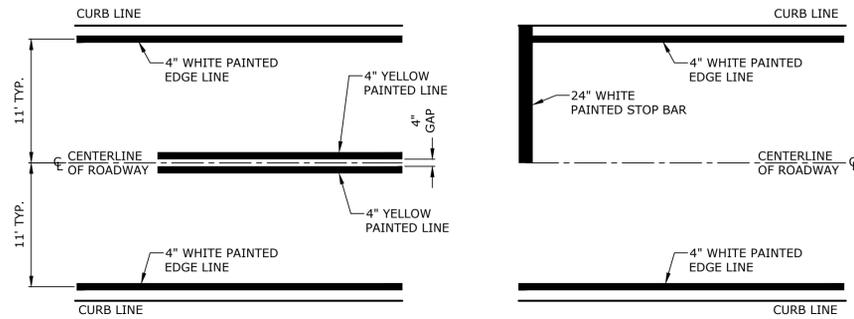
TOWN SIGNAL

Tighe&Bond
 www.tighebond.com
 1000 Bridgeport Avenue
 Suite 320
 Shelton, CT 06484
 (203) 712-1100

ENGINEER	TRAFFIC	DATE	ELECTRICAL	DATE	REV #	INTERSECTION #
TIGHE & BOND						144-XXX
SHELTON, CONNECTICUT						
06-07-2018						
ENERGY BY TOWN			ADDRESS #			
MAINT LEVEL 5			SERVICE POLE SNET 634			
METERED SERVICE						
TOWN:			TRUMBULL		PROJECT NO. 290196-15	
DANIELS FARM ROAD AT STROBEL ROAD			DRAWING TITLE:		DRAWING NO. TCS-002	
			TRAFFIC CONTROL SIGNAL PLAN		SHEET NO. C7.1	

NO.	DATE	REVISION DESCRIPTION

Jan 18, 2019 2:31pm Plotted By: DubucueC
 Tighe & Bond, Inc. J:\170196\15 - Strobel Rd Drawing\Sheet\144-15-C-700-SIGNAL.dwg
 Border Version 11/29/13



4" YELLOW CENTERLINE

24" WHITE STOP BAR

NOTES:

1. INSTALL WHITE EDGE LINES WHERE ROADWAY WIDTH IS 24' OR GREATER. DISCONTINUE WHITE EDGE LINES WHERE ROADWAY WIDTH IS LESS THAN 24'.
2. STOP BARS TO BE 24" MIN. UNLESS OTHERWISE NOTED ON PLANS.
3. STOP BARS TO BE MARKED A MINIMUM OF 4' IN ADVANCE OF NEAREST EDGE OF CROSSWALK.
4. IN ABSENCE OF MARKED CROSSWALK THE STOP BAR SHALL BE PLACED AT THE DESIRED STOPPING POINT. IN NO CASE MORE THAN 30' OR LESS THAN 5' FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY AND 90° TO THE CENTERLINE OF ROADWAY.
5. THE STOP BAR SHALL ORDINARILY BE PLACED IN LINE WITH THE STOP SIGN. HOWEVER, IF THE STOP SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO STOP, THE STOP BAR SHOULD BE PLACED AT THE STOPPING POINT.

PAVEMENT MARKING DETAILS
NO SCALE

Notes:

CLASS "A" CONCRETE: Class "A" concrete shall be used throughout.

ALLOWABLE DESIGN STRESSES:

Class "A" Concrete: Based on $f'_c = 3,000$ psi
Reinforcement: (ASTM A615 Grade 60) $f = 24,000$ psi

JOINTS: Expansion joints shall be provided at intervals not exceeding 60'. Contraction joints shall be located at intervals not exceeding 20'.

SLOPING FILL OR LIVE LOAD SURCHARGE: The wall sections shown for sloping backfill shall also be used for cases where there is a level backfill subject to live load surcharge.

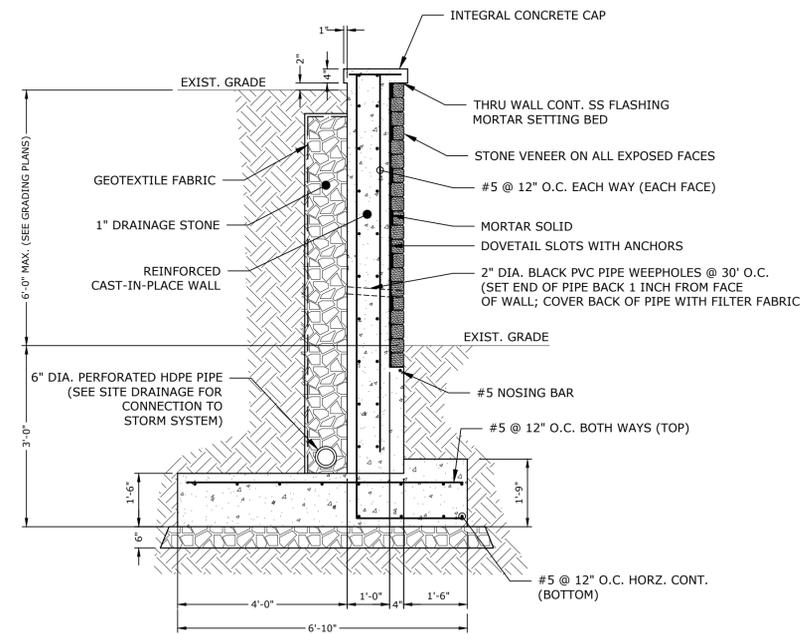
FOUNDATION CONDITIONS: The walls shown are to be founded on earth or rock only. These walls are not to be founded on piles. Organic soil must be removed if encountered.

REINFORCEMENT COVER: All stem reinforcement shall have 2" cover except where noted otherwise. All footing reinforcement shall have 3" cover.

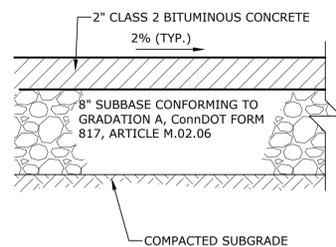
WEEPHOLES: To be located 5' maximum from wall corners, and no more than 30' on center elsewhere along the wall.

* Lap splices to footing dowels for footings founded on compacted gravel fill shall be as shown. Footings founded on other materials shall have two feet added to the required splice length.

** 1/2" x 1/2" Joint seal to extend from the top of footing to the top of the wall and horizontally along this joint to the front face of the wall.



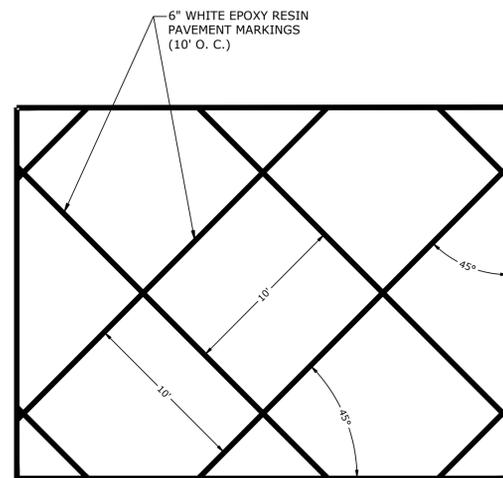
STONE VENEER RETAINING WALL
SCALE: 1" = 2'



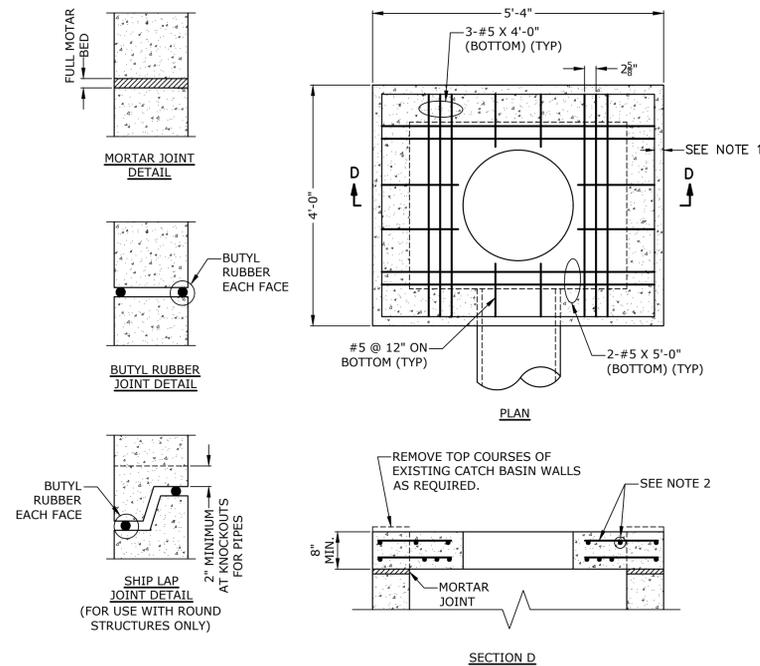
NOTES:

1. ALONG ACCESSIBLE ROUTES CROSS PITCH SHALL NOT EXCEED 5%. LONGITUDINAL SLOPE SHALL NOT EXCEED 5% WITHOUT HANDRAIL, OR 10% WITH HANDRAIL.

BITUMINOUS CONCRETE SIDEWALK
NO SCALE



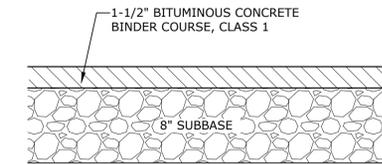
"DO NOT BLOCK INTERSECTION" PAVEMENT MARKING DETAILS
NO SCALE



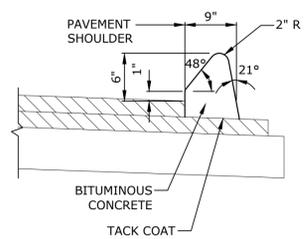
NOTES:

1. ALL REINFORCEMENT SHALL HAVE A MINIMUM CLEAR COVER OF 2", EXCEPT FOR BENEATH BOTTOM REINFORCEMENT IN TOP SLABS, WHERE THE MINIMUM MAY BE 1 1/2".
2. SHRINKAGE AND TEMPERATURE REINFORCEMENT SHALL BE PROVIDED IN THE TOPS OF SLABS. THE TOTAL AREA OF REINFORCEMENT PROVIDED SHALL BE AT LEAST 0.125 SQUARE INCHES PER FOOT IN EACH DIRECTION. THE MAXIMUM SPACING OF THIS REINFORCEMENT SHALL NOT EXCEED 18 INCHES.

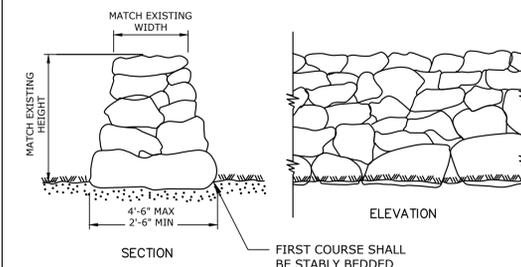
TOP SLAB TO CONVERT CATCH BASIN TO MANHOLE
NO SCALE



TEMPORARY PAVEMENT DETAIL
NO SCALE



BITUMINOUS CONCRETE LIP CURBING
NO SCALE



FARM WALL FENCE
SCALE: NTS

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

BAR IS 1 INCH ON ORIGINAL DRAWING
0 1 INCH
IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-15-C-800-DETL.dwg	
DRAWN BY:	MDS	
CHECKED:	AJM	
APPROVED:	JWB	

DETAILS

SCALE: AS NOTED

C8.2

DOCUMENT ALL LOOP DETECTOR VALUES BOTH CALCULATED AND MEASURED.

DEFINITIONS:

LOOP: #14 AWG WIRE IN SAWCUT, TERMINATED IN HANDHOLE, IMSA SPEC 51-7.
 LEAD-IN: 14/2 SHIELDED TWISTED PAIR CABLE FROM HANDHOLE TO CONTROLLER, IMSA SPEC 50-2.
 LOOP CIRCUIT: LOOP SAWCUT WIRE SPLICED TO 14/2 LEAD-IN CABLE.
 AMPLIFIER: ELECTRONIC DEVICE CONNECTED TO LOOP CIRCUIT. SENSES CHANGE IN RESONANT FREQUENCY AND CREATES AN OUTPUT TO THE CONTROLLER.
 MEGOHMMETER: INSTRUMENT SPECIFICALLY DESIGNED TO TEST THE INSULATION RESISTANCE OF A CIRCUIT. COMMON MANUFACTURERS: AMEC®, AMPROBE®, FLUKE®, MEGGER®.

1: RESISTANCE:

1a: INSULATION RESISTANCE: PERFORM A 600 VOLT (MINIMUM) MEGOHMMETER TEST ON LOOP CIRCUIT. THE LOOP AMPLIFIER MUST BE DISCONNECTED FROM THE LOOP CIRCUIT OR THE LOOP AMPLIFIER WILL BE DAMAGED. THE RESISTANCE OF THE LOOP WIRE TO GROUND MUST BE GREATER THAN 100 MEG OHMS.

1b: WIRE RESISTANCE: MEASURE THE DC RESISTANCE OF THE LOOP CIRCUIT. THE LOOP CIRCUIT MUST BE DISCONNECTED FROM THE AMPLIFIER. USING AN OHMMETER CONNECTED ACROSS THE LOOP CIRCUIT, MEASURE THE DC RESISTANCE OF THE CONDUCTORS. THE RESISTANCE SHOULD BE LESS THAN 4 OHMS.

NOTE: ALL TESTS SHALL BE DONE AT THE CONTROLLER ASSEMBLY (CA), HOWEVER IT IS RECOMMENDED TO PERFORM A PRELIMINARY MEGOHMMETER TEST AT THE HANDHOLE PRIOR TO SEALING THE SAWCUT AND SPLICING TO THE LEAD-IN. IF A DEFECTIVE LOOP WIRE IS FOUND, IT MAY BE EASILY REPLACED.

2: LOOP CIRCUIT INDUCTANCE:

2a: CALCULATE INDUCTANCE OF LOOP (L_{LOOP}) AND LEAD-IN CABLE (L_{14/2}).

LOOP INDUCTANCE (ENGLISH)	LOOP INDUCTANCE (METRIC)
$L_{LOOP} = (P/4) (N^2 + N)$	$L_{LOOP} = (3.28P/4) (N^2 + N)$
LEAD-IN INDUCTANCE	LEAD-IN INDUCTANCE
$L_{14/2} = (0.24 \mu\text{H}/\text{FT}) (D)$	$L_{14/2} = (0.78 \mu\text{H}/\text{m}) (D)$

WHERE:
 L_{LOOP} = INDUCTANCE OF INDIVIDUAL LOOP SEGMENTS IN MICROHENRIES (μH).
 L_{14/2} = INDUCTANCE OF LEAD-IN CABLE.
 P = PERIMETER OF INDIVIDUAL LOOP SEGMENT, IN FEET OR METERS.
 N = NUMBER OF TURNS.
 D = LENGTH OF LEAD-IN CABLE FROM SPLICE IN HANDHOLE TO CONTROLLER, IN FEET OR METERS.
 $L_T = L_1 + L_2 + L_3$ etc.,
 (TOTAL INDUCTANCE OF SEGMENTED LOOP SPLICED IN SERIES.)
 $L_T = 1 / [(1/L_1) + (1/L_2) + (1/L_3) + \text{etc.}]$,
 (TOTAL INDUCTANCE OF SEGMENTED LOOP SPLICED IN PARALLEL.)

WHERE:
 L_T = TOTAL INDUCTANCE OF THE SEGMENTED ARRANGEMENT.
 L₁, L₂, L₃ = INDUCTANCE OF INDIVIDUAL LOOP SEGMENTS.

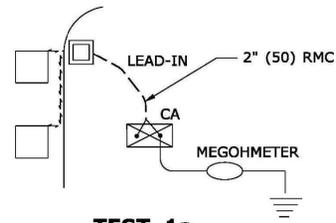
EXAMPLE: (IN ENGLISH)
 6' x 6', 4 TURNS, APPROXIMATELY 300' FROM THE CONTROLLER

$L_{LOOP} = (24/4) (4^2 + 4)$	$L_{14/2} = (0.24 \mu\text{H}/\text{FT}) (300)$
$L_{LOOP} = (6) (20)$	$L_{14/2} = (0.24) (300)$
$L_{LOOP} = 120 \mu\text{H}$	$L_{14/2} = 72 \mu\text{H}$

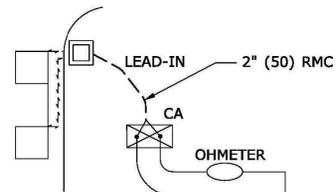
2b: MEASURE INDUCTANCE OF LOOP AND LEAD-IN AT CONTROLLER. USE INSTRUMENT DESIGNED TO MEASURE LOOP CIRCUIT INDUCTANCE.

3: POWER INTERRUPTION:

AFTER THE AMPLIFIER HAS TUNED AND IS OPERATING, DISCONNECT POWER BY REMOVING FUSE OR HARNESS CONNECTOR. RETURN POWER TO THE AMPLIFIER AND CONFIRM IT RE-TUNES AUTOMATICALLY WITHOUT ANY MANUAL ADJUSTMENTS.



TEST 1a



TEST 1b

LOOP NUMBER	RESISTANCE OHMS		INDUCTANCE MICROHENRIES (μH)		AMPLIFIER POWER INTERRUPTION PASS/FAIL (3)
	TO GROUND (1a)	LOOP WIRE (1b)	CALCULATED (2a)	MEASURED (2b)	
D1 FRONT					
D1 REAR					
D2A					
D2B					
D4A FRONT					
D4B REAR					
D5					
D6A					
D6B					

LOOP CIRCUIT TEST DATA (EXAMPLE)

INDUCTIVE LOOP TEST PROCEDURE

PIN	COLOR	FUNCTION
A	WHITE	110 VAC Neutral
B	BROWN	Output Relay Common (moving contact)
C	BLACK	110 VAC (Fused)
D	RED	Loop
E	ORANGE	Loop
F	YELLOW	Output Relay Contact (Closes with moving contact when detecting vehicle)
G	BLUE	Output Relay Contact (Closes with moving contact when detecting vehicle)
H	GREEN	Chassis Ground
J	GREY	110 VAC Delay/Extend Override
Shell		Ground (shall be connected to pin H in the connector)

DETECTOR AMPLIFIER PIN DESIGNATION

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:
 INDUCTIVE LOOP DETECTOR
 SAW CUT
 RIGID METAL CONDUIT
 HANDHOLE

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

NOT TO SCALE

STATE OF CONNECTICUT
 DEPARTMENT OF TRANSPORTATION

SUBMITTED BY: Tracy L. Fogarty
 NAME/DATE/TIME: Tracy L. Fogarty 2009.09.15 08:09:00 -04'00'
 APPROVED BY: John F. Carey
 NAME/DATE/TIME: John F. Carey 2009.09.16 08:16:38 -04'00'

CTDOT
 STANDARD SHEET
 OFFICE OF ENGINEERING

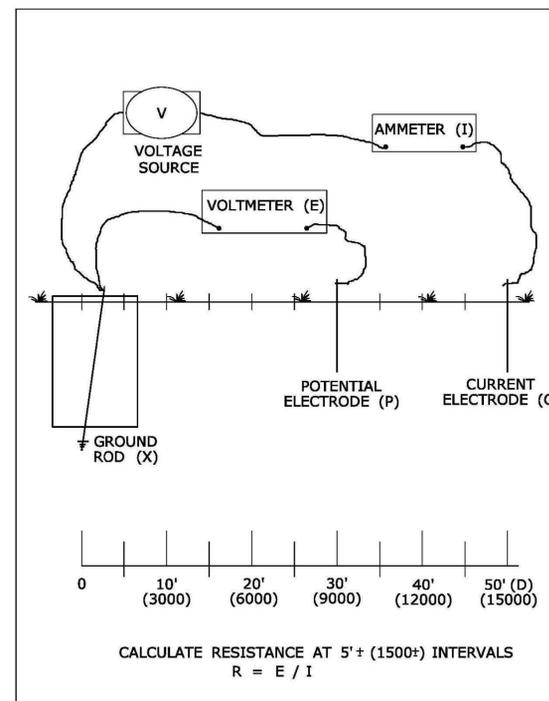
STANDARD SHEET TITLE: GENERAL CLAUSES (TEST PROCEDURES)
 STANDARD SHEET NO.: TR-1000_01
 C 8.3

TEST PROCEDURE:

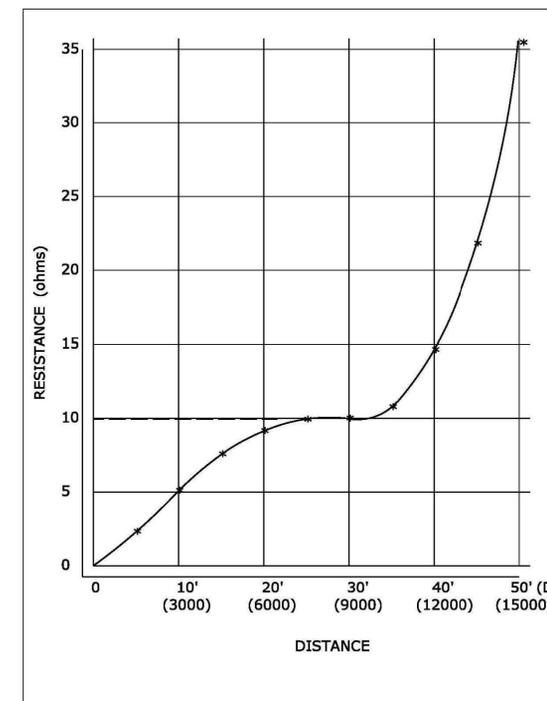
- INSERT ELECTRODE (C) A DISTANCE (D) FROM THE FOUNDATION. RECOMMEND A MINIMUM 50'.
- CONNECT A VOLTAGE SOURCE AND AMMETER BETWEEN THE FOUNDATION GROUND ROD (X) AND C.
- MEASURE THE CURRENT FLOW (I) BETWEEN X AND C.
- INSERT POTENTIAL ELECTRODE (P) AT 5' (1500) INTERVALS IN A STRAIGHT LINE TO ELECTRODE C.
- MEASURE VOLTAGE (E) AT EACH LOCATION OF P.
- CALCULATE RESISTANCE (R) AT EACH LOCATION OF P USING THE FORMULA R=E/I.
- PLOT THE VALUES ON A RxD GROUND RESISTANCE CHART.
- THE ACTUAL GROUND RESISTANCE IS WHERE THE PLOTTED CURVE IS RELATIVELY FLAT, USUALLY AT 62%± OF D. SEE EXAMPLE CHART: CURVE FLATTENS OUT AT 10 OHMS, APPROXIMATELY 30' (9000) FROM FOUNDATION.
- IF GROUND RESISTANCE IS GREATER THAN 10 OHMS, PERFORM CORRECTIVE ACTION AND RE-TEST.

SUGGESTED CORRECTIVE ACTION:

- A. INSTALL ADDITIONAL 10' (3000) GROUND ROD(S). REFER TO NESC SECTION 09, RULE 94.B.2. DRIVE ADDITIONAL GROUND RODS NO CLOSER TO FOUNDATION THAN 6' (1800). IF MORE THAN ONE IS NEEDED, SPACE MINIMUM 6' (1800) APART. BONDS TO ADDITIONAL GROUND ROD(S) SHALL BE MADE BY A CLAMP DESIGN FOR DIRECT BURIAL OR BY EXOTHERMIC WELDING TECHNIQUE. TOP OF ADDITIONAL GROUND ROD(S) SHALL BE 6" (150) BELOW GRADE.
- B. IN AREAS OF SHALLOW BEDROCK, INSTALL A GROUND GRID OR ARRAY CONSISTING OF BURIED WIRE, RODS, STRIPS OR PLATES. REFER TO NESC SECTION 09, RULE 94.B.3. REFER TO NEC SECTION 250. MINIMUM DEPTH OF 18" (450). GRID CONNECTIONS AND BONDS ON GROUND GRID SHALL BE MADE BY CLAMPS DESIGNED FOR DIRECT BURIAL OR BY EXOTHERMIC WELDING TECHNIQUE.



3 POINT GROUND RESISTANCE TEST CIRCUIT

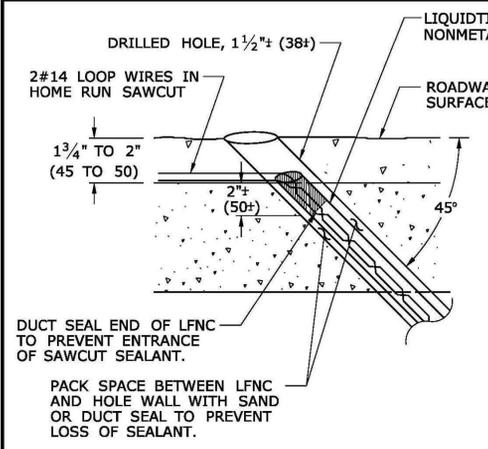


GROUND RESISTANCE CHART (EXAMPLE)

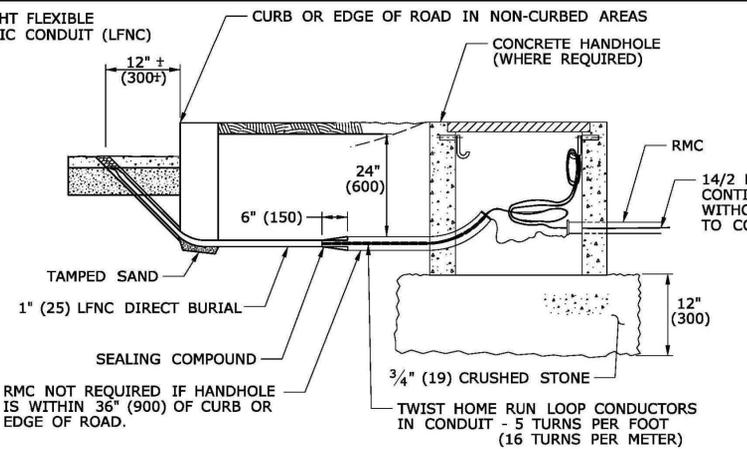
NOTES:

1. DURING THE TEST, THE GROUND ROD SHOULD NOT BE BONDED TO ANY RMC IN THE FOUNDATION.
2. THE VOLTAGE SOURCE, VOLTMETER, AMMETER, ELECTRODES P AND C, AND CONNECTING CABLES ARE AVAILABLE AS A SPECIALIZED TEST INSTRUMENT.
3. REFER TO NATIONAL ELECTRICAL SAFETY CODE (NESC) SECTION 09, GROUNDING METHODS FOR ELECTRIC SUPPLY AND COMMUNICATIONS FACILITIES.
4. REFER TO NATIONAL ELECTRICAL CODE (NEC) CHAPTER 2, ARTICLE 250, GROUNDING.

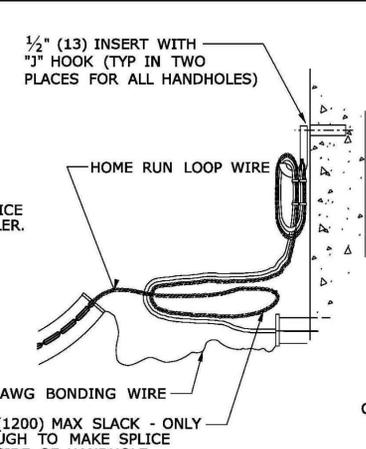
3 POINT FALL-OF-POTENTIAL GROUND RESISTANCE TEST



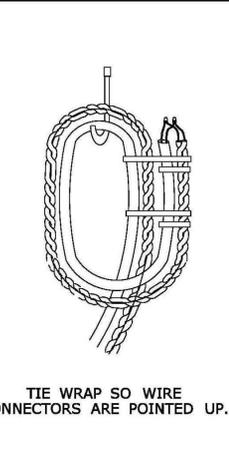
DETAIL "A"



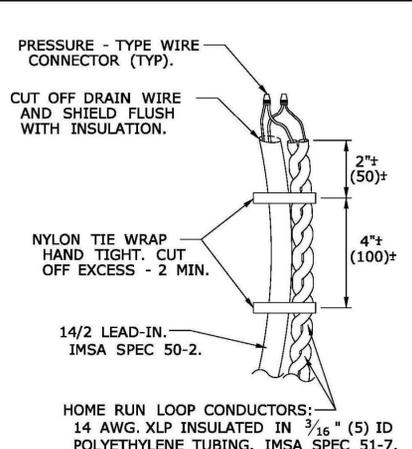
TYPICAL ELEVATION VIEW LOOP DETECTOR SAWCUT AND LEAD-IN



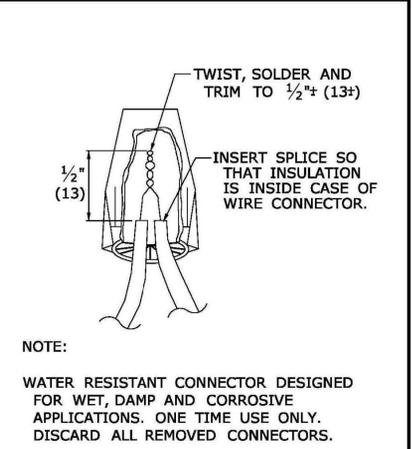
DETAIL "B"



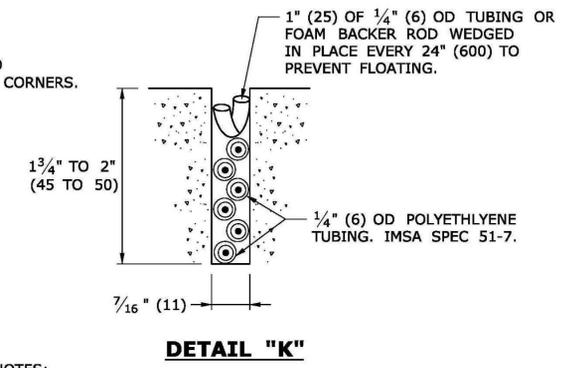
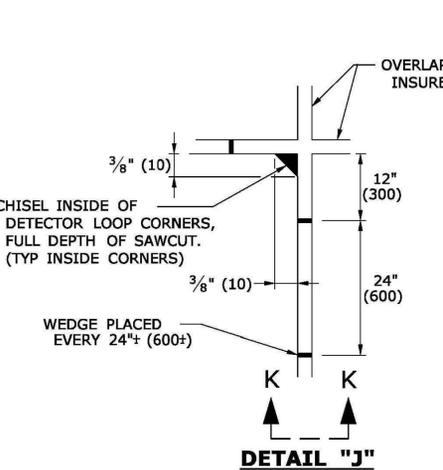
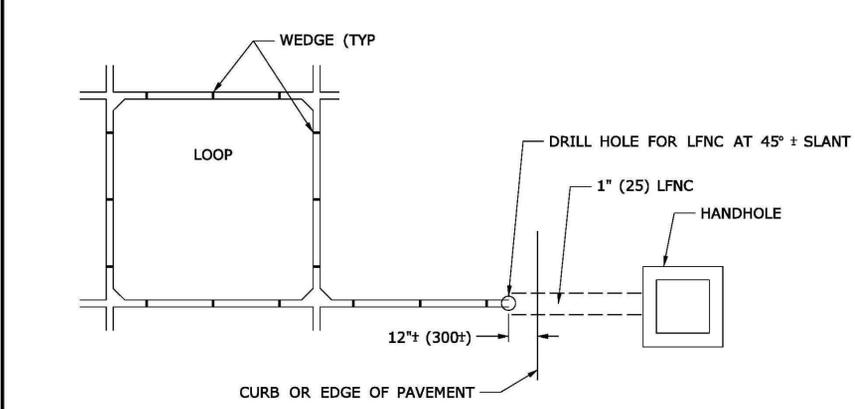
DETAIL "C" FRONT VIEW



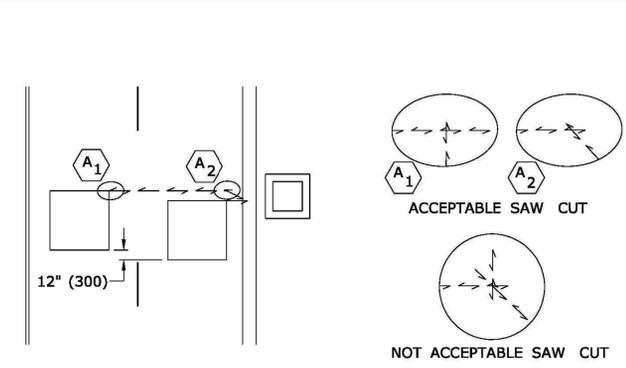
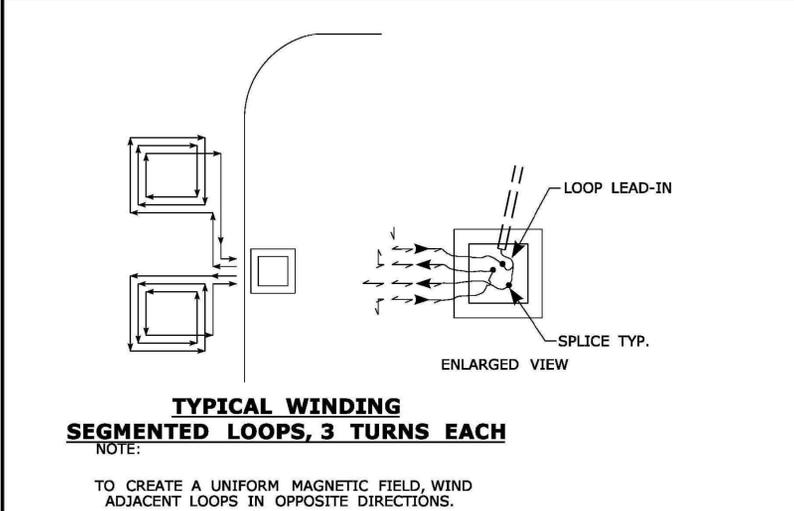
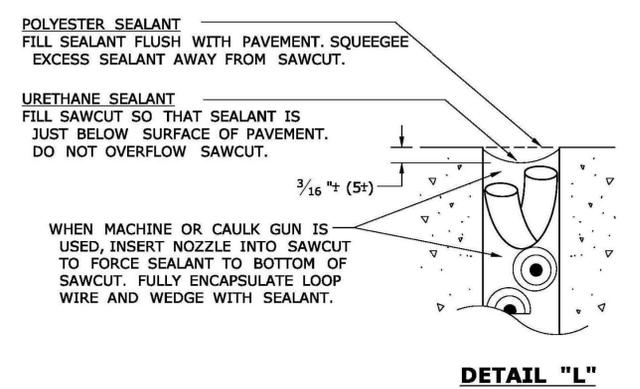
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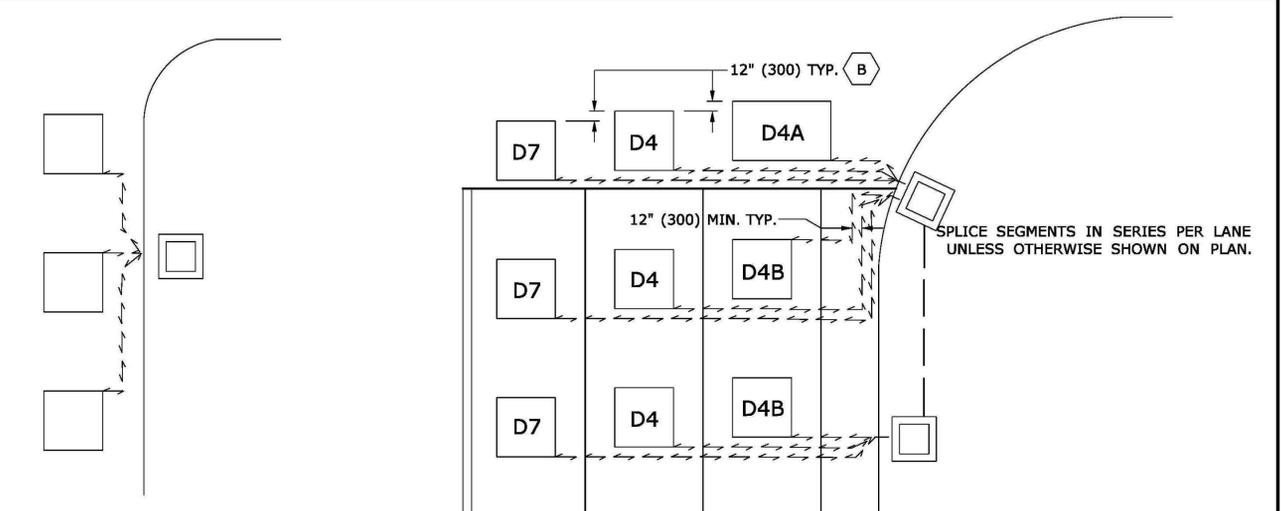
DETAIL "E"



NOTES:
 ONLY USE POLYESTER COMPOUND AS SEALANT, UNLESS OTHER TYPE IS APPROVED BY ENGINEER.
 REFER TO STANDARD SPECIFICATIONS, SECTION 11-11.
 WET SAW UNLESS DRY SAW IS APPROVED BY ENGINEER.
 RECOMMENDED SAW BLADE: 14" x 3/8" (350 x 10) PRODUCES 7/16" (11) SLOT.



A DO NOT OVERLAP MORE THAN TWO SAWCUTS.



LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

- INDUCTIVE LOOP DETECTOR
- - - SAW CUT
- RIGID METAL CONDUIT
- HANDHOLE

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.
 Plotted Date: 10/30/2009

NOT TO SCALE

STATE OF CONNECTICUT
 DEPARTMENT OF TRANSPORTATION
 Filename: CTDOT_TRAFFIC_STD.dgn Model: TR-1111_01

SUBMITTED BY: NAME/DATE/TIME:
 Tracy L. Fogarty Tracy L. Fogarty 2010.01.11 09:28:50 -0500
 APPROVED BY: NAME/DATE/TIME:
 John F. Carey John F. Carey 2010.01.13 11:58:55 -0500

CTDOT
 STANDARD SHEET
 OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
LOOP VEHICLE DETECTOR AND SAWCUT

STANDARD SHEET NO.:
 TR-1111_01
 C 8.4

REV.	DATE	REVISION DESCRIPTION

R1 - SERIES					R2 - SERIES					R3 - SERIES										R4 - SERIES					R5 - SERIES																																																																																																																																																																																																																											
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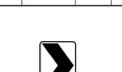
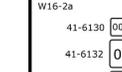
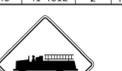
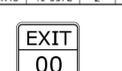
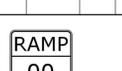
NOTES:

- FOR METRIC SEE CONVERSION CHART.
- THE LEGEND "S.T.C." SHALL APPEAR ON ALL R - SERIES SIGNS EXCEPT WHEN SUFFIXED WITH THE LETTER "Z".
- FOR SPECIFIC SIGN DESIGN CONTACT CONN. D.O.T., DIVISION OF TRAFFIC ENGINEERING. FOR BOLT HOLE PATTERN REFER TO FHWA PUBLICATION "STANDARD HIGHWAY SIGNS". SIGNS OF DIFFERENT DIMENSIONS TO BE ERRECTED ON THE SAME POSTS, OR SPAN/MAST ARM MOUNTED, MAY REQUIRE SPECIAL BOLT HOLE PATTERNS.
- POSTS - SEE TYP. SHEET (SHT #9) - "TYPICAL METAL SIGN POSTS AND SIGN MOUNTING DETAILS."
- POSTS - TYPE A (EXCEPT WHERE NOTED WITH A "B" FOR TYPE B)
- FOR OVERHEAD MOUNTED SIGNS, SEE TYPICAL SIGNAL SHEET - "MISC. TRAFFIC SIGNAL INSTALLATION DETAILS."
- SIGNS SHALL BE FABRICATED OF ONE CONTINUOUS PIECE OF SHEET ALUMINUM. SPLICING OF SHEET ALUMINUM WILL NOT BE ACCEPTED.
- BACKGROUND SHEETING SHALL BE TYPE III RETROREFLECTIVE SHEETING, EXCEPT NOTED BY *.

COLORS:

R-SERIES
 BACKGROUND - WHITE (SILVER) - EXCEPT AS NOTED.
 LEGEND - BLACK - EXCEPT AS NOTED.
 ALL COLORS SHALL BE RETROREFLECTIVE WITH THE EXCEPTION OF BLACK WHICH SHALL BE OPAQUE.
 * SIGNS TO BE "BRIGHT WIDE ANGLE RETROREFLECTIVE SHEETING".

METRIC CONVERSION CHART (1" = 25mm) <table border="1"> <thead> <tr> <th>ENGLISH</th> <th>METRIC</th> <th>ENGLISH</th> <th>METRIC</th> <th>ENGLISH</th> <th>METRIC</th> </tr> </thead> <tbody> <tr><td>12"</td><td>300</td><td>42"</td><td>1050</td><td>72"</td><td>1800</td></tr> <tr><td>18"</td><td>450</td><td>48"</td><td>1200</td><td>78"</td><td>1950</td></tr> <tr><td>24"</td><td>600</td><td>54"</td><td>1350</td><td>84"</td><td>2100</td></tr> <tr><td>30"</td><td>750</td><td>60"</td><td>1500</td><td>90"</td><td>2250</td></tr> <tr><td>36"</td><td>900</td><td>66"</td><td>1650</td><td>96"</td><td>2400</td></tr> </tbody> </table>		ENGLISH	METRIC	ENGLISH	METRIC	ENGLISH	METRIC	12"	300	42"	1050	72"	1800	18"	450	48"	1200	78"	1950	24"	600	54"	1350	84"	2100	30"	750	60"	1500	90"	2250	36"	900	66"	1650	96"	2400	 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION DESIGNER/DRAFTER: D. K. SWINBURNE CHECKED BY: L.N. CONROY Plotted: 4/13/2011 File name: CTDOT_TRAFFIC_GD.dgn		 OFFICE OF ENGINEERING APPROVED BY: _____ DATE: _____		PROJECT TITLE: _____ TOWN: _____ DRAWING TITLE: SIGN FACE SHEET ALUMINUM R-SERIES SIGNS TYPICAL DETAILS SHEET NO. C 8.5	
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THK.</th></tr> <tr><td>6.25</td><td>30</td><td>41-0501</td><td>1</td><td>.080</td></tr> <tr><td>9.00</td><td>36</td><td>41-0502</td><td>1</td><td>.080</td></tr> <tr><td>16.00</td><td>48</td><td>41-0503</td><td>2</td><td>.100</td></tr> </table>				AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-0501	1	.080	9.00	36	41-0502	1	.080	16.00	48	41-0503	2	.100	 <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>6.25</td><td>30</td><td>41-4450</td><td>1</td><td>.080</td></tr> <tr><td>9.00</td><td>36</td><td>41-4448</td><td>1</td><td>.080</td></tr> <tr><td>16.00</td><td>48</td><td>41-4452</td><td>2</td><td>.100</td></tr> </table>				AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-4450	1	.080	9.00	36	41-4448	1	.080	16.00	48	41-4452	2	.100	 <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>6.25</td><td>30</td><td>41-4451</td><td>1</td><td>.080</td></tr> <tr><td>9.00</td><td>36</td><td>41-4449</td><td>1</td><td>.080</td></tr> <tr><td>16.00</td><td>48</td><td>41-4459</td><td>2</td><td>.100</td></tr> </table>				AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-4451	1	.080	9.00	36	41-4449	1	.080	16.00	48	41-4459	2	.100	 ROAD NARROWS <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>6.25</td><td>30</td><td>41-4401</td><td>1</td><td>.080</td></tr> <tr><td>16.00</td><td>48</td><td>41-4404</td><td>2</td><td>.100</td></tr> </table>				AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-4401	1	.080	16.00	48	41-4404	2	.100	 <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>6.25</td><td>30</td><td>41-4333</td><td>1</td><td>.080</td></tr> <tr><td>9.00</td><td>36</td><td>41-4335</td><td>1</td><td>.080</td></tr> </table>				AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-4333	1	.080	9.00	36	41-4335	1	.080	 <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>6.25</td><td>30</td><td>41-4506</td><td>1</td><td>.080</td></tr> <tr><td>16.00</td><td>48</td><td>41-4508</td><td>2</td><td>.100</td></tr> </table>				AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-4506	1	.080	16.00	48	41-4508	2	.100
AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.																																																																																																																																																																																																																																																					
4.69	30	41-2111	1	.080																																																																																																																																																																																																																																																					
6.75	36	41-2112	1	.080																																																																																																																																																																																																																																																					
12.00	48	41-2113	2	.100																																																																																																																																																																																																																																																					
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6.25	30	41-4006	1	.080																																																																																																																																																																																																																																																					
16.00	48	41-4056	2	.100																																																																																																																																																																																																																																																					
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16.00	48	41-4062	2	.100																																																																																																																																																																																																																																																					
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6.25	30	41-4226	1	.080																																																																																																																																																																																																																																																					
9.00	36	41-4236	1	.080																																																																																																																																																																																																																																																					
16.00	48	41-4246	2	.100																																																																																																																																																																																																																																																					
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9.00	36	41-4449	1	.080																																																																																																																																																																																																																																																					
16.00	48	41-4459	2	.100																																																																																																																																																																																																																																																					
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16.00	48	41-4404	2	.100																																																																																																																																																																																																																																																					
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6.25	30	41-4506	1	.080																																																																																																																																																																																																																																																					
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6.25	30	41-4519	1	.080																																																																																																																																																																																																																																																					
9.00	36	41-4520	1	.080																																																																																																																																																																																																																																																					
16.00	48	41-4521	2	.100																																																																																																																																																																																																																																																					
AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.																																																																																																																																																																																																																																																					
9.00	36	41-4443L	1	.080																																																																																																																																																																																																																																																					
9.00	36	41-4440R	1	.080																																																																																																																																																																																																																																																					
16.00	48	41-4444L	2	.100																																																																																																																																																																																																																																																					
16.00	48	41-4445R	2	.100																																																																																																																																																																																																																																																					
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1.77	18	41-2202	1	.080																																																																																																																																																																																																																																																					
7.07	36	41-2201	1	.080																																																																																																																																																																																																																																																					
AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.																																																																																																																																																																																																																																																					
6.25	30	41-4810	1	.080																																																																																																																																																																																																																																																					
9.00	36	41-4811	1	.080																																																																																																																																																																																																																																																					
16.00	48	41-4812	2	.100																																																																																																																																																																																																																																																					
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4.00	24	41-4212	1	.080																																																																																																																																																																																																																																																					
16.00	48	41-4214	2	.100																																																																																																																																																																																																																																																					
AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.																																																																																																																																																																																																																																																					
2.25	18X18	41-5568	1	.080																																																																																																																																																																																																																																																					
4.00	24X24	41-5569	1	.080																																																																																																																																																																																																																																																					
7.50	36X30	41-5570	1	.080																																																																																																																																																																																																																																																					
16.00	48X48	41-5572	2	.100																																																																																																																																																																																																																																																					
AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.																																																																																																																																																																																																																																																					
2.00	24X12	41-6130	.080																																																																																																																																																																																																																																																						
3.75	30X18	41-6132	.080																																																																																																																																																																																																																																																						
7.00	42X24	41-6134	.100																																																																																																																																																																																																																																																						
2.00	24X12	41-2607	.080																																																																																																																																																																																																																																																						
3.75	30X18	41-2608	.080																																																																																																																																																																																																																																																						
3.75	30X18	41-2610	.080																																																																																																																																																																																																																																																						
12.00	48X36	41-6115	2	.100																																																																																																																																																																																																																																																					
 LANE ENDS MERGE LEFT (L)  LANE ENDS MERGE RIGHT (R) <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>9.00</td><td>36</td><td>41-4454L</td><td>1</td><td>.080</td></tr> <tr><td>9.00</td><td>36</td><td>41-4453R</td><td>1</td><td>.080</td></tr> <tr><td>16.00</td><td>48</td><td>41-4456L</td><td>2</td><td>.100</td></tr> <tr><td>16.00</td><td>48</td><td>41-4455R</td><td>2</td><td>.100</td></tr> </table>				AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	9.00	36	41-4454L	1	.080	9.00	36	41-4453R	1	.080	16.00	48	41-4456L	2	.100	16.00	48	41-4455R	2	.100	 <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>6.25</td><td>30</td><td>41-4680</td><td>1</td><td>.080</td></tr> <tr><td>16.00</td><td>48</td><td>41-4692</td><td>2</td><td>.100</td></tr> </table>				AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-4680	1	.080	16.00	48	41-4692	2	.100	 00 - 0" <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>6.25</td><td>30</td><td>41-4704</td><td>1</td><td>.080</td></tr> <tr><td>9.00</td><td>36</td><td>41-4706</td><td>1</td><td>.080</td></tr> <tr><td>16.00</td><td>48</td><td>41-4708</td><td>2</td><td>.100</td></tr> </table>				AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-4704	1	.080	9.00	36	41-4706	1	.080	16.00	48	41-4708	2	.100	 EXIT 00 M.P.H. <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>20.00</td><td>48X60</td><td>41-5596</td><td>2</td><td>.100</td></tr> </table>				AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	20.00	48X60	41-5596	2	.100	 RAMP 00 M.P.H. <table border="1"> <tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr> <tr><td>20.00</td><td>48X60</td><td>41-5598</td><td>2B</td><td>.100</td></tr> </table>				AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	20.00	48X60	41-5598	2B	.100																																																																																																																																																						
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20.00	48X60	41-5598	2B	.100																																																																																																																																																																																																																																																					

NOTES:
 FOR METRIC SEE CONVERSION CHART.
 1. FOR SPECIFIC SIGN DESIGN CONTACT CONN. D.O.T., DIVISION OF TRAFFIC ENGINEERING.
 FOR BOLT HOLE PATTERN REFER TO FHWA PUBLICATION "STANDARD HIGHWAY SIGNS".
 SIGNS OF DIFFERENT DIMENSIONS TO BE ERRECTED ON THE SAME POSTS, OR SPAN/MAST ARM MOUNTED, MAY REQUIRE SPECIAL BOLT HOLE PATTERNS.
 2. POSTS - SEE TYP. SHEET (SHT #9) - "TYPICAL METAL SIGN POSTS AND SIGN MOUNTING DETAILS."
 3. POSTS - TYPE A (EXCEPT WHERE NOTED WITH A "B" FOR TYPE B)
 4. SIGNS SHALL BE FABRICATED OF ONE CONTINUOUS PIECE OF SHEET ALUMINUM.
 SPlicing OF SHEET ALUMINUM WILL NOT BE ACCEPTED.

COLORS:
 R-SERIES
 BACKGROUND - YELLOW - EXCEPT AS NOTED.
 LEGEND - BLACK - EXCEPT AS NOTED.
 ALL COLORS SHALL BE TYPE III RETROREFLECTIVE WITH THE EXCEPTION OF BLACK WHICH SHALL BE OPAQUE.

METRIC CONVERSION CHART (1" = 25mm)

ENGLISH	METRIC	ENGLISH	METRIC
12"	300	42"	1050
18"	450	48"	1200
24"	600	54"	1350
30"	750	60"	1500
36"	900	66"	1650

DESIGNER/DRAFTER:
D. K. SWINBURNE

CHECKED BY:
L.N. CONROY

Plotted: 4/13/2011

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

OFFICE OF ENGINEERING

SIGNATURE/BLOCK:
APPROVED BY: DATE:

PROJECT TITLE:

TOWN:

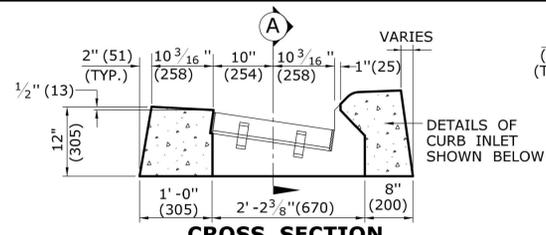
PROJECT NO.

DRAWING NO.

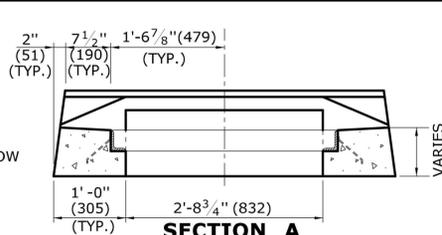
DRAWING TITLE:
**SIGN FACE SHEET ALUMINUM
S&W SERIES SIGNS TYPICAL DETAILS**

SHEET NO.
C8.6

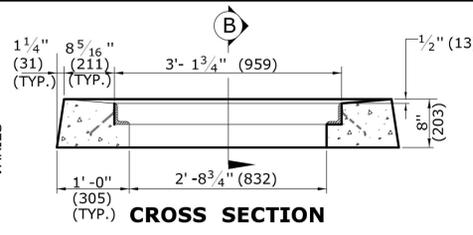
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**CROSS SECTION
TYPE "C" CATCH BASIN TOP**

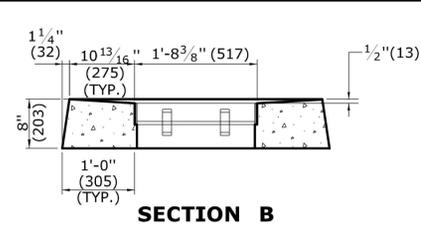


SECTION A



CROSS SECTION

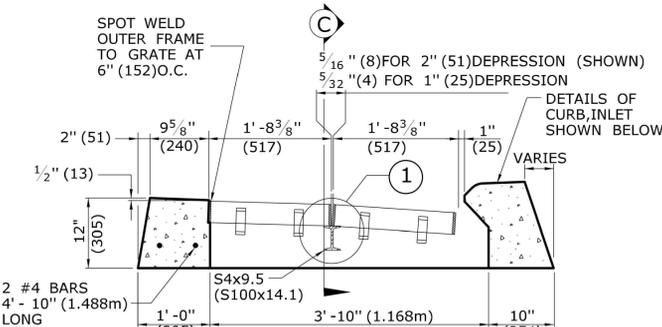
TYPE "C-L" CATCH BASIN TOP



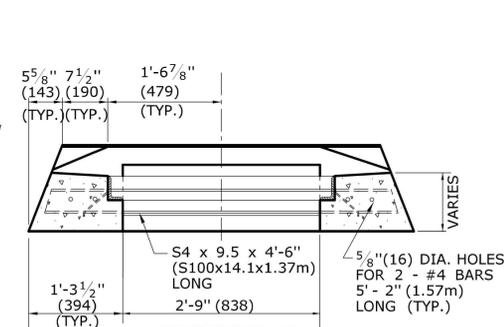
SECTION B

GENERAL NOTES:

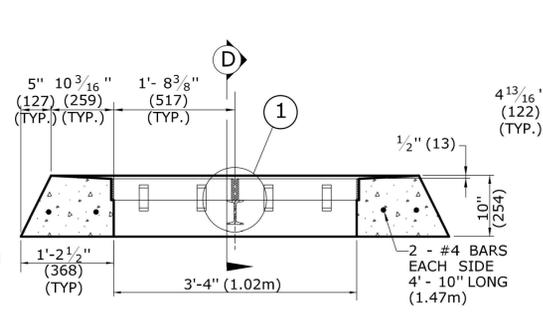
1. FOR DETAILS OF FRAME AND GRATE SEE STANDARD SHEET HW-507-08.
2. ALL STEEL, EXCEPT REINFORCING BARS, SHALL BE GALVANIZED IN CONFORMANCE WITH SECTION M06.03 OF CONNECTICUT'S STANDARD SPECIFICATIONS.
3. ALL BARS SHALL HAVE A MINIMUM 2" (51) COVER.



**CROSS SECTION
TYPE "C" CATCH BASIN DOUBLE GRATE - TYPE I TOP**

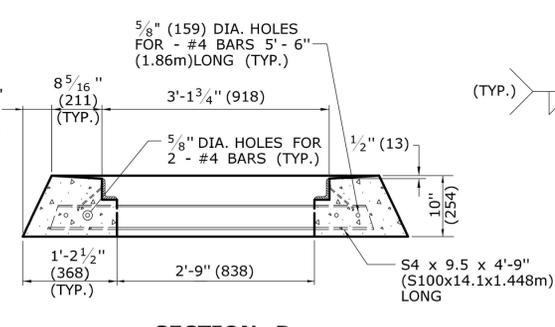


SECTION C

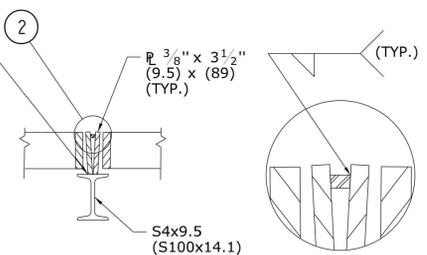


CROSS SECTION

TYPE "C-L" CATCH BASIN DOUBLE GRATE - TYPE I TOP

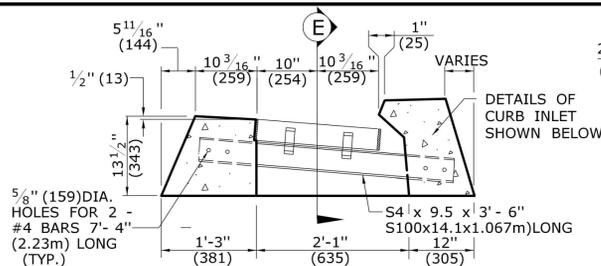


SECTION D

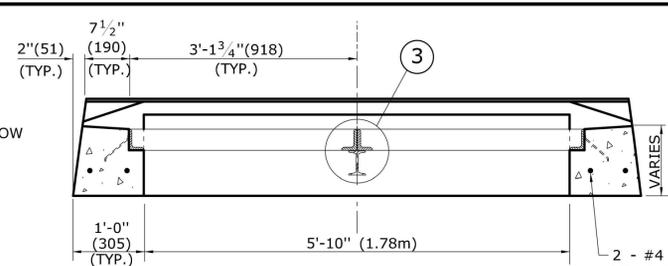


DETAIL "1"

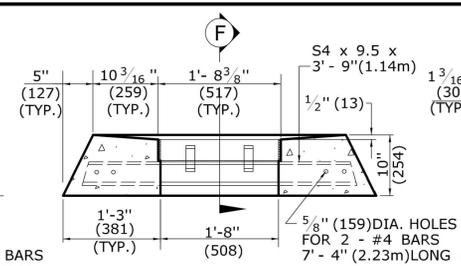
DETAIL "2"



**CROSS SECTION
TYPE "C" CATCH BASIN DOUBLE GRATE - TYPE II TOP**

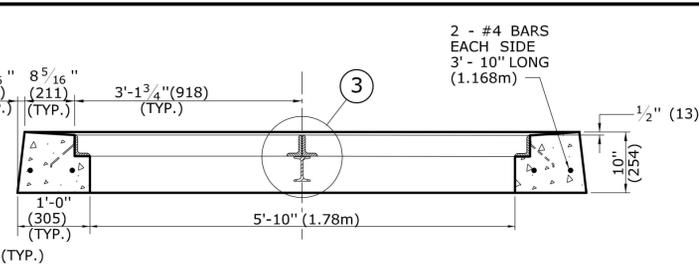


SECTION E

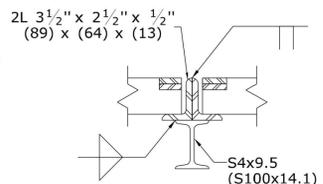


CROSS SECTION

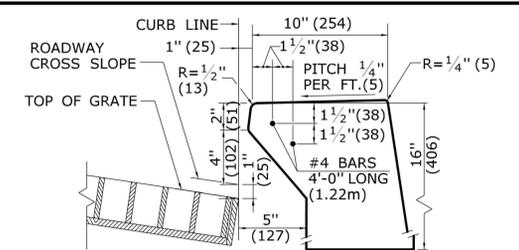
TYPE "C-L" CATCH BASIN DOUBLE GRATE - TYPE II TOP



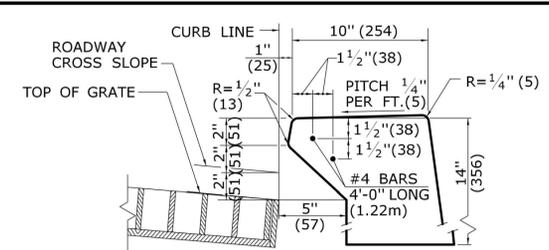
SECTION F



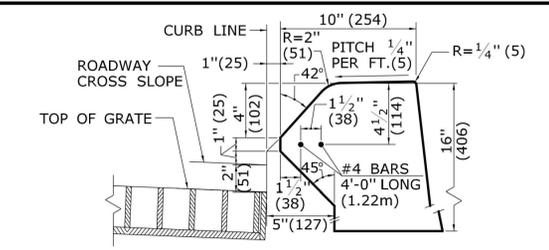
DETAIL "3"



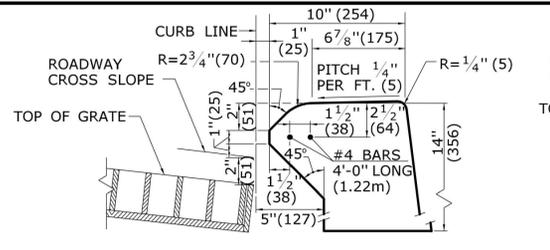
**INLET WITH 6" (152) CONCRETE OR
STONE CURBING FOR TYPE "C" CB**



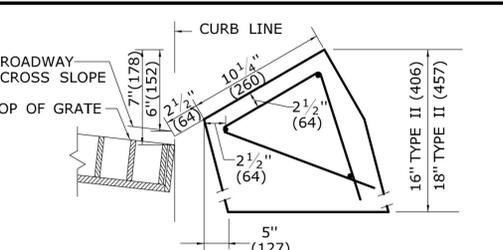
**INLET WITH NO CURBING
(PLAIN TYPE) FOR TYPE "C" CB**



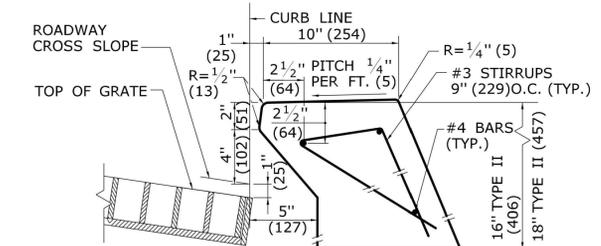
**INLET WITH 6" (152) BITUMINIOUS
CONCRETE LIP CURBING FOR TYPE "C" CB**



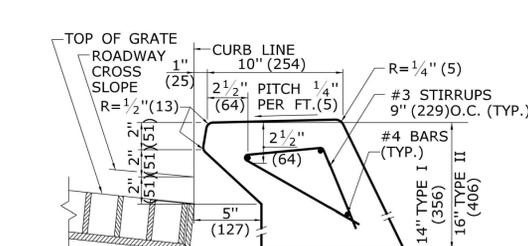
**INLET WITH 4" (102) CONCRETE
PARK CURBING FOR TYPE "C" CB**



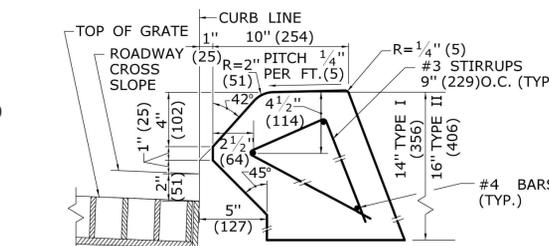
**INLET WITH GRANITE
SLOPE CURB FOR TYPE "C" CB**



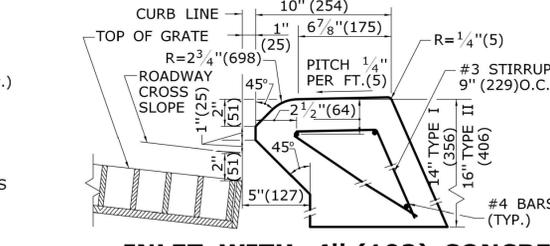
**INLET WITH 6" (152) CONCRETE OR
STONE CURBING FOR TYPE "C" CB
DOUBLE GRATE TYPE I & II**



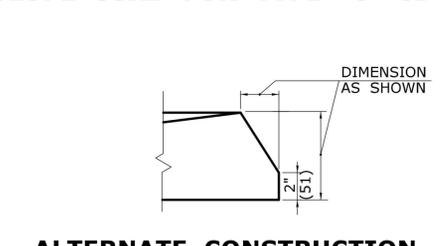
**INLET WITH NO CURBING
(PLAIN TYPE) FOR TYPE "C" CB
DOUBLE GRATE TYPE I & II**



**INLET WITH 6" (152) BITUMINIOUS
CONCRETE LIP CURBING FOR TYPE "C" CB
DOUBLE GRATE TYPE I & II**



**INLET WITH 4" (102) CONCRETE
PARK CURBING FOR TYPE "C" CB
DOUBLE GRATE TYPE I & II**



**ALTERNATE CONSTRUCTION
OF TYPE II TOP**

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
2	7/28/11	REMOVE MIN. DROP NOTE
1	6/01/10	REVISE CALL-OUT

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

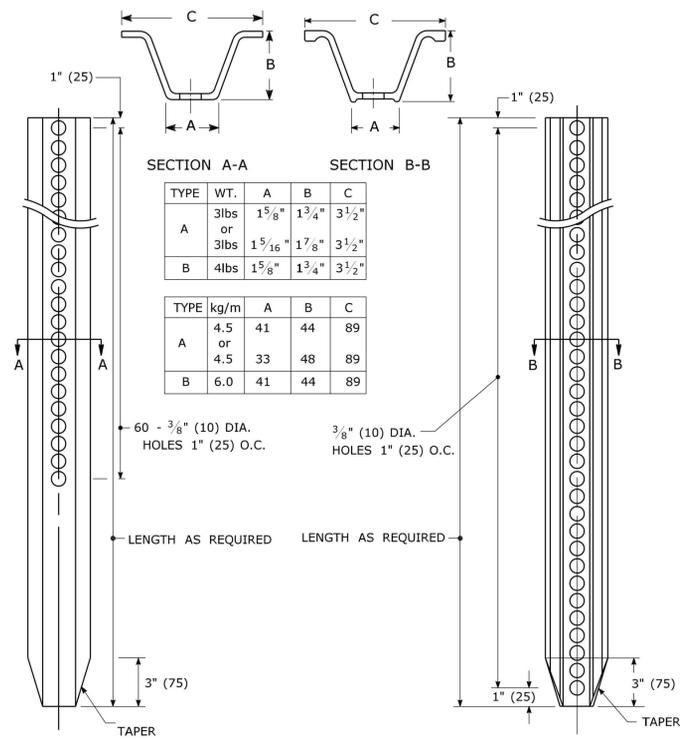
SUBMITTED BY:	NAME/DATE/TIME:
Leo Fontaine	2011.11.10 10:04:01 -0500
APPROVED BY:	NAME/DATE/TIME:
James H. Norman	2011.11.10 10:19:36 -0500

CTDOT
STANDARD SHEET
OFFICE OF ENGINEERING

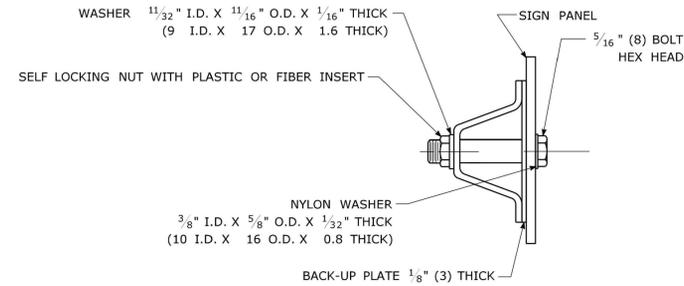
TYPE "C" & "C-L" CATCH BASIN
TOPS AND CURBS

STANDARD SHEET NO.:
HW-507_07
C8.8

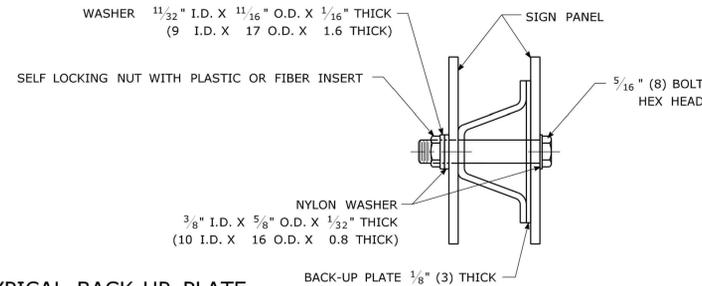
TYPICAL METAL SIGN POSTS



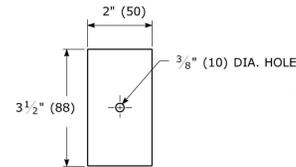
TYPICAL SIGN PANEL ATTACHMENT



TYPICAL BACK TO BACK SIGN PANEL ATTACHMENT



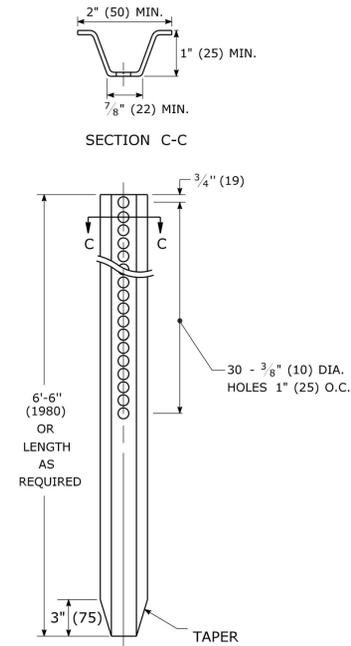
TYPICAL BACK-UP PLATE



BOLTS - STAINLESS STEEL CONFORMING TO ASTM F593, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).
 SELF LOCKING NUTS - STAINLESS STEEL CONFORMING TO ASTM F594, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).
 WASHERS - STAINLESS STEEL CONFORMING TO ASTM A240, (ALLOY TYPES 304 OR 316).

METAL DELINEATOR POST

WT./FT. = 1.12 LBS. MIN.
 (MASS/m = 1.67 kg/m MIN.)



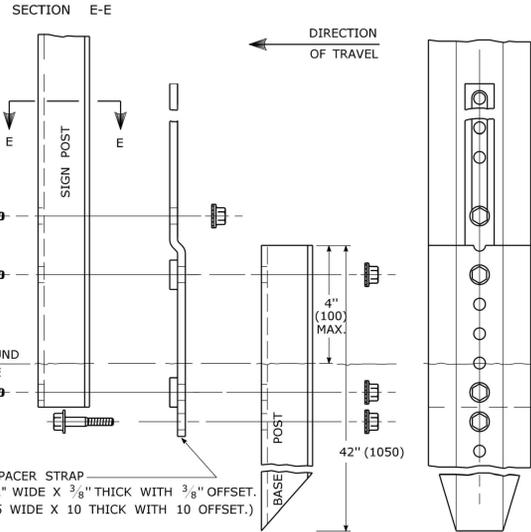
GENERAL NOTES:

- STEEL FOR DELINEATOR POSTS SHALL BE ASTM A36/A36(m) STEEL. STEEL FOR ALL OTHER POSTS SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A 499 GRADE 60 AND TO THE CHEMICAL REQUIREMENTS OF ASTM A1 CARBON STEEL TEE RAIL HAVING NOMINAL WEIGHT (MASS) OF 91lbs. (45 kg.) OR GREATER PER LINEAR YARD (METER).
- AFTER FABRICATION, ALL STEEL POSTS, STRAPS AND PLATES SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A123/A123(m).
- WASHERS FOR BREAKAWAY INSTALLATIONS SHALL MEET ASTM F436, TYPE 1.
- ALL BOLTS, NUTS, AND WASHERS FOR BREAKAWAY INSTALLATIONS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A153/A153(m).
- ALL SIGN POSTS SHALL HAVE BREAKAWAY FEATURES THAT MEET AASHTO REQUIREMENTS CONTAINED IN THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS." THE BREAKAWAY FEATURES SHALL BE STRUCTURALLY ADEQUATE TO CARRY THE SIGNS SHOWN IN THE PLANS AT 60 mph (97 km/h) WIND LOADINGS. INSTALLATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
- TYPE A POSTS - 3 lbs/ft (4.5 kg/m) TYPE B POSTS - 4 lbs/ft (6 kg/m).

BREAKAWAY TYPE I INSTALLATION

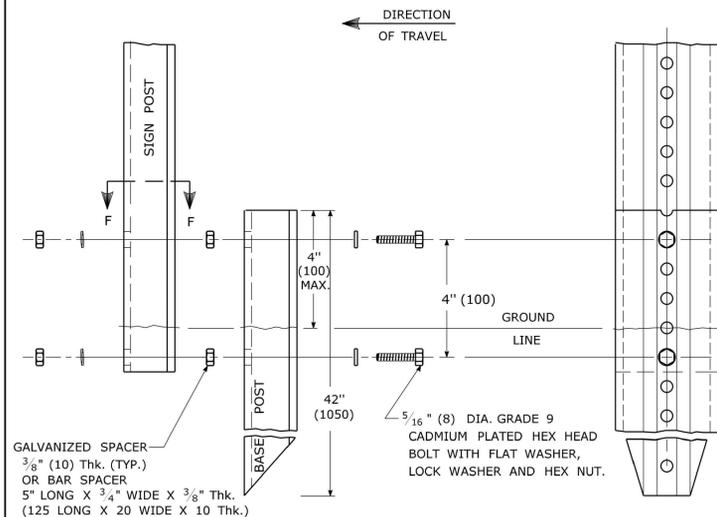
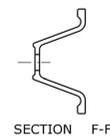
FOR 3 & 4 LB. POSTS
 (FOR 4.5 & 6.0 kg/m POSTS)

BOLTS - HEX HEAD, INTEGRAL FLANGE CONFORMING TO ASTM A354, SIZE IS 5/16" (8) - 18 UNC X 1 3/4" (44) GRADE BC FOR 3 LBS/FT (4.5kg/m) POSTS & 5/16" (8) - 18 UNC X 2" (50) GRADE BD FOR 4 LBS./FT (6.0 kg/m) POSTS.
 NUTS - 5/16" (8) - 18 UNC HEX HEAD, INTEGRAL FLANGE CONFORMING TO ASTM A563, GRADE DH.
 LOCKWASHERS - 3/8" (10) HEAVY DUTY EXTERNAL TYPE.



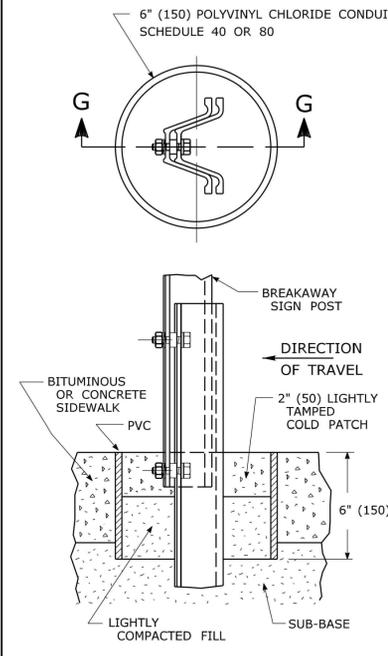
BREAKAWAY TYPE II INSTALLATION

FOR 3 & 4 LB. POSTS
 (FOR 4.5 & 6.0 kg/m POSTS)

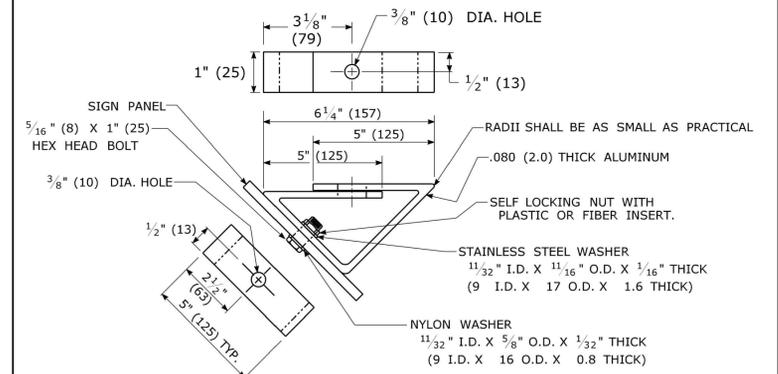


GALVANIZED SPACER - 3/8" (10) THK. (TYP.) OR BAR SPACER - 5" LONG X 3/4" WIDE X 3/8" THK. (125 LONG X 20 WIDE X 10 THK.)
 5/16" (8) DIA. GRADE 9 CADMIUM PLATED HEX HEAD BOLT WITH FLAT WASHER, LOCK WASHER AND HEX NUT.

TYPICAL SLEEVE FOR PAVED AREAS



45° SUBMOUNTING BRACKET



REV.	DATE	REVISION DESCRIPTION
1	2-2011	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

DIMENSIONS ARE IN ENGLISH (") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: - OVER 1" TO NEAREST 5 mm - UNDER 1" TO NEAREST 1 mm.

STATE OF CONNECTICUT
 DEPARTMENT OF TRANSPORTATION

NOT TO SCALE

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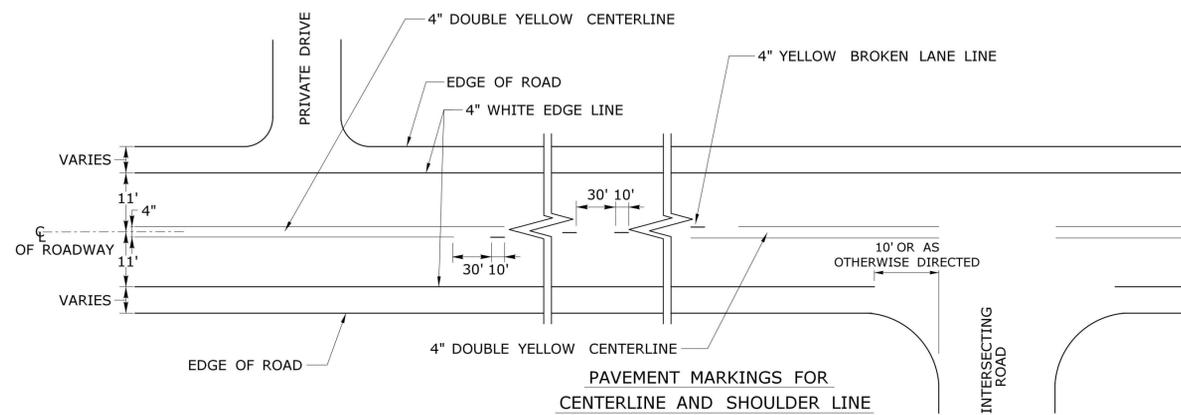
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 NAME/DATE/TIME: 2011.03.22 13:20:01 -04'00'

APPROVED BY: John F. Carey
 NAME/DATE/TIME: 2011.03.24 09:53:08 -04'00'

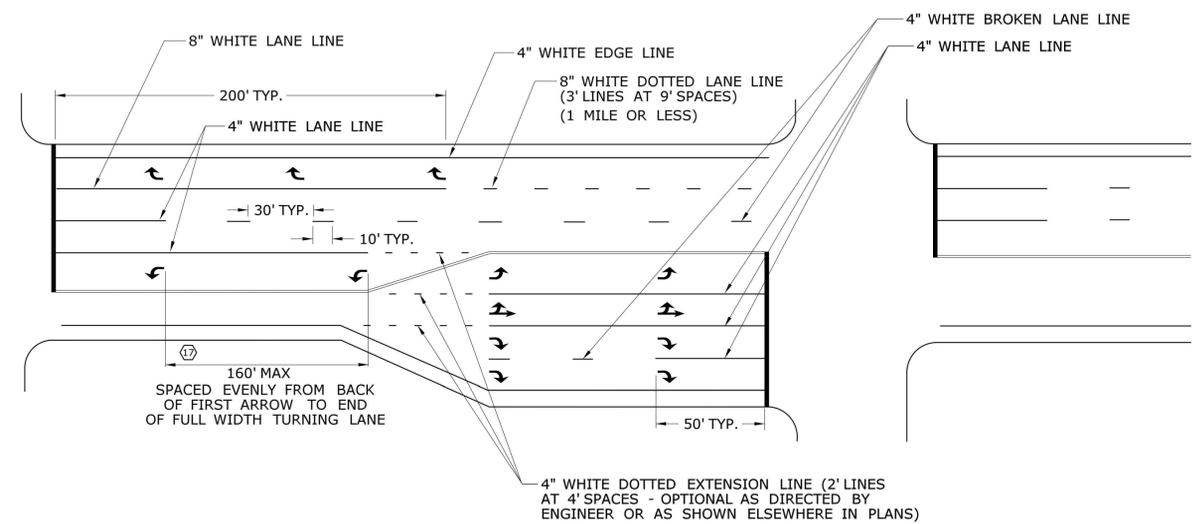
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 STANDARD SHEET
 OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
METAL SIGN POSTS AND SIGN MOUNTING DETAILS

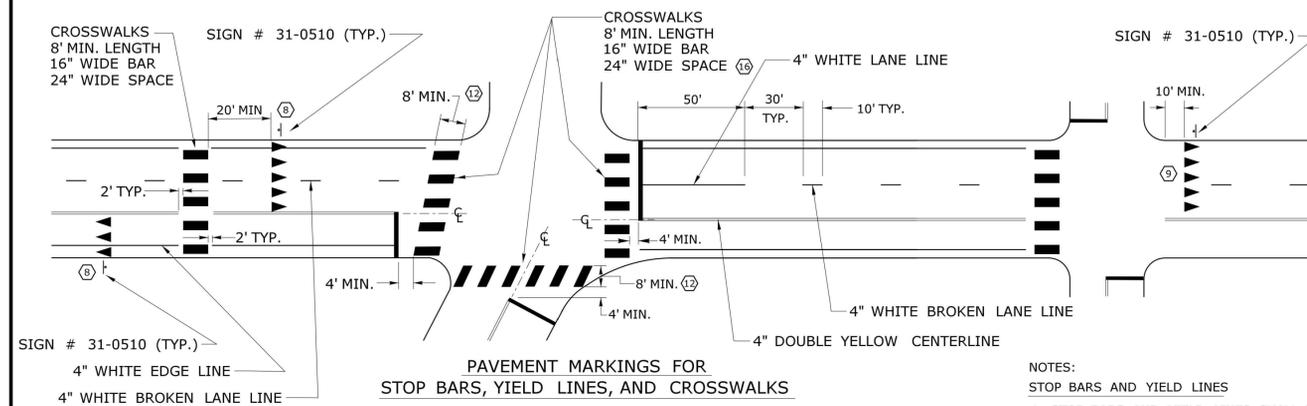
STANDARD SHEET NO.:
TR-1208_02 C8.9



PAVEMENT MARKINGS FOR CENTERLINE AND SHOULDER LINE



PAVEMENT MARKINGS FOR TURNING LANES AND ROAD LANE-DROP



PAVEMENT MARKINGS FOR STOP BARS, YIELD LINES, AND CROSSWALKS

NOTES:
STOP BARS AND YIELD LINES

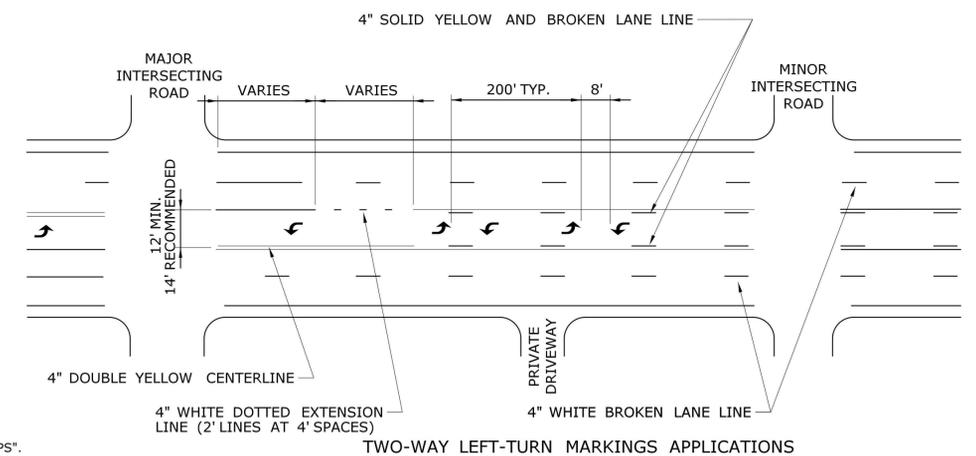
- STOP BARS AND YIELD LINES SHALL BE WHITE.
- STOP BARS SHALL BE 12" MIN. UNLESS OTHERWISE NOTED ON PLANS.
- STOP BARS TO BE PLACED A MINIMUM OF 4' IN ADVANCE OF THE NEAREST EDGE OF CROSSWALK AND SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY.
- IN THE ABSENCE OF A MARKED CROSSWALK THE STOP BAR SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY, AT THE DESIRED STOPPING POINT AT LEAST 5' AND NO MORE THAN 30' FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY.
- THE STOP SIGN SHOULD BE PLACED IN LINE WITH THE STOP BAR. HOWEVER, IF THE STOP SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO STOP, THE STOP BAR SHOULD BE PLACED AT THE STOPPING POINT.
- FOR STOP BARS AT RAMP SEE DETAILS "R", "S", "T", & "U" AND NOTES ON TRAFFIC STANDARD SHEET TR-1210 07 "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS EXIT RAMP".
- FOR YIELD LINE INSTALLATIONS, ONLY FULL TRIANGLES ARE TO BE INSTALLED.
- MID-BLOCK CROSSWALKS ARE CROSSWALKS LOCATED MORE THAN 50 FEET FROM A SIGNALIZED OR UNSIGNALIZED INTERSECTION. YIELD LINES ASSOCIATED WITH MIDBLOCK CROSSWALKS SHOULD BE INSTALLED 20 TO 50 FEET IN ADVANCE OF THE NEAREST CROSSWALK LINE OR AS DIRECTED BY THE ENGINEER. SIGN # 31-0510 IS REQUIRED.
- FOR CROSSWALKS AT UNSIGNALIZED INTERSECTIONS WITH MINOR STREET STOP CONTROL, YIELD LINES SHALL BE INSTALLED ON MULTI-LANE APPROACHES, BUT NOT SINGLE LANE APPROACHES.
- THE YIELD SIGN SHOULD BE PLACED IN LINE WITH A YIELD LINE. HOWEVER, IF THE YIELD SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO YIELD, THE YIELD LINE SHOULD BE PLACED AT THE YIELDING POINT.

CROSSWALKS

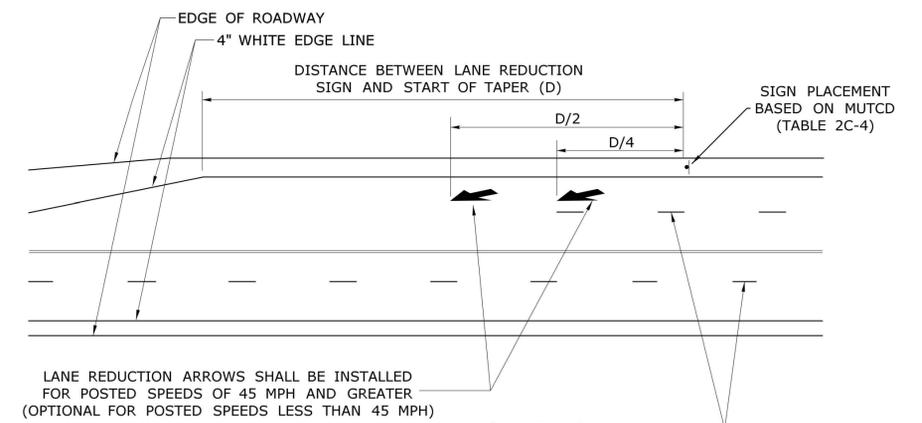
- CROSSWALK MARKINGS SHALL BE WHITE.
- AT LOCATIONS WHERE THE CROSSWALK IS SKEWED, BARS TO BE PARALLEL TO ϕ AND ENDS OF BARS TO BE PARALLEL. THE LENGTH OF THE BARS WILL VARY DEPENDING ON THE ANGLE OF SKEW.
- BARS SHOULD BE NO CLOSER THAN 1' FROM EDGE OF ROAD.
- ONLY FULL LENGTH BARS ARE TO BE INSTALLED.
- DECORATIVE CROSSWALKS SHALL BE BANDED FROM CURB TO CURB WITH A MINIMUM 12" WIDE WHITE TRANSVERSE LINE ALONG EACH EDGE.
- 24" WIDE SPACE TO BE CENTERED ON YELLOW CENTERLINE.

PAVEMENT MARKINGS FOR TURNING LANES

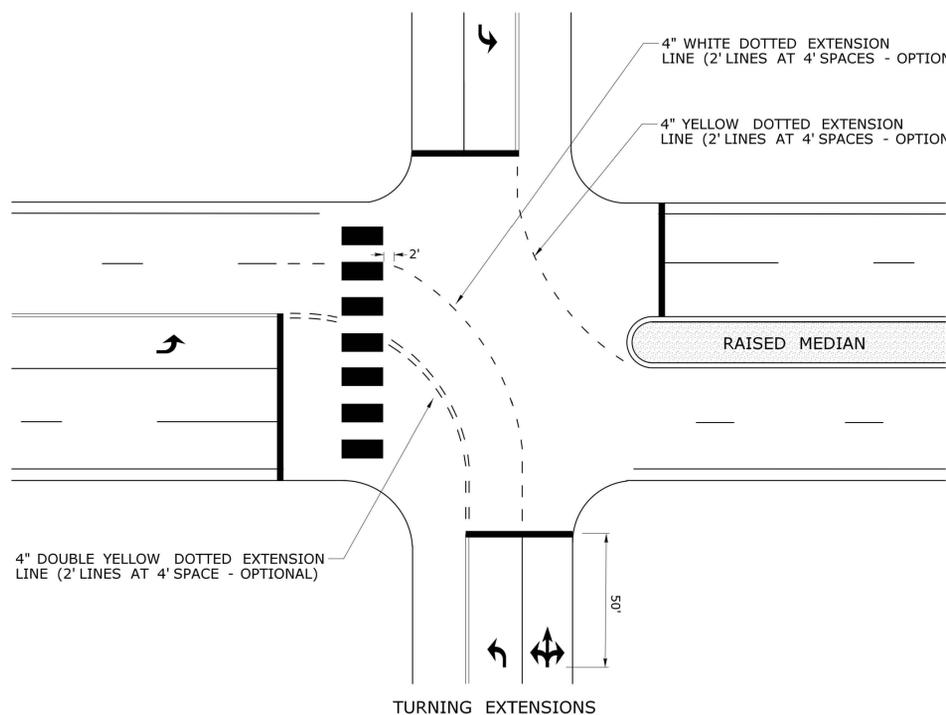
- INSTALL AT LEAST TWO ARROWS PER LANE WHERE STORAGE LENGTH IS GREATER THAN 150 FEET.



TWO-WAY LEFT-TURN MARKINGS APPLICATIONS



LANE REDUCTION TRANSITIONS



TURNING EXTENSIONS

REV.	DATE	REVISION DESCRIPTION

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 4/3/2017

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: TR-1210_08.DGN Model: TR-1210_05

SUBMITTED BY: Mark F. Makuch, P.E. 2017.04.19 11:43:11-0400'

APPROVED BY: Gregory M. Dorosh, P.E. 2017.04.20 13:25:03-0400'

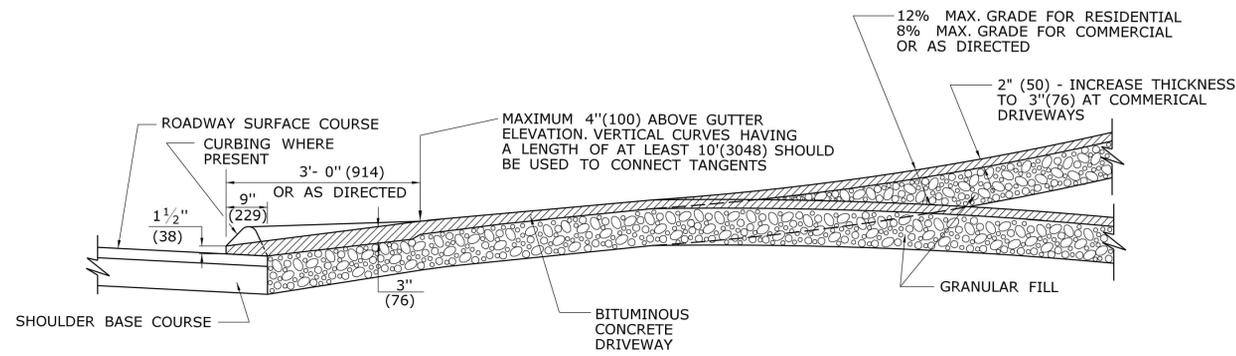
CTDOT
STANDARD SHEET
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
PAVEMENT MARKINGS FOR NON FREEWAYS

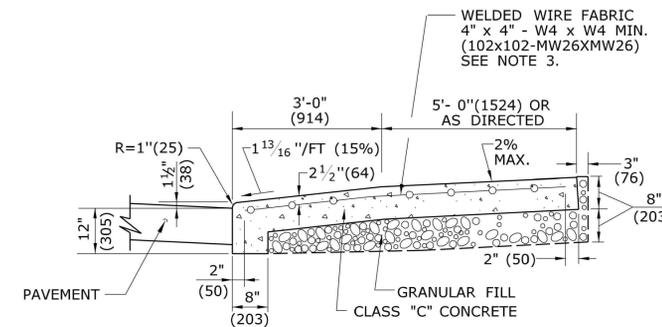
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C8.10

GENERAL NOTES:

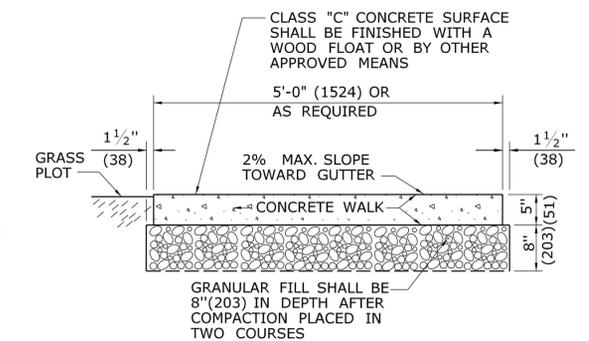
1. DRIVEWAY ENTRANCE SHALL BE A MINIMUM OF 12' (3658) WIDE, EXCLUDING CURBING WHEN PRESENT.
2. SIDEWALK RAMP SHALL BE A MINIMUM OF 36" (914) TO 40" (1016) MAXIMUM, WITH A MAXIMUM SLOPE OF 12:1. THERE SHALL BE NO LIP AT THE DRIVEWAY SIDEWALK INTERFACE.
3. WELDED WIRE FABRIC MATS WITH REINFORCING AT CLOSER SPACING MAY BE USED.



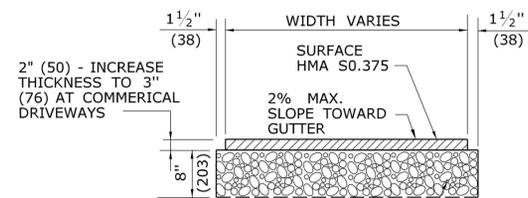
SECTION A



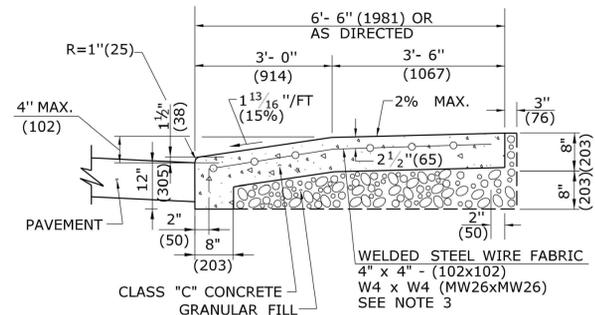
SECTION C



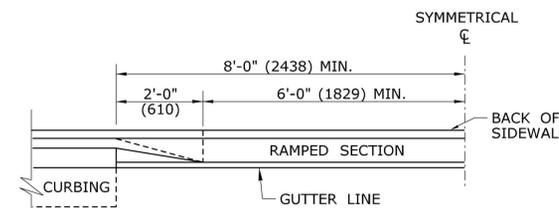
SECTION D



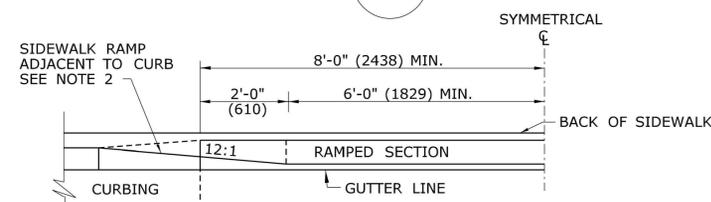
**TYPICAL SECTION
BITUMINOUS CONCRETE
SIDEWALK AND DRIVE**



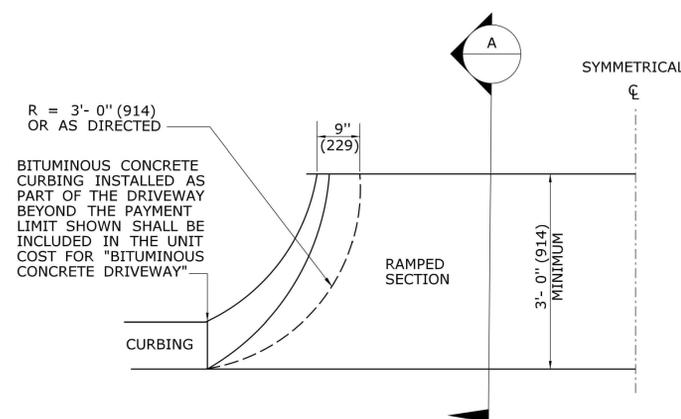
SECTION B



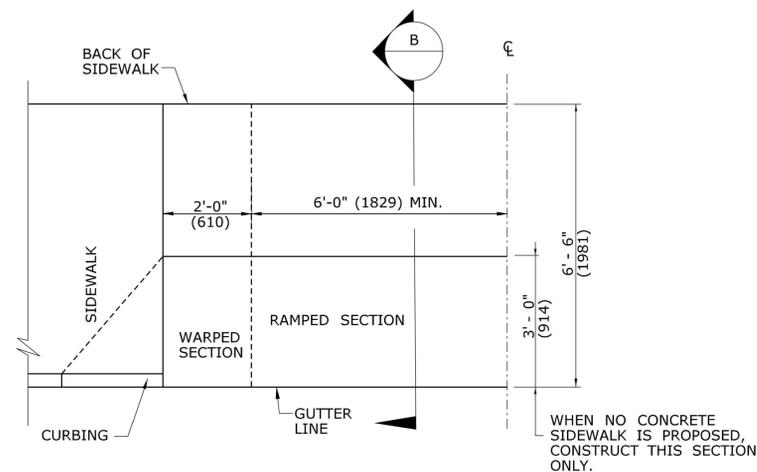
HALF ELEVATION



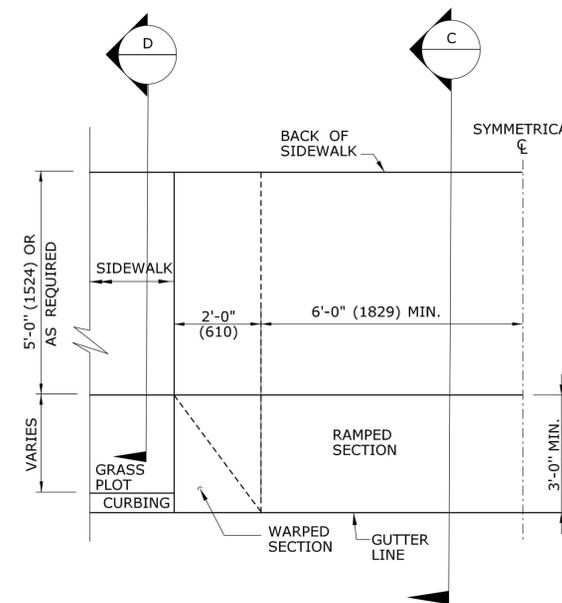
HALF ELEVATION



**HALF BITUMINOUS CONCRETE
DRIVEWAY PLAN**



**HALF PLAN OF
CONCRETE DRIVEWAY RAMP WHERE
SIDEWALK ADJOINS CURBING**



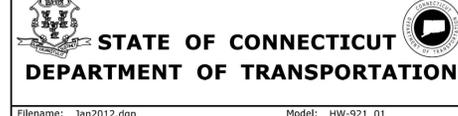
**HALF PLAN OF
CONCRETE DRIVEWAY RAMP WHERE
CURB IS SEPARATED FROM
SIDEWALK BY GRASS PLOT**

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
1	6/01/10	REVISED BORDER TITLE
2	6/01/10	REVISED HALF ELEVATION DETAILS
3	1/12	REVISE 2% MAX. SLOPE NOTE
-	-	-
-	-	-
-	-	-

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NOT TO SCALE



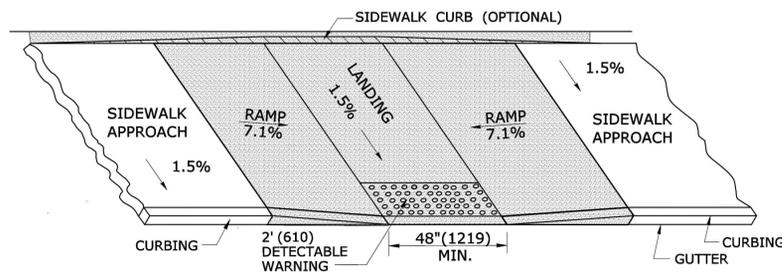
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Leo Fontaine
2012.01.13 13:14:45 -05'00'

APPROVED BY: NAME/DATE/TIME:
James H. Norman
2012.01.26 15:32:44 -05'00'

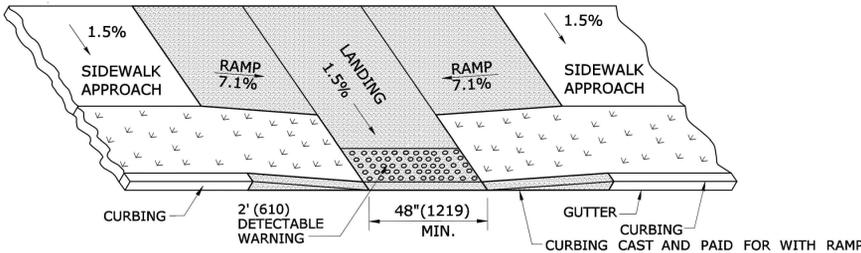
CTDOT
STANDARD SHEET
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
DRIVEWAY RAMPS AND SIDEWALKS

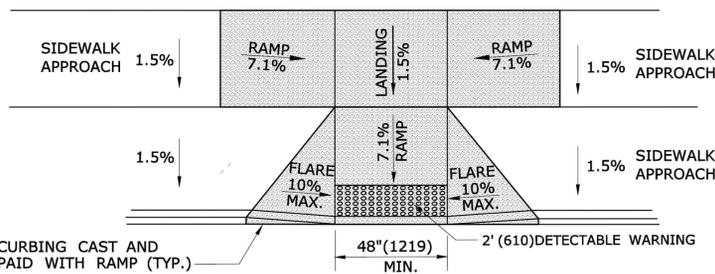
STANDARD SHEET NO.:
HW-921_01
C8.11



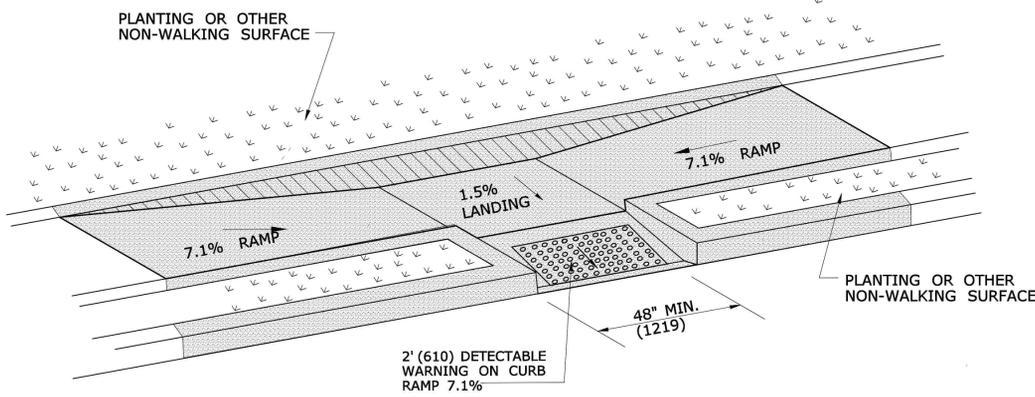
PARALLEL SIDEWALK RAMP (TYPE 1) NO UTILITY STRIP



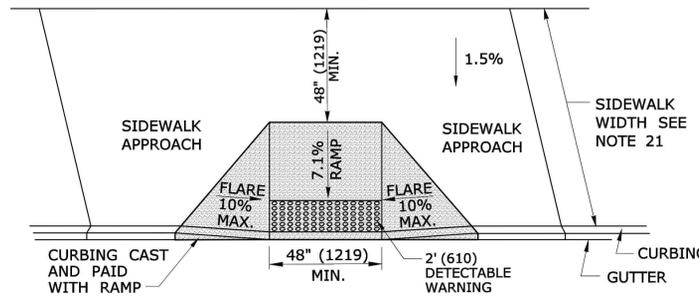
PARALLEL SIDEWALK RAMP (TYPE 1a) WITH UTILITY / GRASS STRIP



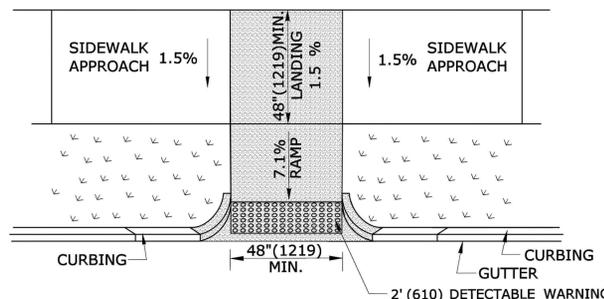
PARALLEL/PERPENDICULAR SIDEWALK RAMP NO UTILITY/GRASS STRIP (TYPE 1b)



PARALLEL SIDEWALK RAMP (TYPE 1c) WITH UTILITY / GRASS STRIP

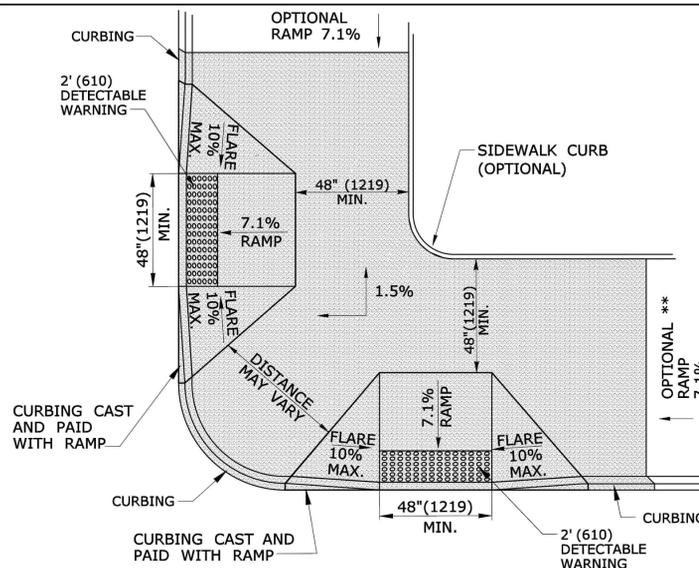


PERPENDICULAR SIDEWALK RAMP W/ 48" (1219) MIN. BY PASS LANDING (TYPE 2)



PERPENDICULAR SIDEWALK RAMP W/CURB RETURNS / UTILITY GRASS STRIP (TYPE 2a)

* OPTIONAL FLARE ONE SIDE OF RAMP

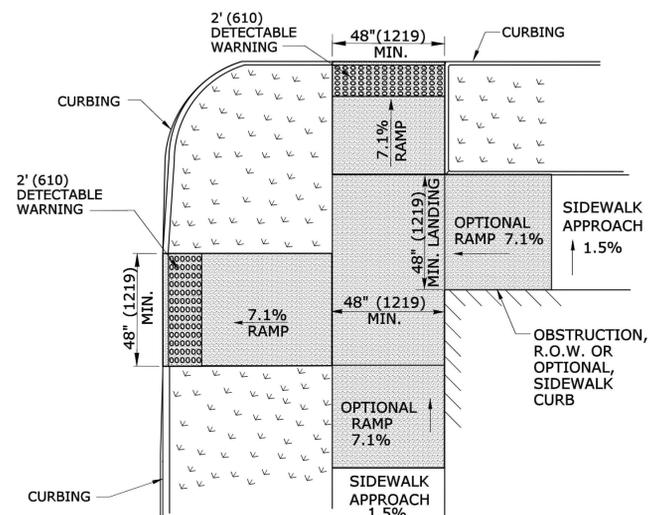


DUAL PERPENDICULAR SIDEWALK RAMP (TYPE 3)

SEE NOTES 19
* OPTIONAL CURB RETURN ON ONE SIDE OF RAMP
** SEE NOTE 23

GENERAL NOTES:

- MAXIMUM SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE SIDEWALK RAMP SHOULD NOT EXCEED 5%. THE MAXIMUM GRADE DIFFERENCE BETWEEN THE GUTTER AND CURB RAMP SHALL NOT EXCEED 13%. SEE DETAIL 1 ON SHEET 4.
- RAMP GRADE SHALL BE UNIFORM, FREE OF SAGS AND ABRUPT GRADE CHANGES. RUNNING SLOPES OF RAMPS SHALL NOT EXCEED 8.33% AND SHALL NOT EXCEED 15' (4.5m) WITHOUT PROVIDING A LANDING.
- ALL RAMPS SHALL BE CONSTRUCTED OF CLASS "F" CONCRETE IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
- SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP. THE SURFACE OF ALL SIDEWALK RAMPS SHALL BE STABLE, FIRM AND SLIP RESISTANT. SURFACE DISCONTINUITIES SHALL NOT EXCEED 1/2" (13) MAX. VERTICAL DISCONTINUITIES BETWEEN 1/4" (6.4) AND 1/2" (13) MAX. SHALL BE BEVELED 1:2 MINIMUM APPLIED ACROSS THE ENTIRE LEVEL CHANGE.
- DIAGONAL SIDEWALK RAMPS AT MARKED CROSSINGS SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS, EXCLUDING ANY FLARED SIDES. DIAGONAL AND PERPENDICULAR RAMPS SHALL HAVE THE RAMP CUT PERPENDICULAR TO THE TANGENT OF THE CURB RADIUS FOR THE DESIGNATED ACCESSIBLE ROUTE. BOTH LONGITUDINAL SIDES OF THE RAMP SHOULD BE THE SAME LENGTH. SKEWED RAMPS SHOULD BE AVOIDED. FLARES ARE NOT CONSIDERED PART OF PEDESTRIAN ACCESS ROUTE. DIAGONAL RAMPS SHOULD NOT BE INSTALLED WHERE CURB RADII IS LESS THAN 20'(6096).
- REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT. 8.3% MAXIMUM SLOPE MAY NOT BE ACHIEVABLE DUE TO EXISTING SIDEWALK GRADE. IN RECOGNITION OF THIS, A LIMIT OF 15' (4572) FOR REMOVAL SHALL BE USED UNLESS OTHERWISE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER. SAW CUT REQUIRED FOR DUMMY JOINTS SHALL BE INCLUDED IN THE COST OF "CONCRETE SIDEWALK RAMP" OR "CONCRETE SIDEWALK".
- EXPANSION JOINTS IN CONCRETE SHALL MATCH THOSE IN ADJACENT SIDEWALKS BUT IN NO CASE SHALL THE SPACING BETWEEN EXPANSION JOINTS EXCEED 12' (3658) UNLESS OTHERWISE NOTED.
- CONCRETE SIDEWALK RAMPS, SHALL BE PAID FOR UNDER THE ITEM "CONCRETE SIDEWALK RAMP", AS DEFINED BY THE CONSTRUCTION LIMITS ON THE PLANS AND SHALL BE FIELD VERIFIED.
- SIDEWALK RAMPS SHALL BE CONSTRUCTED WITH THE TOE AT THE GUTTER CAST INTEGRALLY WITH RAMP UNLESS DIRECTED OTHERWISE BY THE ENGINEER (SEE TYPICAL SECTION ON SHEET 3). CURB REMOVAL AND CAST IN PLACE CURBING REQUIRED FOR THE RAMP, SHALL BE INCLUDED WITH PAY ITEM "CONCRETE SIDEWALK RAMP". CURBING OUTSIDE LIMITS OF RAMP OR LANDING SHOWN ON SHEET 3 SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
- PREFERRED LOCATION TO INSTALL DETECTABLE WARNING STRIP SHALL BE 6" (152) FROM THE EDGE OF ROAD ALONG THE FULL WIDTH OF THE RAMP. FOR ALTERNATE LOCATIONS, REFER TO DETECTABLE WARNING PLACEMENT DETAILS ON SHEET 4.
- TO PERMIT WHEELCHAIR WHEELS TO ROLL BETWEEN DOMES, ALIGN DOMES ON A SQUARE GRID IN THE DIRECTION OF RUNNING SLOPE (PERPENDICULAR TO CURB OR SLOPE BREAK). THE TRANSITION FROM RAMP TO GUTTER SHALL BE FLUSH WITHOUT A LIP.
- WHERE COMMERCIAL DRIVEWAYS ARE PROVIDED WITH TRAFFIC SIGNALS AND THE SIDEWALK IS CONTINUOUS THROUGH DRIVEWAY, DETECTABLE WARNINGS ARE REQUIRED AT THE JUNCTION BETWEEN THE PEDESTRIAN ROUTE AND DRIVEWAY.
- CONSTRUCT A SIDEWALK CURB WHEN THERE IS INSUFFICIENT BUFFER AVAILABLE TO GRADE OR WHEN CALLED FOR IN PLANS. PAID FOR WITH SIDEWALK RAMP WHEN REQUIRED FOR GRAD.
- THE TOP AND BOTTOM OF RAMPS SHOULD BE PROVIDED WITH A 4' x 4' (1219 x 1219) MINIMUM LEVEL LANDING AREA WITH A CROSS SLOPE LESS THAN OR EQUAL TO 2% IN ANY DIRECTION.
- UTILITY POLES, LUMINAIRE, PEDESTRIAN OR SIGNAL POLES, GRATES, ACCESS COVERS, AND OTHER APPURTENANCES SHALL NOT BE LOCATED ON RAMPS, LANDINGS, BLENDED TRANSITIONS, AND @ GUTTERS WITHIN THE PEDESTRIAN ACCESS ROUTE.
- APPROACH SIDEWALK WIDTHS, GRASS STRIP OR UTILITY STRIP WIDTHS MAY VARY.
- APPROACH SIDEWALK AND LANDING CROSS SLOPE SHALL NOT EXCEED 2%.
- THE RUNNING OR CROSS SLOPES ON LANDINGS AT MID BLOCK CROSSING MAY BE WARPED TO MEET STREET OR HIGHWAY GRADE.
- FOR PERPENDICULAR CURB RAMPS A MIN. 4'(1.2m) x 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE TOP OF CURB RAMP. WHERE THE LEVEL LANDING IS RESTRICTED AT THE BACK OF SIDEWALK THE LEVEL LANDING SHALL BE 4'(1.2m) x 5'(1.5m) WITH THE 5'(1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE RAMP RUN.
- FOR PARALLEL CURB RAMPS, A MIN. 4'(1.2m) x 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE BOTTOM OF CURB RAMP. IF THE LEVEL LANDING IS RESTRICTED ON 2 OR MORE SIDES, THE LEVEL LANDING SHALL BE 4'(1.2m) x 5'(1.5m) WITH THE 5'(1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE PEDESTRIAN STREET CROSSING.
- WHEN WIDTH OF SIDEWALK IS ≥48" AND A PERPENDICULAR SIDEWALK RAMP IS INSTALLED, THE FLARED SIDES SHALL BE 10% MAX. IF WIDTH OF SIDEWALK IS <48" THE FLARED SIDES MUST NOT EXCEED 8.33% (12:1).
- SHADED AREAS ARE TYPICAL PAY LIMITS FOR CONCRETE SIDEWALK RAMP BUT, MAY VARY AS DIRECTED BY THE ENGINEER.
- OPTIONAL RAMP, WHEN REQUIRED, SHALL BE PAID FOR AS PART OF CONCRETE SIDEWALK RAMP.



DUAL PERPENDICULAR SIDEWALK RAMP (TYPE 3a) WITH UTILITY / GRASS STRIP

SEE NOTE 20

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 6/17/2014
1	7/13	Created new sheets (4 total).		

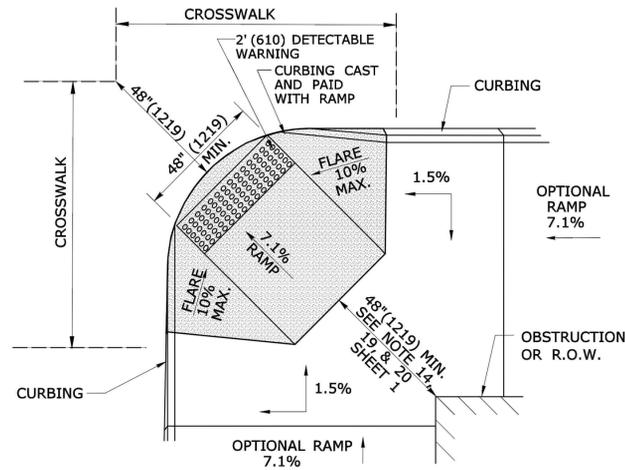
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MGB/EMK
CHECKED BY:
LLF

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

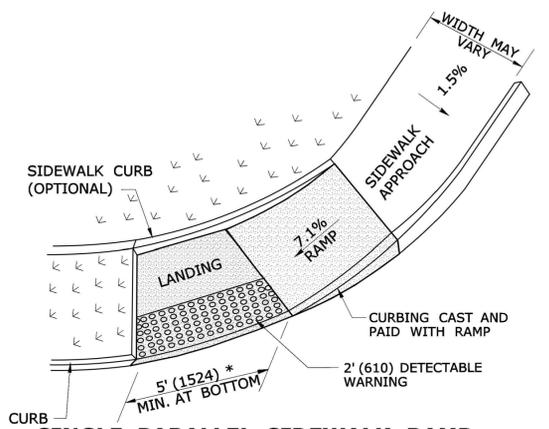
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OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE:
SIDEWALK RAMPS SHEET 1

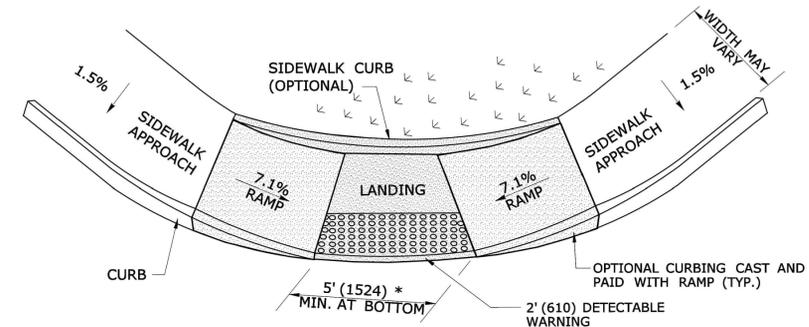
TOWN:
PROJECT NO.
DRAWING NO.
SHEET NO.
C8.12



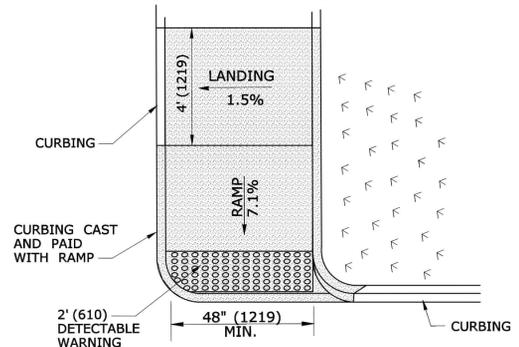
**DIAGONAL SIDEWALK RAMP (TYPE 4)
W/LANDING AT TOP**



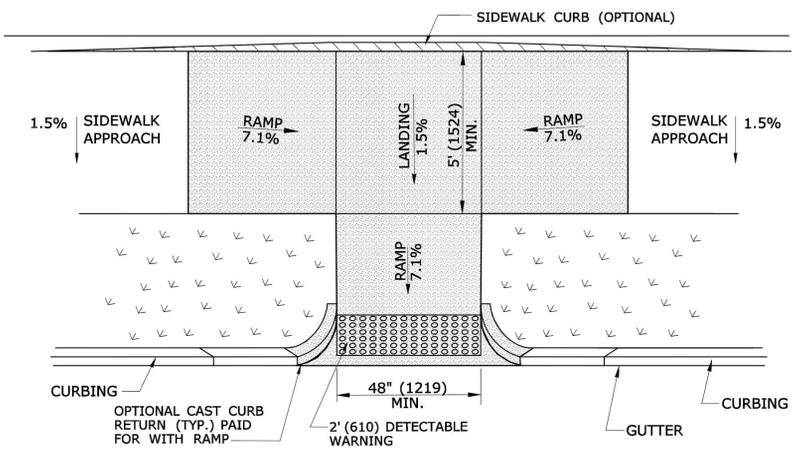
**SINGLE PARALLEL SIDEWALK RAMP
W/LANDING AT BOTTOM ON
CORNER (TYPE 4c)**
* SEE NOTE 20 SHEET 1



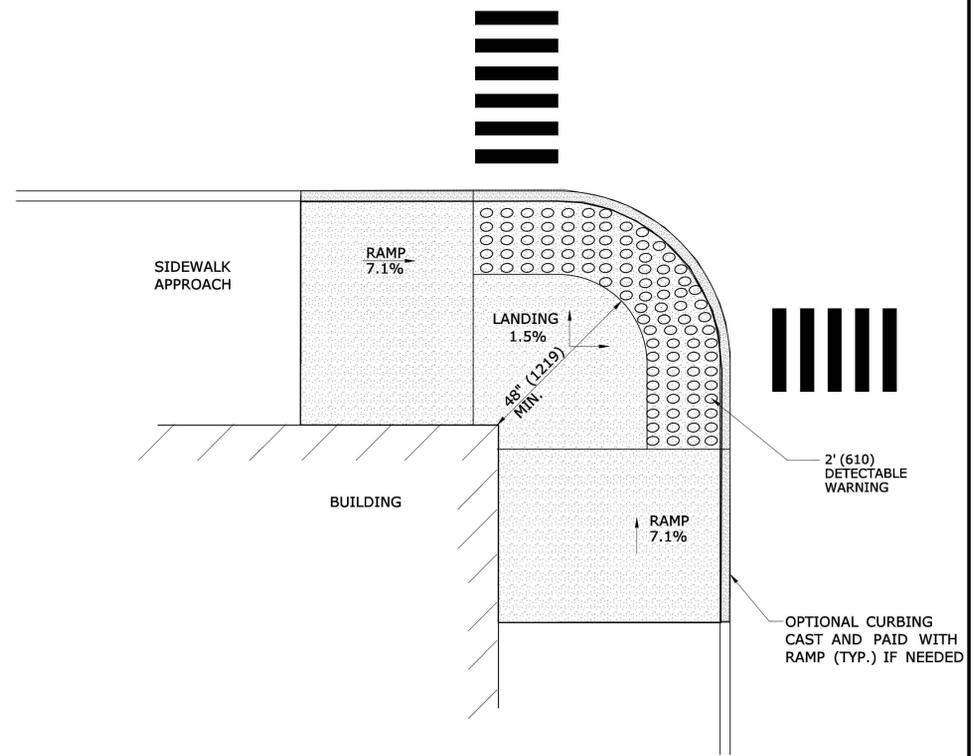
**DOUBLE PARALLEL SIDEWALK RAMP
W/LANDING AT BOTTOM ON CORNER (TYPE 4f)**
* SEE NOTE 20 SHEET 1



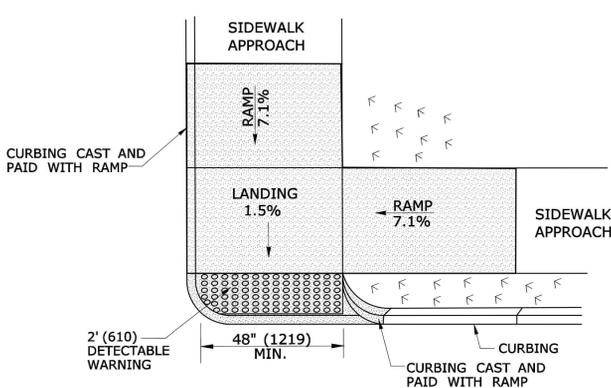
**SINGLE DIRECTION
PERPENDICULAR SIDEWALK RAMP
NO / UTILITY GRASS STRIP
(TYPE 4a)**



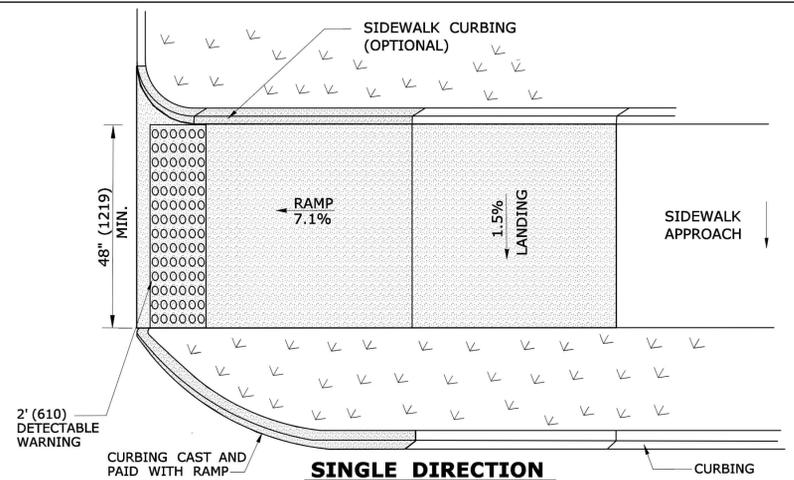
**PARALLEL/PERPENDICULAR SIDEWALK RAMP
COMBINATION W/ CURB RETURNS (TYPE 4d)**
* OPTIONAL FLARE ONE SIDE



**RESTRICTED CONDITION
DIAGONAL SIDEWALK RAMP
(TYPE 4g)**



**DOUBLE DIRECTION
PARALLEL SIDEWALK RAMP
NO / UTILITY GRASS STRIP
(TYPE 4b)**
SEE NOTE 20 SHEET 1



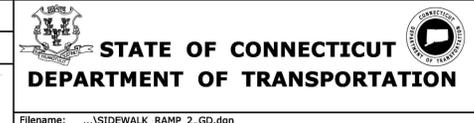
**SINGLE DIRECTION
PERPENDICULAR SIDEWALK RAMP
W/ UTILITY GRASS STRIP (TYPE 4e)**
REFER TO DETECTABLE WARNING PLACEMENT ON SHEET 4

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

Plotted Date: 5/27/2014

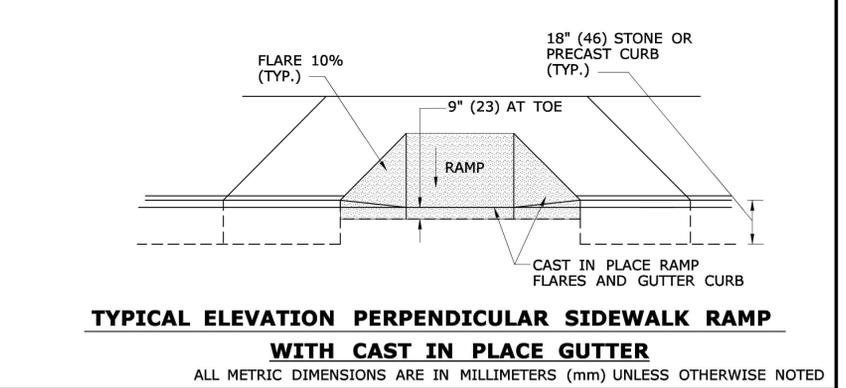
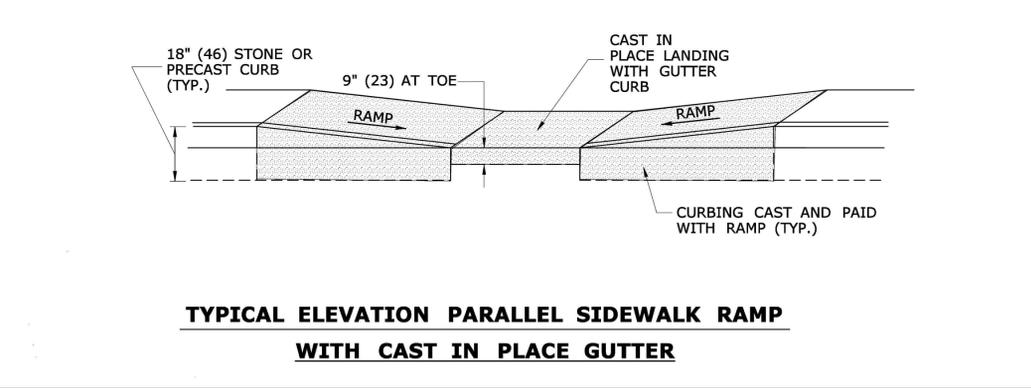
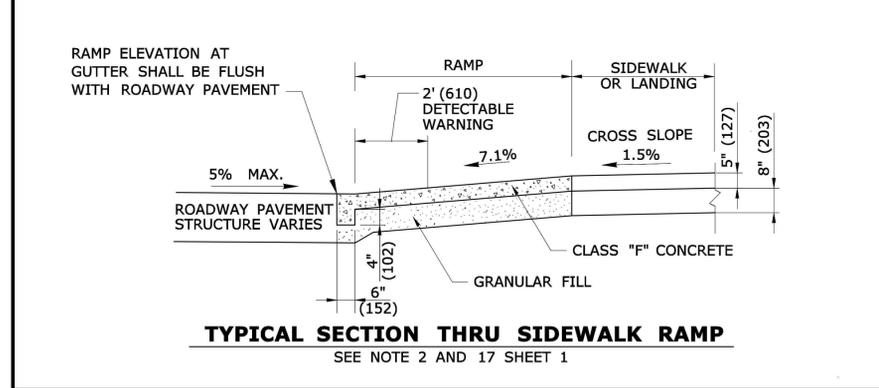
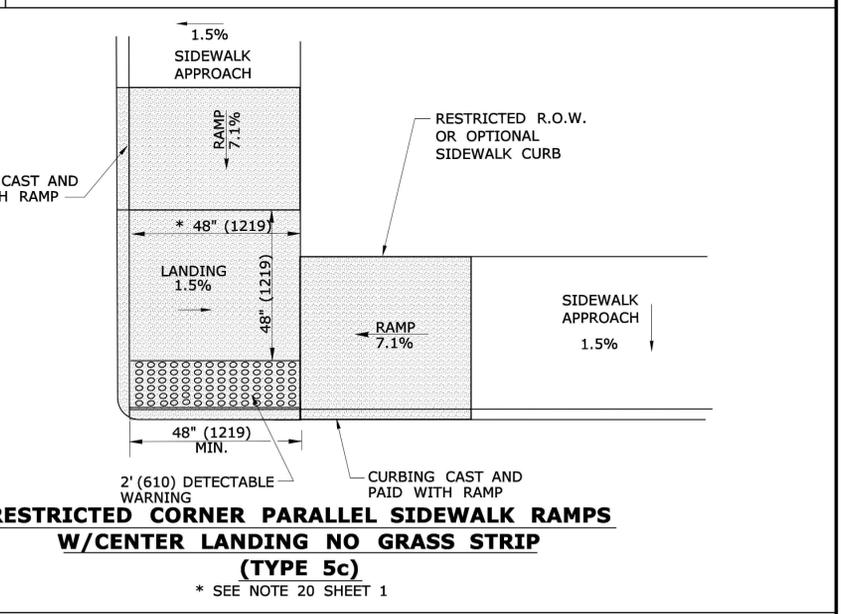
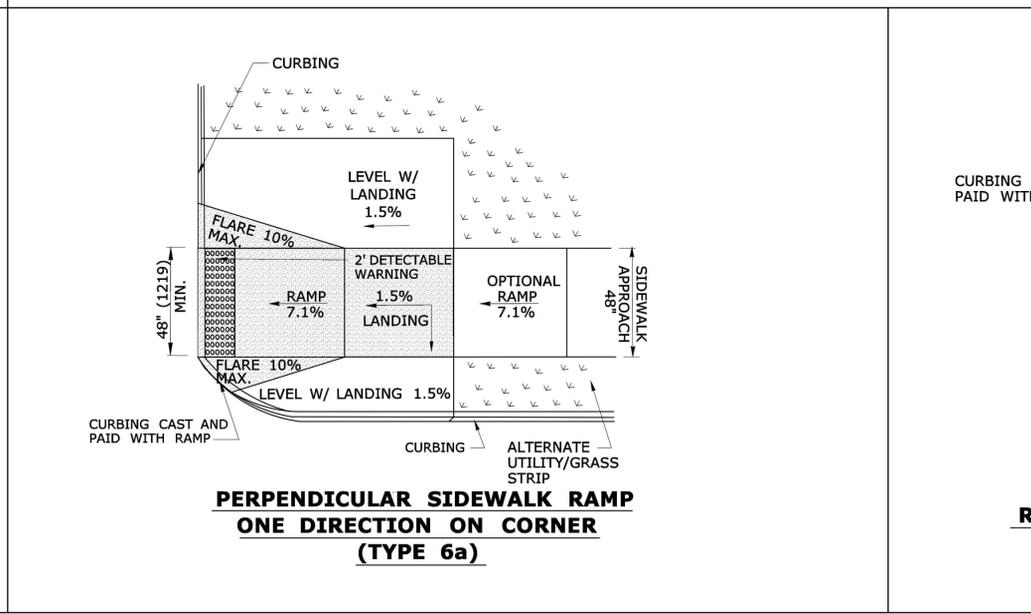
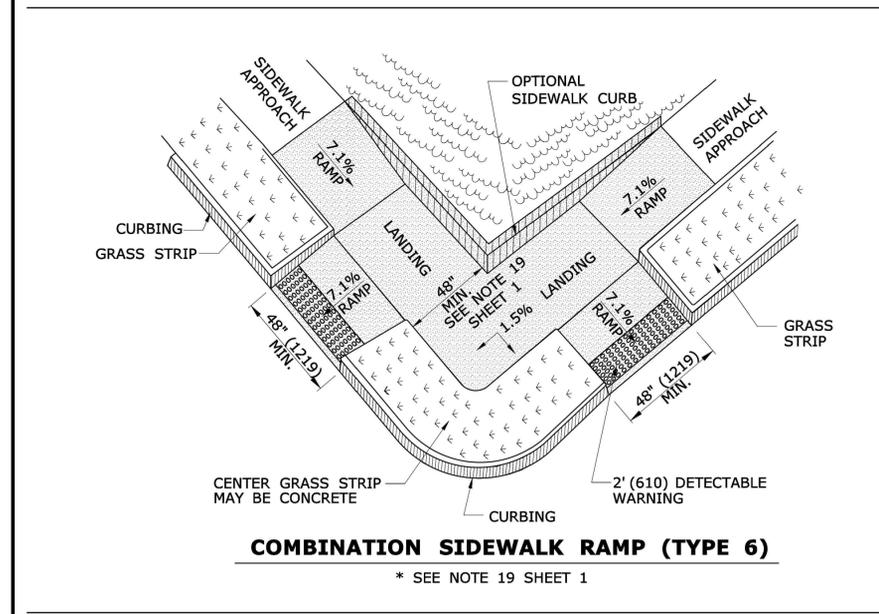
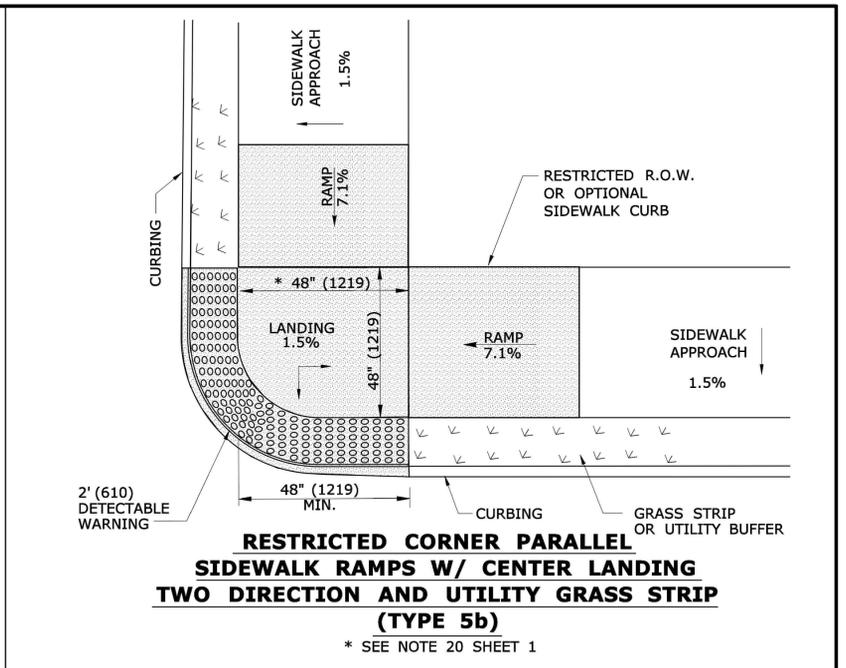
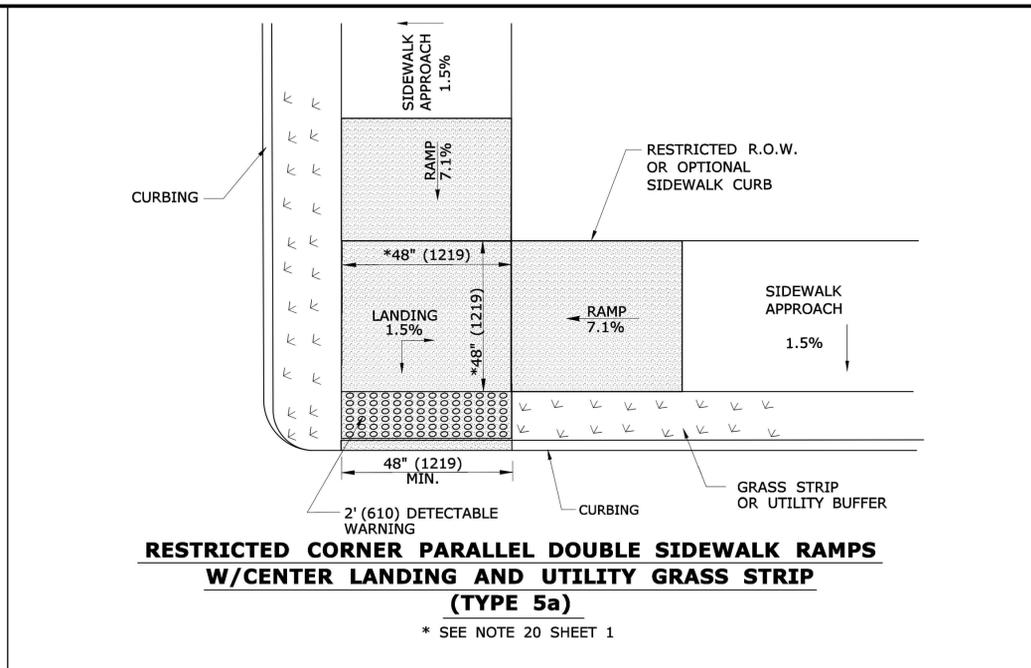
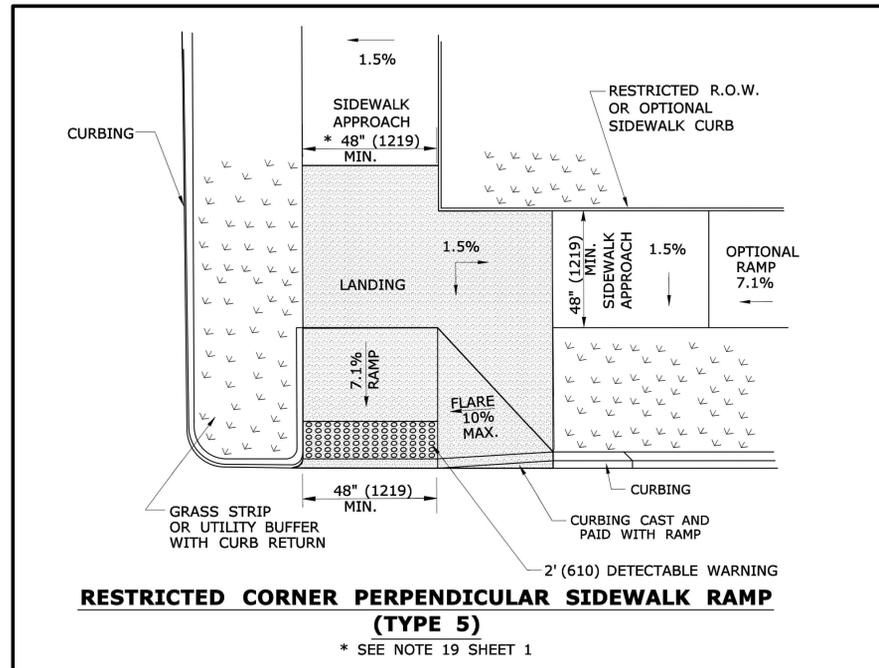
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CHECKED BY:
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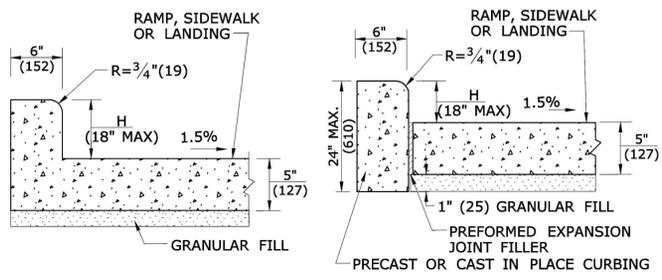
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OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE:
**SIDEWALK RAMP
SHEET 2**

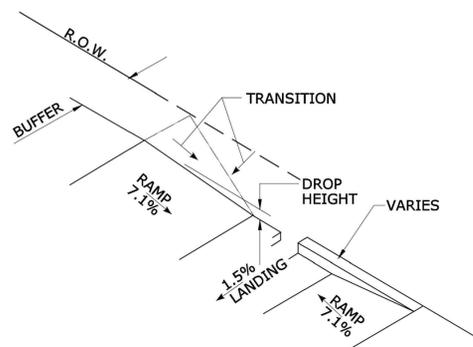
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PROJECT NO.
DRAWING NO.
SHEET NO.
C8.13



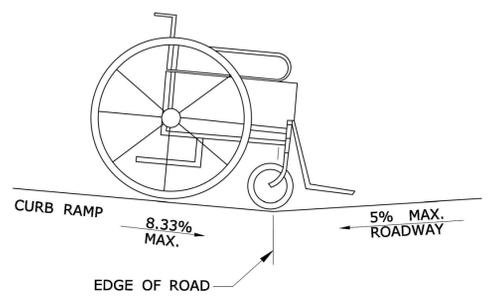
THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	DESIGNER/DRAFTER: MGB/EMK CHECKED BY: LLF	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	SIGNATURE/BLOCK: OFFICE OF ENGINEERING APPROVED BY:	PROJECT TITLE:	TOWN:	PROJECT NO.:
REV. DATE REVISION DESCRIPTION SHEET NO. Plotted Date: 5/27/2014	FILENAME: ...SIDEWALK_RAMP 3_GD.dgn	DRAWING TITLE: SIDEWALK RAMP SHEET 3	DRAWING NO.:	SHEET NO.: C8.14		



MONOLITHIC CAST CURB **SEPARATELY CAST CURB**
SIDEWALK CURB OPTIONS AT BACK OF SIDEWALK



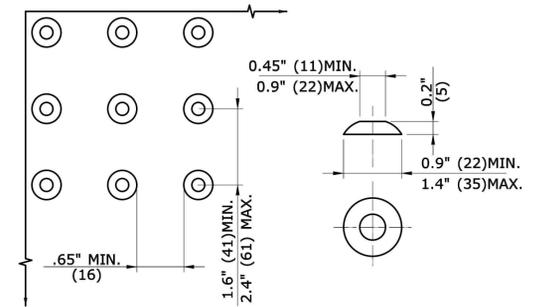
BACK OF SIDEWALK CURB OR BUFFER TRANSITION



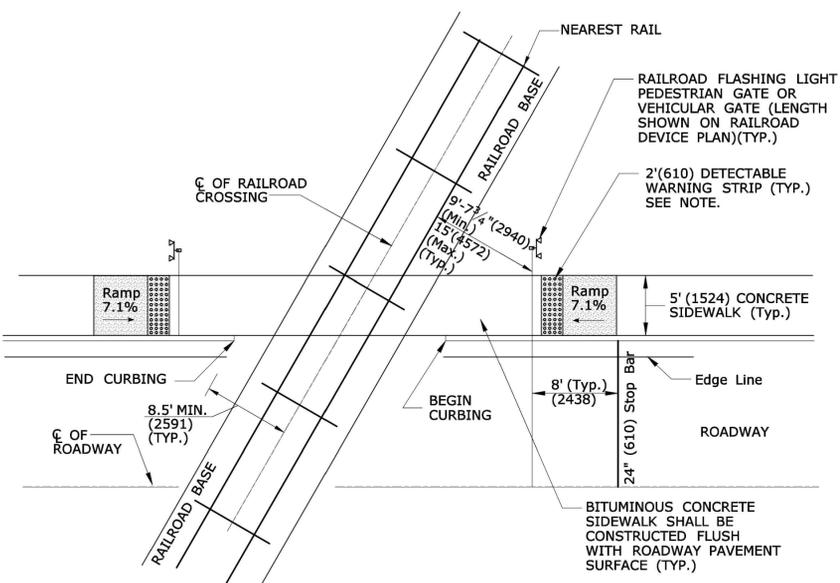
DETAIL 1
SEE GRADE CHANGE AT ROADWAY INTERFACE
 SEE NOTE 1 SHEET HW-921 02a

GENERAL NOTES:

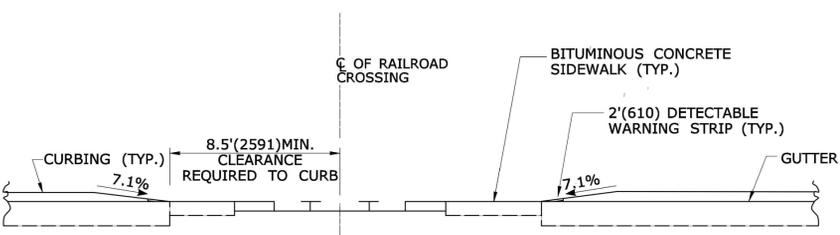
1. RAMPED MEDIANS SHALL HAVE A CURB RAMP AT EITHER END AND LEVEL LANDING A MINIMUM OF 5' x 5' (1.5m x 1.5m) IN BETWEEN. CUT-THROUGH MEDIANS SHALL BE A MINIMUM OF 6' (1.8m) LONG AND 5' (1.5m) WIDE. FOR ALL MEDIANS, CUT-THROUGH OR RAMPED, A 2' (610) STRIP OF DETECTABLE WARNINGS SHALL BE INSTALLED AT THE ENTRANCE AND EXIT.
2. SEE GENERAL NOTES ON SHEET 1.



DOME SPACING **DOME SECTION**
STANDARD DOME ON DETECTABLE WARNING TILES



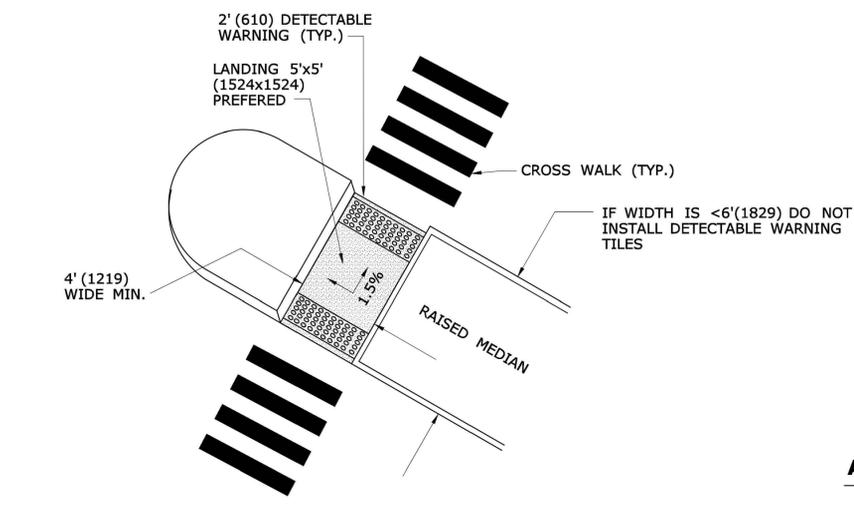
PLAN VIEW



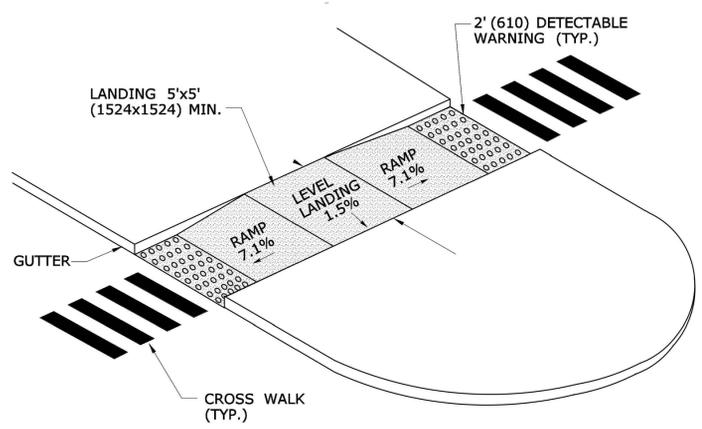
ELEVATION VIEW

DETECTABLE WARNINGS AT RAILROAD CROSSING

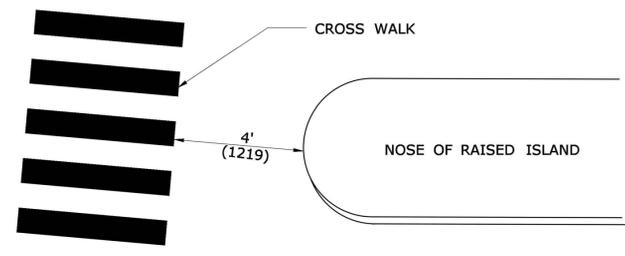
NOTE: WHEN NO GATE IS PRESENT, INSTALL DETECTABLE WARNING SURFACE 12' (3.6m) FROM THE NEAREST RAIL. IF GATE IS PRESENT, INSTALL DETECTABLE WARNING 2' (610) PRIOR TO GATE. THE ROWS OF TRUNCATED DOMES IN A DETECTABLE WARNING SURFACE SHALL BE INSTALLED PARALLEL WITH THE DIRECTION OF PEDESTRIAN TRAVEL.



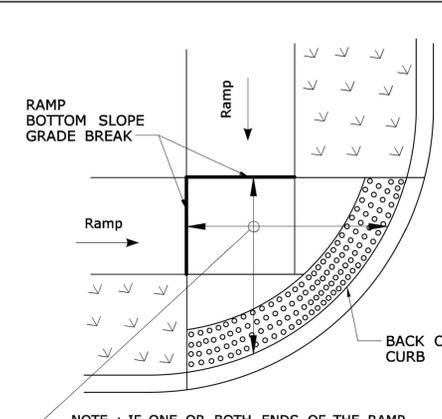
CUT-THROUGH MEDIAN ISLAND



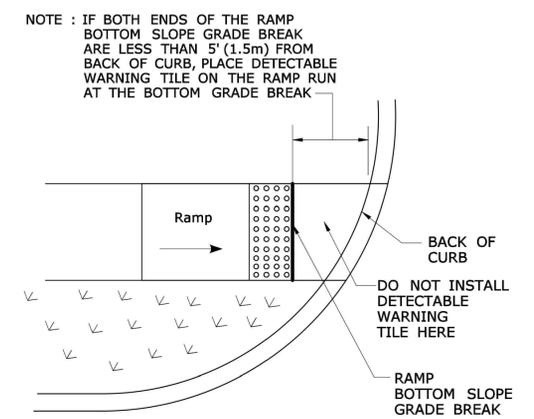
RAISED MEDIAN ISLAND WITH LANDING AND RAMPS



ALTERNATE CROSSWALK WITH MEDIAN ISLAND PULLED BACK



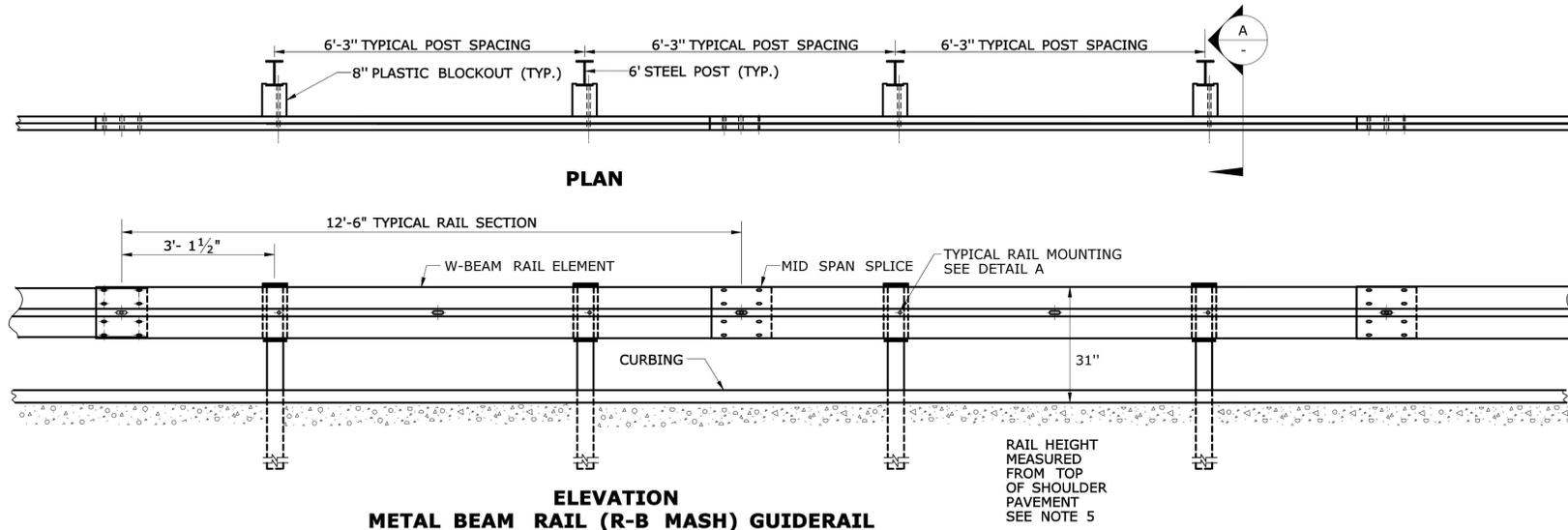
DETECTABLE WARNING PLACEMENT DETAIL 1



DETECTABLE WARNING PLACEMENT DETAIL 2

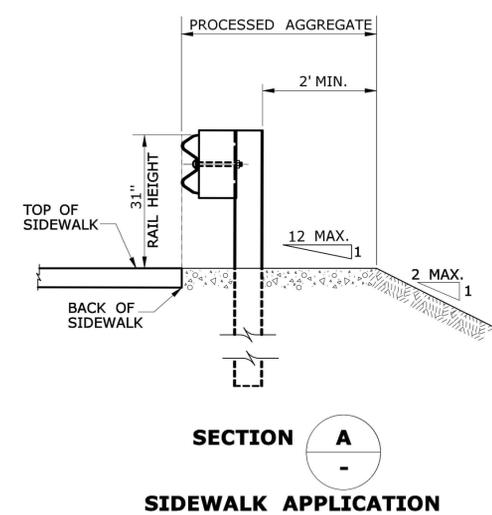
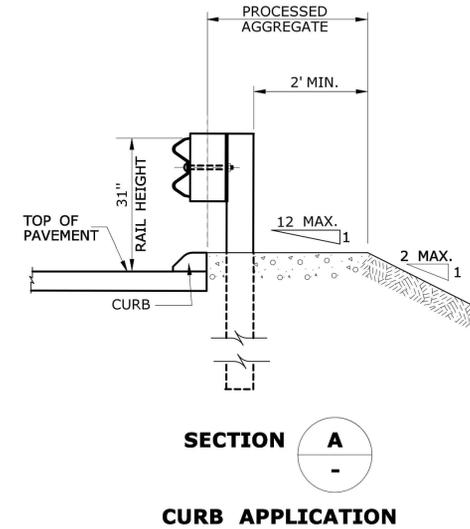
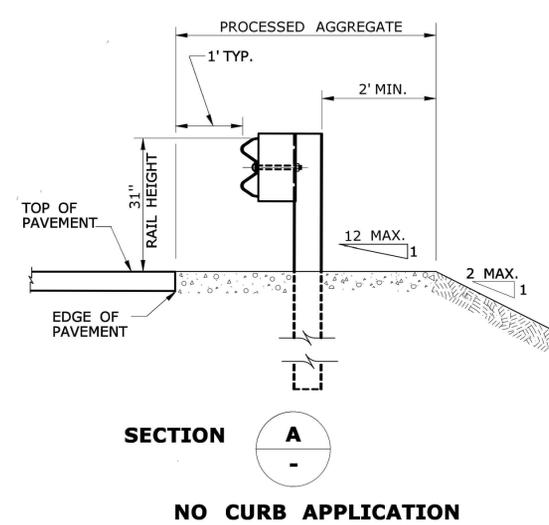
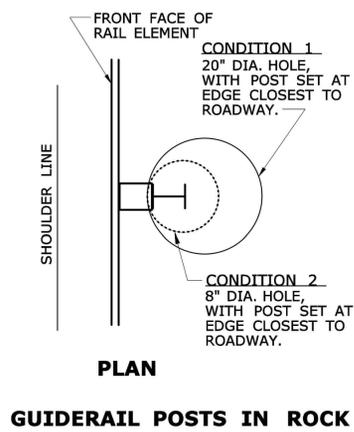
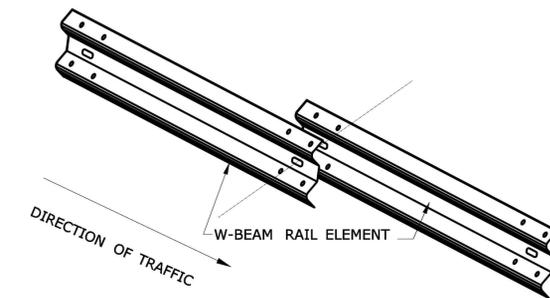
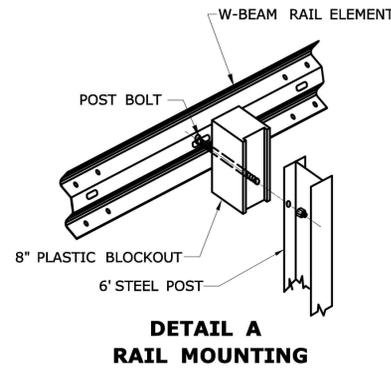
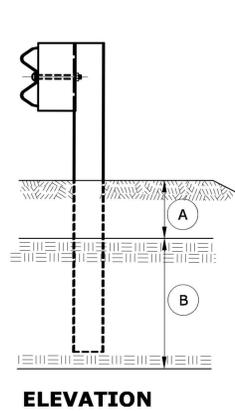
ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: MGB/EMK CHECKED BY: LLF	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	SIGNATURE/BLOCK: OFFICE OF ENGINEERING APPROVED BY:	PROJECT TITLE: TOWN:	PROJECT NO.: DRAWING NO.: SHEET NO.: C8.15
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 5/27/2014	DRAWING TITLE: SIDEWALK RAMP SHEET 4	



GENERAL NOTES:

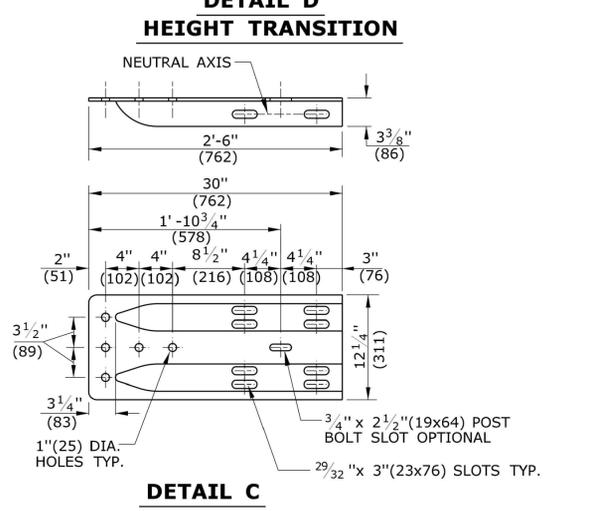
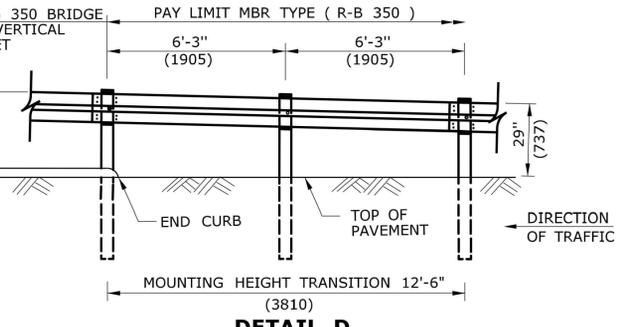
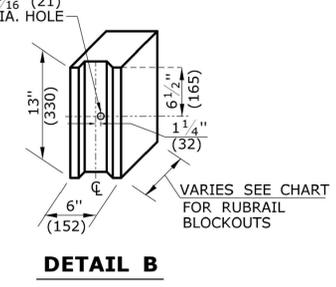
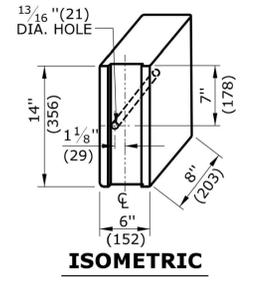
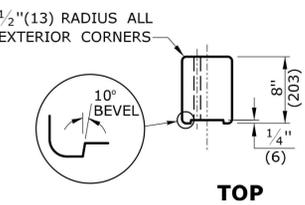
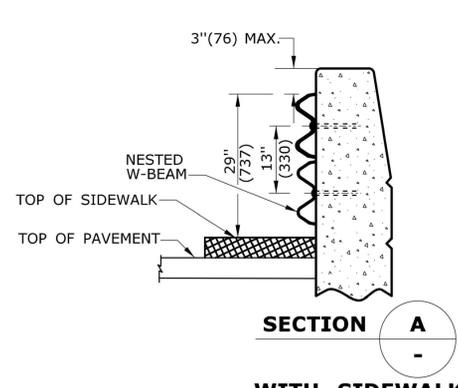
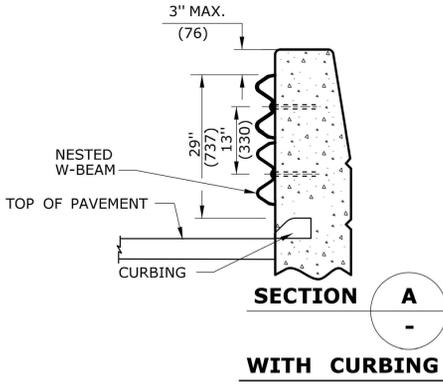
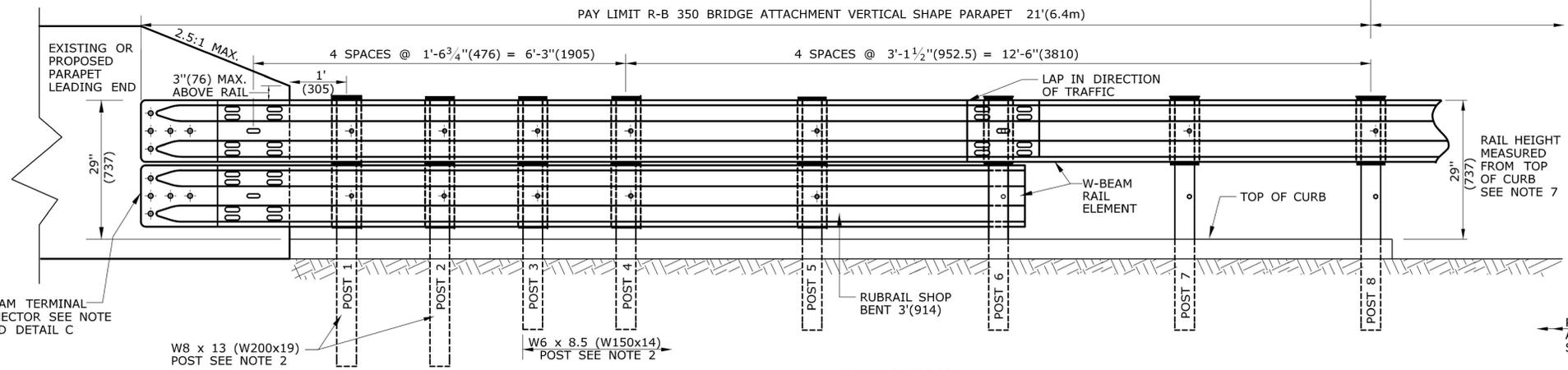
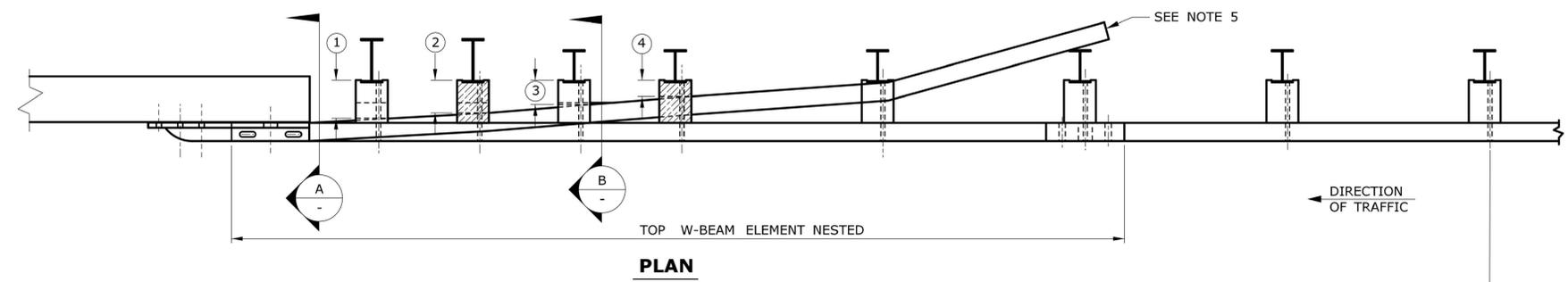
1. SEE SHEET HW-910.20 FOR MASH W-BEAM HARDWARE AND W-BEAM DELINEATOR DETAILS.
2. THREE BLOCKOUTS MAY BE USED FOR ONE POST ONLY. TWO BLOCKOUTS MAY BE USED FOR A SERIES OF POSTS. THE COST OF ADDITIONAL BLOCKOUTS AND LONGER BOLTS SHALL BE INCLUDED IN THE PRICE PER FOOT OF GUIDERRAIL. EXTRA BLOCKOUTS AT TRANSITIONS TO BRIDGE PARAPETS SHOULD BE AVOIDED. DO NOT USE ADDITIONAL BLOCKS IF IT CAUSES THE POST TO BE DRIVEN BEYOND AN EMBANKMENT HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.
3. IF BLOCKOUTS DO NOT AVOID POST FROM OBSTRUCTION, ONE POST MAY BE OMITTED IF 50 FEET OF GUIDERRAIL EXISTS ON BOTH SIDES OF LOCATION. USE METAL BEAM RAIL SPAN SECTION TYPE II OR III FOR MORE THAN ONE CONSECUTIVE OMITTED POST, SEE SHEET HW-910.24.
4. W-BEAM GUIDERRAIL MAY BE PLACED 1' OR MORE FROM THE EDGE OF PAVEMENT ONLY ON SLOPES 10:1 OR FLATTER AND WITHOUT CURBING.
5. IF THE RAIL IS INSTALLED WITHIN 2' OF THE EDGE OF PAVEMENT, THE RAIL HEIGHT IS MEASURED FROM THE SHOULDER SLOPE EXTENDED TO THE RAIL. IF THE RAIL IS INSTALLED BEYOND 2' FROM THE EDGE OF PAVEMENT, THE RAIL HEIGHT IS MEASURED FROM THE GROUND DIRECTLY BELOW THE RAIL.
6. RAIL HEIGHT CONSTRUCTION TOLERANCE IS +/- 1 INCH.



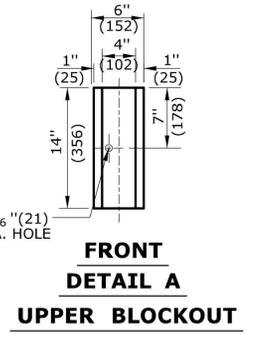
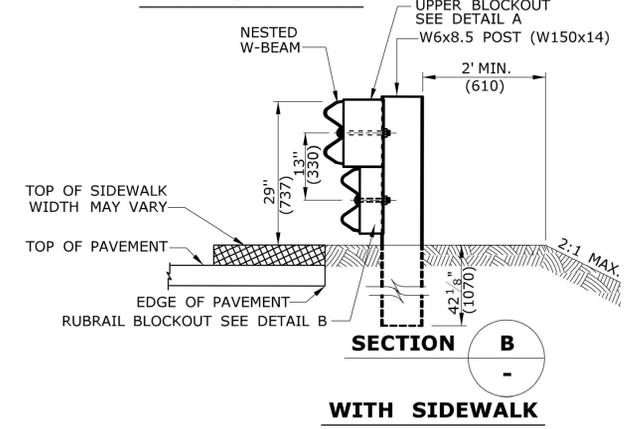
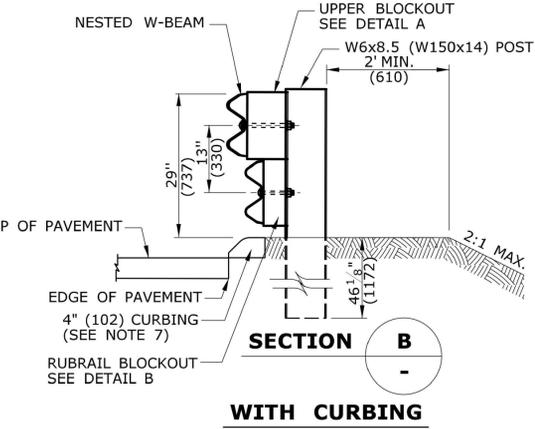
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GENERAL NOTES:

- THIS R-B 350 GUIDERAIL TRANSITION IS APPROPRIATE FOR CONNECTION AT THE FOLLOWING LOCATIONS:
 - WHEN ANY SAFETY SHAPE (F-SHAPED OR JERSEY SHAPE) PARAPET HAS AN ELECTRICAL JUNCTION BOX WITHIN 8' (2438) OF THE END OF THE PARAPET, THE END OF THE PARAPET SHALL BE MODIFIED OR TRANSFORMED TO A VERTICAL SHAPE PRIOR TO GUIDERAIL ATTACHMENT.
 - VERTICAL WALL OR ABUTMENT FACE.
 - VERTICAL CONCRETE PARAPET WITH SIDEWALK.
 - VERTICAL FACE FOR LEADING AND TRAILING ENDS ON DUAL DIRECTION ROADWAYS.
- POSTS 1 AND 2 ARE W8 x 13 (W200x19), 7'-6" (2286) LONG. ALL OTHER POSTS IN TRANSITION ARE W6 x 8.5 (W150x14), 6' (1829) LONG.
- POSTS 1 THROUGH 5 REQUIRE AN ADDITIONAL HOLE TO ATTACH LOWER BLOCKOUTS AND/OR LOWER RUBRAIL.
- RUBRAIL BLOCKOUTS FOR POSTS 1 THROUGH 4 ARE ATTACHED TO POST AND RAIL WITH A 5/8" (16) BUTTONHEAD BOLTS (SEE CHART FOR BOLT LENGTH). RUBRAIL ONLY IS ATTACHED TO POST 5 WITH A 5/8" x 1 1/4" (16 x 32) BUTTONHEAD BOLT.
- THE RUBRAIL MAY BE SHOP BENT IN THE LAST 3' (914) TO FACILITATE INSTALLATION. DO NOT ATTACH RUBRAIL TO BACK OF POST 6.
- USE CLASS B (10 GAUGE) TYPE II W-BEAM RAIL ELEMENTS FOR INSTALLATIONS ON EXPRESSWAYS AND RAMP.
- FOR THIS APPLICATION WHEN CURBING IS USED, R-B 350 RAIL HEIGHT MUST BE MEASURED FROM THE TOP OF CURBING TO THE TOP OF RAIL. SEE DETAIL D FOR HEIGHT TRANSITION.
- FOR NEW CONSTRUCTION WHERE CURBING IS NEEDED, USE EITHER 4" (102) BITUMINOUS CONCRETE PARK CURBING OR PRECAST CONCRETE TRANSITION CURBING SET WITH A 4" (102) REVEAL. THE PREFERRED CURBING FOR HIGH SPEED ROADWAYS (>45 MPH (72kph)) IS 4" (102). HOWEVER, ON LOW SPEED ROADWAYS (<45 MPH (72kph)) A 6" (152) CURBING MAY BE USED. ADJUST RAIL HEIGHT AS REQUIRED.
- ANCHORAGE:
 - AT EXISTING PARAPETS EACH W-BEAM TERMINAL CONNECTOR SHALL BE ANCHORED USING FOUR 7/8" x 12" (22 x 305) CHEMICALLY ANCHORED BOLTS WITH WASHERS OR AS DETAILED ON STRUCTURE SHEETS. MAXIMUM BOLT PROJECTION BEYOND THE NUT SHALL BE 1/2" (13). THE 12" (305) MINIMUM LENGTH OF CHEMICALLY ANCHORED BOLTS SHALL INCLUDE A MINIMUM EMBEDMENT DEPTH OF 10" (254) INTO SUITABLY REINFORCED CONCRETE OR AS RECOMMENDED BY THE MANUFACTURER OF BONDING MATERIAL.
 - FOR NEW PARAPETS OR BARRIERS, THE W-BEAM TERMINAL CONNECTORS SHALL BE ANCHORED AS DETAILED ON THE STRUCTURE SHEETS.
- ADDITIONAL BLOCKOUTS WITH POSTS 1 THROUGH 6 SHOULD BE AVOIDED.
- FOR SINGLE DIRECTION ROADWAY:
 - INSTALL W-BEAM TERMINAL CONNECTOR BETWEEN NESTED GUIDERAIL ELEMENTS.
- FOR DUAL DIRECTION ROADWAY FOR APPROACHING TRAFFIC:
 - INSTALL W-BEAM TERMINAL CONNECTOR BETWEEN NESTED GUIDERAIL ELEMENTS.
- FOR TRAILING END:
 - INSTALL W-BEAM TERMINAL CONNECTOR OUTSIDE OF THE NESTED GUIDERAIL ELEMENTS.
- MINIMUM RAIL HEIGHT FOR NEW CONSTRUCTION SHALL BE 29" (737) ± 1" (25).



RUBRAIL BLOCKOUTS 13"(330)HIGH x 6"(152)WIDE		
POST	THICKNESS	BOLT LENGTH
①	7"(178)	9"(229)
②	6"(152)	8"(203)
③	4 1/2"(114)	6"(152)
④	3"(76)	5"(127)



SEE NOTE 11
ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
1	6/11	REVISED NOTE 9 FOR USE OF CHEMICAL ANCHORS
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Plotted Date: 5/10/2011

Filename: CTDOT_HIGHWAY_STD_JUNE2011.dgn Model: 42 - HW-910_07

SUBMITTED BY: NAME/DATE/TIME: James H. Norman 2011.06.09 15:14:46 -04'00'

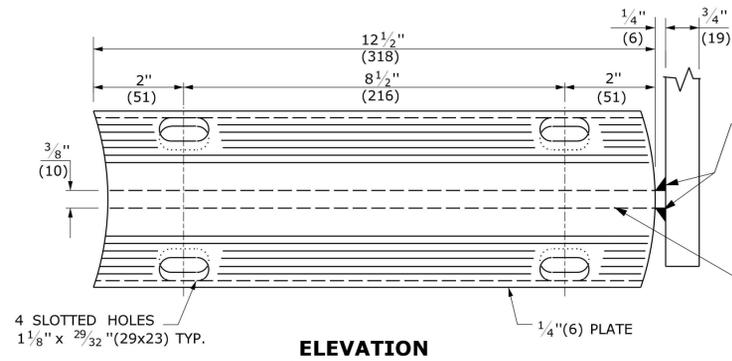
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CTDOT STANDARD SHEET
OFFICE OF ENGINEERING

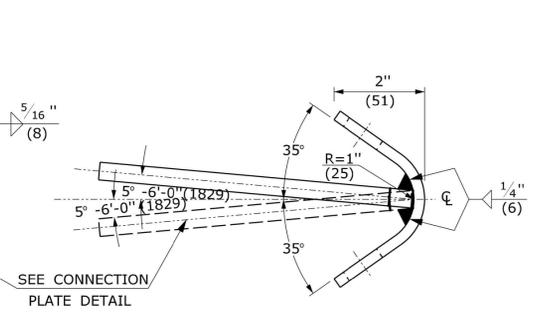
STANDARD SHEET TITLE: R-B 350 BRIDGE ATTACHMENT VERTICAL SHAPE PARAPET

STANDARD SHEET NO.: HW-910_07 C8.18

Tight & Bond Inc. \\ \T0196\15_Strobel Rd\Drawing\Sheet\T0196-15-C-801-DET.dwg Layout Name: C8.19 Plotted by: DubuqueC Plotted on: Jan 18, 2019--2:35pm

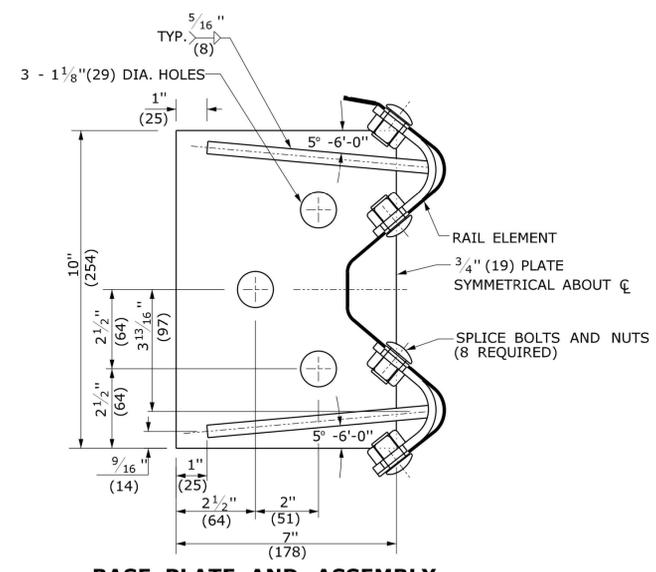


ELEVATION

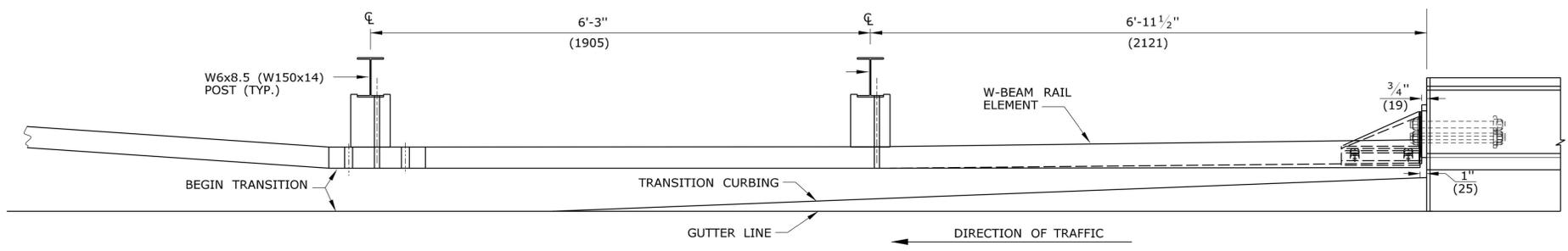


SIDE

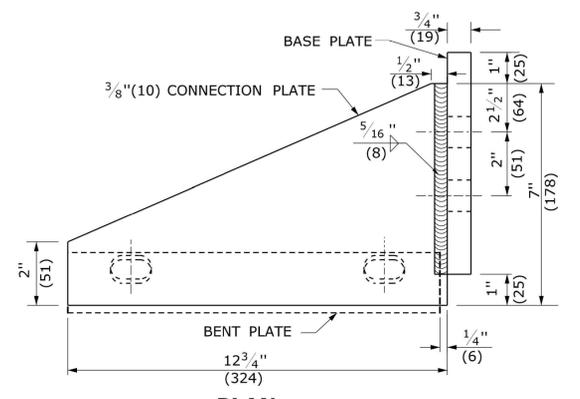
BENT PLATE
(2 REQUIRED)



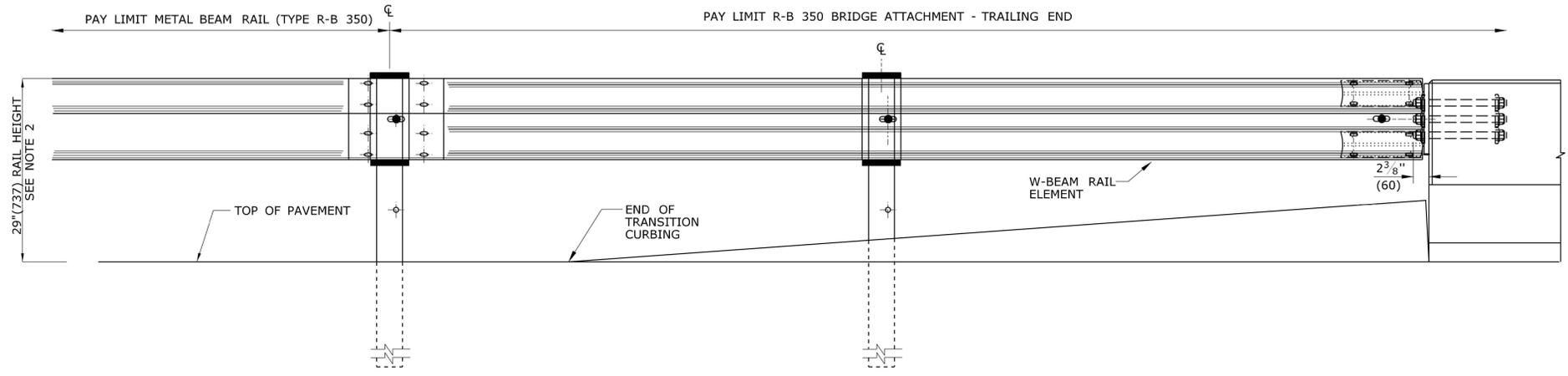
BASE PLATE AND ASSEMBLY



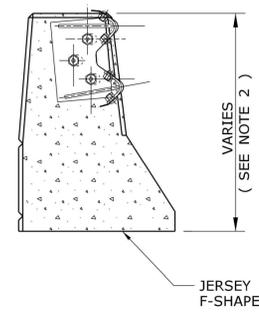
PLAN



CONNECTION PLATE DETAIL
(2 REQUIRED)



ELEVATION
ATTACHMENT AT TRAILING END OF BRIDGE PARAPET
(ONE-WAY ROADWAY ONLY)



BASE PLATE AND ASSEMBLY ATTACHMENT TO TRAILING END OF CONCRETE BARRIER OR BRIDGE PARAPET (SEE NOTE 5)

- GENERAL NOTES:**
- SEE SHEET HW-910.01 FOR HARDWARE AND DELINEATOR DETAILS.
 - IF PROPOSED R-B 350 GUIDERAIL IS BEING ATTACHED TO A PARAPET USING EXISTING ANCHOR BOLTS FROM R-1 GUIDERAIL INSTALLED AT A 33" (838) RAIL HEIGHT, A 4" (102) HEIGHT TRANSITION OVER 25' (7.62m) SHALL BE PROVIDED.
 - FOR EXISTING PARAPETS, USE THREE 1" (25) DIA. x 14" (356) LONG, FULLY THREADED, CHEMICALLY ANCHORED BOLTS AND WASHERS WITH 12" (305) EMBEDMENT DEPTH OR AS RECOMMENDED BY MANUFACTURER OF BONDING MATERIAL TO OBTAIN FULL STRENGTH OF ANCHOR BOLTS. FOR ANCHORAGE IN NEW PARAPETS, SEE STRUCTURE SHEETS.
 - USE CLASS B TYPE II (10 GAUGE) W-BEAM RAIL ELEMENT FOR INSTALLATIONS ON EXPRESSWAYS AND RAMP.
 - TRAILING END ATTACHMENT MAY ALSO BE INSTALLED AT A VERTICAL FACE PARAPET OR RETAINING WALL ON ONE-WAY ROADWAYS ONLY.
 - MINIMUM RAIL HEIGHT FOR NEW CONSTRUCTION SHALL BE 29" (737) ± 1" (25).

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
1	6/11	REVISED NOTE 3 FOR USE OF CHEMICAL ANCHORS
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 5/10/2011

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY STD_JUNE2011.dgn Model: 43 - HW-910_08

SUBMITTED BY: [Signature] NAME/DATE/TIME: [Blank]

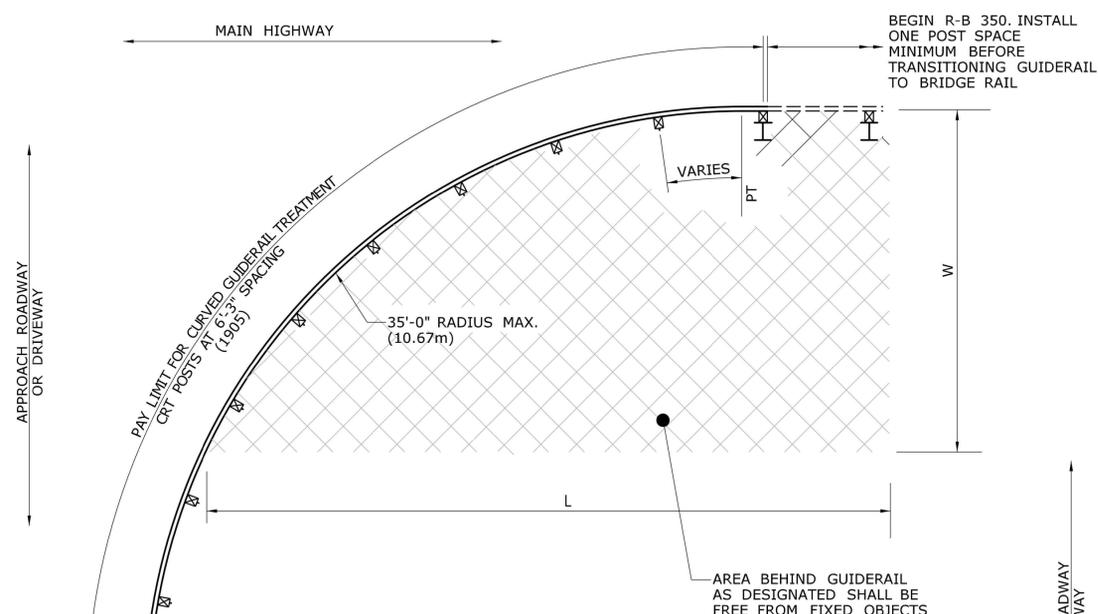
APPROVED BY: [Signature] NAME/DATE/TIME: James H. Norman 2011.06.09 15:15:09 -04'00

CTDOT
STANDARD SHEET

OFFICE OF ENGINEERING

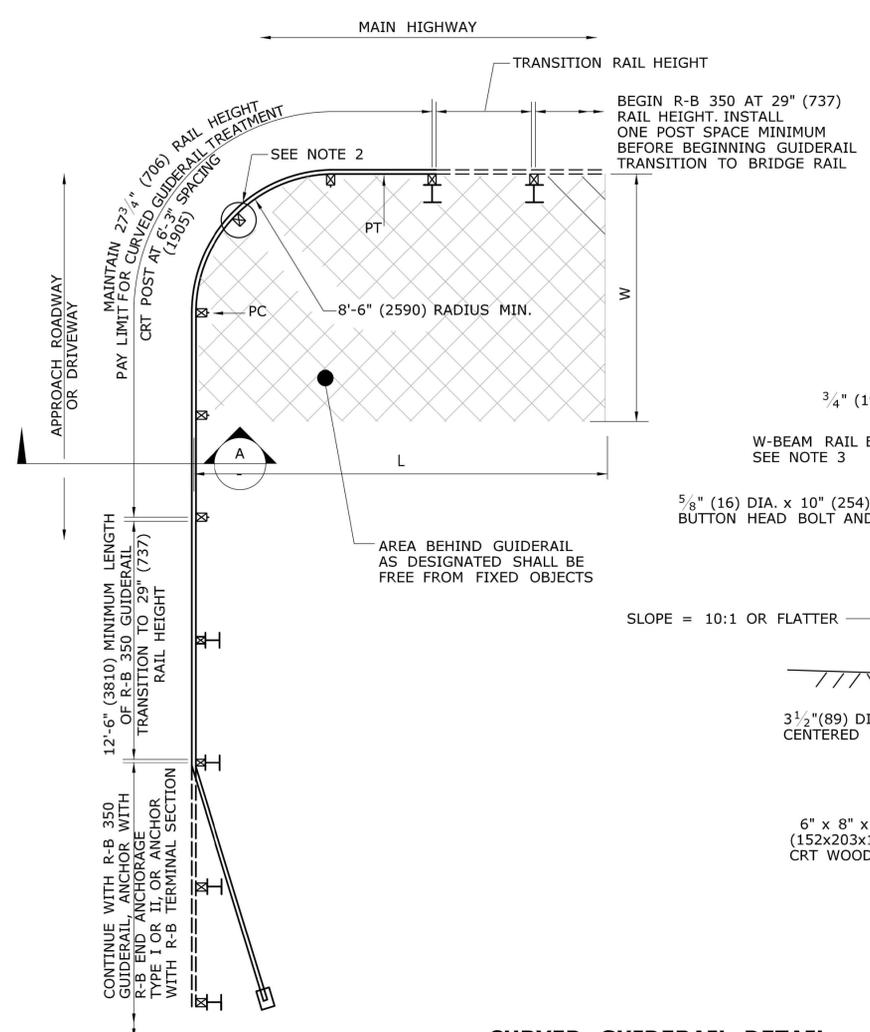
STANDARD SHEET TITLE: **R-B 350 BRIDGE ATTACHMENT TRAILING END**

STANDARD SHEET NO.: **HW-910_08**
C8.19

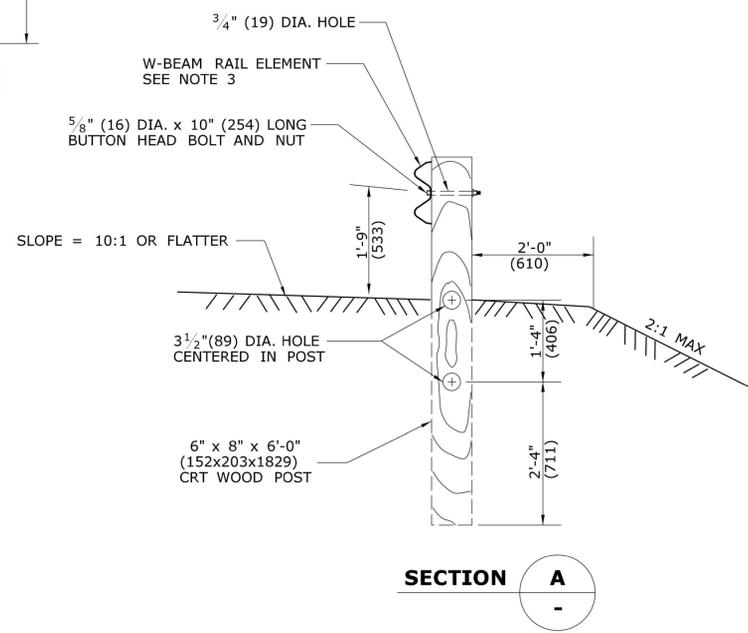


RADIUS	NO. OF CRT POSTS	REQUIRED AREA FREE OF FIXED OBJECTS	
		L	W
8'-6" (2591)	5	25' (7.62m)	15' (4.57m)
17'-0" (5182)	6	30' (9.14)	15' (4.57m)
25'-6" (777m)	8	40' (12.19m)	20' (6.10m)
35'-0" (10.67m)	11	50' (15.24m)	20' (6.10)

**CURVED GUIDERAIL DETAIL
(35'-0" (10.67m) RADIUS MAX.)**



**CURVED GUIDERAIL DETAIL
(8'-6" (2590) RADIUS MIN.)**



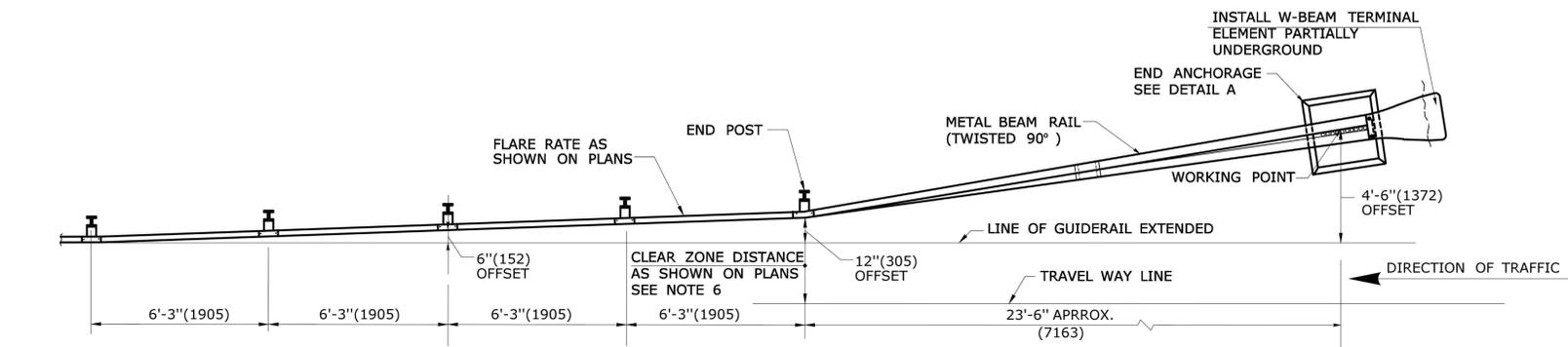
GENERAL NOTES:

1. NO WASHERS ARE USED ON THE 5/8" (16) DIA. BUTTON HEAD BOLTS CONNECTING THE RAIL TO THE CONTROLLED RELEASING TERMINAL (CRT) POSTS.
2. THE RAIL IS NOT BOLTED TO THE CRT POST AT THE CENTER OF THE NOSE AS SHOWN FOR THE 8'-6" (2590) RADIUS CURVED GUIDERAIL TREATMENT ONLY.
3. THE CURVED GUIDERAIL SECTION SHALL BE SHOP BENT.
4. THE SLOPE FROM THE EDGE OF THE SHOULDER TO THE FACE OF THE RAIL SHALL BE 10:1 OR FLATTER. NO CURBING SHALL BE INSTALLED WITHIN THE PAY LIMIT OF THE CURVED GUIDERAIL TREATMENT.
5. THIS SYSTEM SHALL BE USED ONLY ON ROADS WITH DESIGN SPEEDS ≤ 50 mph (80 kph).
6. MAINTAIN MINIMUM 27 3/4" (706) RAIL HEIGHT THROUGH RADIUS.

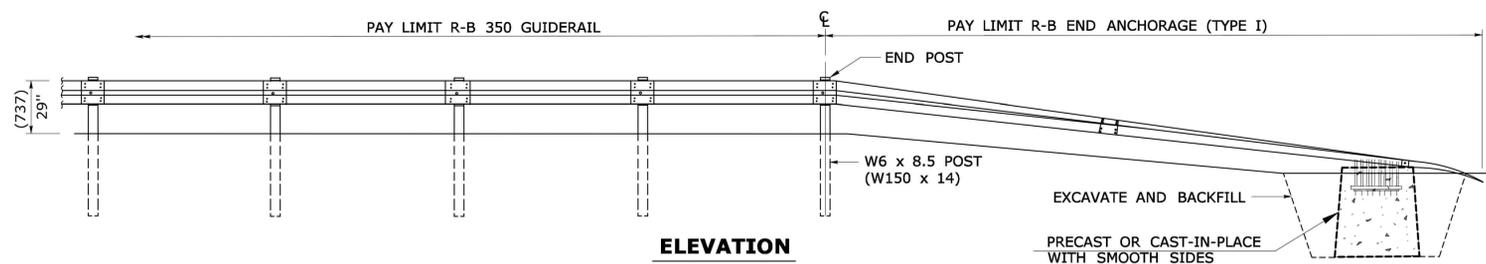
ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

Title & Bond Inc. J:\10196\15_Strobel Rd\Drawing\Sheet\10196-15-C-801-DET.dwg Layout Name: C8.20 Plotted by: DubuqueC Plotted on: Jan 18, 2019--2:35pm

		<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p> <p>Filename: CTDOT_HIGHWAY_STD_JUNE2011.dgn Model: 46 - HW-910.11</p>	<p>SUBMITTED BY: </p> <p>NAME/DATE/TIME: James H. Norman 2012.07.25 12:47:32 -04'00'</p> <p>APPROVED BY: </p> <p>NAME/DATE/TIME: James H. Norman 2012.07.25 12:47:32 -04'00'</p>	<p>CTDOT STANDARD SHEET</p> <p>OFFICE OF ENGINEERING</p>	<p>STANDARD SHEET TITLE:</p> <p>CURVED GUIDERAIL TREATMENT DETAIL</p>	<p>STANDARD SHEET NO.:</p> <p>HW-910_11 C8.20</p>																																	
<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>REV.</th> <th>DATE</th> <th>REVISION DESCRIPTION</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td></tr> </tbody> </table>	REV.	DATE	REVISION DESCRIPTION																																				
REV.	DATE	REVISION DESCRIPTION																																					



PLAN

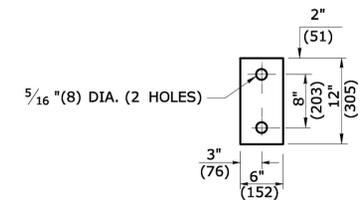


ELEVATION

**R-B END ANCHORAGE TYPE I
(ROADSIDE APPLICATION)**

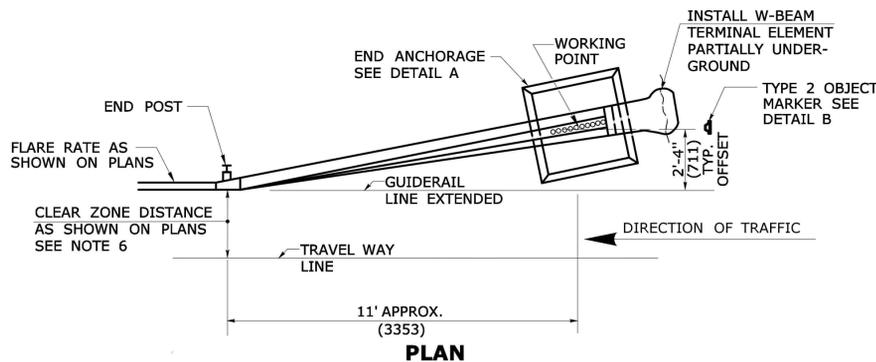
GENERAL NOTES:

1. R-B END ANCHORAGE TYPE I INSTALLED ON FREEWAYS AND RAMPS SHALL USE CLASS B (10 GAUGE) TERMINAL AND W-BEAM RAIL ELEMENTS. ALL OTHER R-B END ANCHORAGE TYPE I SHALL USE CLASS A (12 GAUGE) TERMINAL AND W-BEAM RAIL ELEMENTS.
2. R-B END ANCHORAGE TYPE II SHALL USE CLASS A (12 GAUGE) TERMINAL AND W-BEAM RAIL ELEMENTS.
3. OTHER RADII WHICH CAN BE DEMONSTRATED TO PROVIDE THE INSTALLATIONS SHOWN FOR END ANCHORAGE TYPE II MAY BE APPROVED.
4. J-HOOK BOLTS MAY BE SUBSTITUTED FOR BOTTOM PLATE ANCHORAGE IN CONCRETE END ANCHORS USING THE SAME SIZE, STRENGTH, AND LENGTH AS NOTED ON THE PLANS.
5. MINIMUM RAIL HEIGHT FOR NEW CONSTRUCTION SHALL BE 29" (737) + 1' (25).
6. R-B TYPE I END ANCHOR: INSTALL LEADING END USING DESIGN CLEAR ZONE DISTANCE MEASURED FROM TRAVEL WAY LINE TO END POST AS SHOWN ON PLANS, THEN PLACE CONCRETE ANCHOR 4'-6" (1372) FROM THE LINE OF GUIDERAIL EXTENSION. INSTALL TRAILING END ON DUAL DIRECTION ROADWAYS IN THE SAME MANNER EXCEPT MEASURE CLEAR ZONE FROM THE DOUBLE YELLOW LINE. INSTALL TRAILING END ON SINGLE DIRECTION ROADWAYS MEASURED FROM THE GUIDERAIL LINE EXTENDED AND OFFSET 4'-6" (1372).
- R-B TYPE II END ANCHOR: INSTALL ONLY ON ROADWAYS WITH DESIGN SPEEDS < 45mph (72kph). FOR DUAL DIRECTION ROADWAYS, INSTALL IN THE SAME MANNER AS TYPE I END ANCHOR FOR BOTH LEADING AND TRAILING END USING DESIGN CLEAR ZONE DISTANCE EXCEPT PLACE CONCRETE ANCHOR 2'-4" (711) MEASURED FROM THE LINE OF GUIDERAIL EXTENDED. INSTALL TRAILING END ON SINGLE DIRECTION ROADWAYS MEASURED FROM THE GUIDERAIL LINE EXTENDED AND OFFSET 2'-4" (711).
7. SEE HW-911-04 FOR TYPICAL END ANCHOR GRADING PLAN.

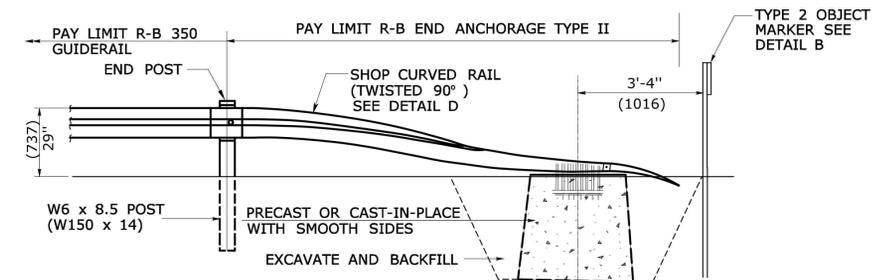


**DETAIL B
TYPE 2 OBJECT MARKER
SIGN # 51-5030**

OBJECT MARKER FACE SHALL BE YELLOW TYPE III RETROREFLECTORIZED. THE BOTTOM OF TYPE 2 OBJECT MARKER SHALL BE 4'(1219) ABOVE GROUND LEVEL MOUNTED ON METAL DELINEATOR POST. IT SHALL BE USED TO IDENTIFY CULVERT ENDS AND TYPE II END ANCHORS, OR AS DIRECTED BY THE ENGINEER.

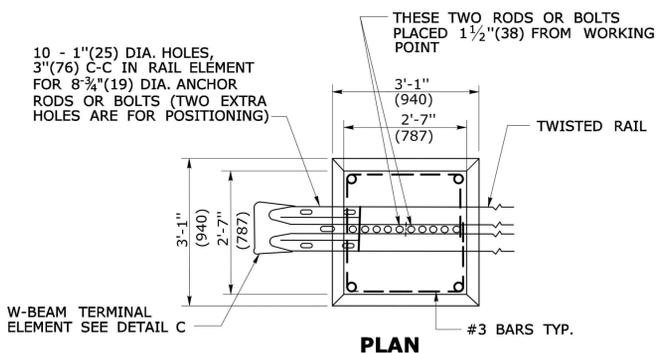


PLAN

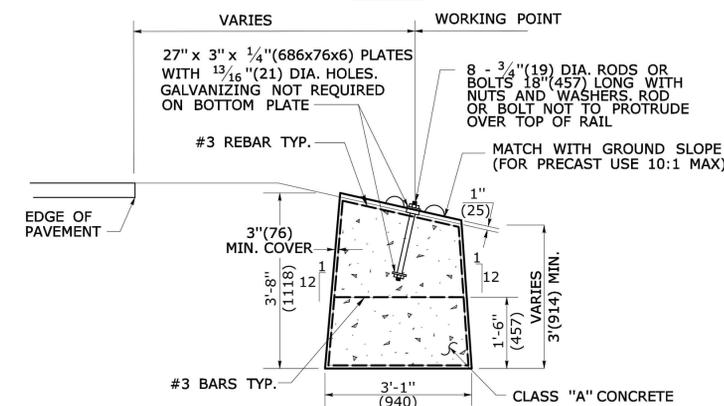


ELEVATION

**R-B END ANCHORAGE TYPE II
SEE NOTE 2**

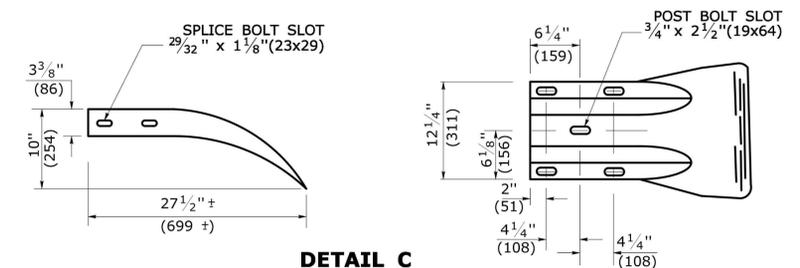


PLAN

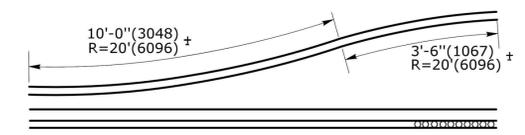


ELEVATION

**DETAIL A
ROADSIDE CONCRETE END ANCHOR
SEE NOTE 4**



**DETAIL C
W-BEAM TERMINAL ELEMENT**



DETAIL D

**SHOP CURVED RAIL
SEE NOTE 3**

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
1	6/11	REVISED TYPE I AND II ANCHOR FOR CLEAR ZONE PLACEMENT
2	7/13	ADD POST OFFSET DISTANCE
-	-	-
-	-	-
-	-	-
-	-	-

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NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Plotted Date: 6/13/2013

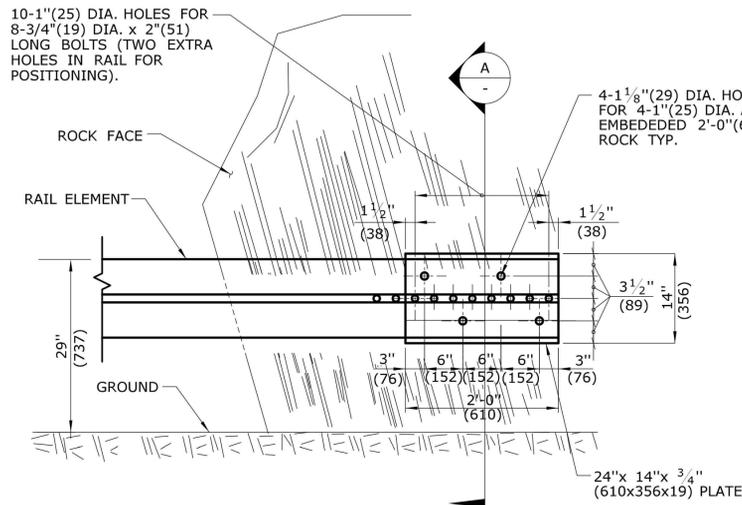
Filename: CTDOT-HIGHWAY-STD2013.dgn Model: 70 - HW-911_01

SUBMITTED BY: NAME/DATE/TIME:
APPROVED BY: NAME/DATE/TIME:
James H. Norman
2013.07.24 14:55:36-04'00'

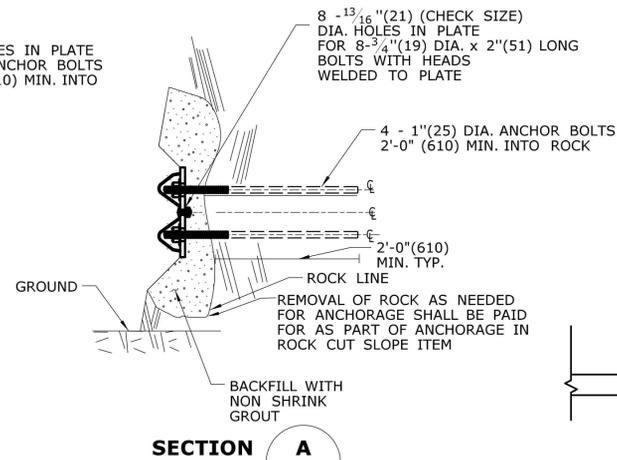
CTDOT
STANDARD SHEET
OFFICE OF ENGINEERING

R-B END ANCHORAGE
TYPE I AND II

STANDARD SHEET NO.:
HW-911_01
C8.21

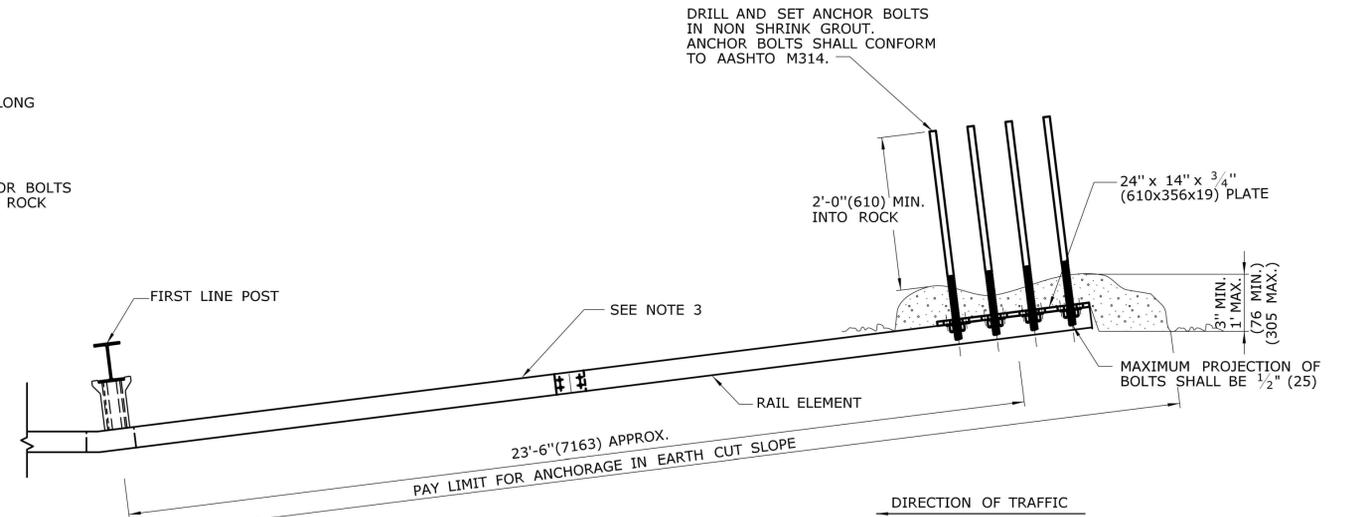


ELEVATION

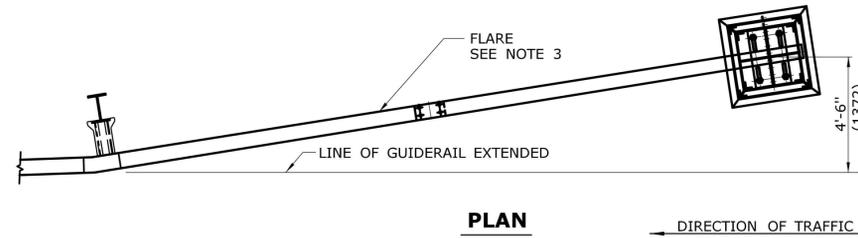


SECTION A

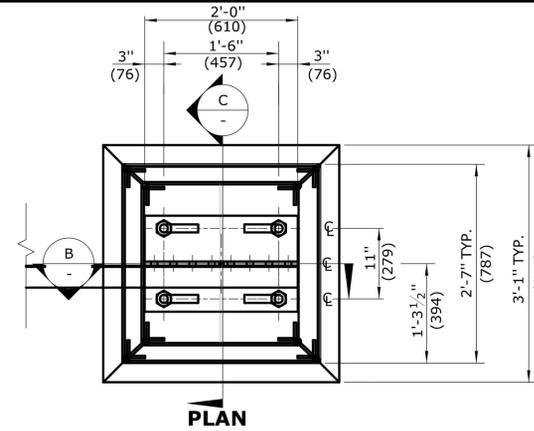
DETAILS OF ANCHORAGE IN ROCK CUT SLOPE



PLAN



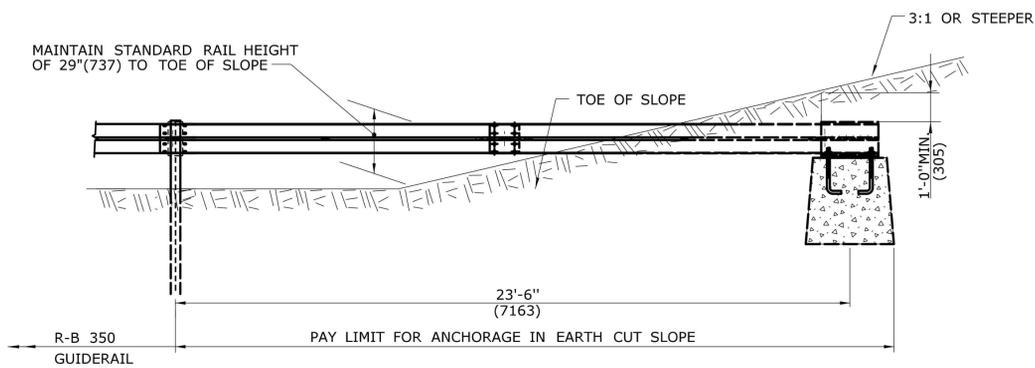
PLAN



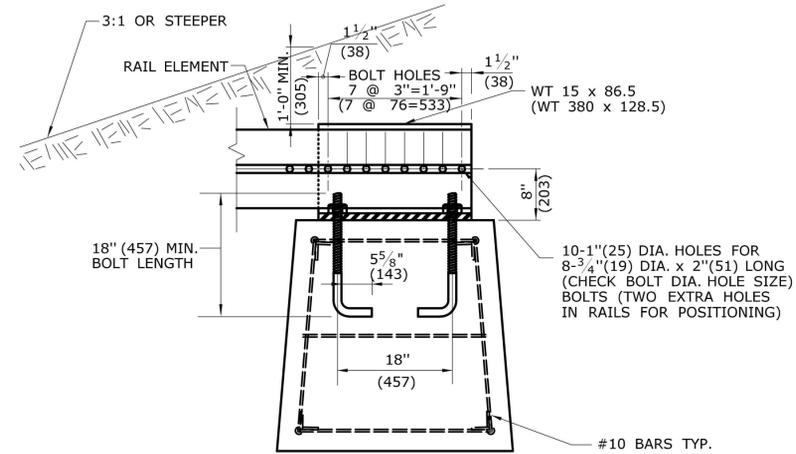
SECTION B

GENERAL NOTES:

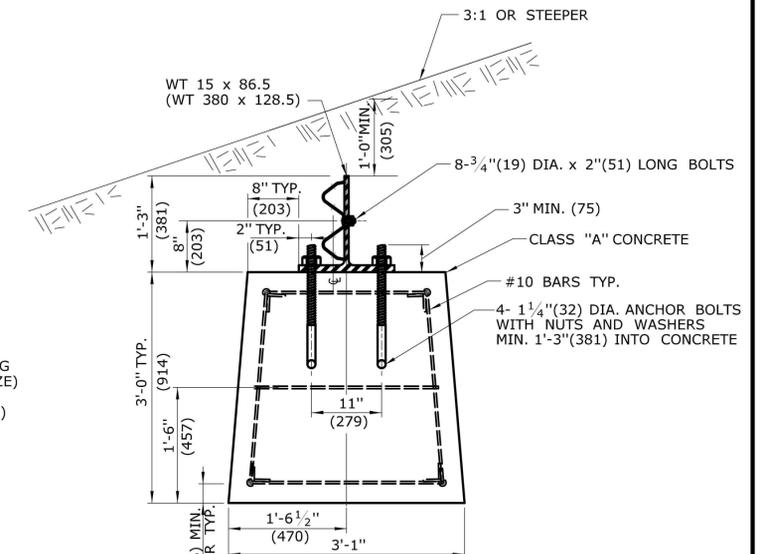
1. ANCHORAGE IN AN EARTH CUT SLOPE SHALL BE IN A CUT SLOPE 3:1 OR STEEPER. FULL RAIL HEIGHT MUST BE MAINTAINED TO TOE OF SLOPE AND A MINIMUM 1'(305) COVER SHALL BE PROVIDED OVER THE RAIL ELEMENT.
2. ANCHORAGE IN EARTH CUT SLOPE AND ROCK CUT SLOPE INSTALLED ON LIMITED ACCESS HIGHWAYS AND RAMPS SHALL USE CLASS B TYPE II (10 GAUGE) W-BEAM RAIL ELEMENTS.
3. THE MAXIMUM FLARE RATES FOR THESE TERMINAL TREATMENTS SHALL CONFORM TO THE SAME REQUIREMENTS AS STANDARD GUIDERAIL IN ACCORDANCE WITH THE CTDOT HIGHWAY DESIGN MANUAL.
4. MINIMUM RAIL HEIGHT FOR NEW CONSTRUCTION SHALL BE 29"(737) ± 1" (25).



ELEVATION



SECTION B

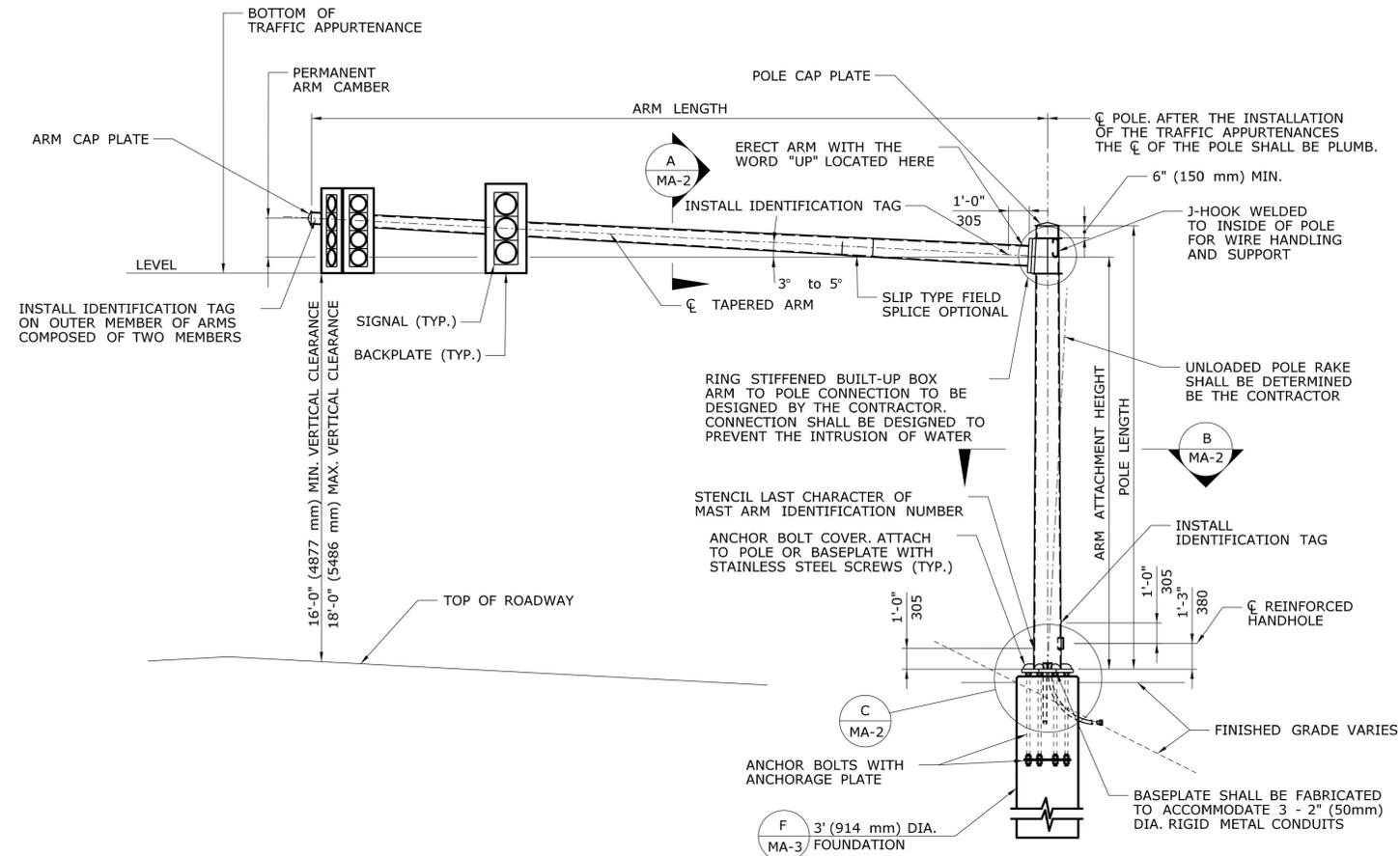


SECTION C

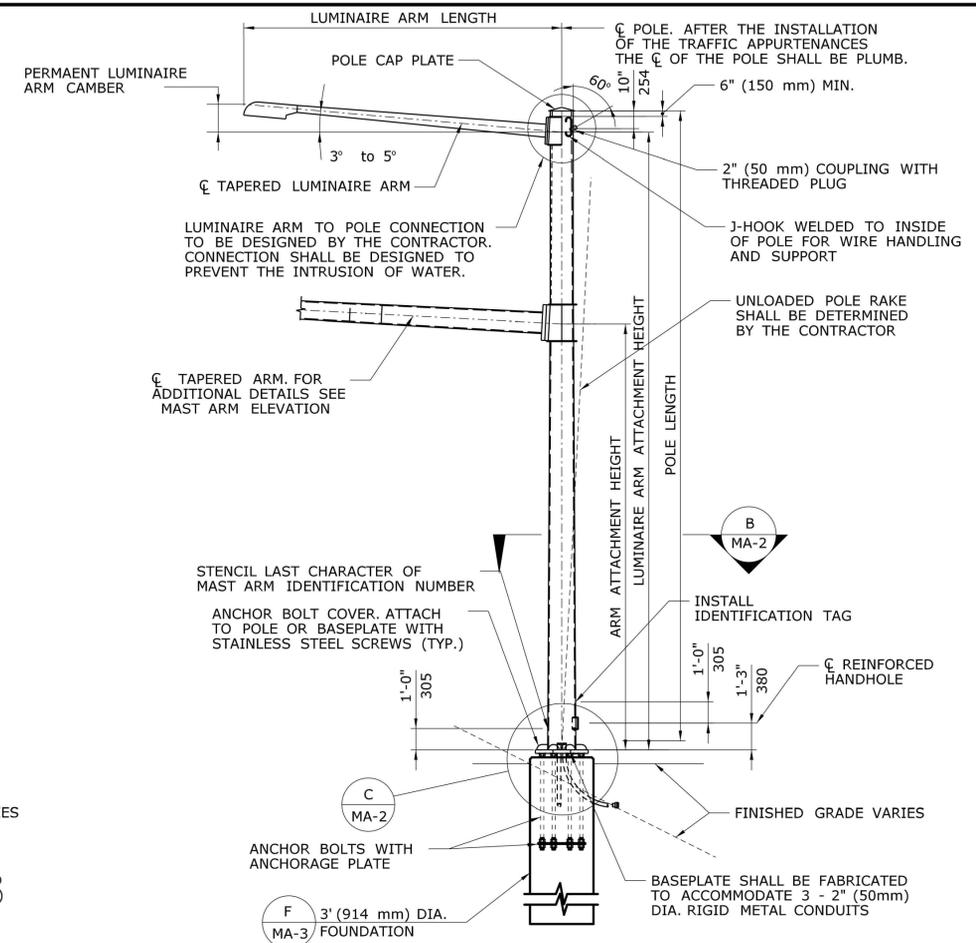
DETAILS OF ANCHORAGE IN EARTH CUT SLOPE

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

<p>THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.</p> <p>Plotted Date: 9/30/2010</p>	<p>NOT TO SCALE</p>	<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>	<p>SUBMITTED BY: NAME/DATE/TIME: James H. Norman</p> <p>APPROVED BY: NAME/DATE/TIME: James H. Norman</p>	<p>CTDOT STANDARD SHEET OFFICE OF ENGINEERING</p>	<p>STANDARD SHEET TITLE: ANCHOR IN EARTH CUT SLOPE ANCHOR IN ROCK CUT SLOPE</p>	<p>STANDARD SHEET NO.: HW-911_03 C8.22</p>
<p>REV. DATE REVISION DESCRIPTION</p>						



**ELEVATION
MAST ARM**
SCALE: 1/4" = 1'-0"



**ELEVATION
COMBINATION MAST ARM**
SCALE: 1/4" = 1'-0"

MAST ARM ASSEMBLY NOTES

THE MAST ARM, INCLUDING THE ANCHORAGE TO THE FOUNDATION, SHALL BE DESIGNED, FABRICATED AND INSTALLED BY THE CONTRACTOR, OF THE SPAN SPECIFIED, IN ACCORDANCE WITH THE SPECIAL PROVISION "XX STEEL MAST ARM ASSEMBLY" OR "XX STEEL COMBINATION MAST ARM ASSEMBLY".

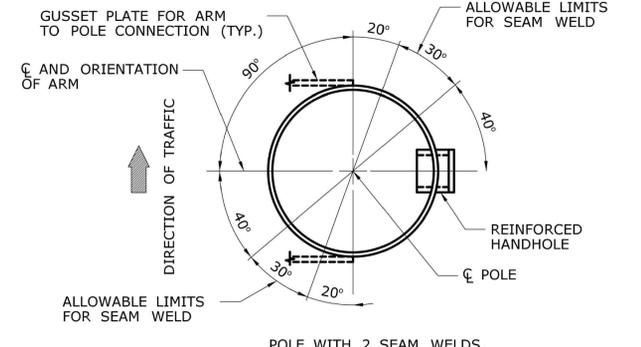
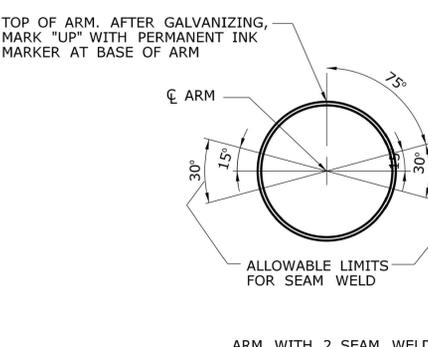
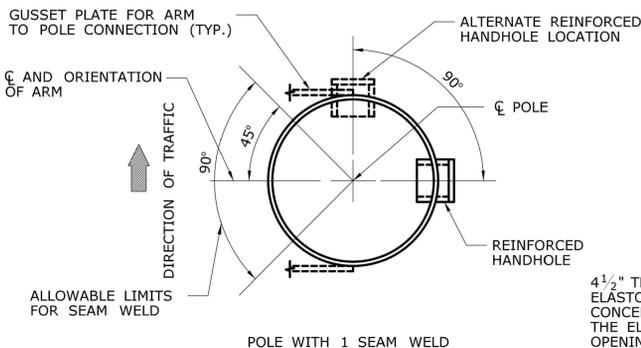
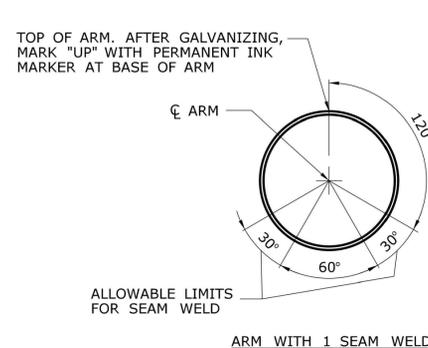
THE DIMENSIONS OF THE MAST ARM ASSEMBLY AND DETAILS OF THE TRAFFIC APPURTENANCES SUPPORTED BY THE MAST ARM ASSEMBLY ARE SHOWN ON THE TRAFFIC SIGNAL PLANS, ELEVATIONS, CROSS-SECTIONS OR IN THE SPECIAL PROVISIONS. THE ARM AND POLE LENGTHS AND THE ATTACHMENT HEIGHTS SHALL BE VERIFIED BY THE CONTRACTOR BASED ON THE FINISHED GRADE AT THE SITE, TOP OF FOUNDATION ELEVATION, THE LOCATIONS OF OVERHEAD UTILITY CABLES AND THE TRAFFIC APPURTENANCE MOUNTING HEIGHTS. IF EITHER THE ARM OR POLE LENGTH IS INADEQUATE, THE CONTRACTOR SHALL NOTIFY THE ENGINEER.

THE MAST ARMS SHALL BE DESIGNED IN ACCORDANCE WITH THE LATEST EDITION OF THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, INCLUDING THE LATEST INTERIM SPECIFICATIONS, AS AMENDED BY THE AS SPECIAL PROVISION "XX STEEL MAST ARM ASSEMBLY" OR "XX STEEL COMBINATION MAST ARM ASSEMBLY".

THE MAST ARM SHALL BE DESIGNED FOR THE LOAD EFFECTS DUE TO THE ACTUAL TRAFFIC APPURTENANCES (SIGNALS, SIGNS, LUMINAIRES, CAMERAS, ETC.). THE MAST ARMS SHALL ALSO BE DESIGNED FOR THE EFFECTS OF TRAFFIC APPURTENANCES DURING ALL STAGES OF CONSTRUCTION THAT MAY EXIST DURING THE PROJECT UNDER WHICH THE MAST ARMS ARE INSTALLED.

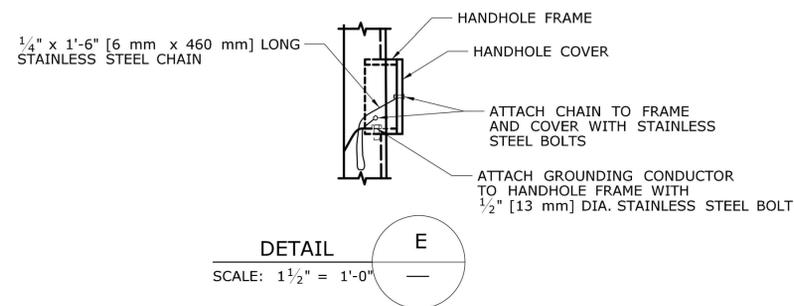
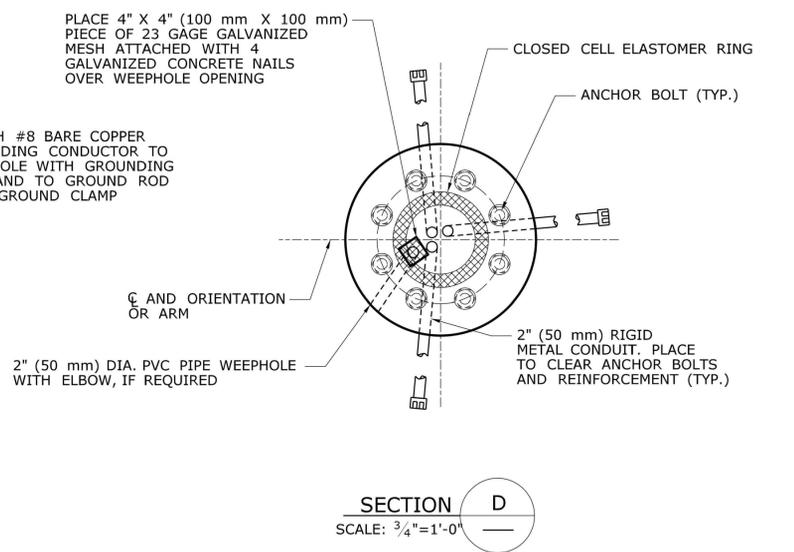
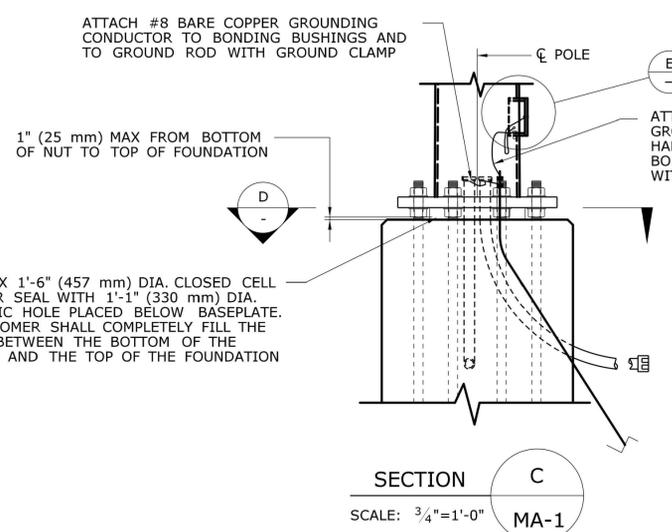
THE MAST ARMS SHALL BE DESIGNED TO SUPPORT TRAFFIC APPURTENANCES WITH PROPERTIES NO LESS THAN THOSE SHOWN IN THE TABLE ENTITLED "TRAFFIC APPURTENANCE PROPERTIES - MINIMUM DESIGN VALUES".

DESIGNER/DRAFTER: -		<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>	SIGNATURE/ BLOCK: -	PROJECT TITLE: -	TOWN: -	PROJECT NO. -	
CHECKED BY: -			<p>OFFICE OF ENGINEERING</p>	APPROVED BY: -	DATE: -	DRAWING TITLE: MAST ARM ASSEMBLY ELEVATION	DRAWING NO. MA-1
SCALE AS NOTED		Filename: ...XXXXXXX_SB_MastArm_MA1_Elev.dgn			SHEET NO. C8.23		
REV.	DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 4/14/2010			



SECTION A
SCALE: 1 1/2" = 1'-0"
MA-1

SECTION B
SCALE: 1 1/2" = 1'-0"
MA-1



IDENTIFICATION TAG
NTS

TRAFFIC APPURTENANCE PROPERTIES
MINIMUM DESIGN VALUES

	2'-0" 610	2'-0" 610	2'-0" 610	3'-2" 965	WIDTH HEIGHT
					SHEET ALUMINUM SIGN PANEL
	3 SECTION, 12" (305) DIA. TRAFFIC SIGNAL W/ BACKPLATE	4 SECTION, 12" (305) DIA. TRAFFIC SIGNAL W/ BACKPLATE	5 SECTION, 12" (305) DIA. TRAFFIC SIGNAL W/ BACKPLATE	5 SECTION, 12" (305) DIA. TRAFFIC SIGNAL W/ BACKPLATE	
WEIGHT, INCLUDING MOUNTING HARDWARE	65 LBS (29.48 kg)	80 LBS (36.29 kg)	95 LBS (43.09 kg)	105 LBS (47.63 kg)	4 LBS/SQ.FT. (19.53 kg/m ²)
TOTAL SURFACE AREA	28.04 SQ. FT. (2.61 m ²)	35.46 SQ. FT. (3.29 m ²)	45.16 SQ. FT. (4.20 m ²)	41.04 SQ. FT. (3.81 m ²)	BASED ON PANEL DIMENSIONS
PROJECTED AREA, FRONT FACE	8.62 SQ. FT. (0.80 m ²)	10.91 SQ. FT. (1.01 m ²)	13.34 SQ. FT. (1.24 m ²)	13.72 SQ. FT. (1.28 m ²)	BASED ON PANEL DIMENSIONS
PROJECTED AREA, BOTTOM FACE	1.18 SQ. FT. (0.11 m ²)	1.18 SQ. FT. (0.11 m ²)	1.18 SQ. FT. (0.11 m ²)	2.58 SQ. FT. (0.24 m ²)	BASED ON PANEL DIMENSIONS

NOTES:
THE TABULATED VALUES ARE THE MINIMUM VALUES THAT SHALL BE USED FOR THE DESIGN.
MAST ARMS SHALL BE DESIGNED ASSUMING ALL TRAFFIC SIGNALS ARE COMPOSED OF 12" (305 mm) DIAMETER SECTIONS WITH BACKPLATES.
THE PROJECTED FRONT FACE AREA IS IN A PLANE PARALLEL TO THE PLANE FORMED BY THE ARM AND THE POLE.
IF MULTIPLE APPURTENANCES ARE ATTACHED AT THE SAME LOCATION, THE MINIMUM DESIGN VALUE SHALL BE NO LESS THAN THE SUM OF THE CORRESPONDING TRAFFIC APPURTENANCE PROPERTIES.
FOR TRAFFIC APPURTENANCES NOT SHOWN, THE PROPERTIES SHALL BE DETERMINED BY THE CONTRACTOR AND SUBMITTED FOR REVIEW WITH THE WORKING DRAWING SUBMITTAL.

DESIGNER/DRAFTER: - CHECKED BY: -	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	SIGNATURE/ BLOCK: - APPROVED BY: DATE:	PROJECT TITLE: -	TOWN: -	PROJECT NO. -
THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	SCALE AS NOTED	Plotted Date: 4/14/2010	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	DRAWING TITLE: MAST ARM ASSEMBLY DETAILS	DRAWING NO. MA-2 SHEET NO. C8.24
REV. DATE	REVISION DESCRIPTION	SHEET NO.	Filename: ...XXXXXXX.SB.MastArm.MA1.Elev.dgn		

FOUNDATION NOTES

THE MAST ARM FOUNDATION IS DESIGNED IN ACCORDANCE WITH THE LATEST EDITION OF THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, WITH THE LATEST INTERIM SPECIFICATIONS.

THE FOUNDATION EMBEDMENT IS DESIGNED FOR MAXIMUM LOAD EFFECTS, APPLIED AT THE TOP OF THE FOUNDATION, NO GREATER THAN THE FOLLOWING:

- AXIAL COMPRESSIVE FORCE: 5.00 K
- RESULTANT SHEAR FORCE: 6.30 K
- TORSION: 120 FT-K
- RESULTANT BENDING MOMENT: 113 FT-K

THE USE OF THE FOUNDATION IS NOT PERMITTED IF THE COMPUTED REACTIONS FROM THE CONTRACTOR DESIGNED MAST ARM ASSEMBLY EXCEED THE ABOVE LOAD EFFECTS.

THE ENGINEER SHALL BE NOTIFIED IF THE SLOPE OF THE FINISHED GRADE AT THE FOUNDATION EXCEEDS THE MAXIMUM PERMITTED SLOPE.

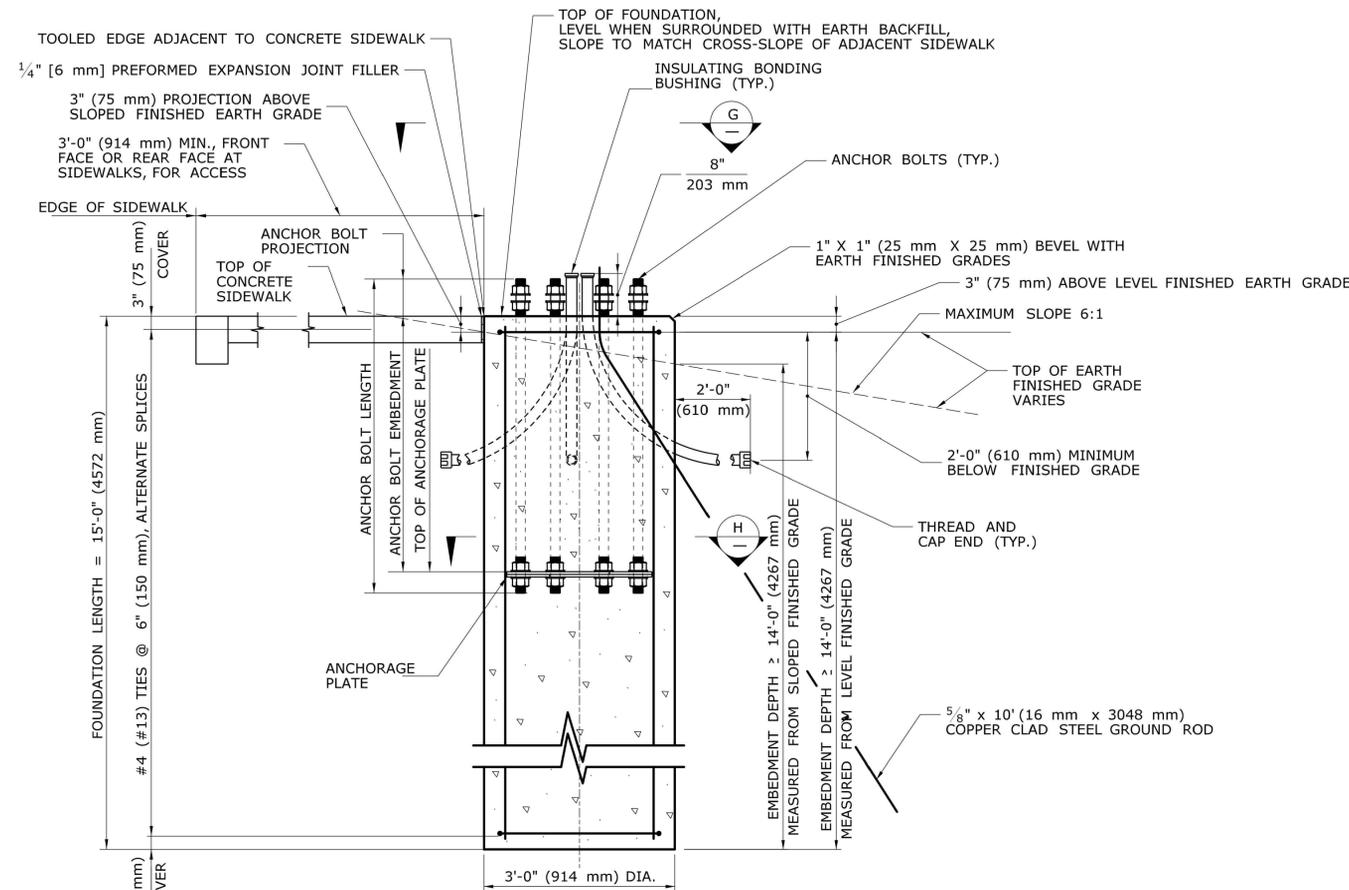
THE CONCRETE FOR THE FOUNDATION SHALL CONFORM TO "CLASS "A" CONCRETE".

THE REINFORCEMENT SHALL BE UNCOATED AND CONFORM TO ASTM A615, GRADE 60 (ASTM A615M, GRADE 420). THE REINFORCEMENT SHALL BE ASSEMBLED WITH WIRE TIES. WELDING TO ASSEMBLE REINFORCEMENT IS NOT PERMITTED. ALL REINFORCEMENT SHALL HAVE 3" (75 mm) COVER, UNLESS OTHERWISE NOTED.

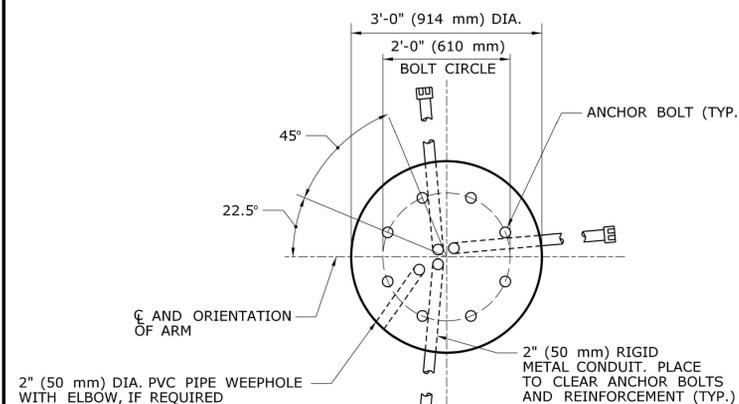
THE CONCRETE SHALL BE PLACED IN A AUGERED HOLE AGAINST UNDISTURBED EARTH.

THE MAST ARM SHALL NOT BE ERECTED ON THE FOUNDATION UNTIL AFTER THE CONCRETE HAS ATTAINED A 28 DAY COMPRESSIVE STRENGTH, f_c , GREATER THAN OR EQUAL TO 3000 PSI (21 MPa).

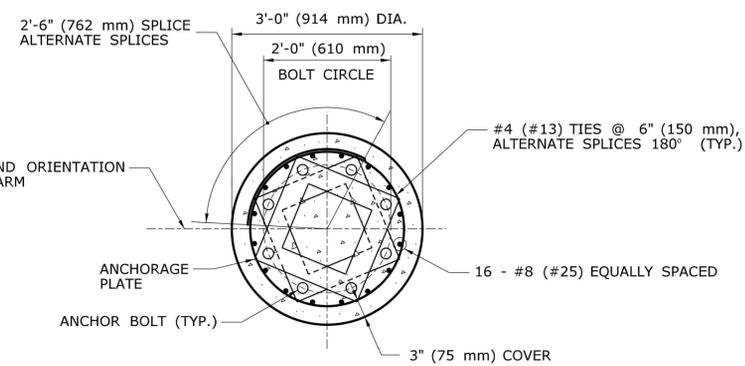
THE COST OF THE FOUNDATION, INCLUDING THE EXCAVATION, CONCRETE AND REINFORCEMENT, SHALL BE PAID FOR UNDER THE ITEM "TRAFFIC CONTROL FOUNDATION - MAST ARM".



SECTION F
SCALE: 3/4"=1'-0"
MA-1



SECTION G
SCALE: 3/4"=1'-0"

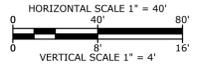
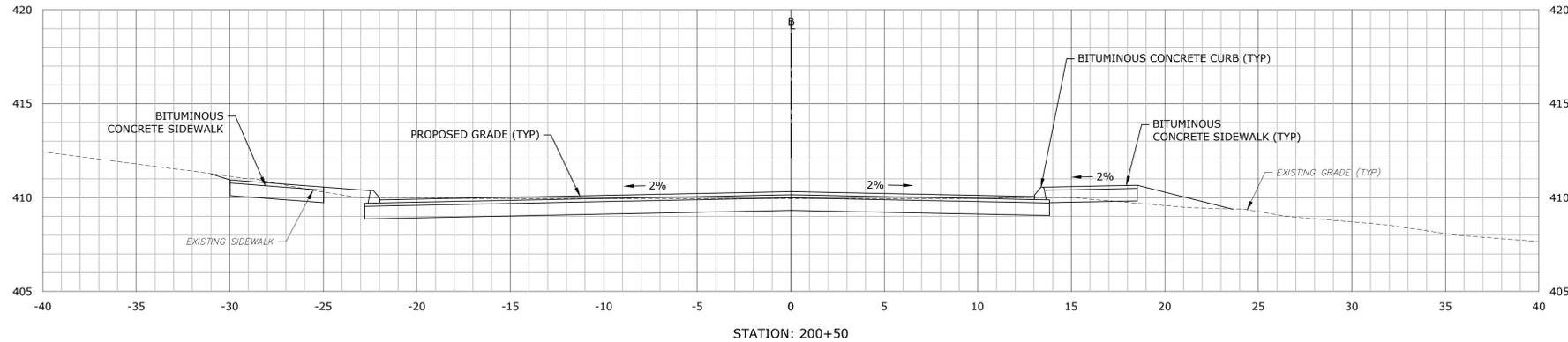
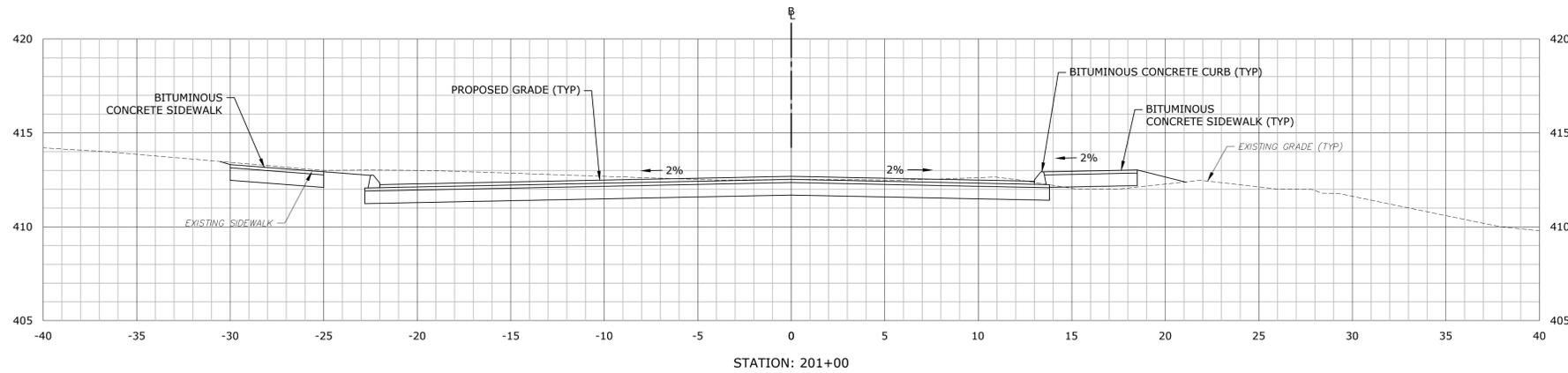
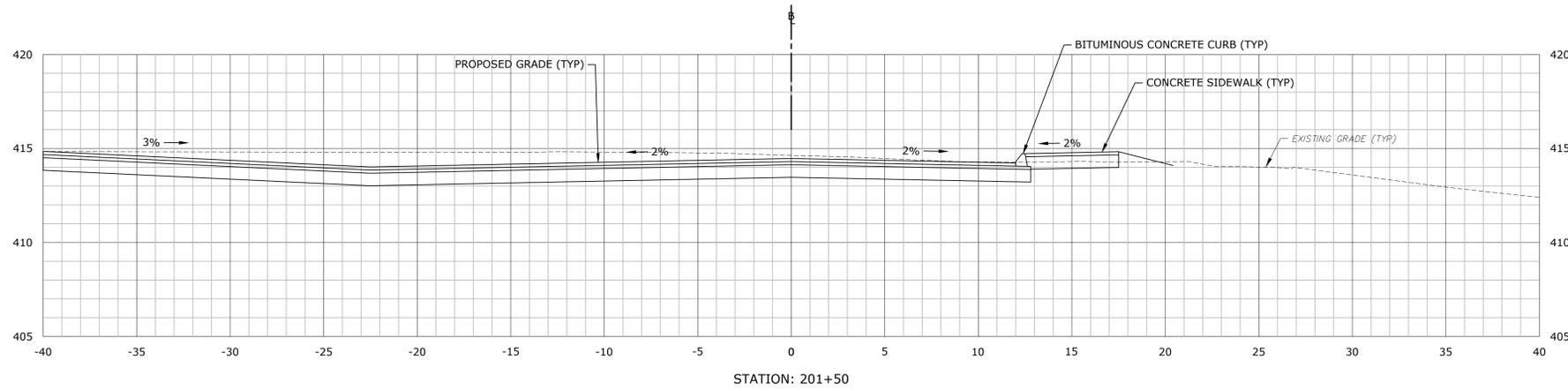


SECTION H
Scale: 3/4"=1'-0"

ATTENTION USER:

THE USER OF THESE FOUNDATION DETAILS IS RESPONSIBLE FOR DETERMINING THE VALUES FOR THE MAXIMUM LOAD EFFECTS APPLIED AT THE TOP OF THE FOUNDATION AND ADDING THE VALUES TO THE ABOVE NOTES. THE USER SHALL ENSURE THAT THE FOUNDATION IS ONLY USED FOR MAST ARM ASSEMBLIES WITH COMPUTED REACTIONS THAT DO NOT EXCEED THESE MAXIMUM LOAD EFFECTS. THIS NOTE SHALL BE REMOVED, BY THE USER, PRIOR TO INCORPORATING THESE FOUNDATION DETAILS INTO THE CONTRACT DOCUMENTS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: - CHECKED BY: -	<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>	SIGNATURE/BLOCK: - APPROVED BY: _____ DATE: _____	PROJECT TITLE: -	TOWN: -	PROJECT NO.: -
REV. DATE REVISION DESCRIPTION SHEET NO.	Plotted Date: 4/14/2010	SCALE AS NOTED		FILENAME: ...XXXXXX-SB-MastArm.MA1-Elev.dgn	DRAWING TITLE: MAST ARM ASSEMBLY FOUNDATION DETAILS	DRAWING NO.: MA-3	SHEET NO.: C8.25



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

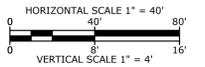
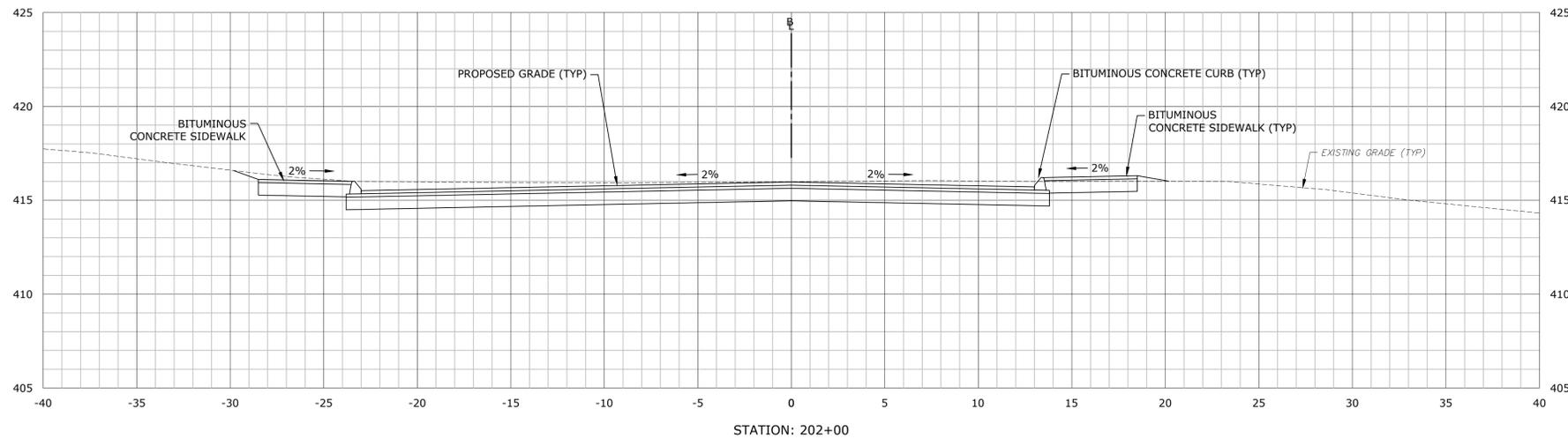
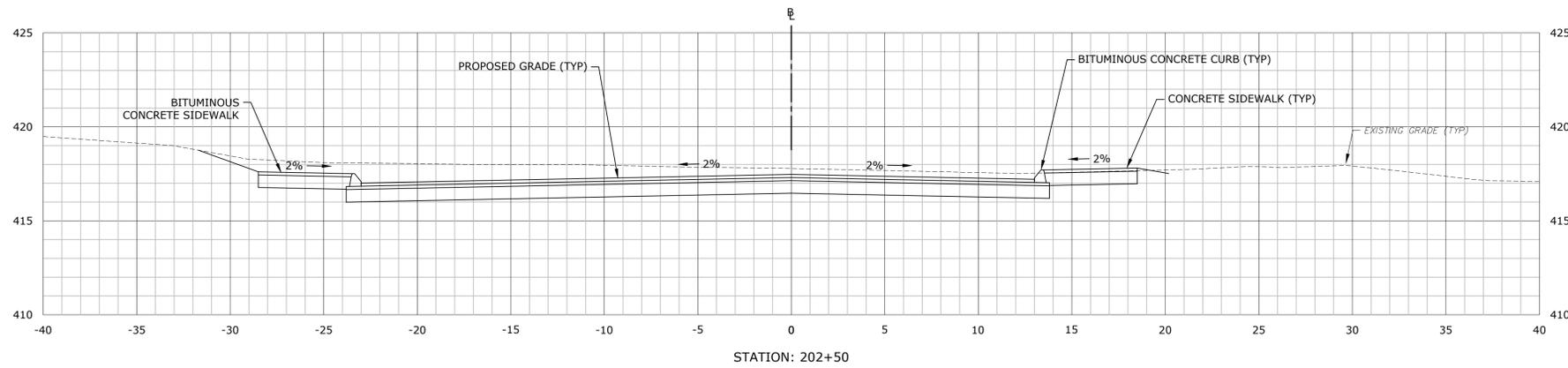
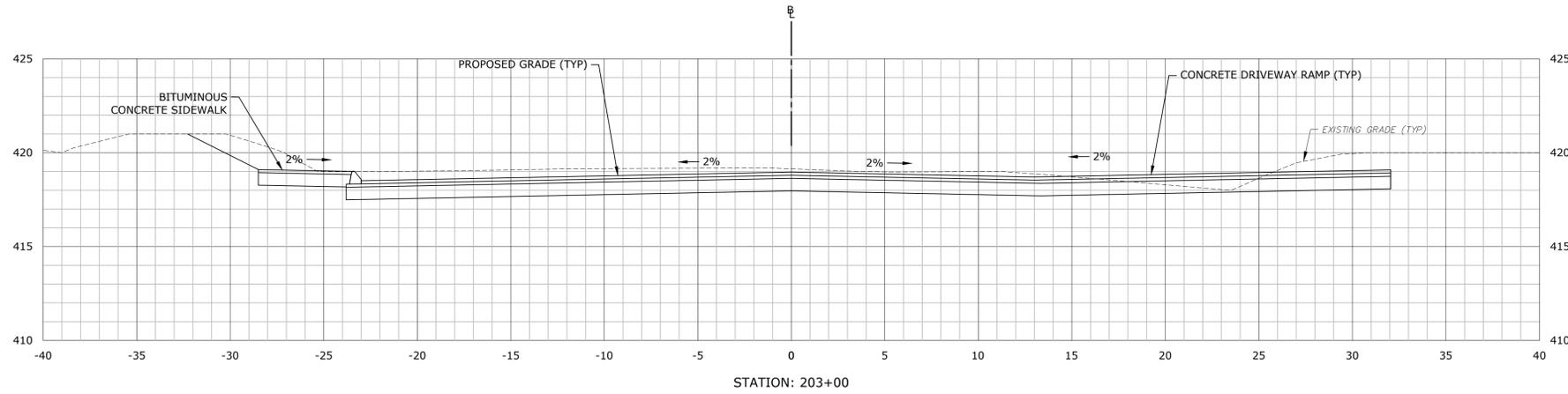
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	CRD
CHECKED BY:	CRD
APPROVED BY:	AJM

**DANIELS FARM ROAD
 CROSS SECTIONS
 STA: 200+50 TO 201+50**

SCALE: 1" = 4'

C9.0



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

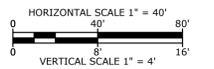
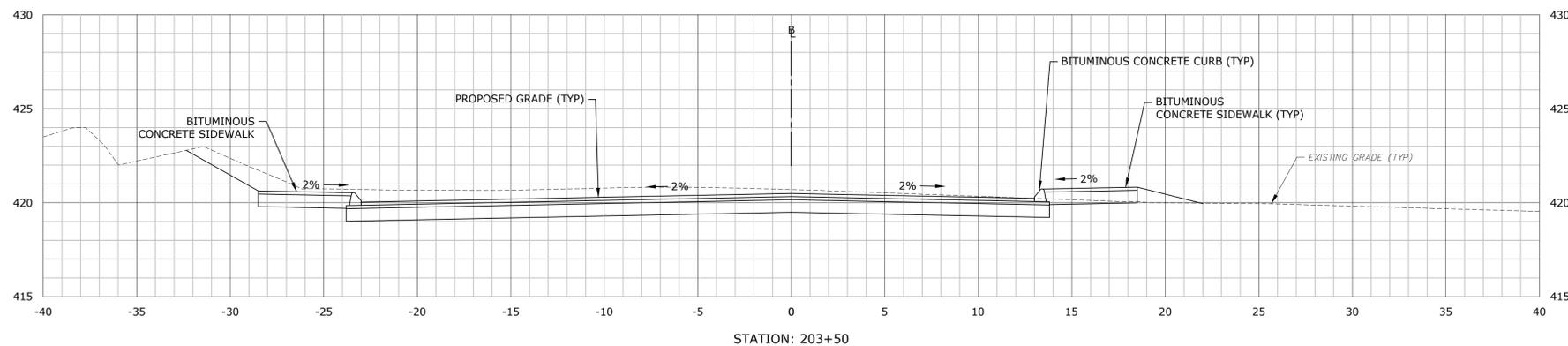
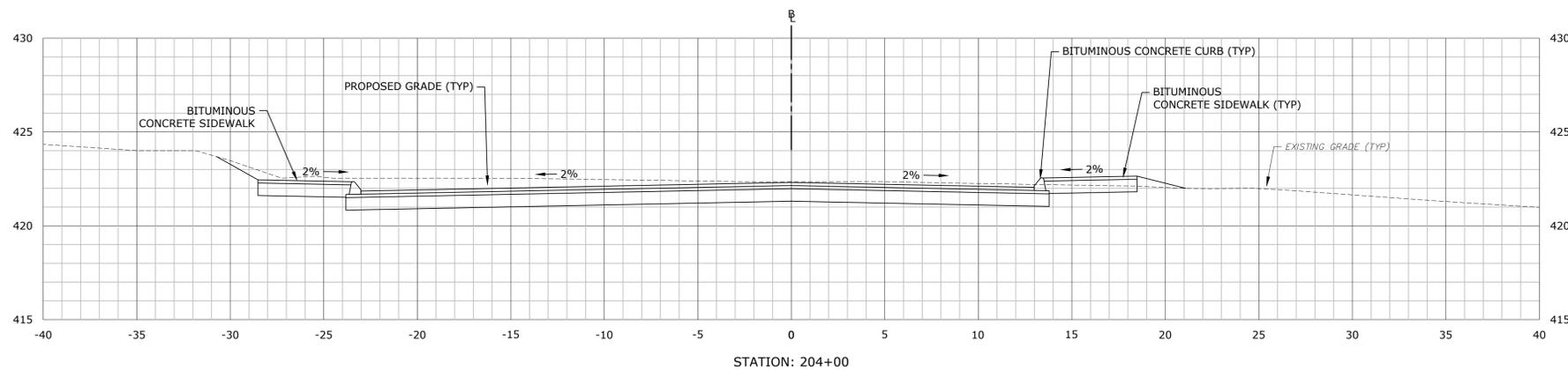
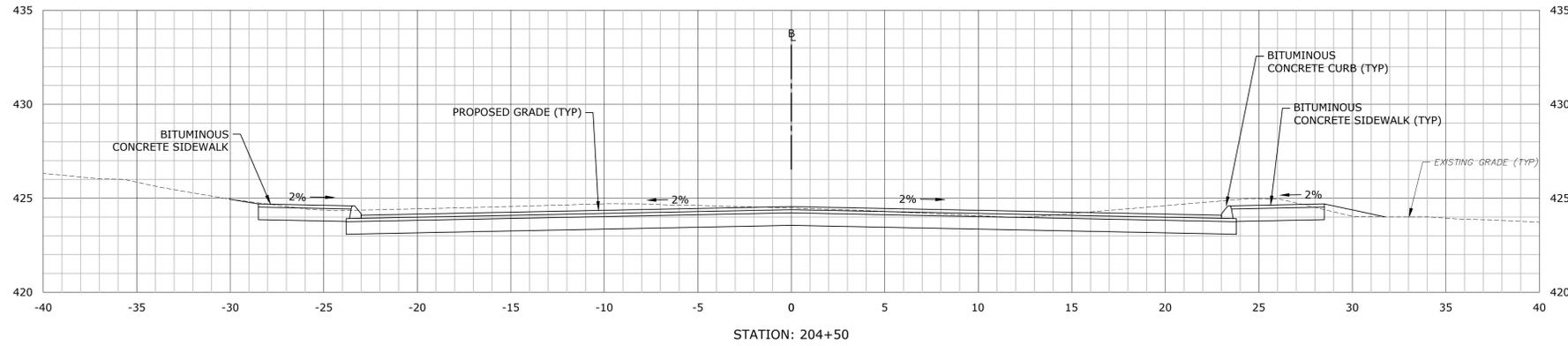
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	CRD
CHECKED:	CRD
APPROVED:	AJM

**DANIELS FARM ROAD
 CROSS SECTIONS
 STA: 202+00 TO 203+00**

SCALE: 1" = 4'

C9.1



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

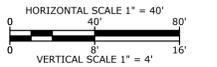
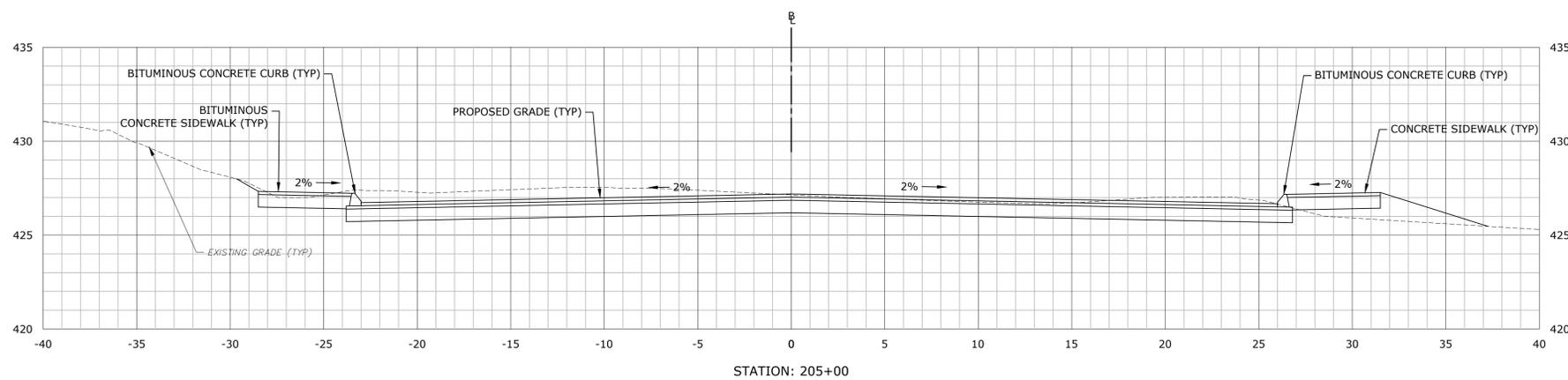
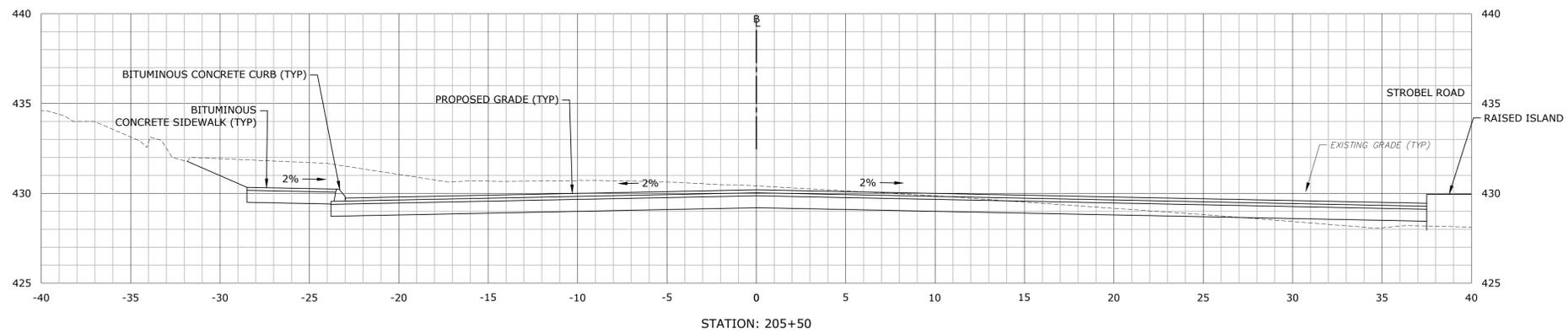
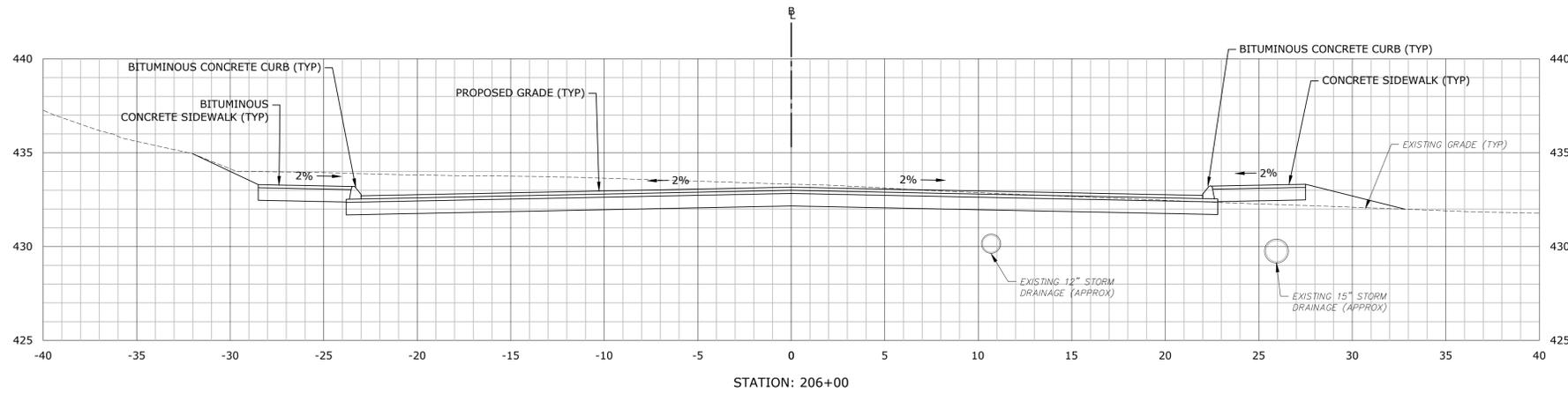
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	CRD
CHECKED:	CRD
APPROVED:	AJM

**DANIELS FARM ROAD
 CROSS SECTIONS
 STA: 203+50 TO 204+50**

SCALE: 1" = 4'

C9.2



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

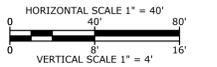
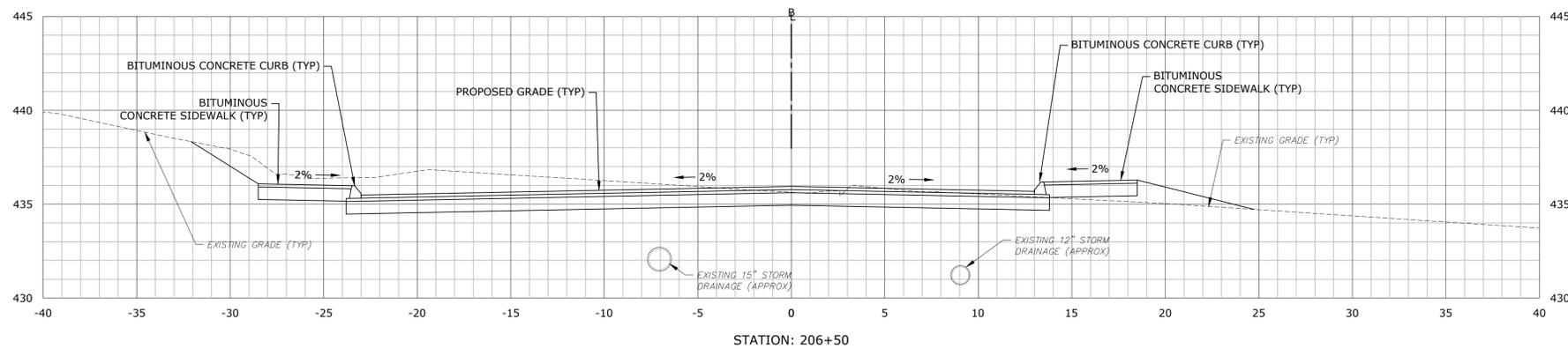
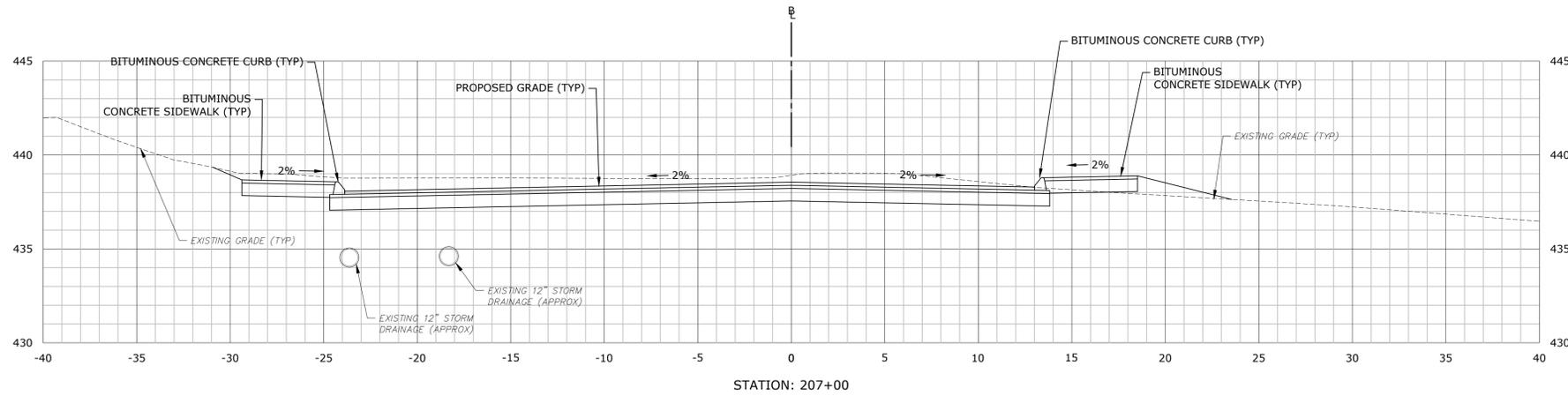
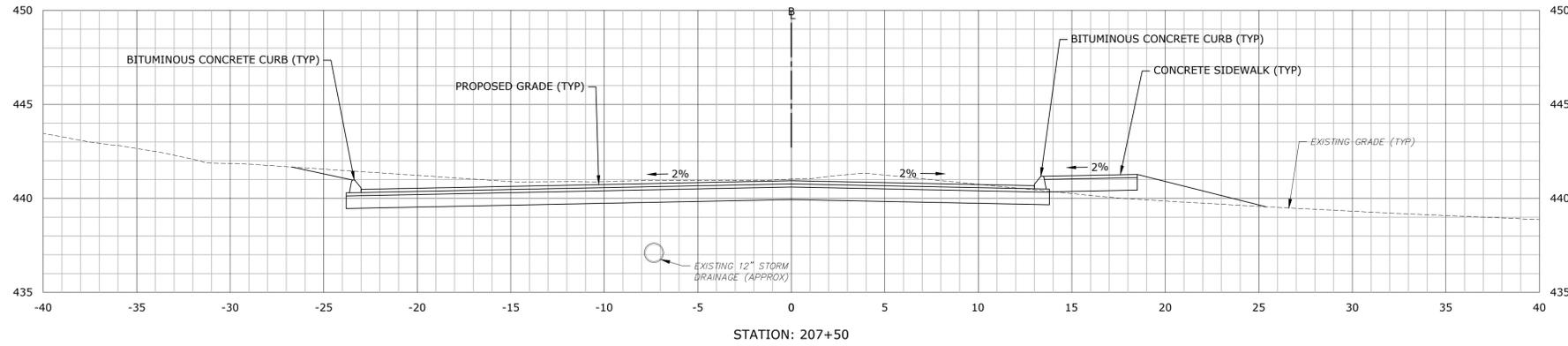
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PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	CRD
CHECKED BY:	CRD
APPROVED BY:	AJM

DANIELS FARM ROAD
 CROSS SECTIONS
 STA: 205+00 TO 206+00

SCALE: 1" = 4'

C9.3



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

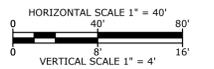
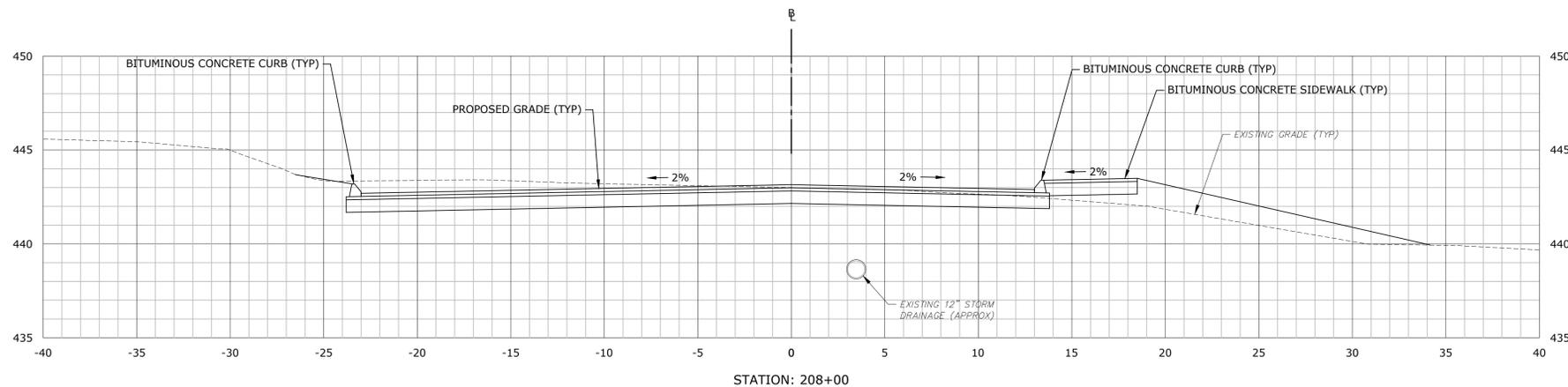
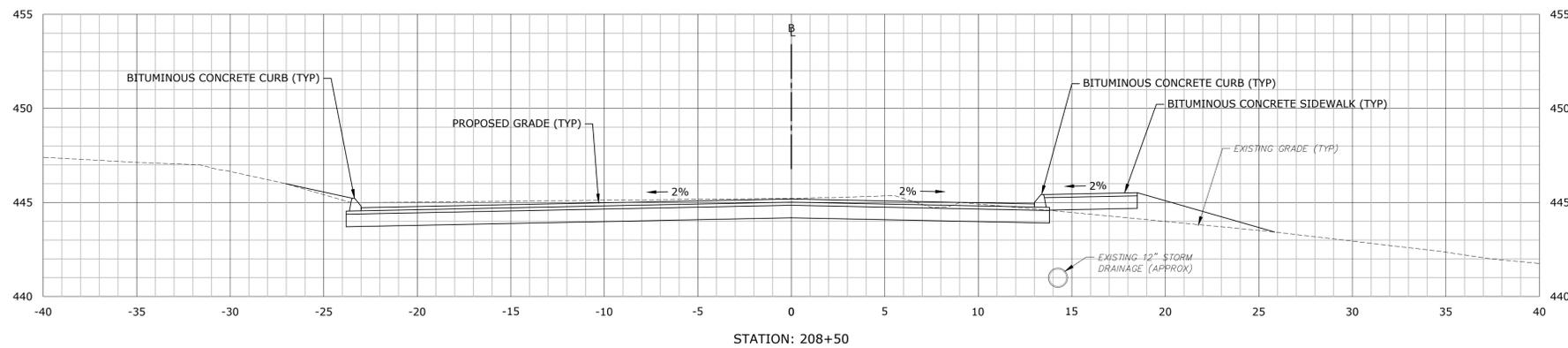
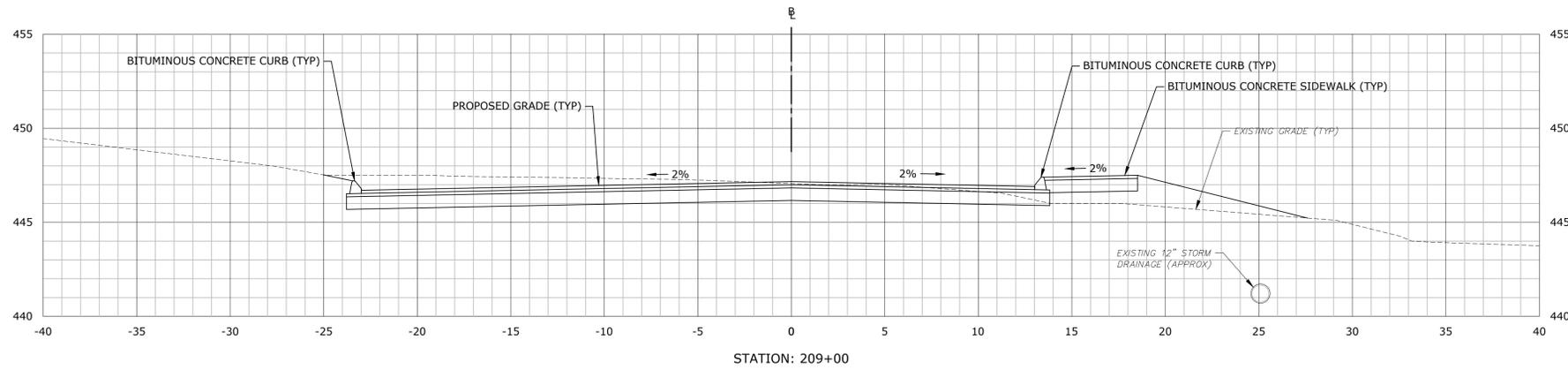
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	CRD
CHECKED BY:	CRD
APPROVED BY:	AJM

**DANIELS FARM ROAD
 CROSS SECTIONS
 STA: 206+50 TO 207+50**

SCALE: 1" = 4'

C9.4



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

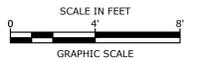
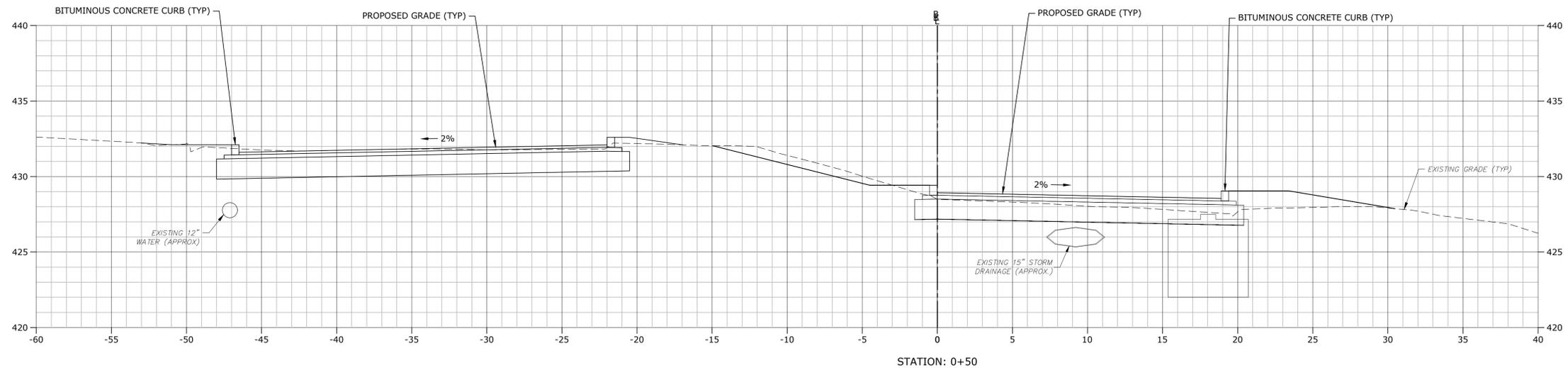
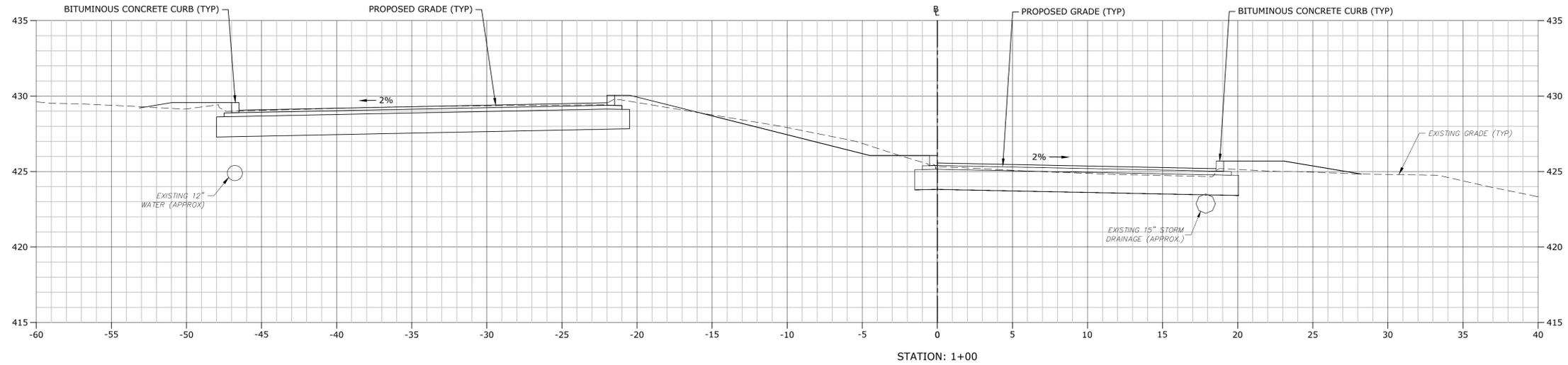
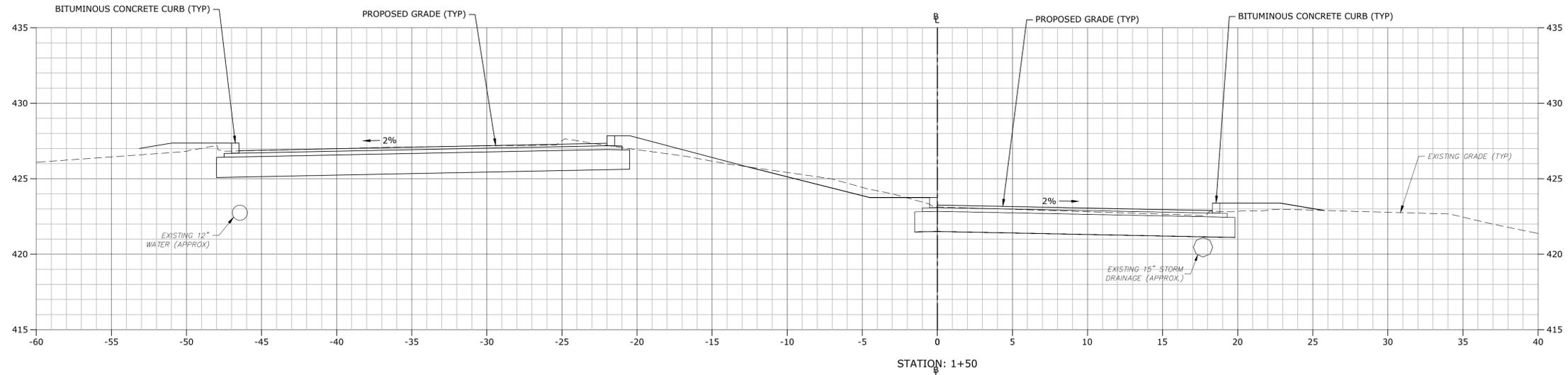
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: CRD
 CHECKED: CRD
 APPROVED: AJM

**DANIELS FARM ROAD
 CROSS SECTIONS
 STA: 208+00 TO 209+00**

SCALE: 1" = 4'

C9.5



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON
 ORIGINAL DRAWING
 IF NOT ONE INCH ON
 THIS SHEET, ADJUST
 SCALES ACCORDINGLY

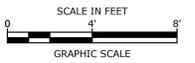
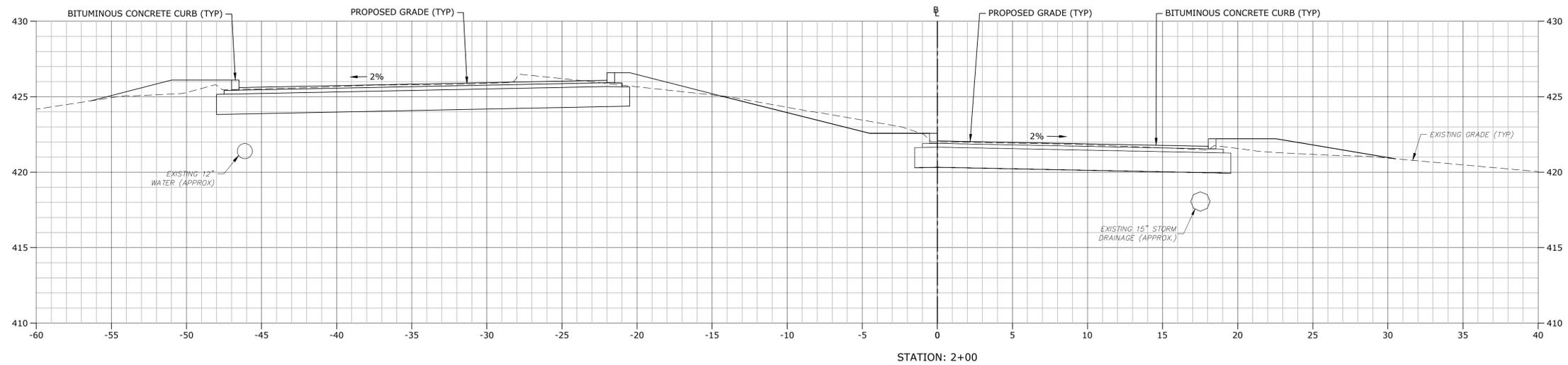
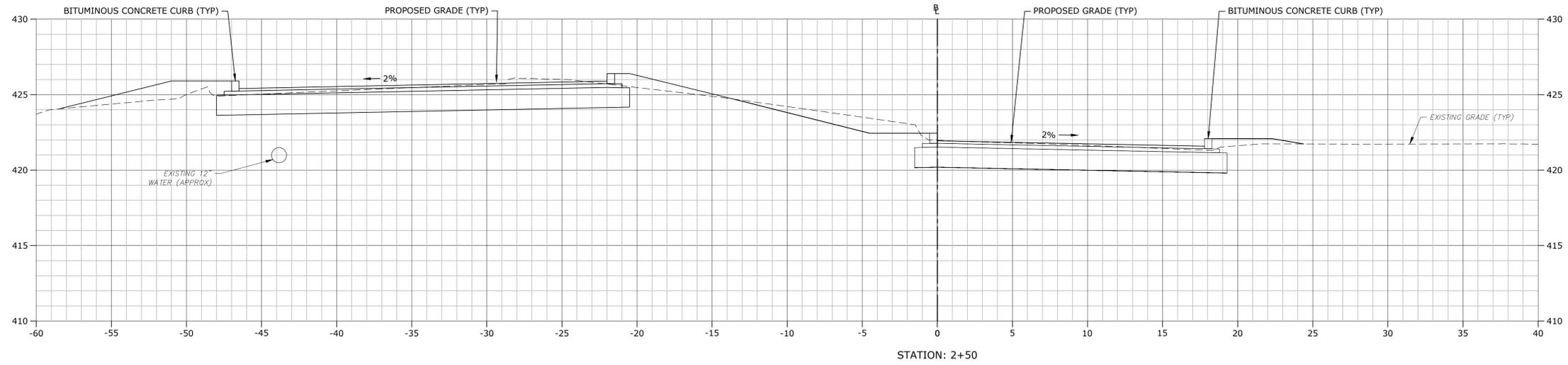
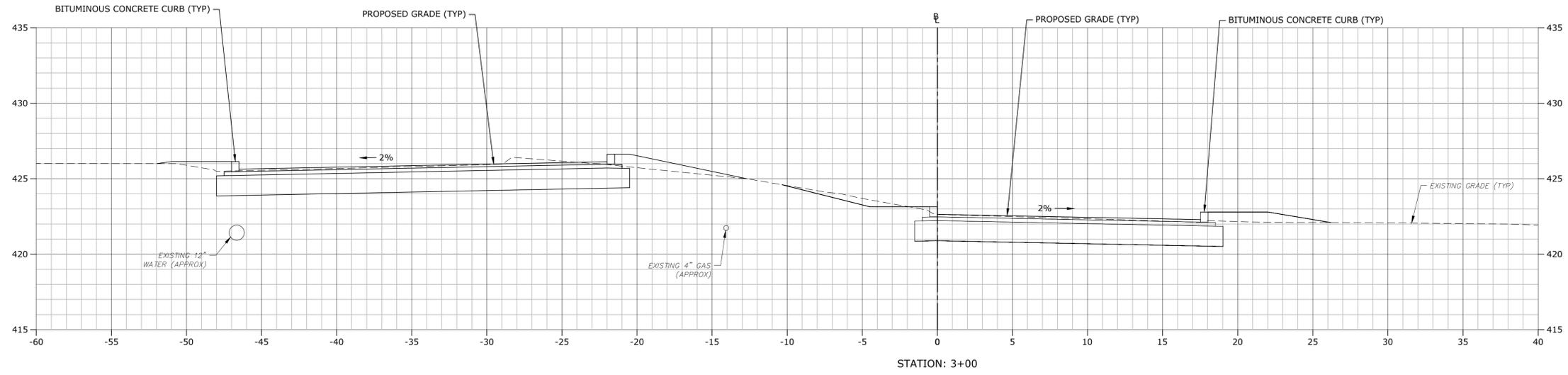
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 0+50 TO 1+50**

SCALE: 1" = 4'

C9.7



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

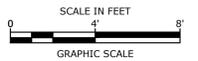
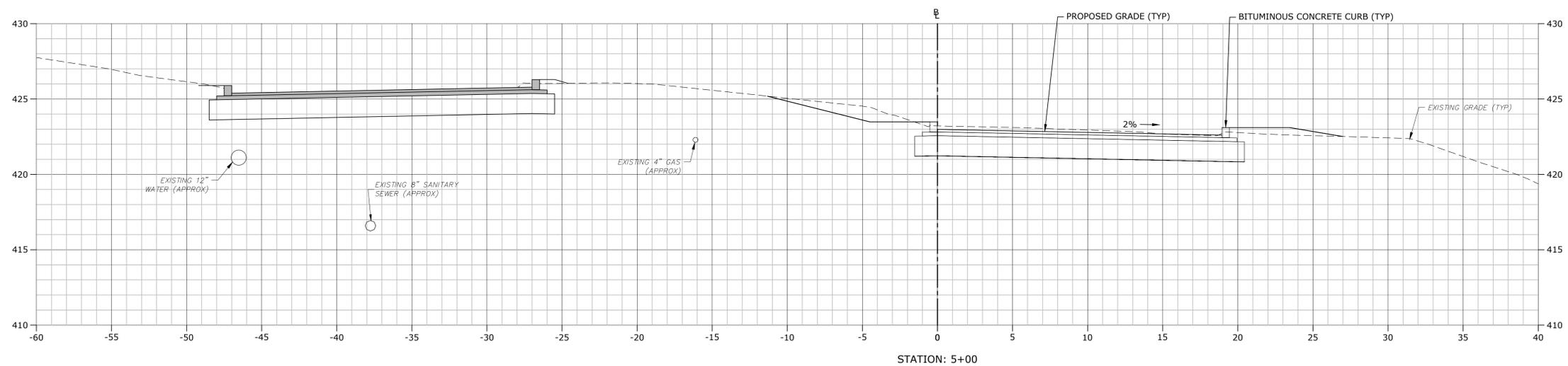
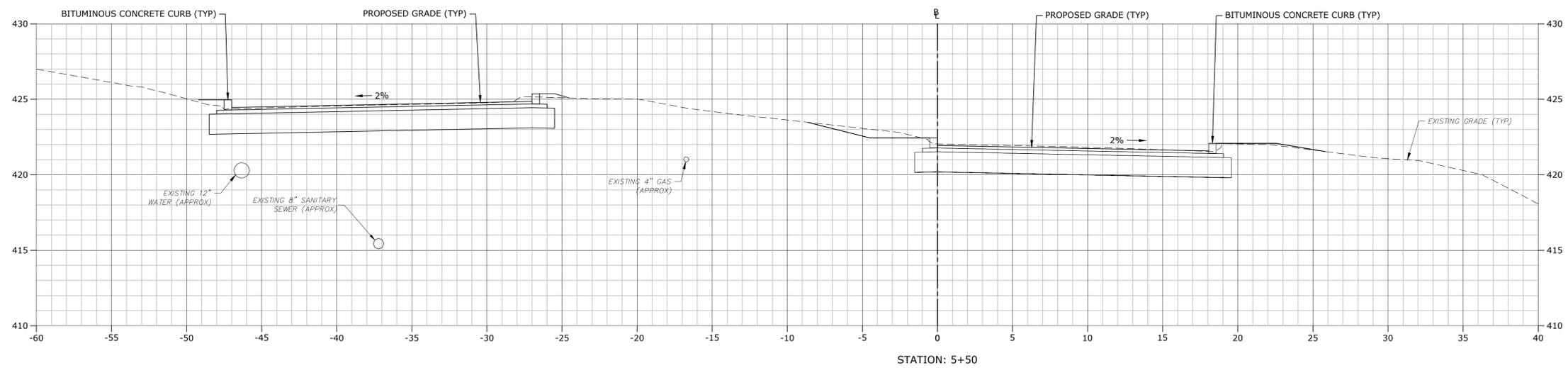
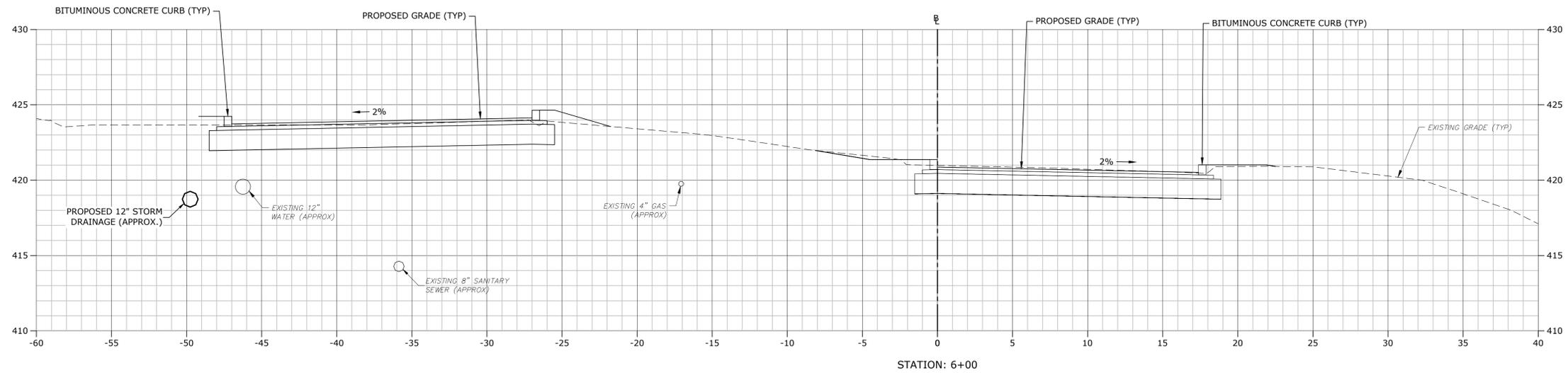
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 2+00 TO 3+00**

SCALE: 1" = 4'

C9.8



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

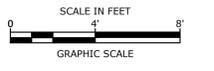
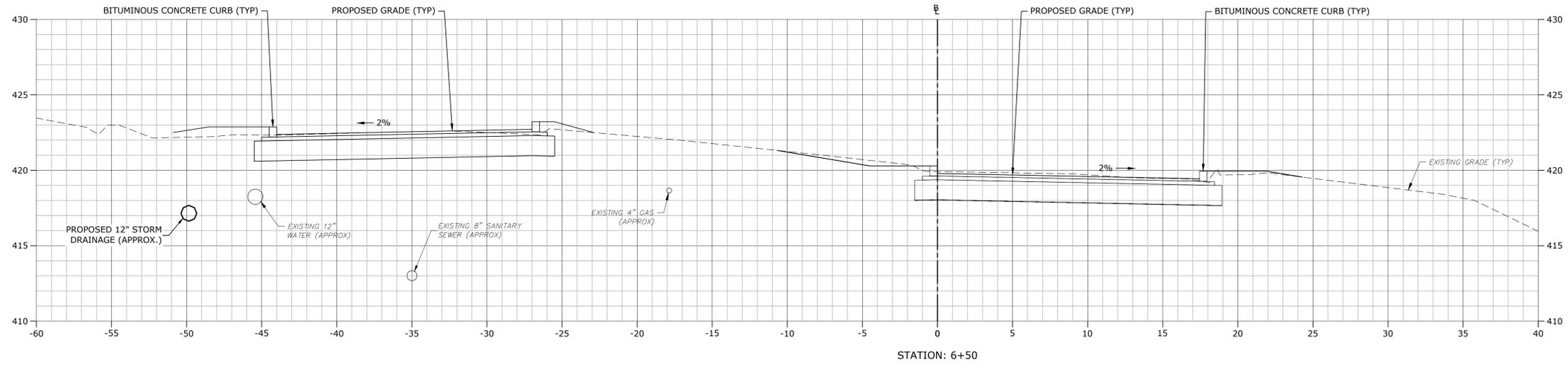
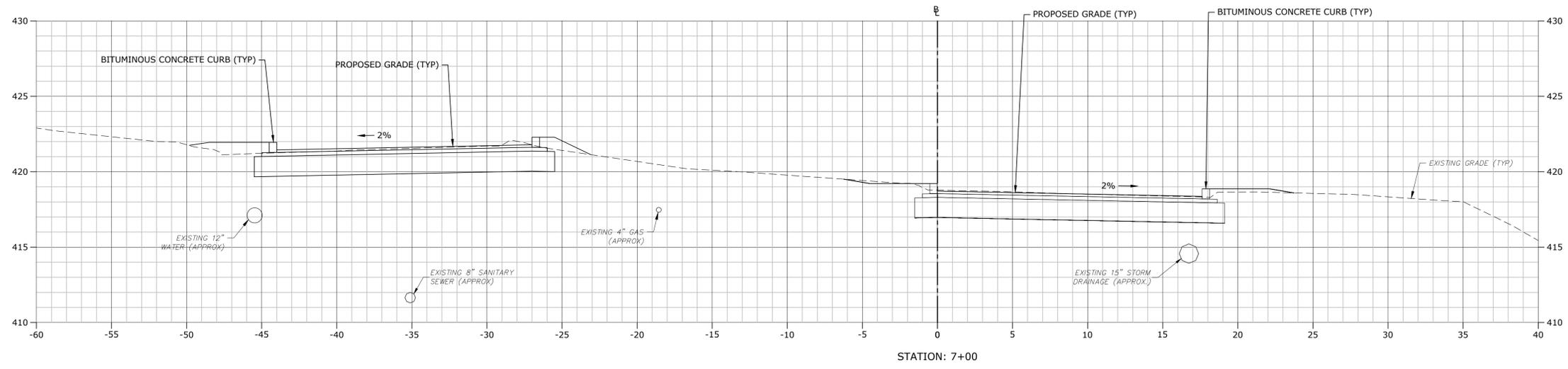
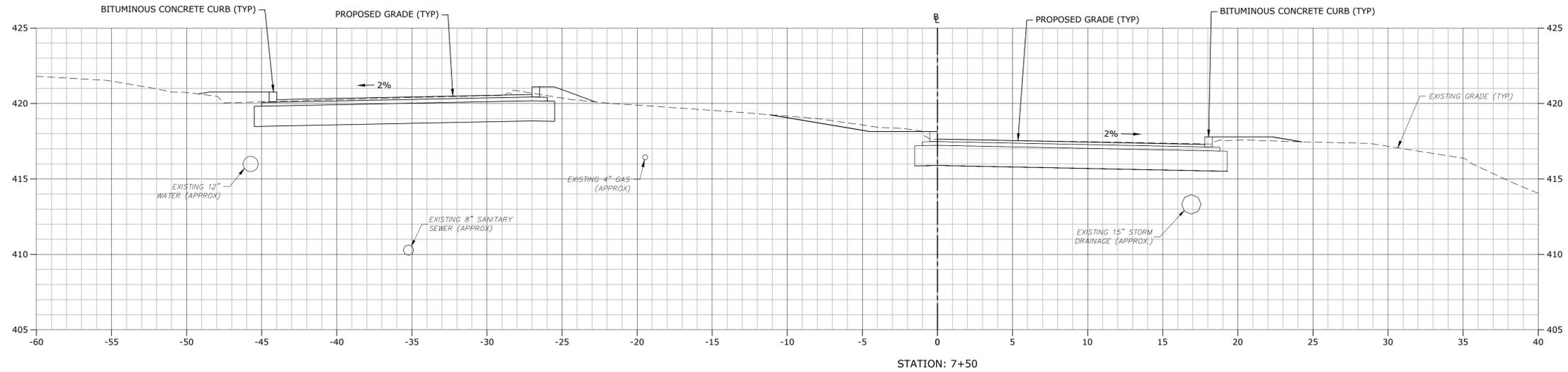
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 5+00 TO 6+00**

SCALE: 1" = 4'

C9.10



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

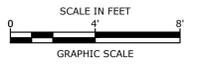
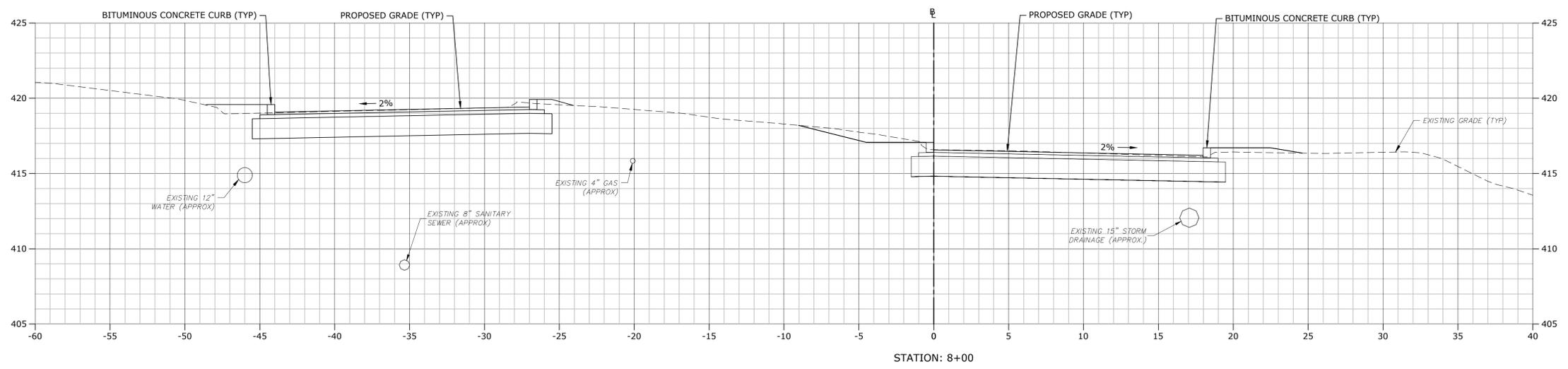
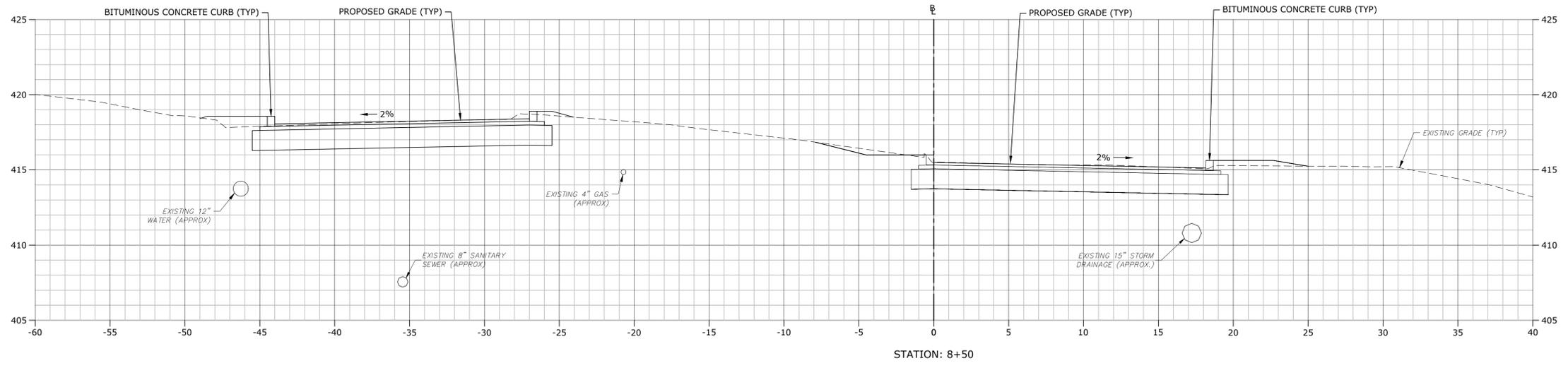
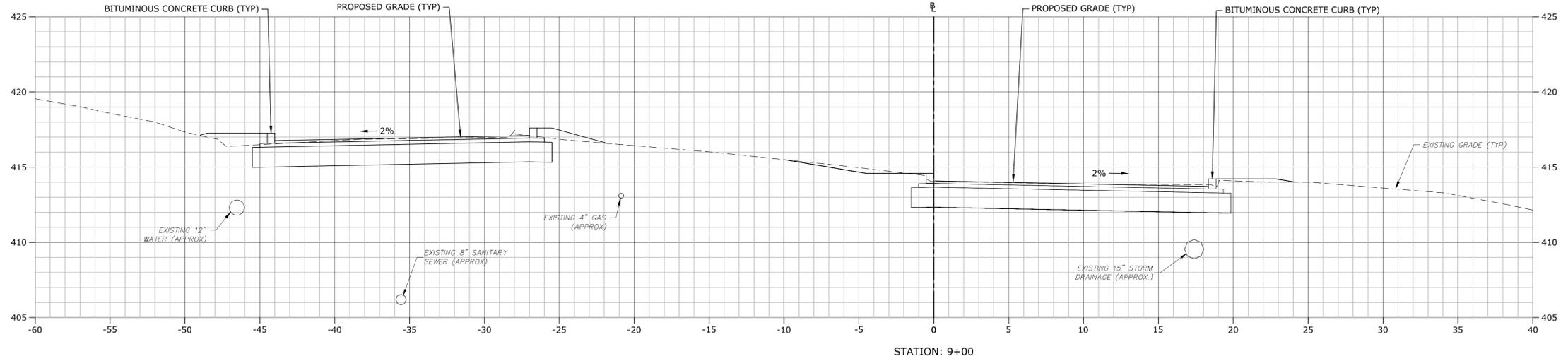
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-15-C-900-XS.dwg	
DRAWN BY:	MDS	
CHECKED:	CRD	
APPROVED:	JWB	

**STROBEL ROAD
 CROSS SECTIONS
 STA: 6+50 TO 7+50**

SCALE: 1" = 4'

C9.11



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

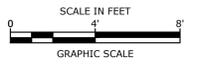
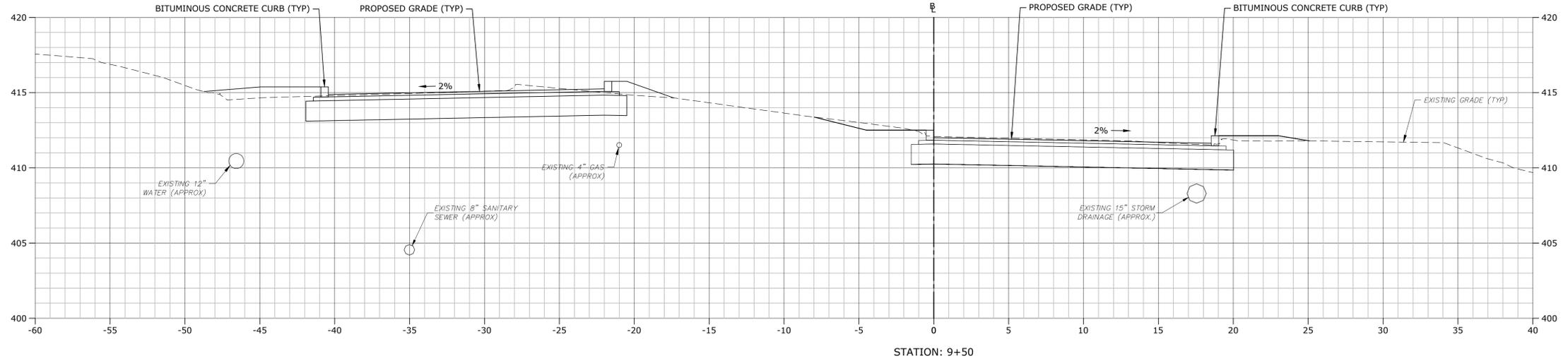
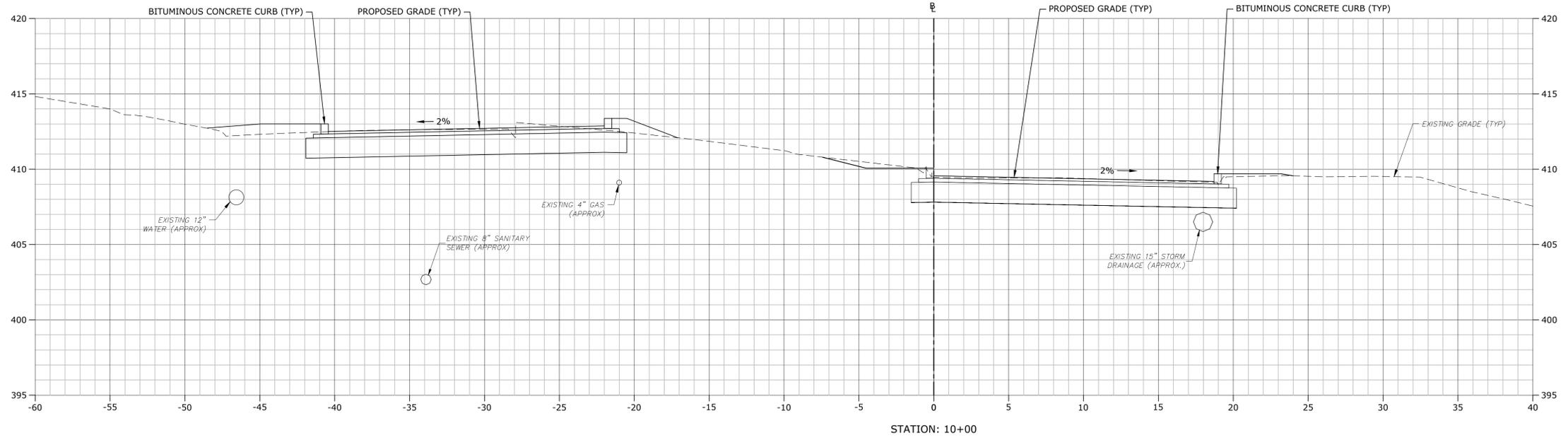
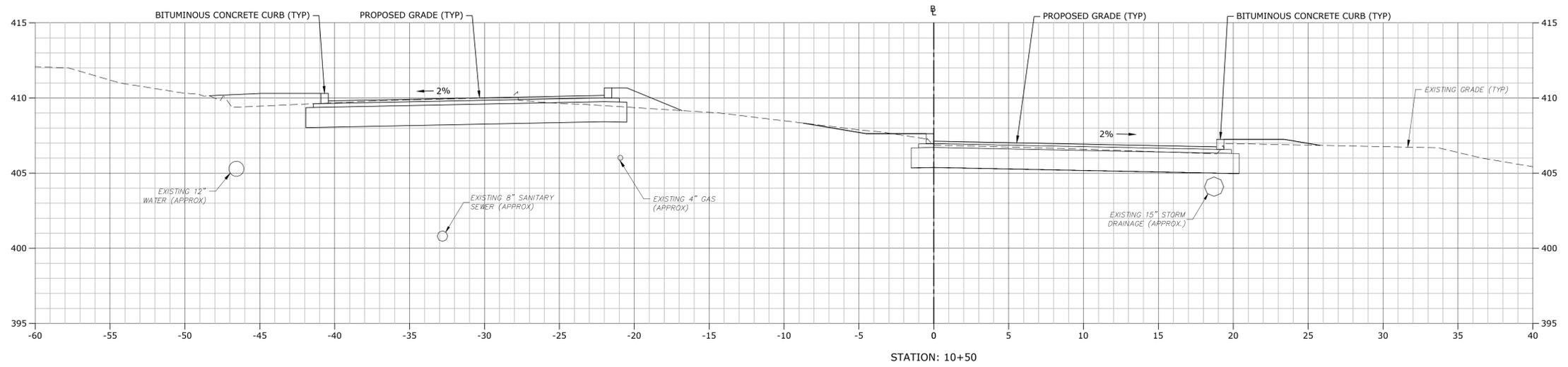
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 8+00 TO 9+00**

SCALE: 1" = 4'

C9.12



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

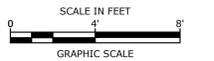
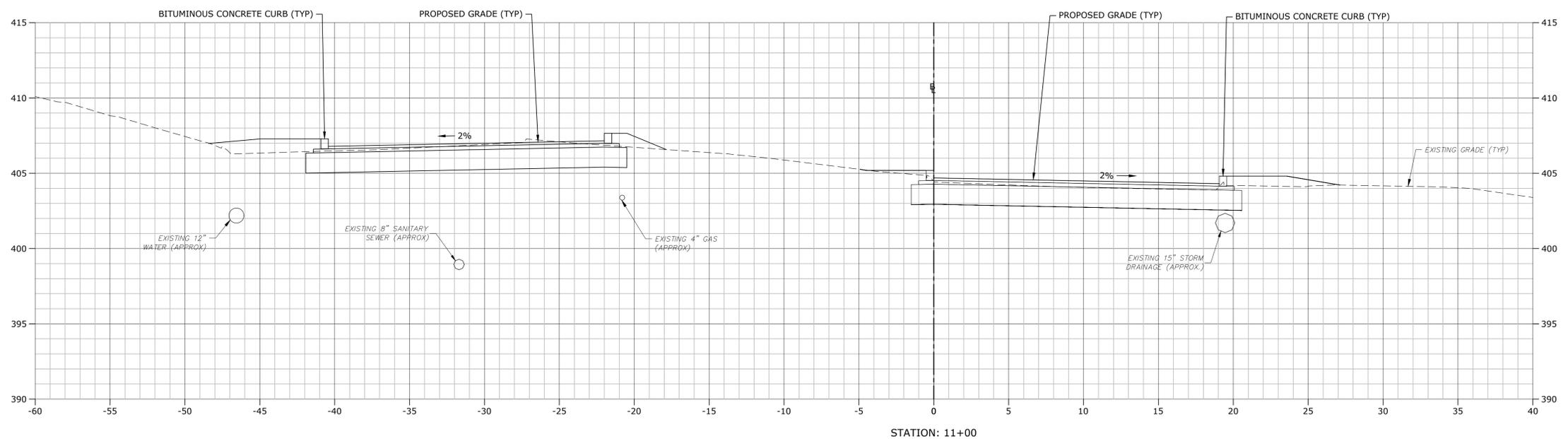
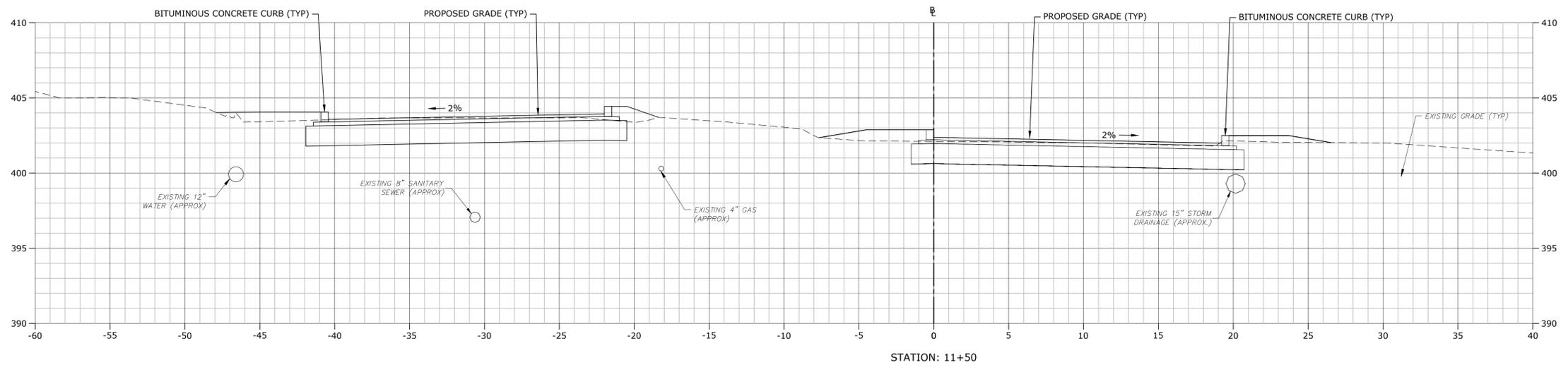
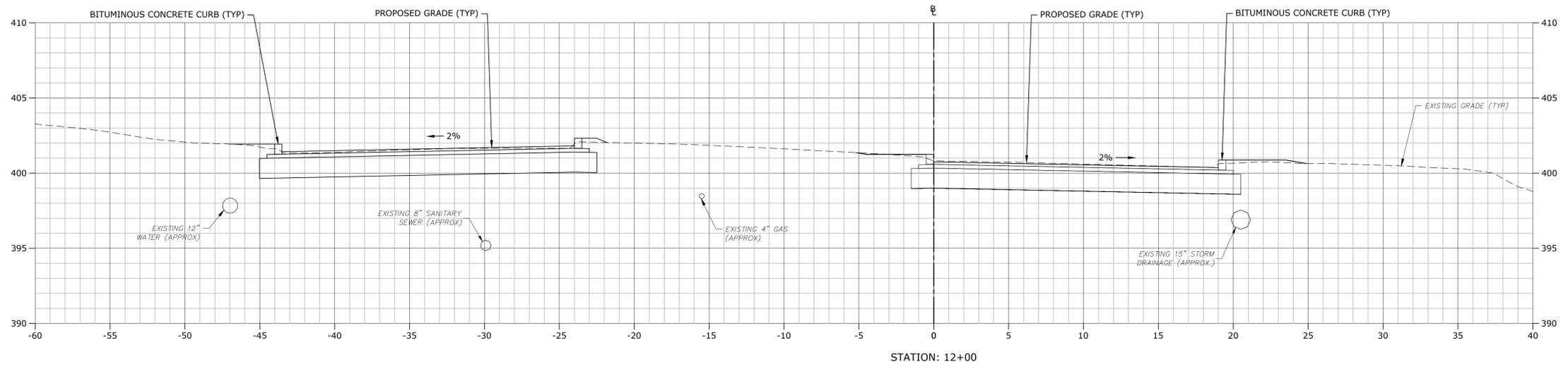
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 9+50 TO 10+50**

SCALE: 1" = 4'

C9.13



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

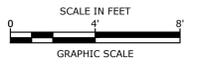
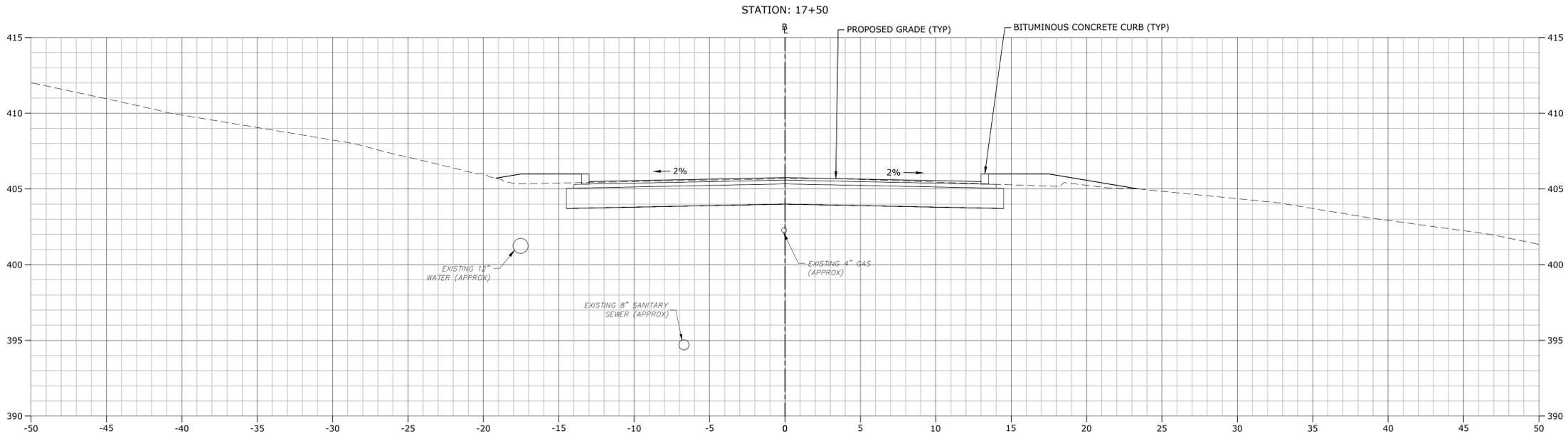
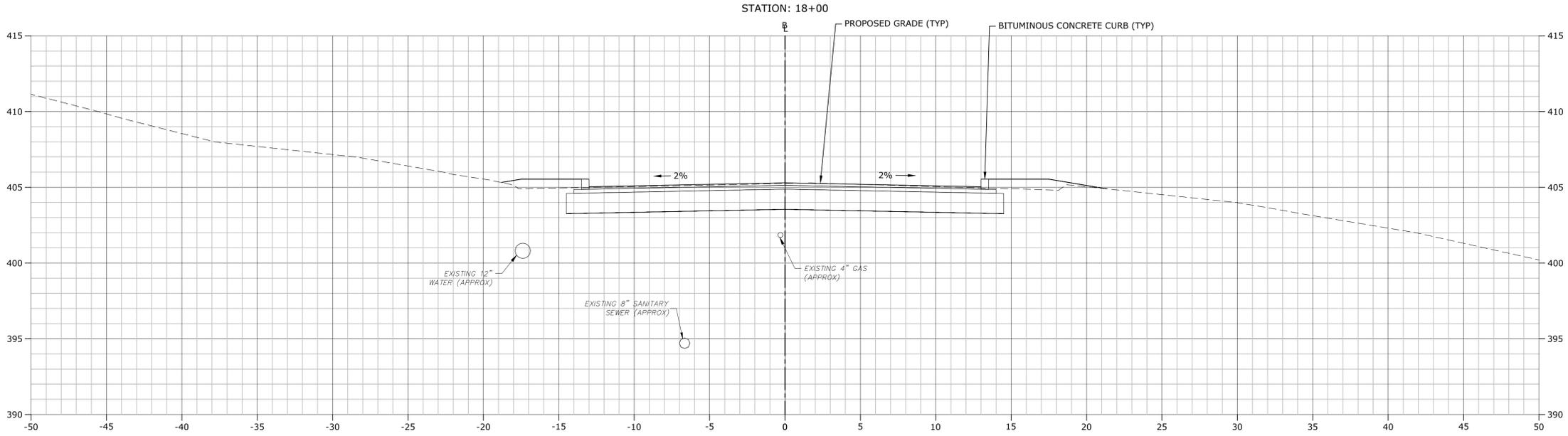
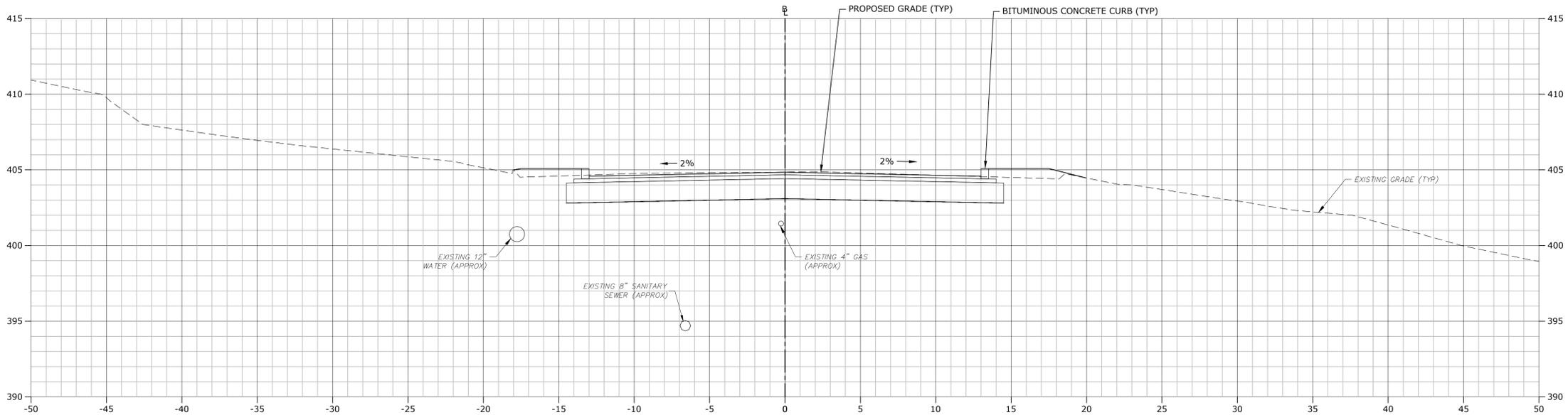
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-15-C-900-XS.dwg	
DRAWN BY:	MDS	
CHECKED:	CRD	
APPROVED:	JWB	

**STROBEL ROAD
 CROSS SECTIONS
 STA: 11+00 TO 12+00**

SCALE: 1" = 4'

last saved: 2/6/2019 11:17:20am By: Duhayoc
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**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

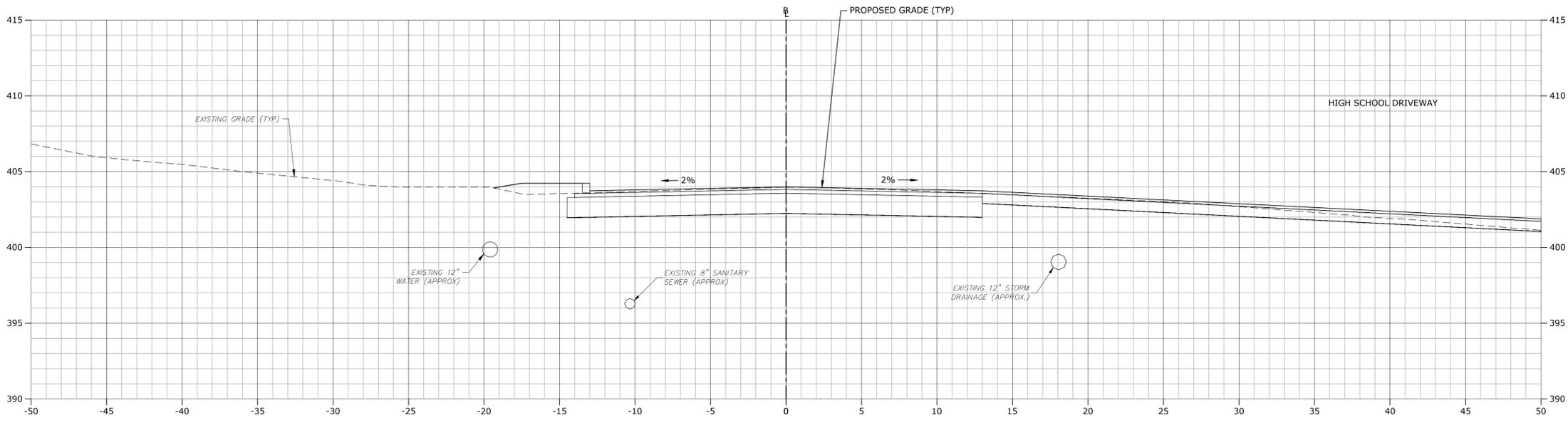
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 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

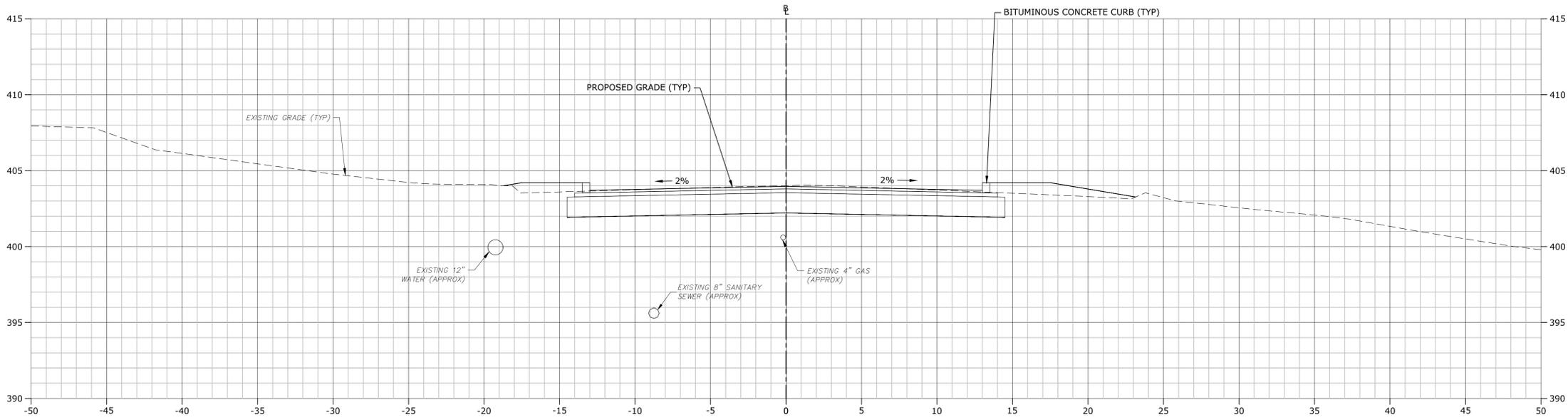
PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 17+00 TO 18+00**

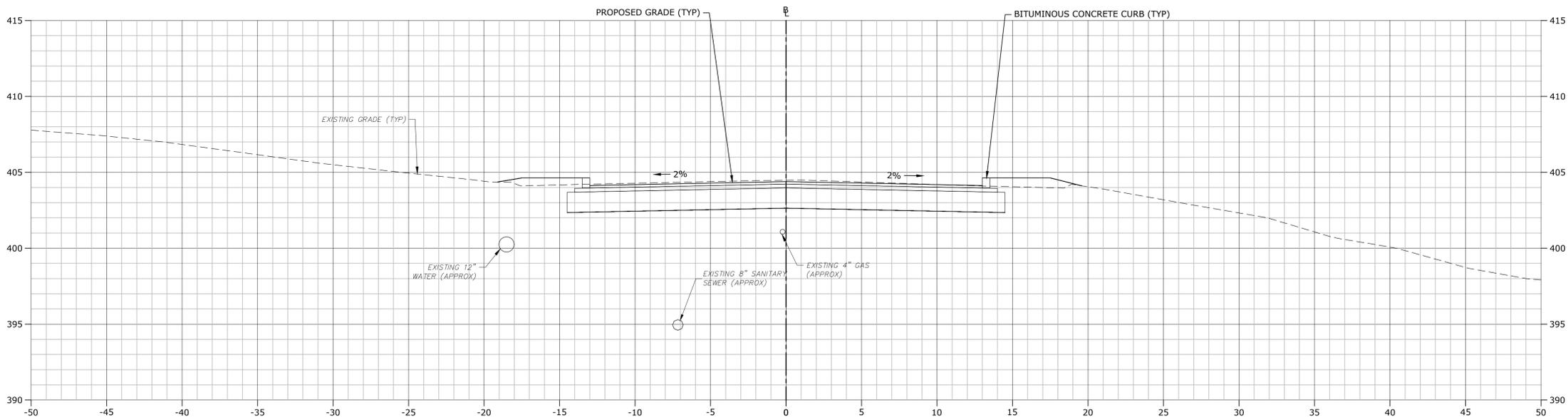
SCALE: 1" = 4'



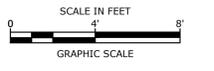
STATION: 19+50



STATION: 19+00



STATION: 18+50



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

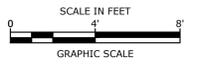
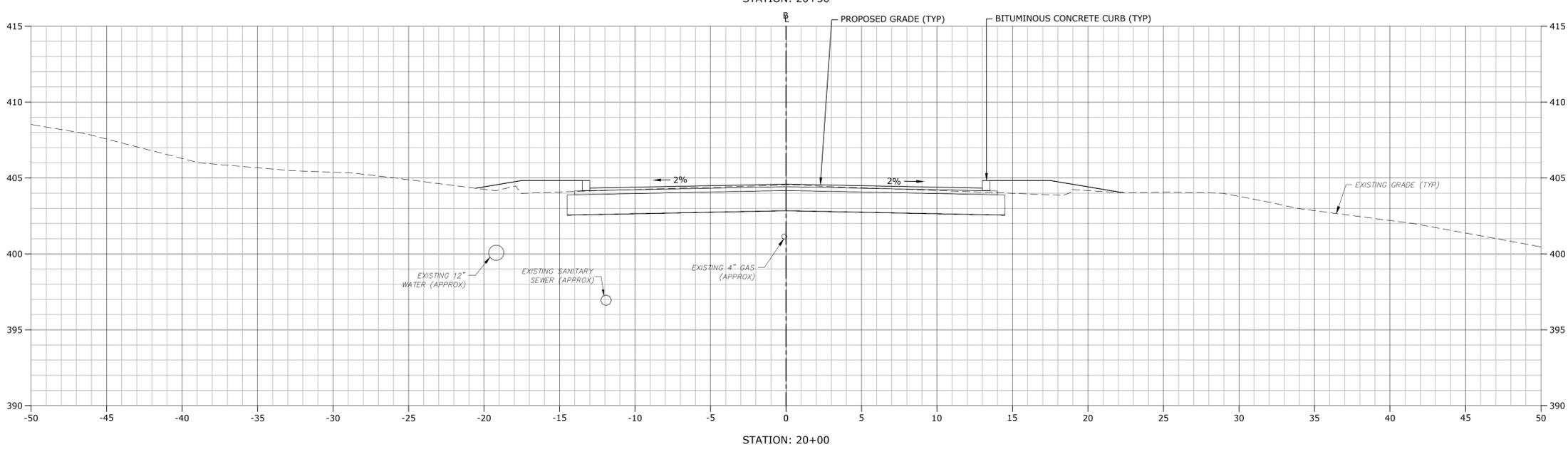
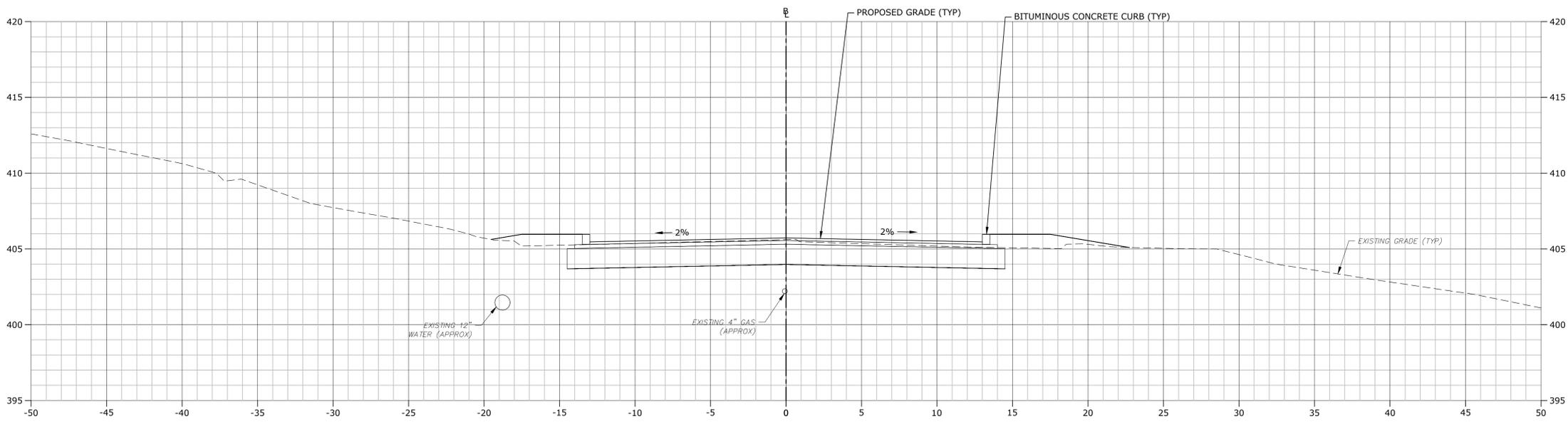
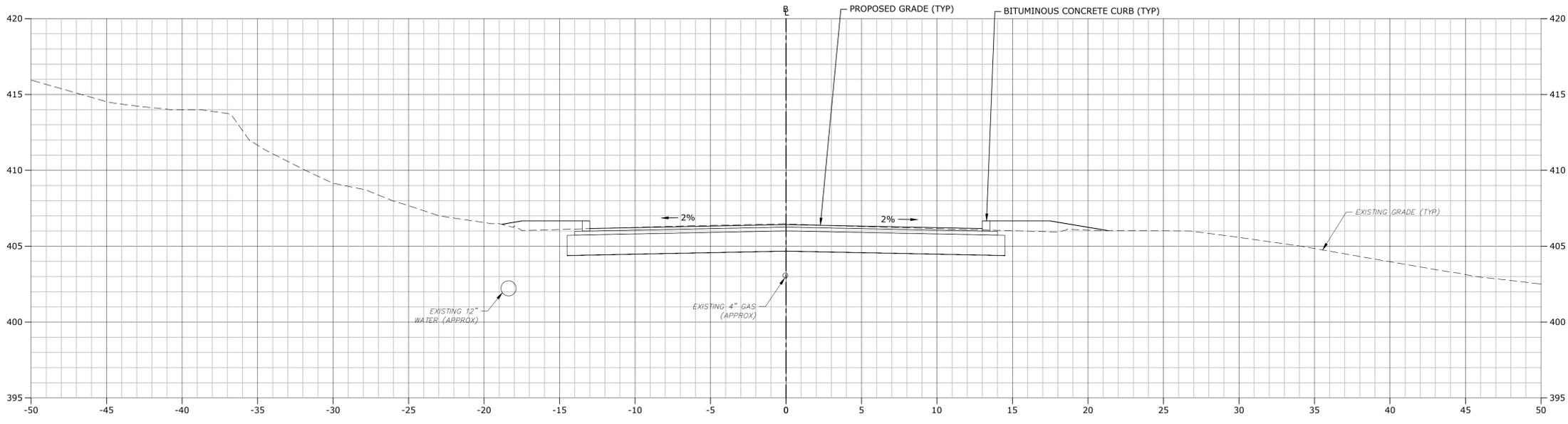
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 18+50 TO 19+50**

SCALE: 1" = 4'

Plot Saved: 2/6/2019 10:19:11:20am By: Duhayoc
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawings\Sheet\T0196-15-C-900-XS.dwg



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

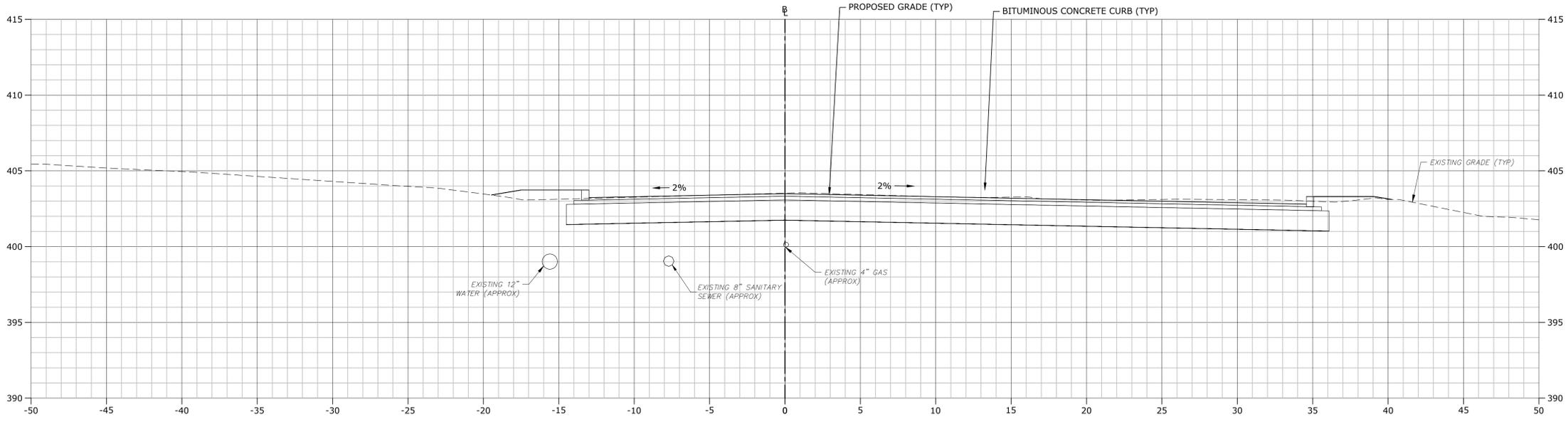
VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
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FILE:	T0196-15-C-900-XS.dwg	
DRAWN BY:	MDS	
CHECKED:	CRD	
APPROVED:	JWB	

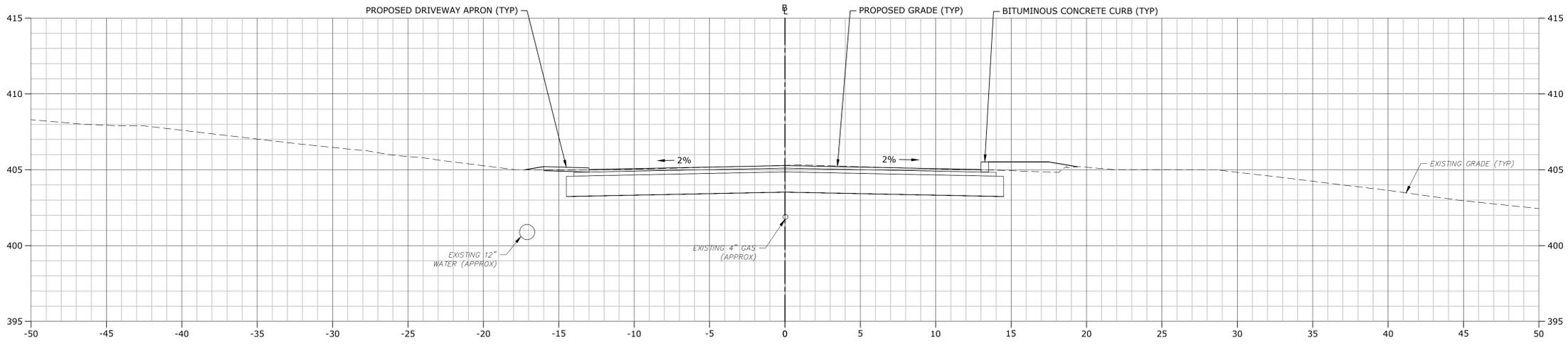
**STROBEL ROAD
 CROSS SECTIONS
 STA: 20+00 TO 21+00**

SCALE: 1" = 4'

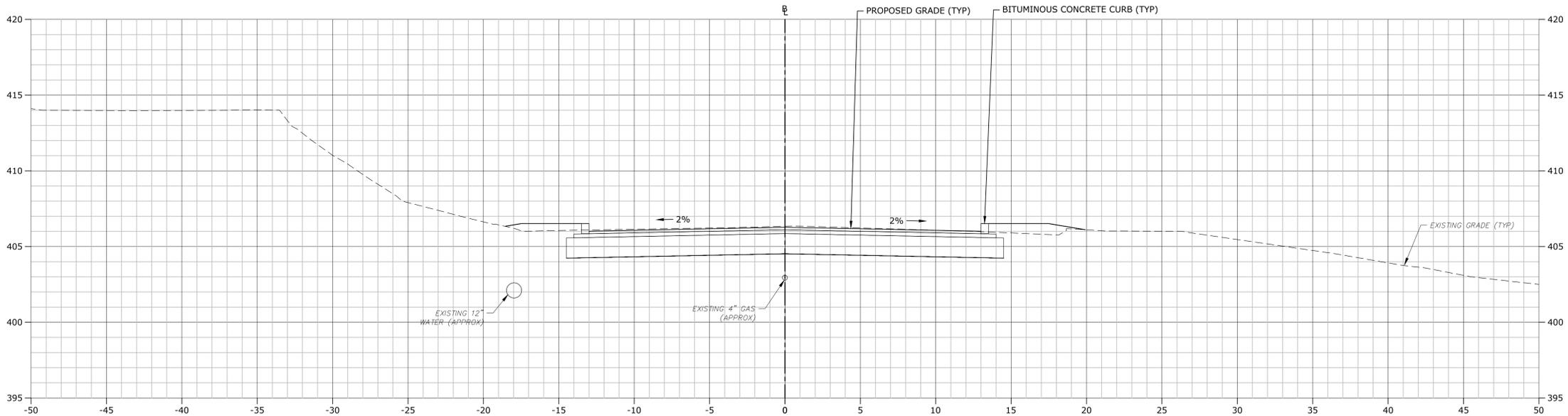
C9.20



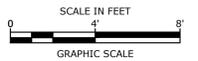
STATION: 22+50



STATION: 22+00



STATION: 21+50



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

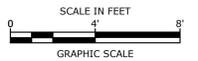
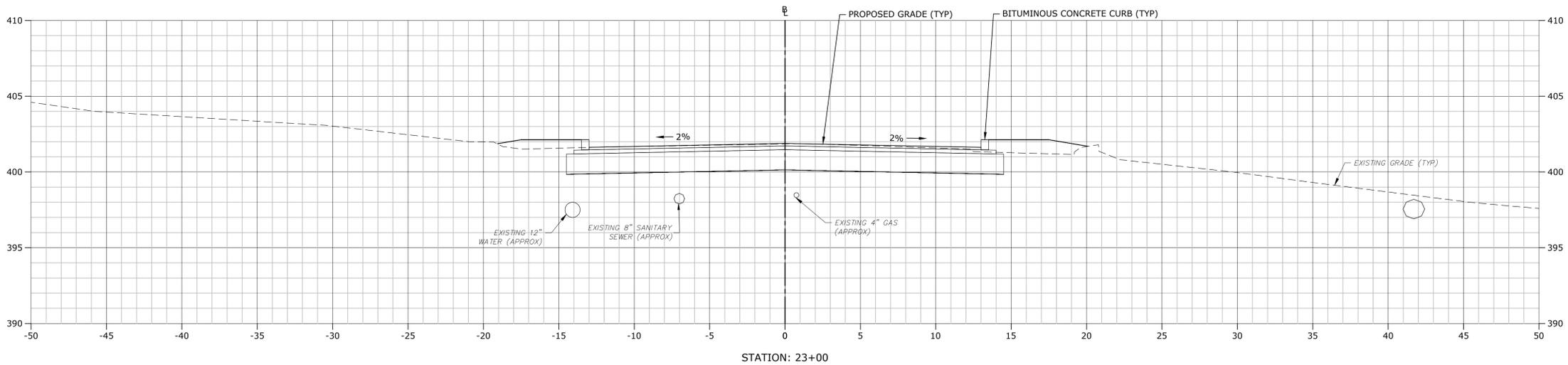
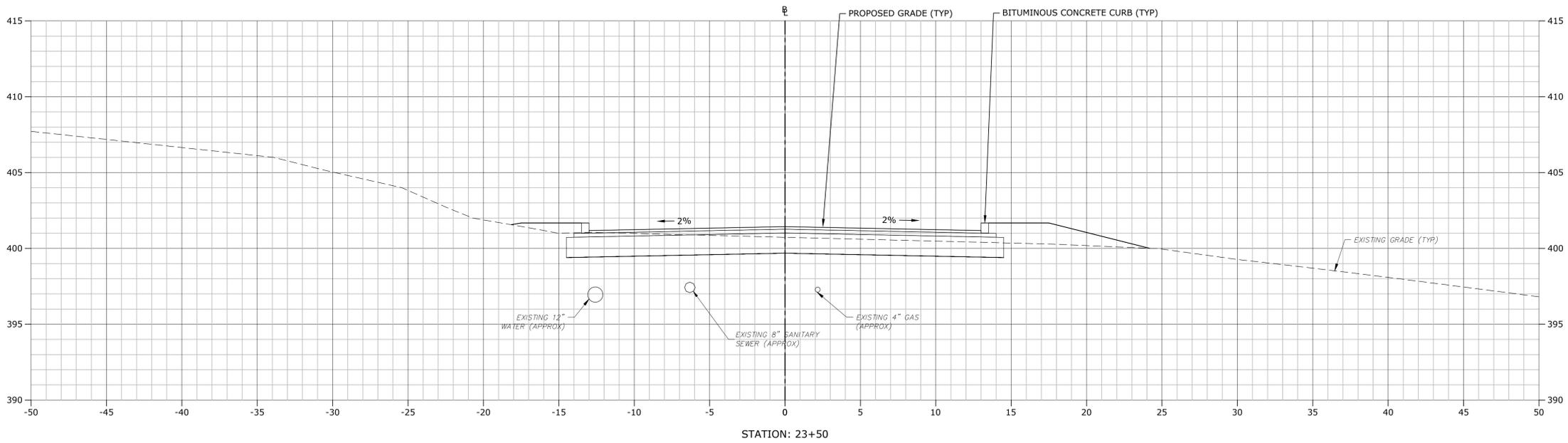
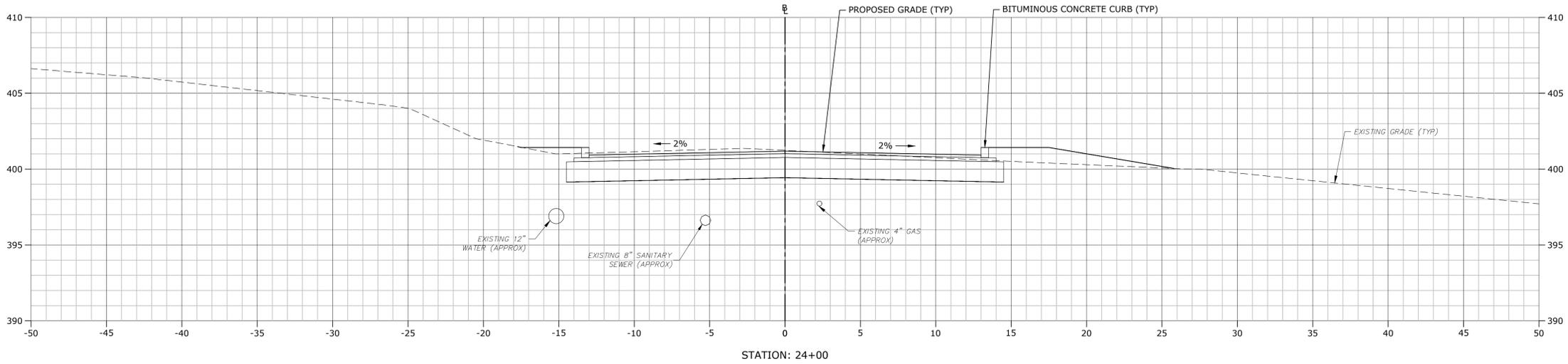
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 21+50 TO 22+50**

SCALE: 1" = 4'

Plot Saved: 2/6/2019 11:20am By: Duhayoc
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawing\Sheet\T0196-15-C-900-XS.dwg



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

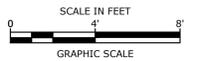
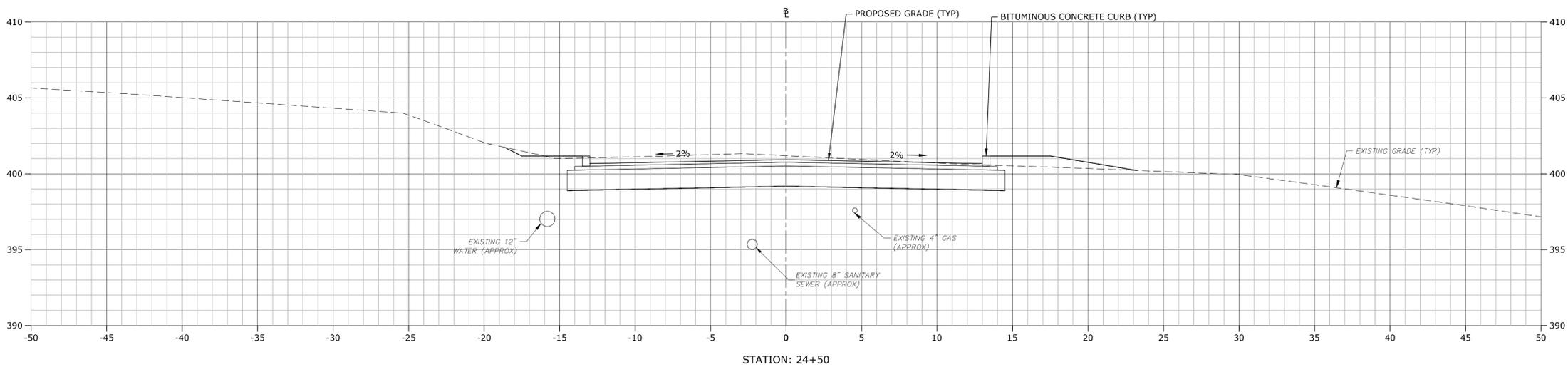
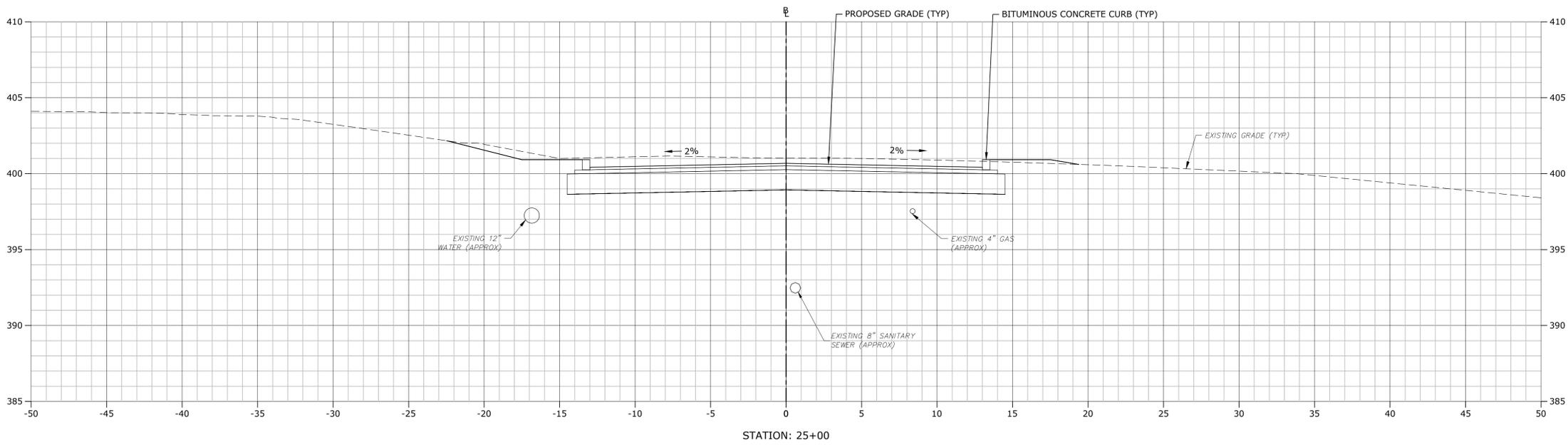
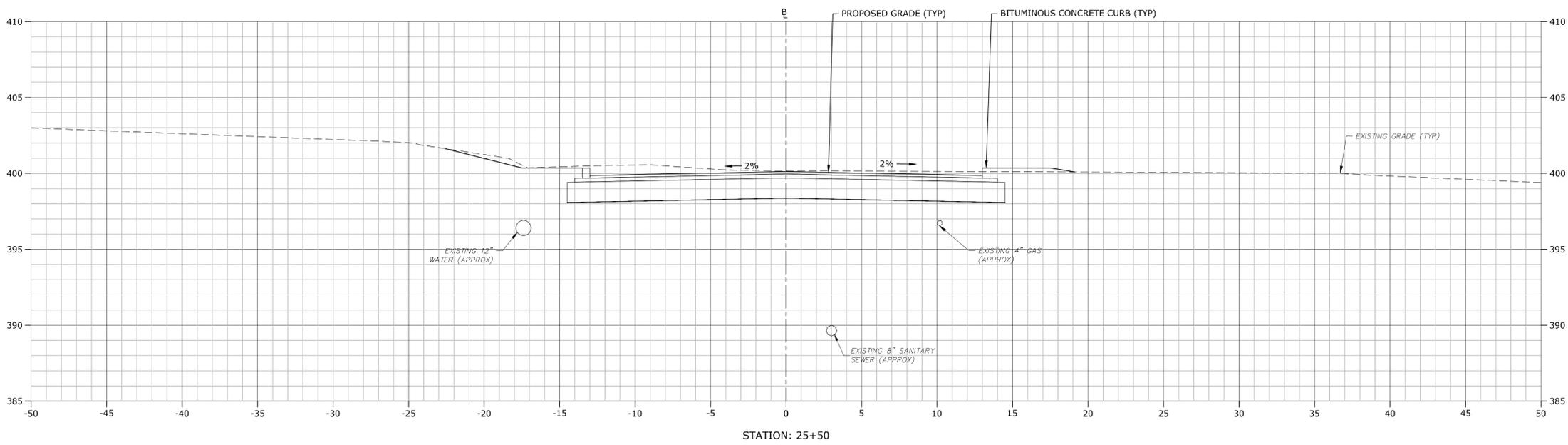
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 23+00 TO 24+00**

SCALE: 1" = 4'

Last Saved: 2/6/2019 11:21:11 AM By: Duhayoc
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawings\Sheet\T0196-15-C-900-XS.dwg



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

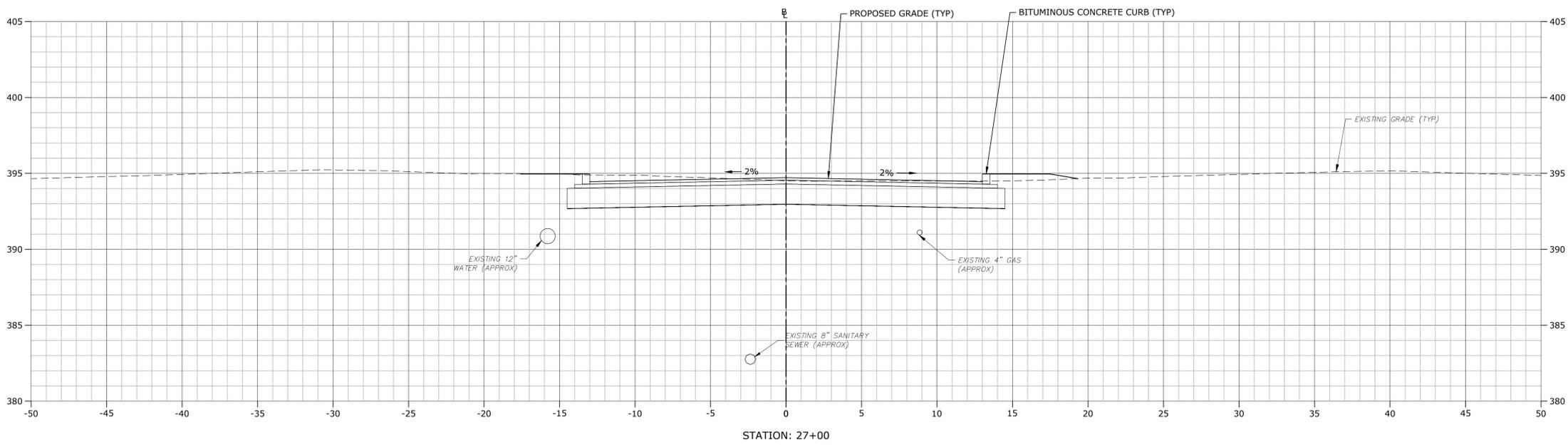
MARK	DATE	DESCRIPTION

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FILE:	T0196-15-C-900-XS.dwg
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CHECKED:	CRD
APPROVED:	JWB

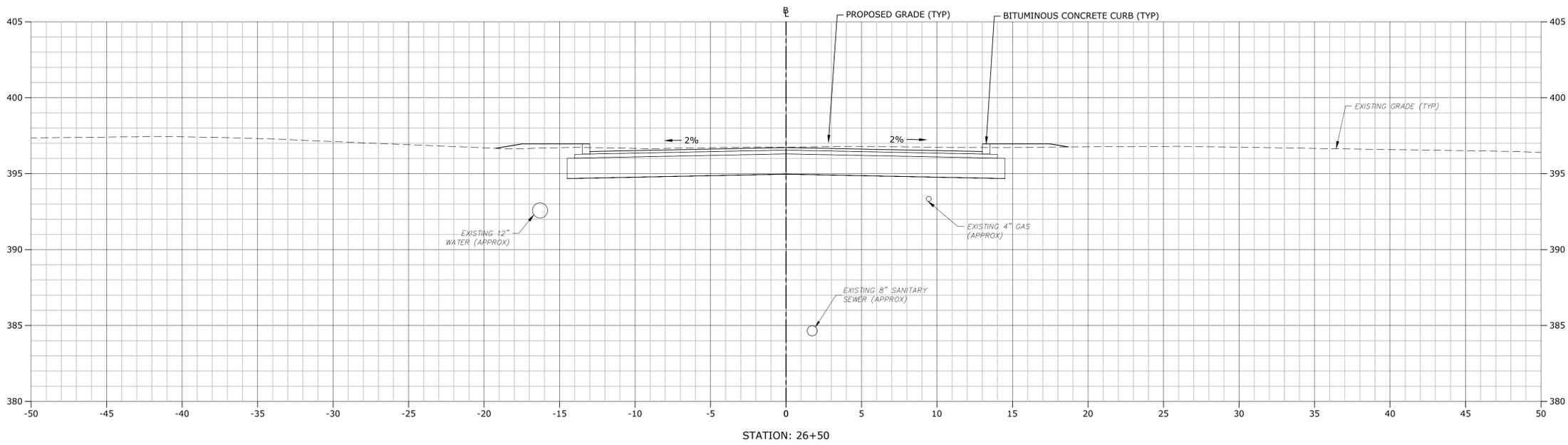
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 CROSS SECTIONS
 STA: 24+50 TO 25+50**

SCALE: 1" = 4'

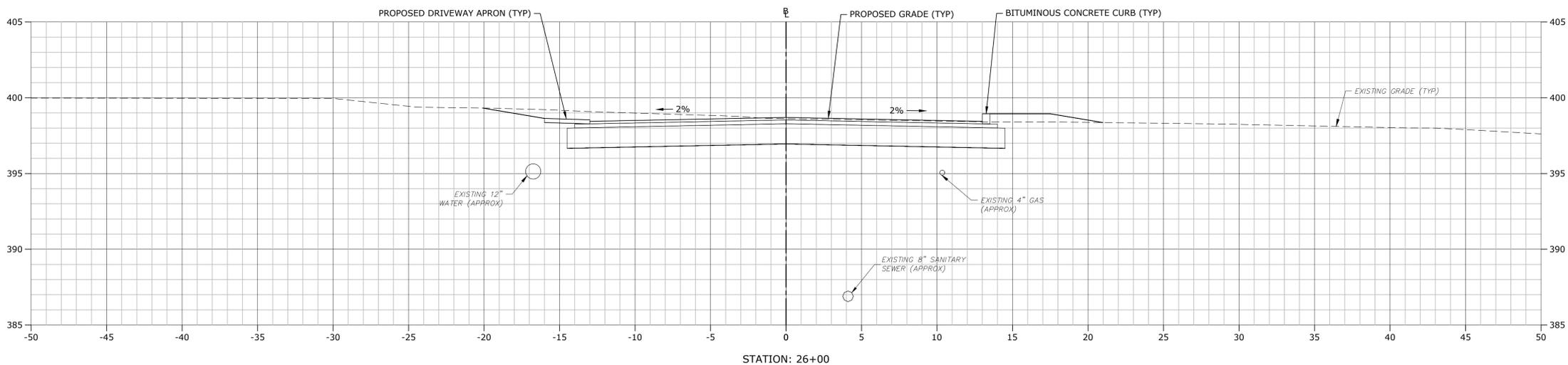
Plot Sheet: 2/2/2019 10:19:11:21am By: Duhon@ct Tighe & Bond: J:\T0196\15_Strobel Rd\Drawing\Sheet\T0196-15-C-900-XS.dwg



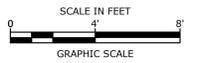
STATION: 27+00



STATION: 26+50



STATION: 26+00



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

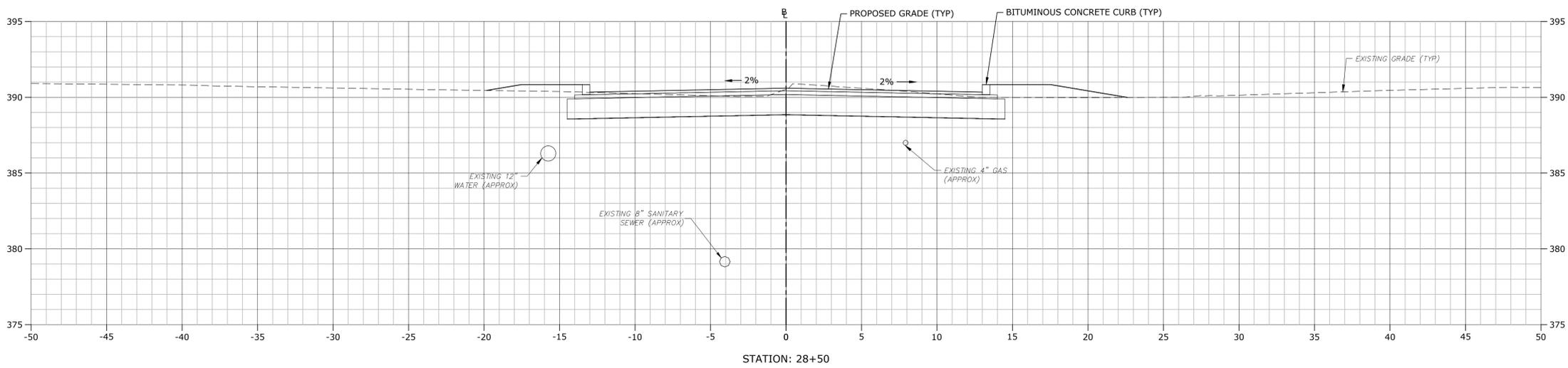
January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

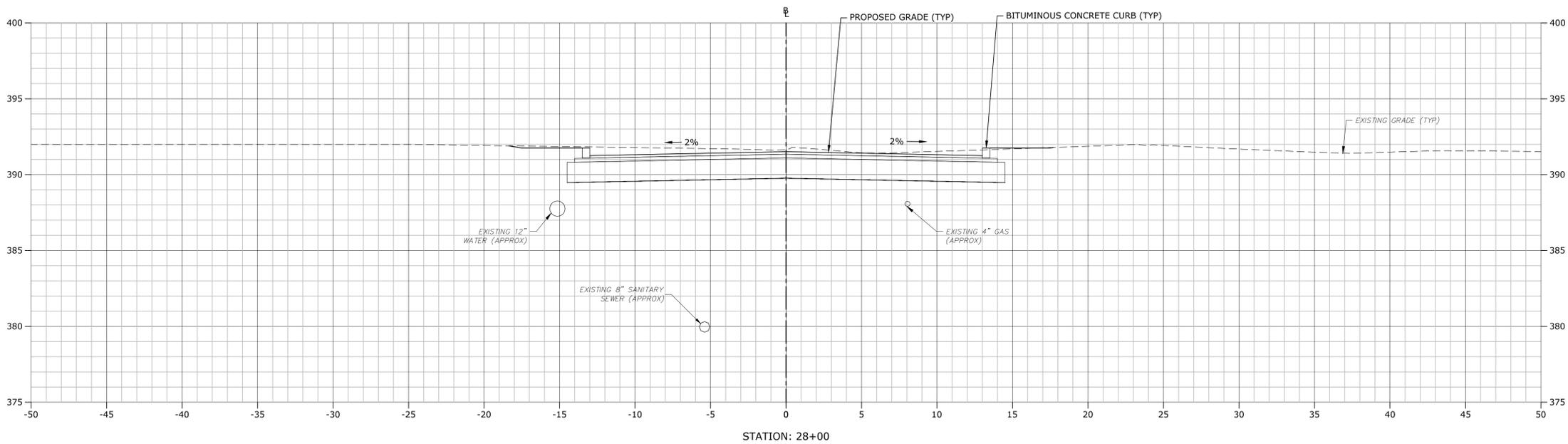
MARK	DATE	DESCRIPTION

**STROBEL ROAD
 CROSS SECTIONS
 STA: 26+00 TO 27+00**

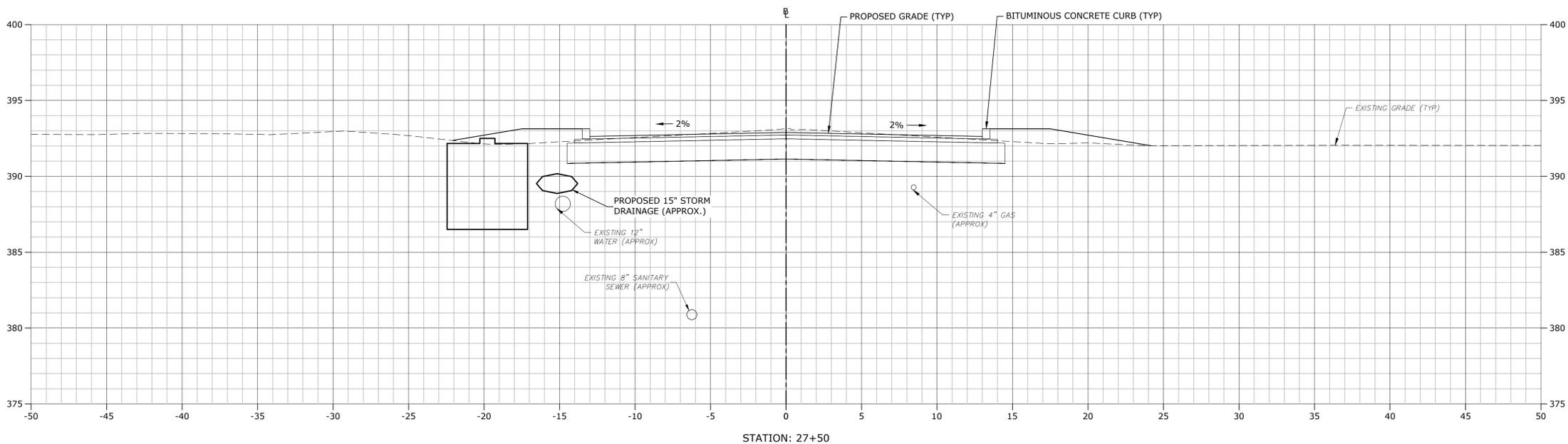
SCALE: 1" = 4'



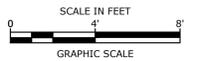
STATION: 28+50



STATION: 28+00



STATION: 27+50



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

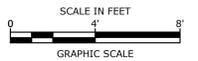
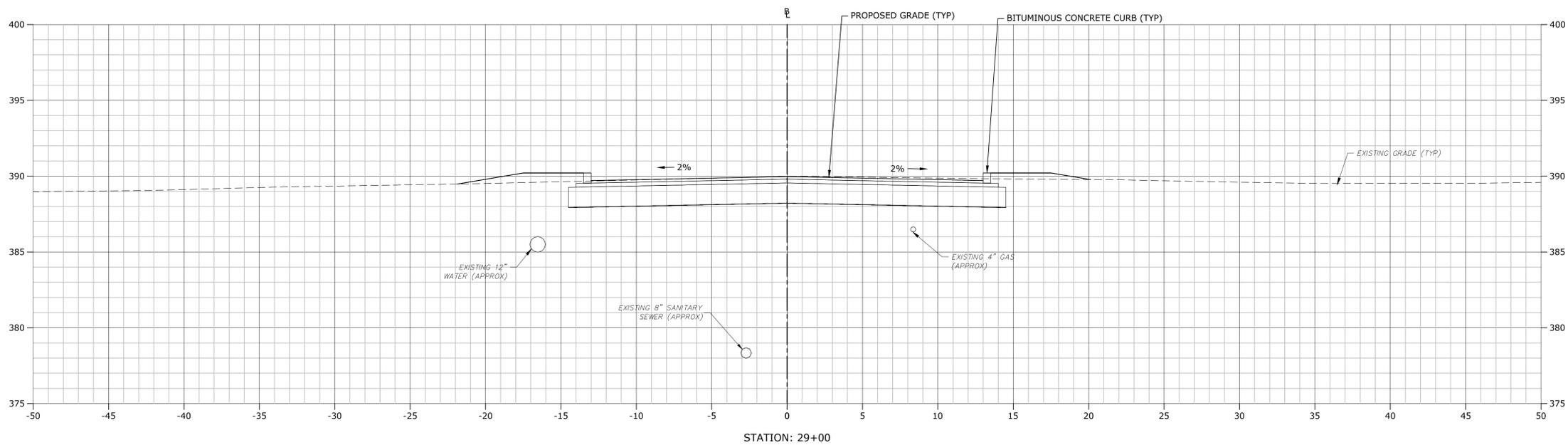
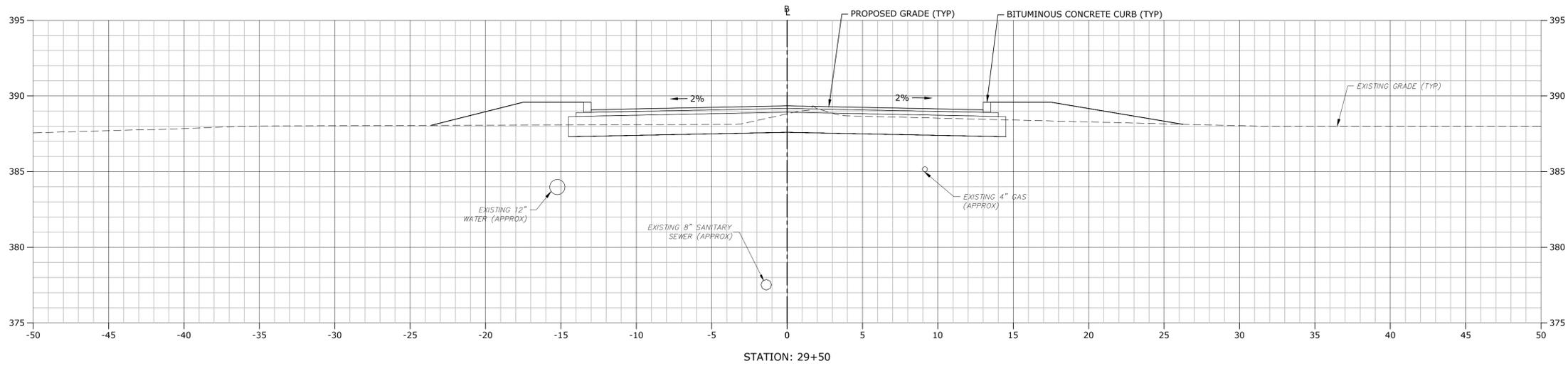
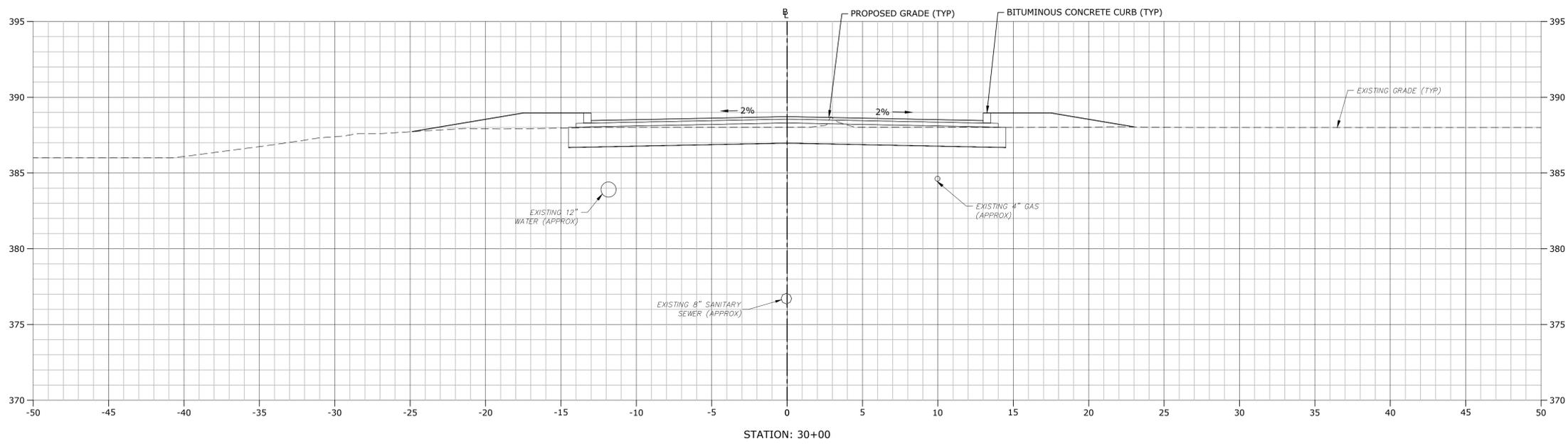
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 27+50 TO 28+50**

SCALE: 1" = 4'

C9.25



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

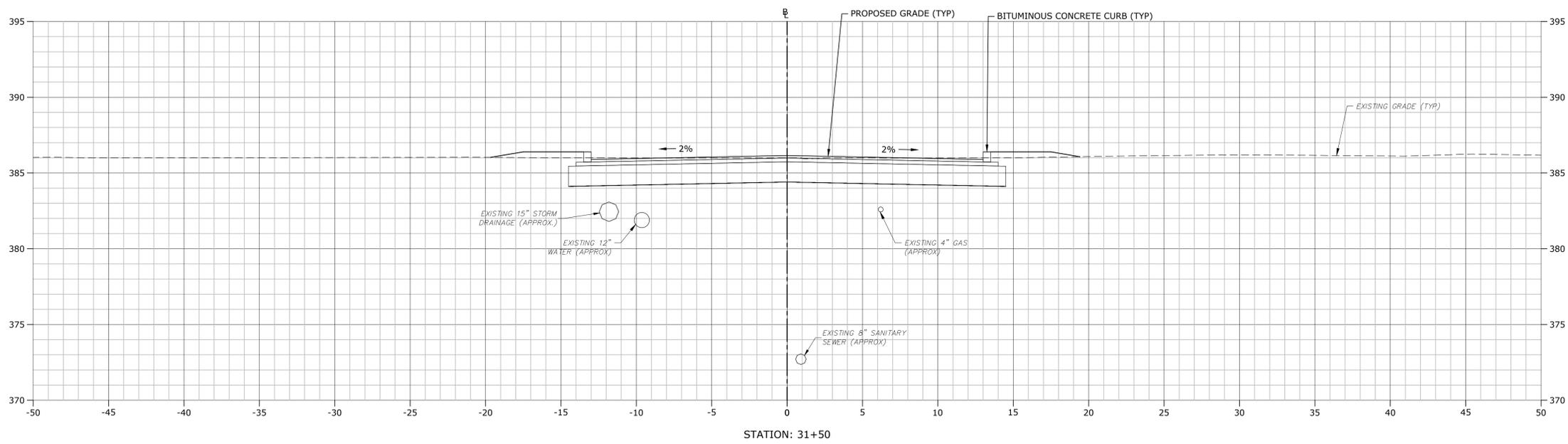
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
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 APPROVED: JWB

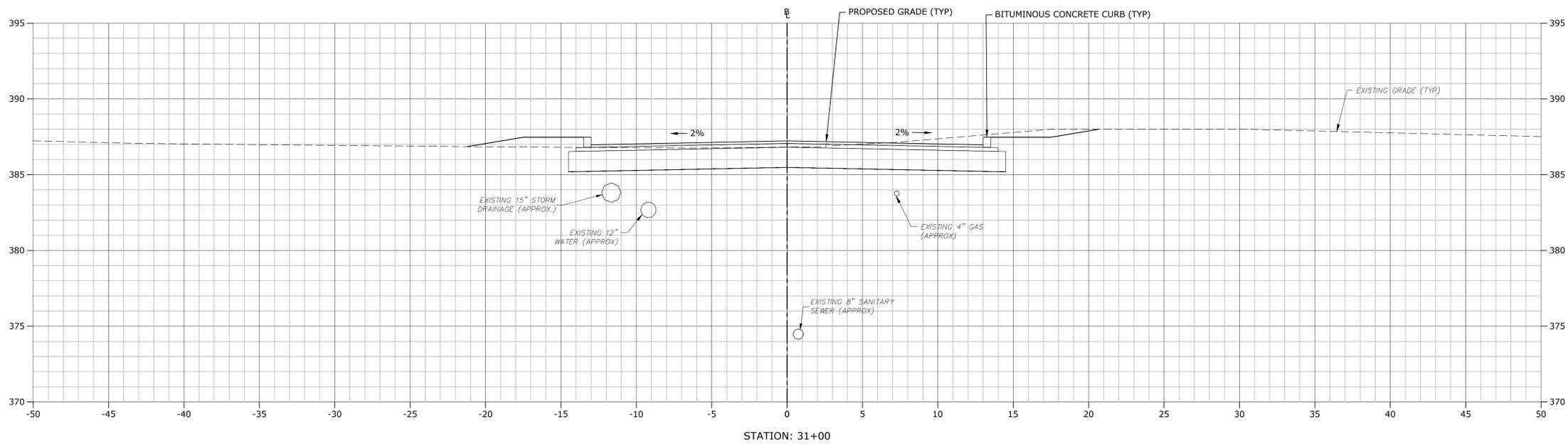
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 CROSS SECTIONS
 STA: 29+00 TO 30+00**

SCALE: 1" = 4'

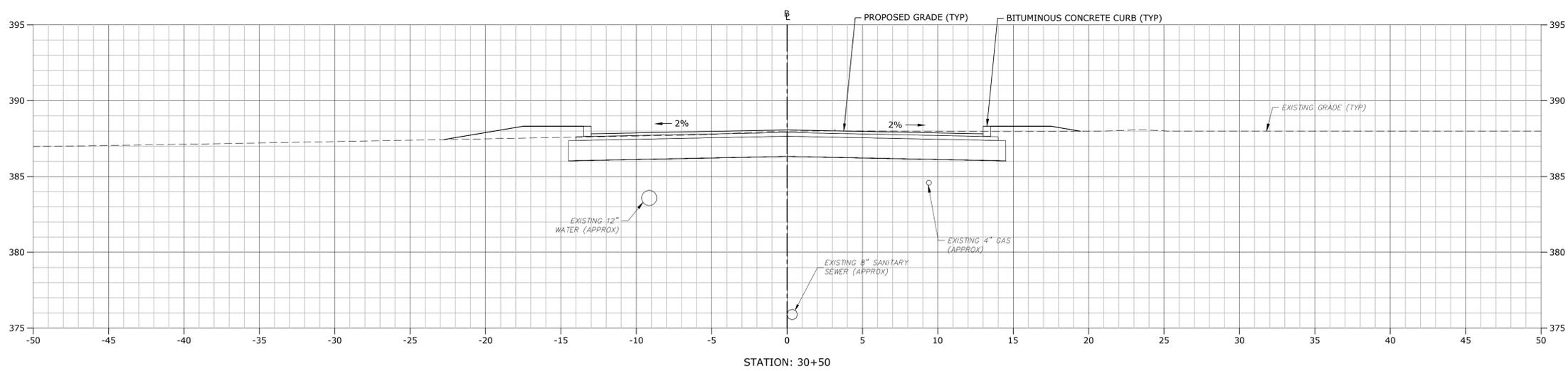
last saved: 2/6/2019 10:19:11:23am By: Duhon,ec
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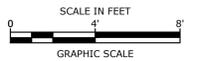
STATION: 31+50



STATION: 31+00



STATION: 30+50



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

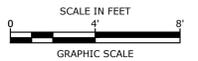
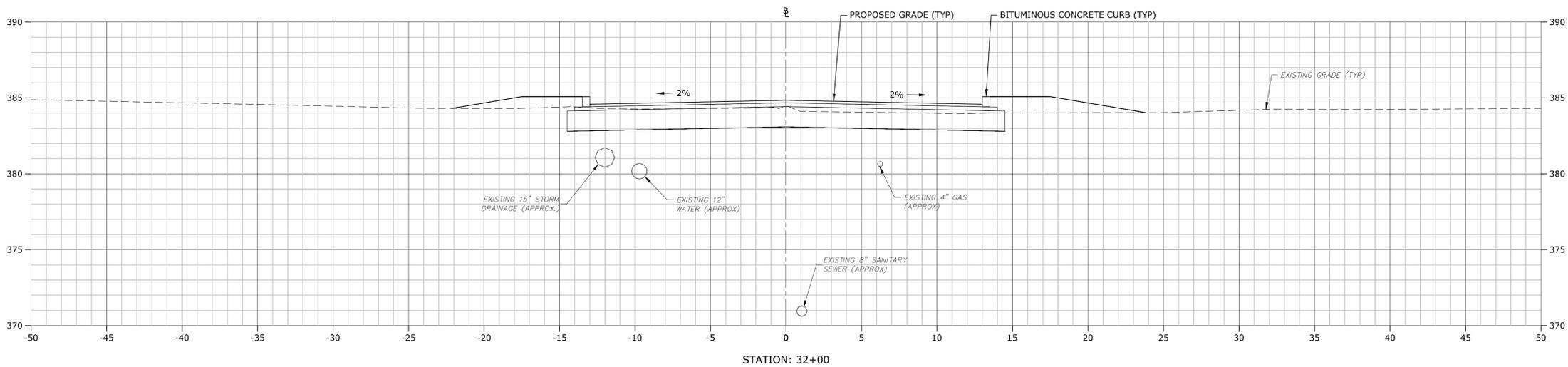
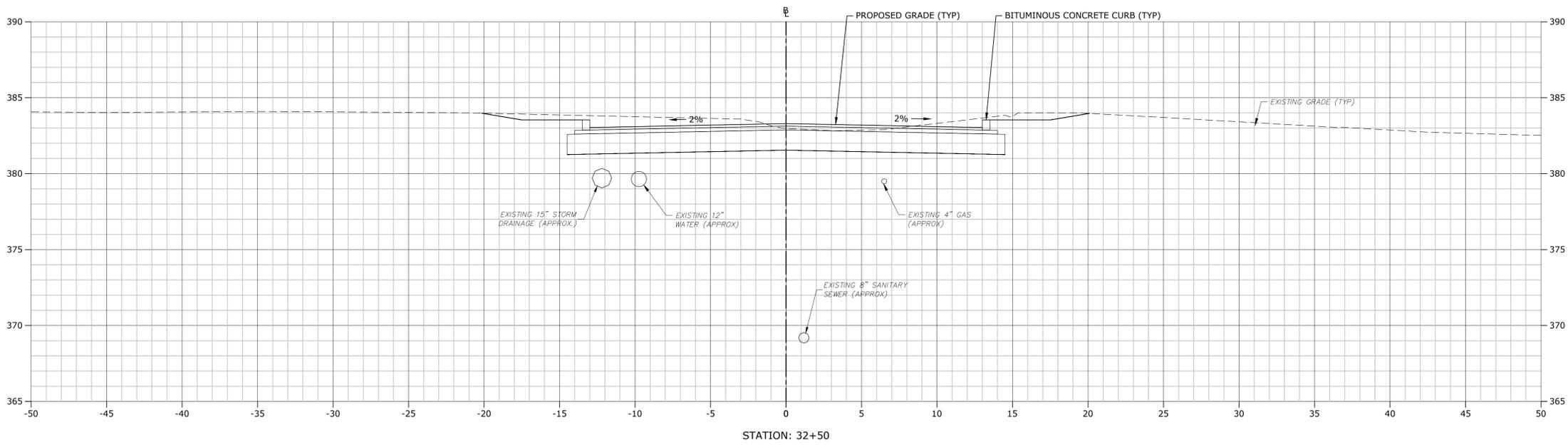
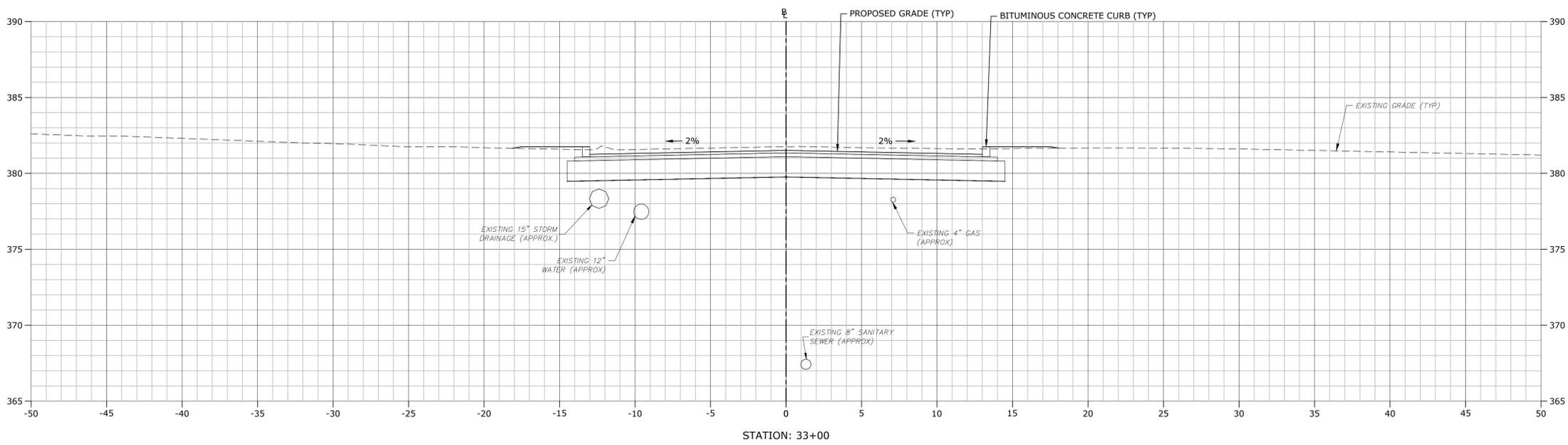
VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 30+50 TO 31+50**

SCALE: 1" = 4'



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

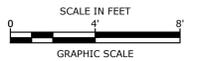
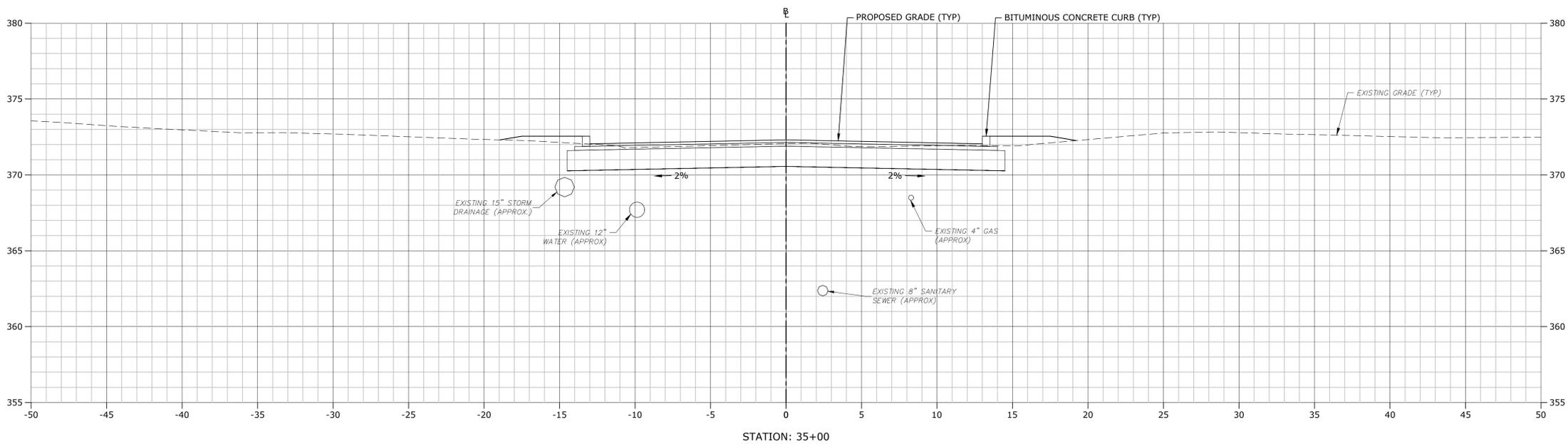
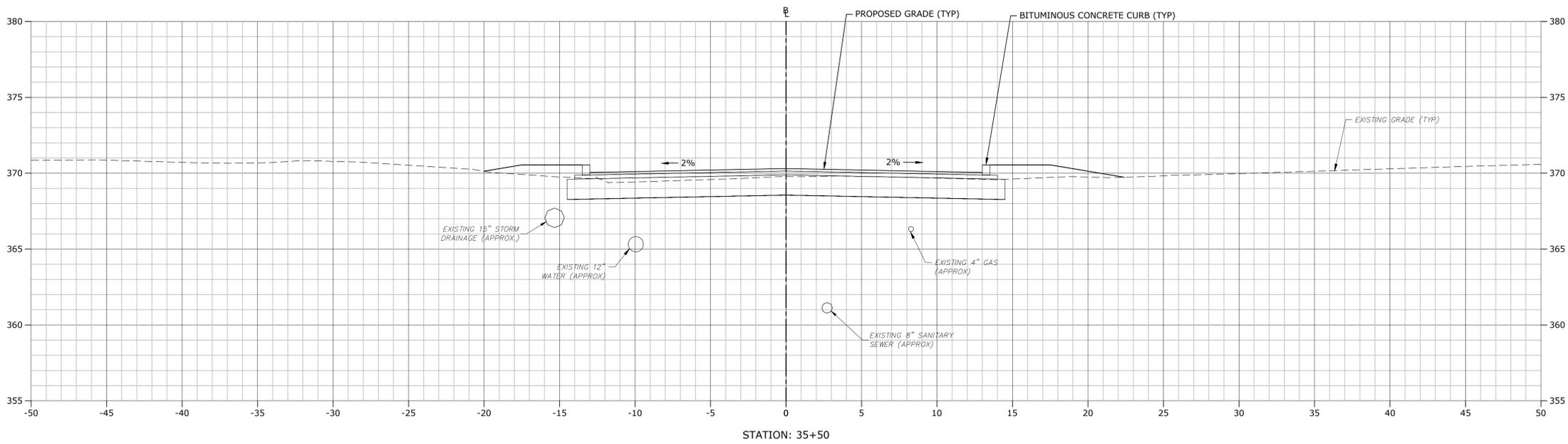
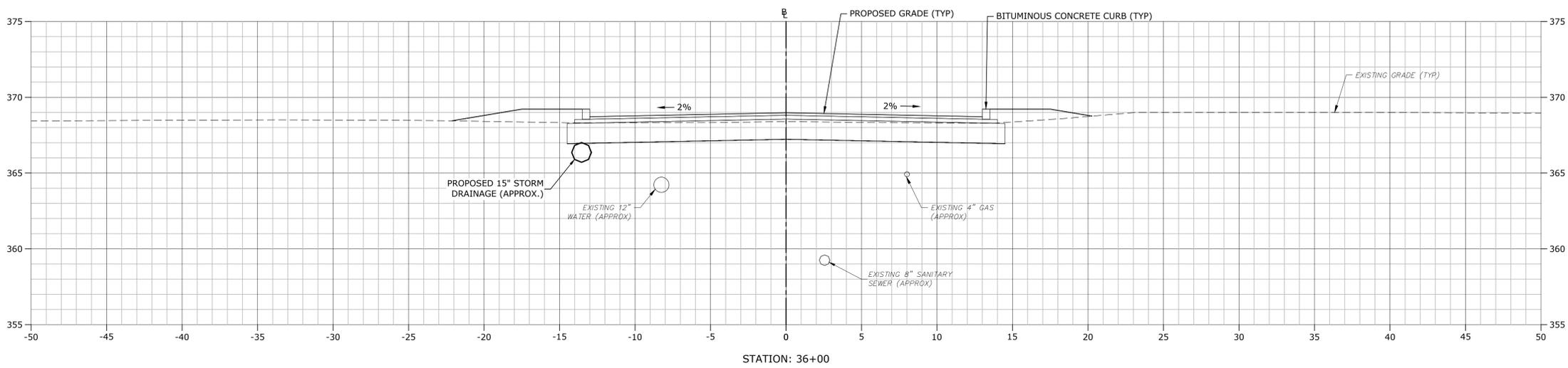
VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

**STROBEL ROAD
 CROSS SECTIONS
 STA: 32+00 TO 33+00**

SCALE: 1" = 4'

Last Saved: 2/6/2019 11:11:23am By: Duhon, C
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawings\Sheet\T0196-15-C-900-XS.dwg



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

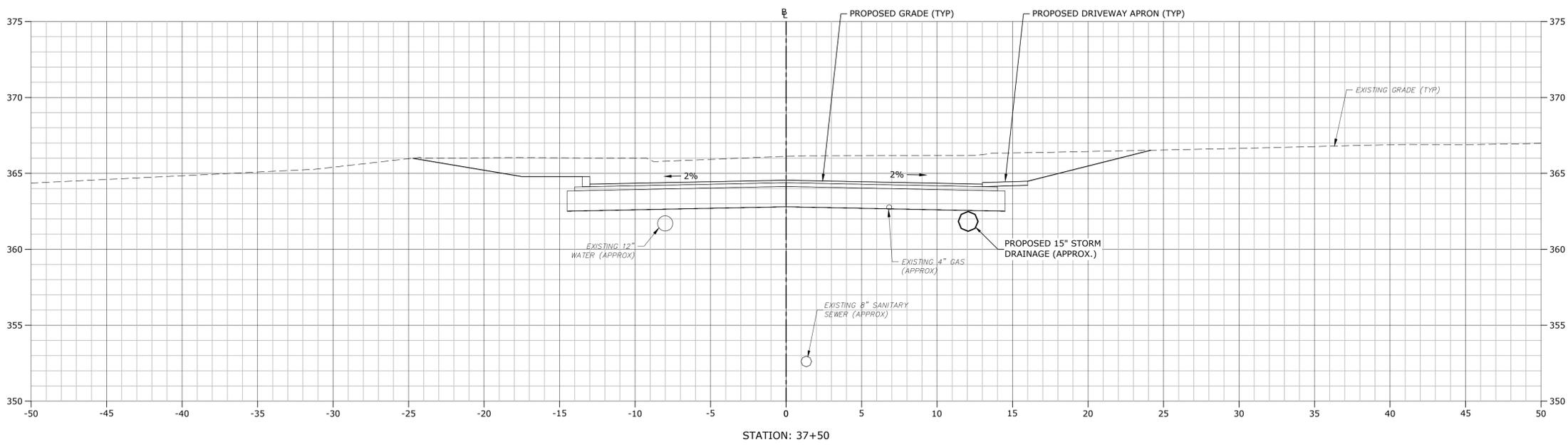
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

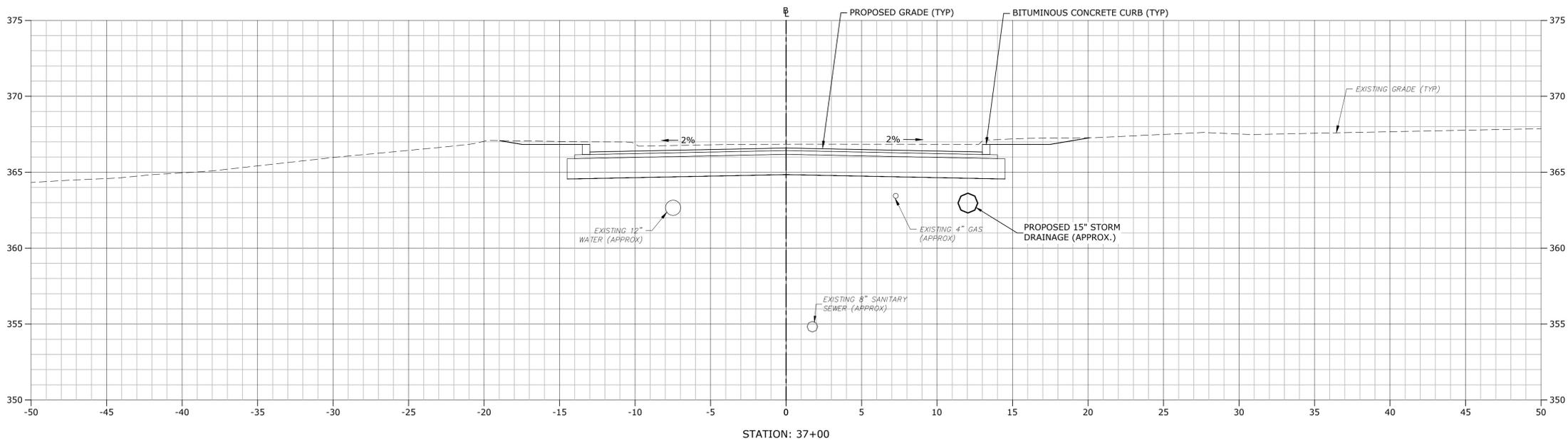
**STROBEL ROAD
 CROSS SECTIONS
 STA: 35+00 TO 36+00**

SCALE: 1" = 4'

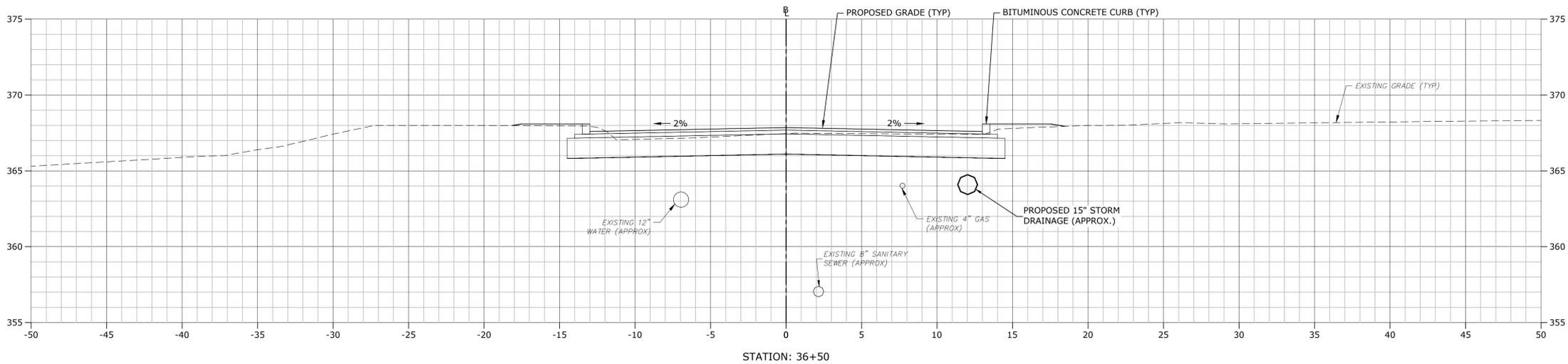
Last Saved: 2/6/2019 11:24am By: Duhon@C
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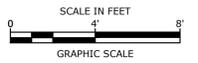
STATION: 37+50



STATION: 37+00



STATION: 36+50



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

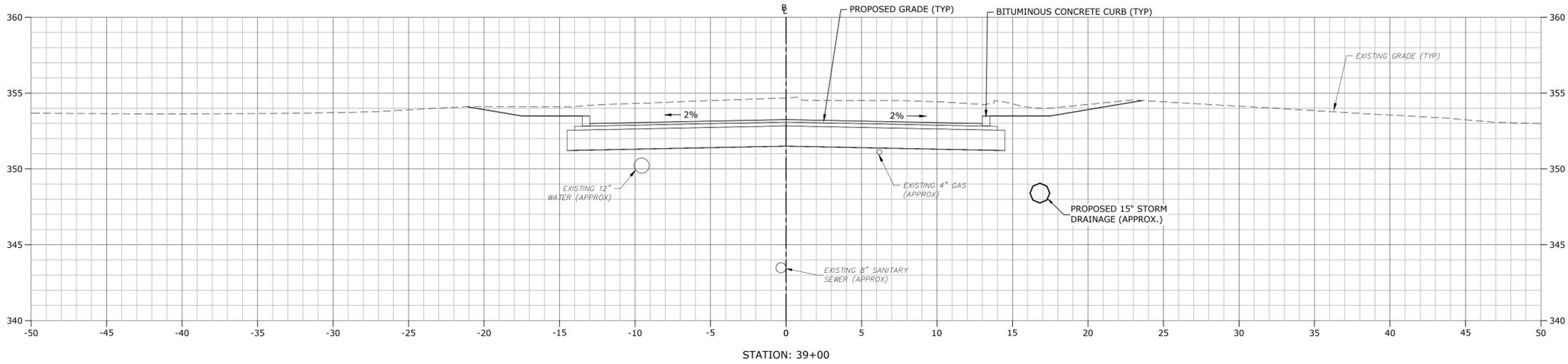
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

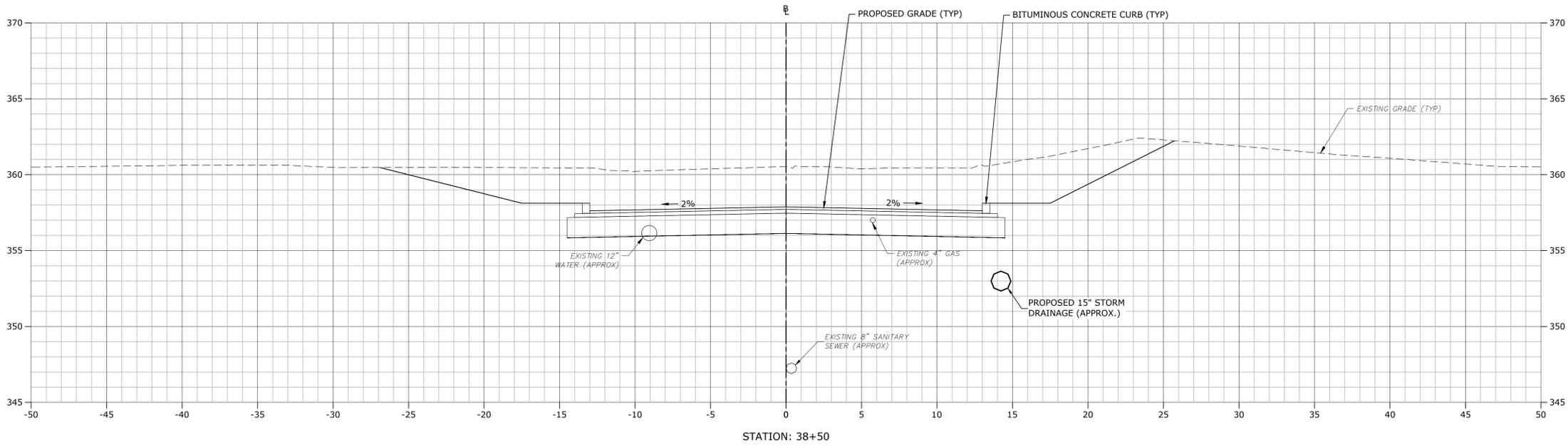
**STROBEL ROAD
 CROSS SECTIONS
 STA: 36+50 TO 37+50**

SCALE: 1" = 4'

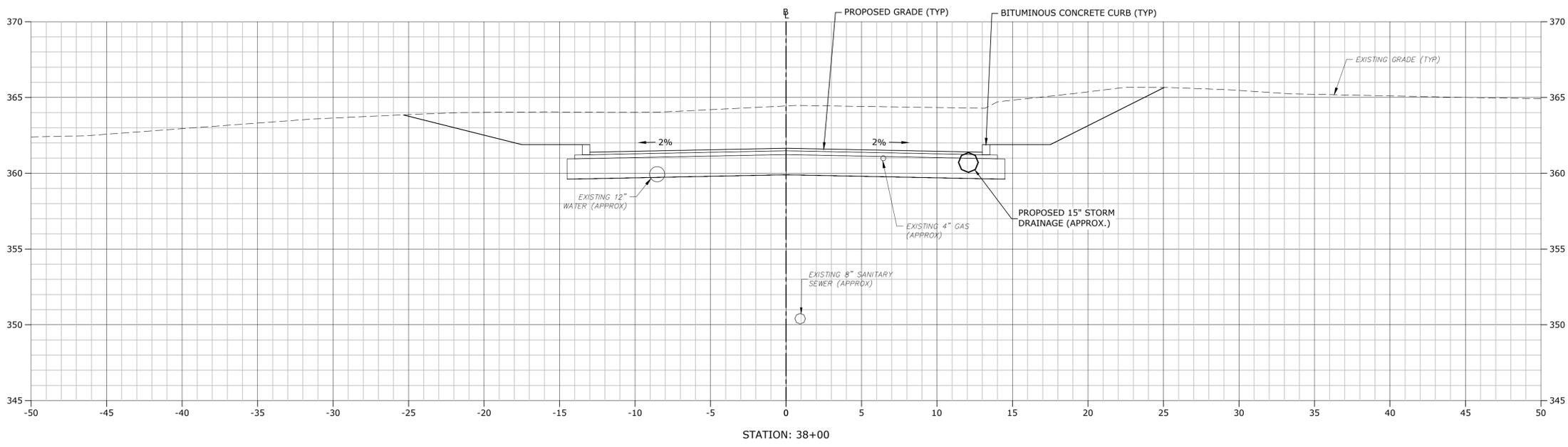
C9.30



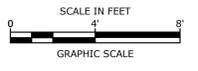
STATION: 39+00



STATION: 38+50



STATION: 38+00



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

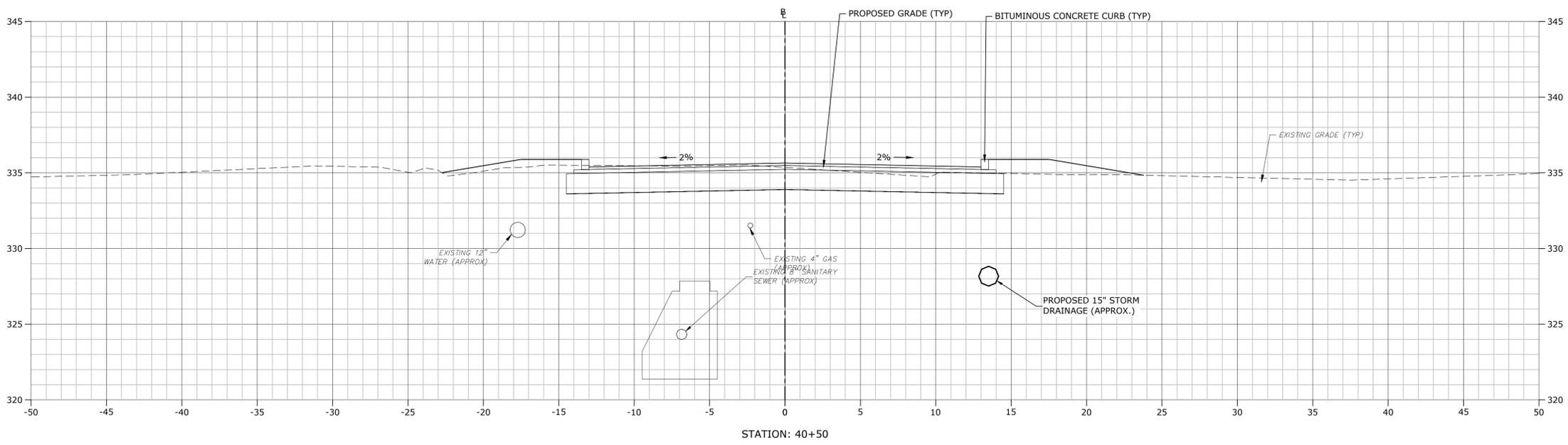
VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-15-C-900-XS.dwg	
DRAWN BY:	MDS	
CHECKED:	CRD	
APPROVED:	JWB	

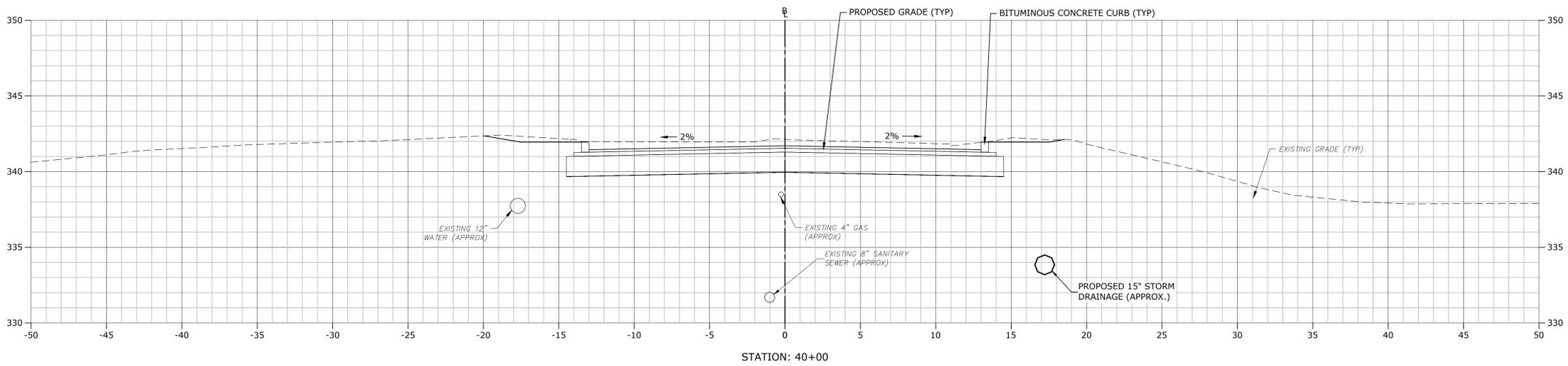
**STROBEL ROAD
 CROSS SECTIONS
 STA: 38+00 TO 39+00**

SCALE: 1" = 4'

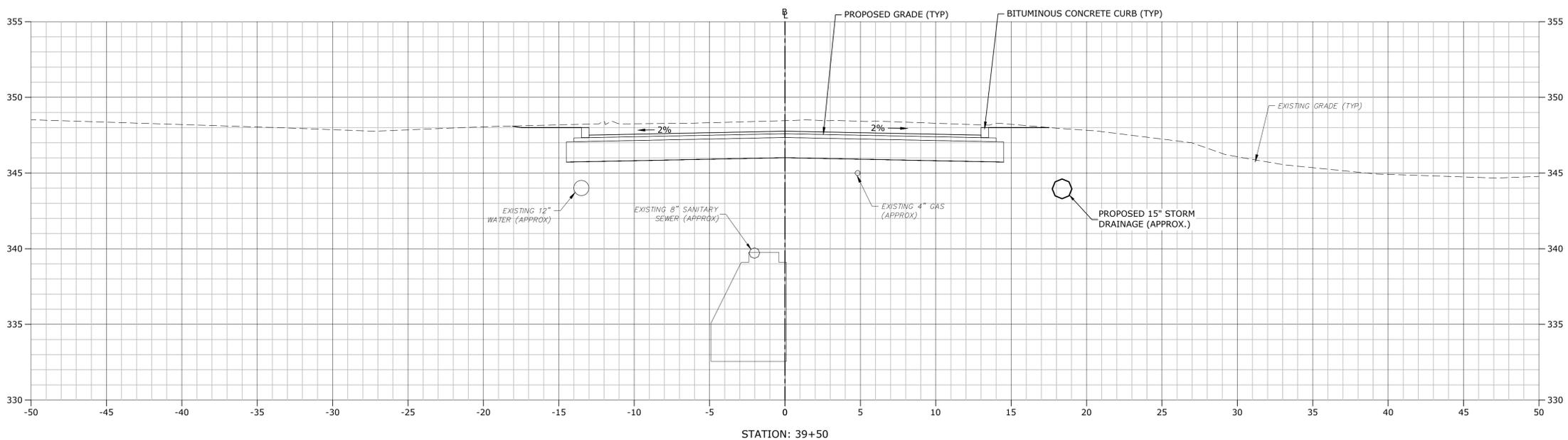
C9.31



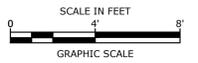
STATION: 40+50



STATION: 40+00



STATION: 39+50



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

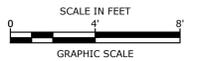
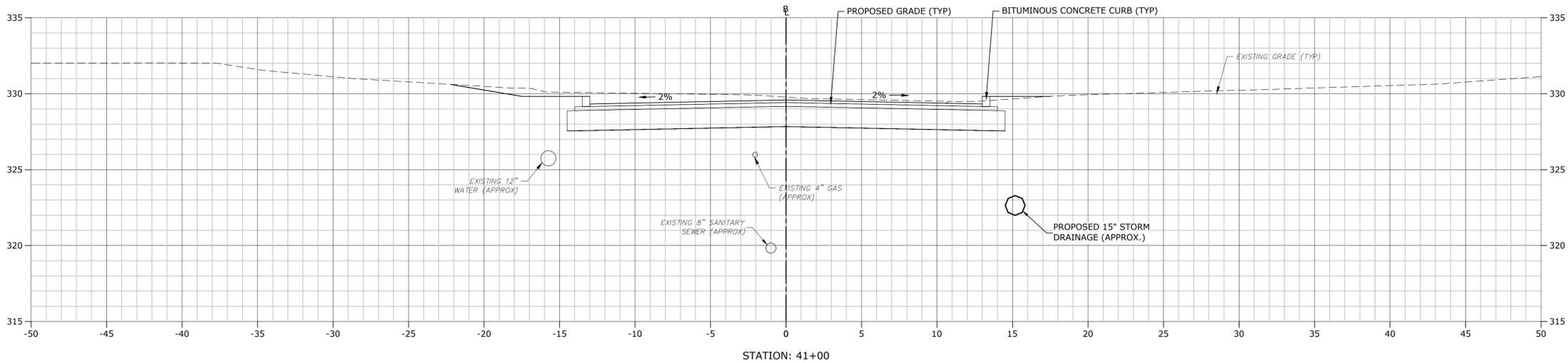
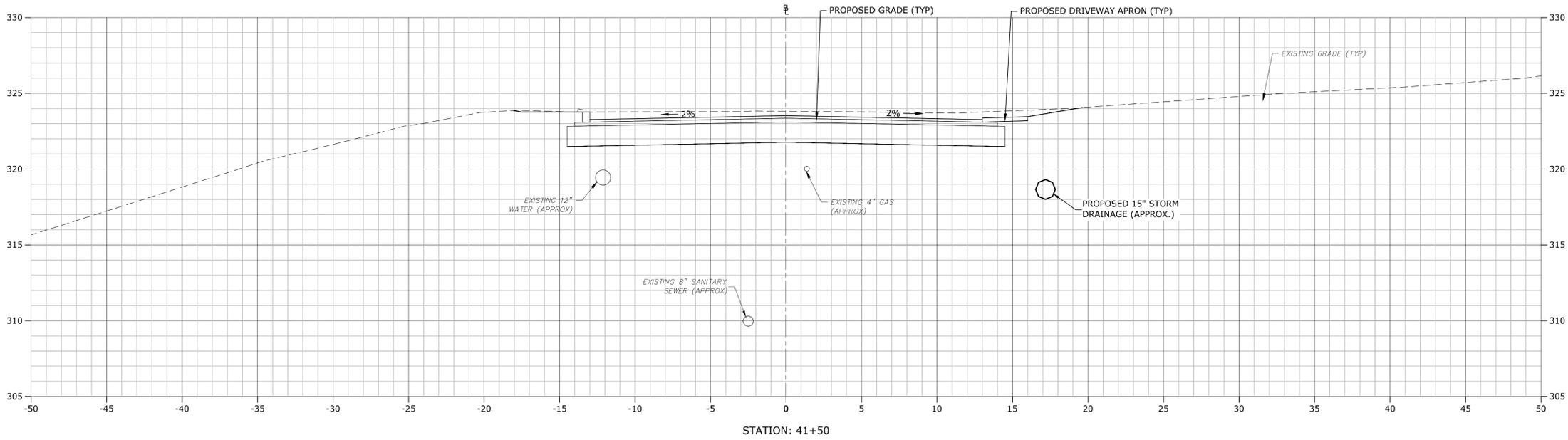
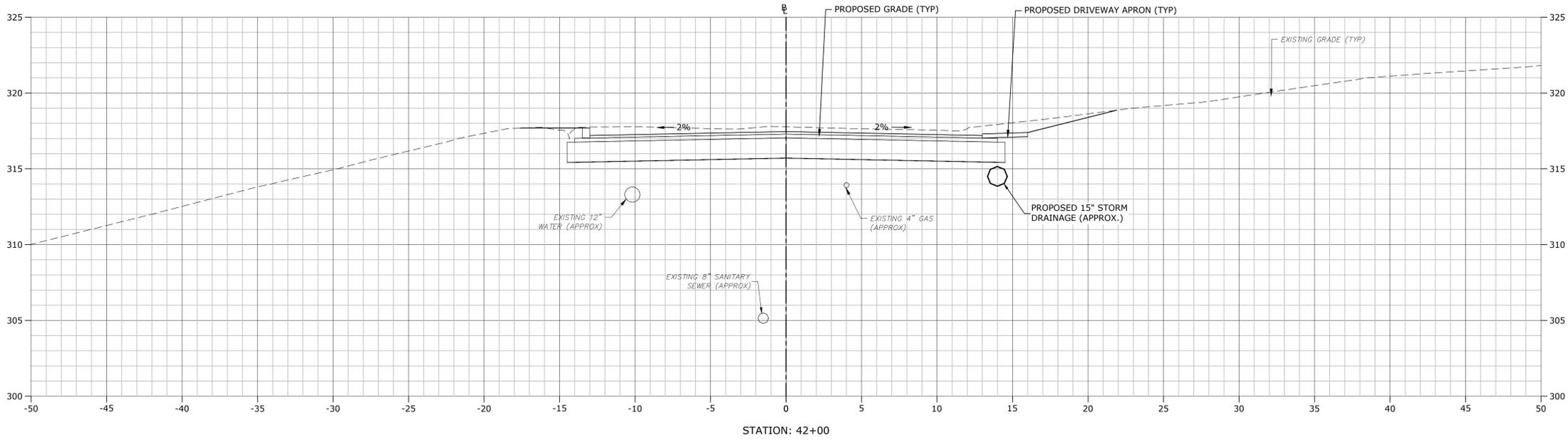
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 39+50 TO 40+50**

SCALE: 1" = 4'

C9.32



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

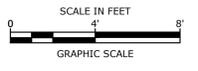
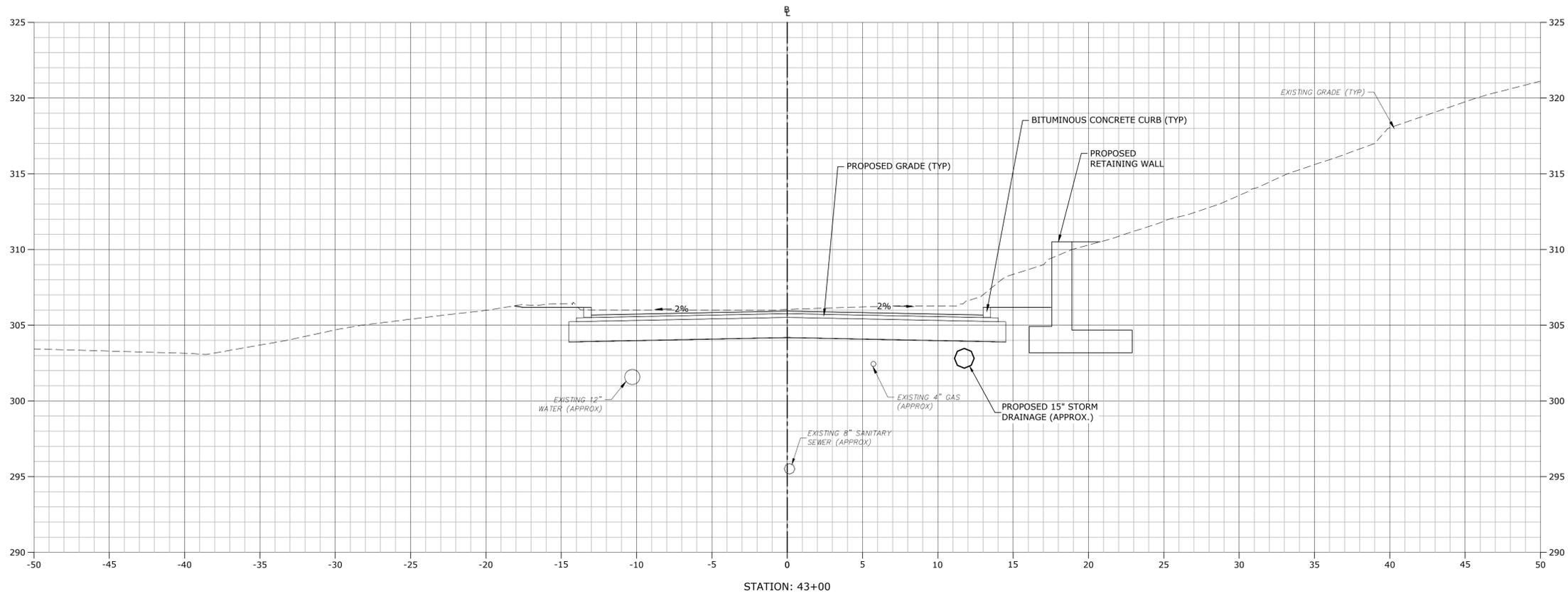
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 41+00 TO 42+00**

SCALE: 1" = 4'

last saved: 2/6/2019 11:11:26am By: Duhonuec
 Tighe & Bond: \\T0196\15_Strobel Rd\Drawings\Sheet\T0196-15-C-900-XS.dwg

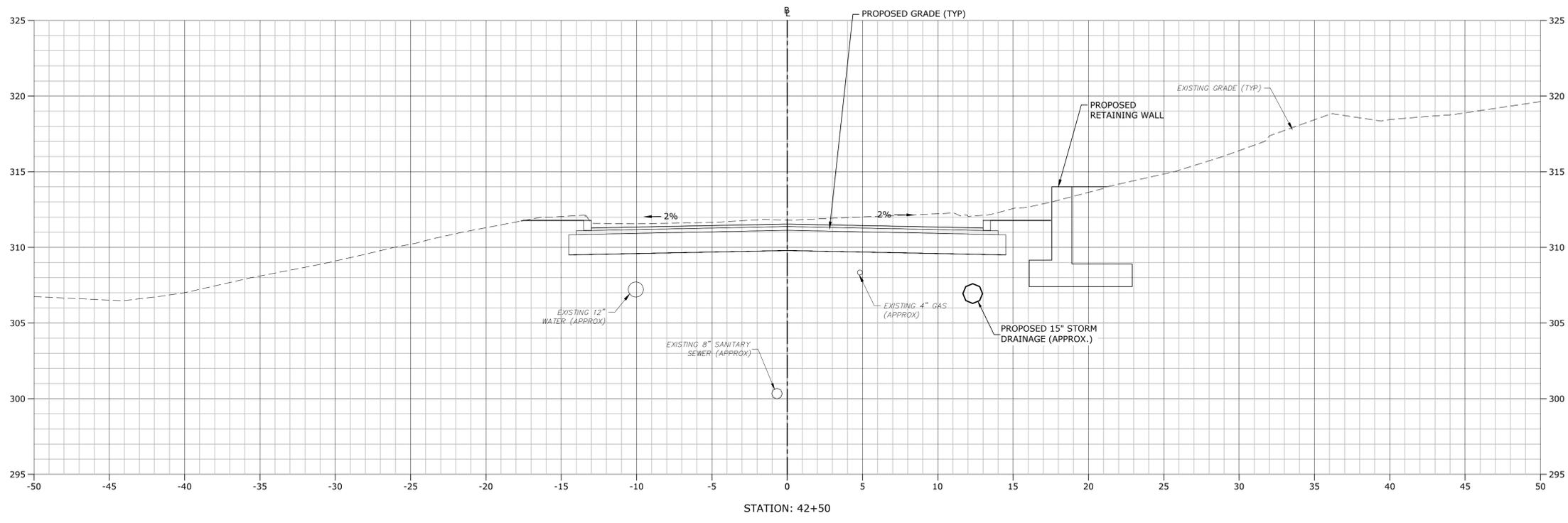


**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY



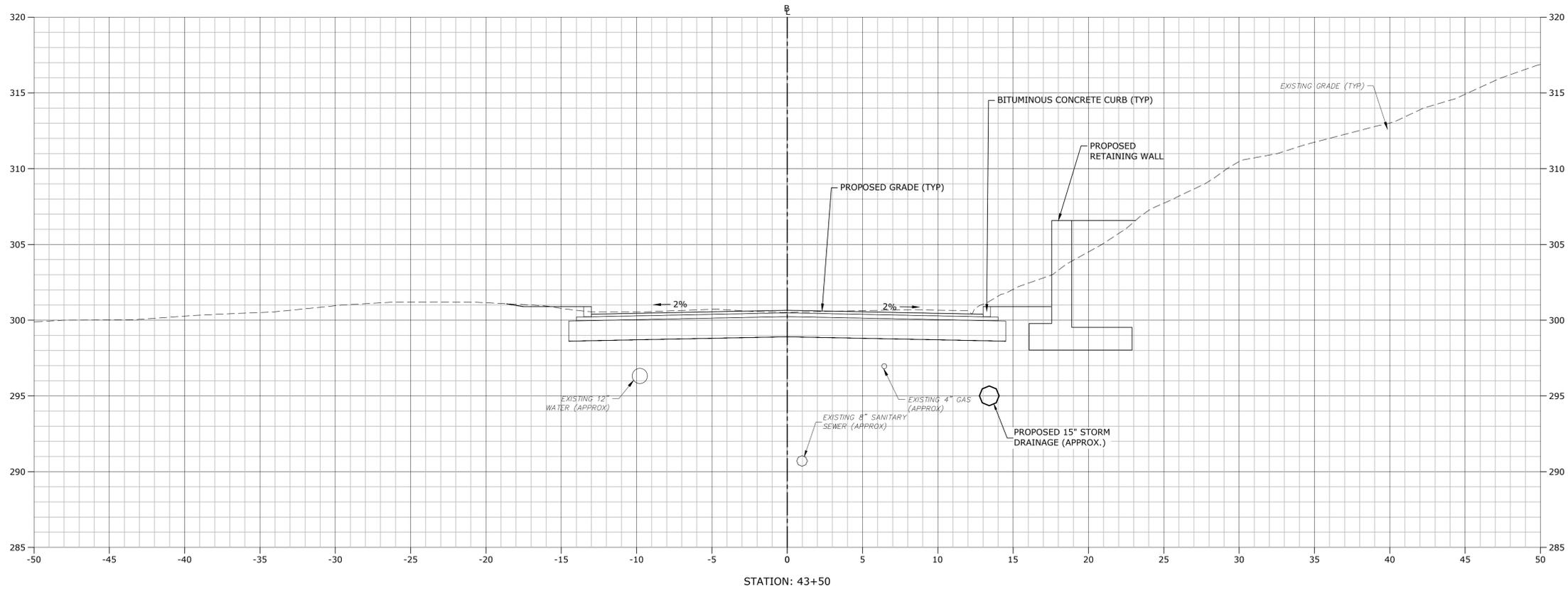
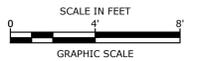
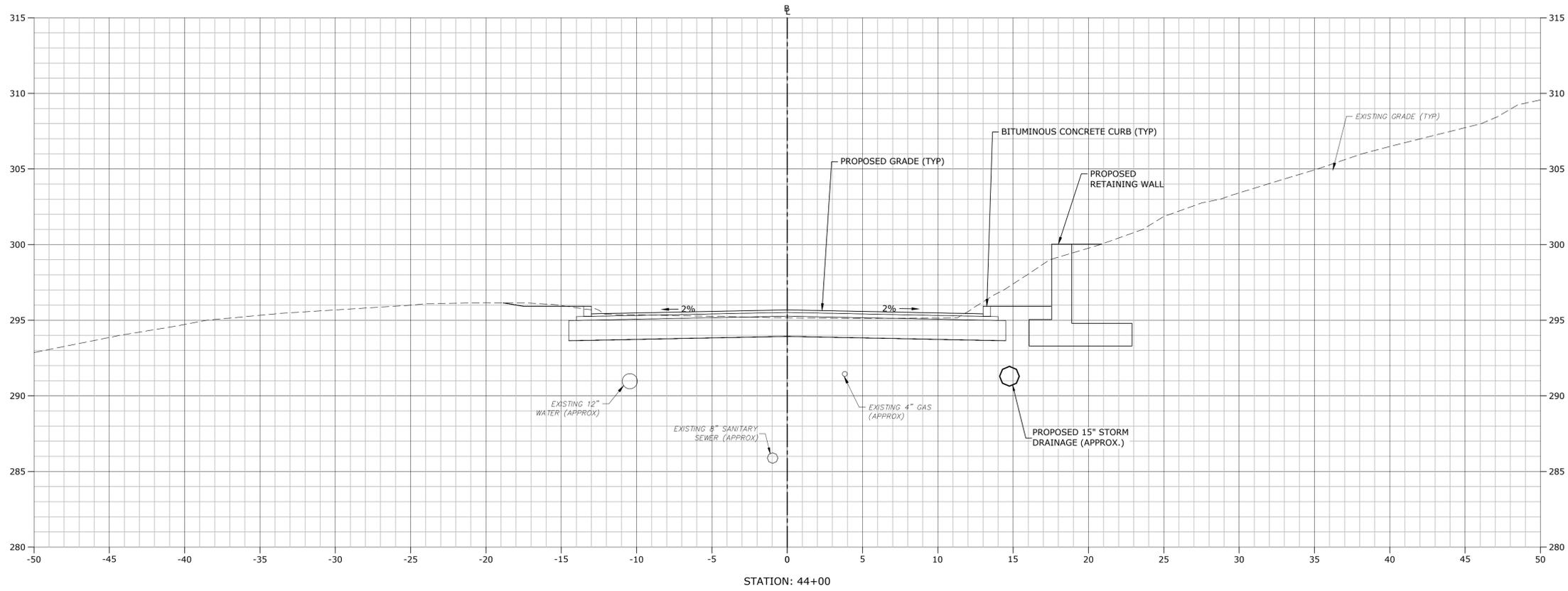
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 42+50 TO 43+00**

SCALE: 1" = 4'

Plot: 2/18/2019 11:26am By: Duhon, C
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawings\Sheet\T0196-15-C-900-XS.dwg



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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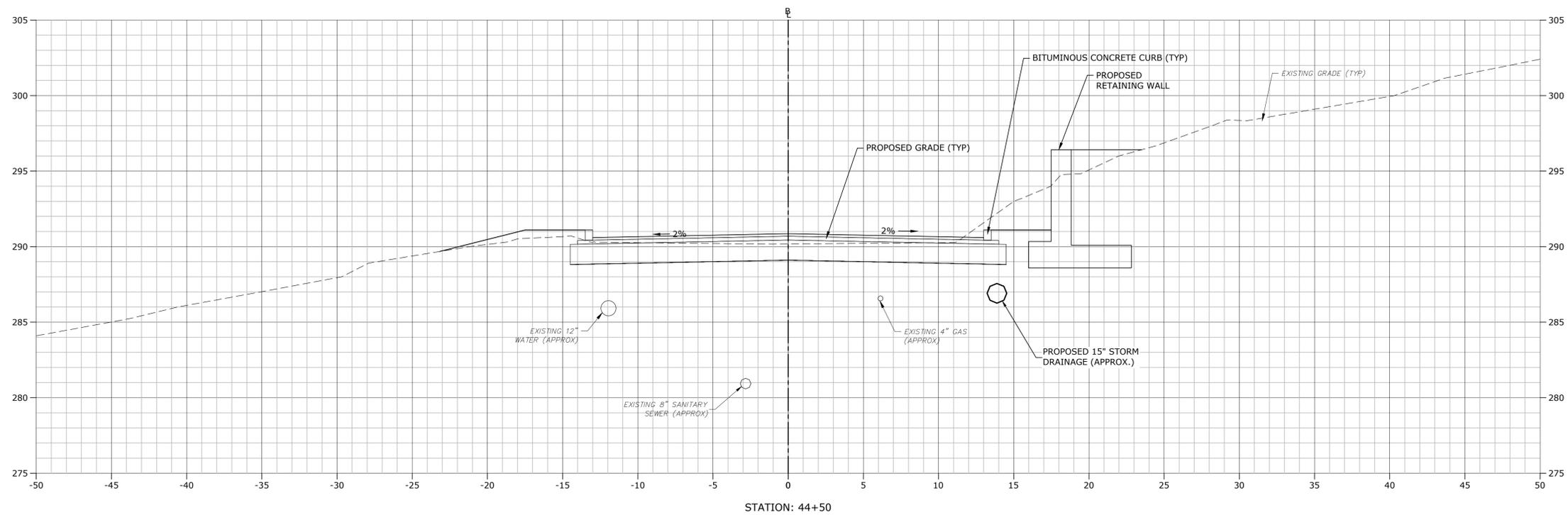
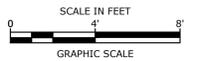
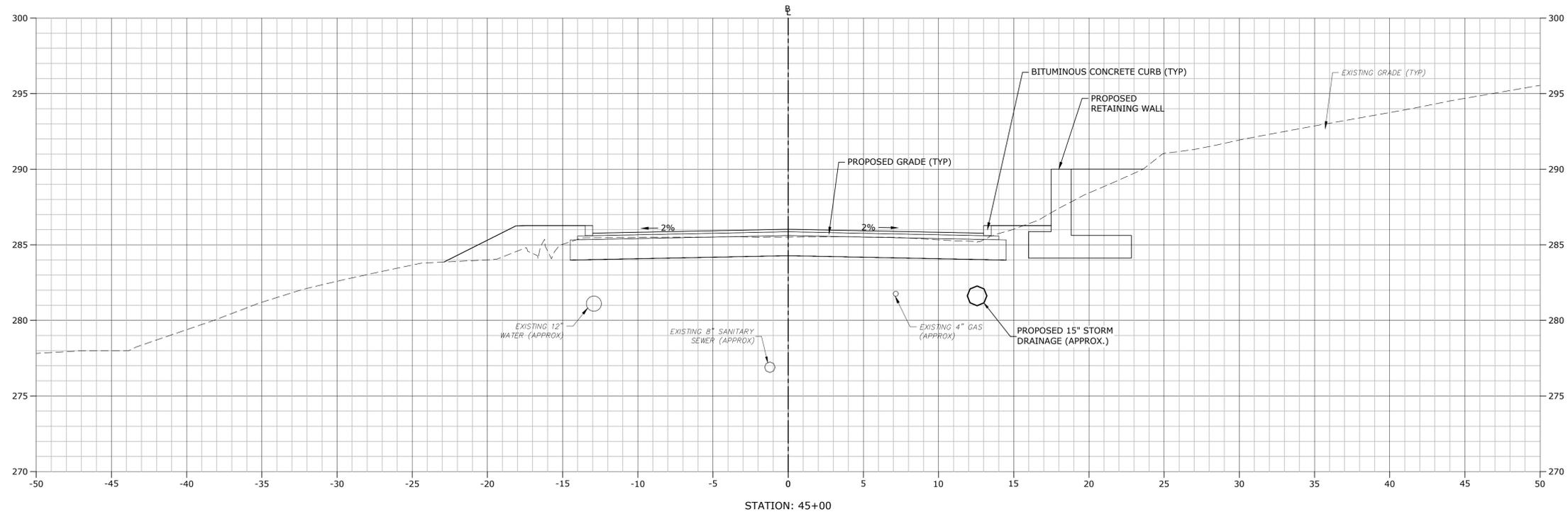
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 43+50 TO 44+00**

SCALE: 1" = 4'

C9.35



VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

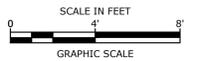
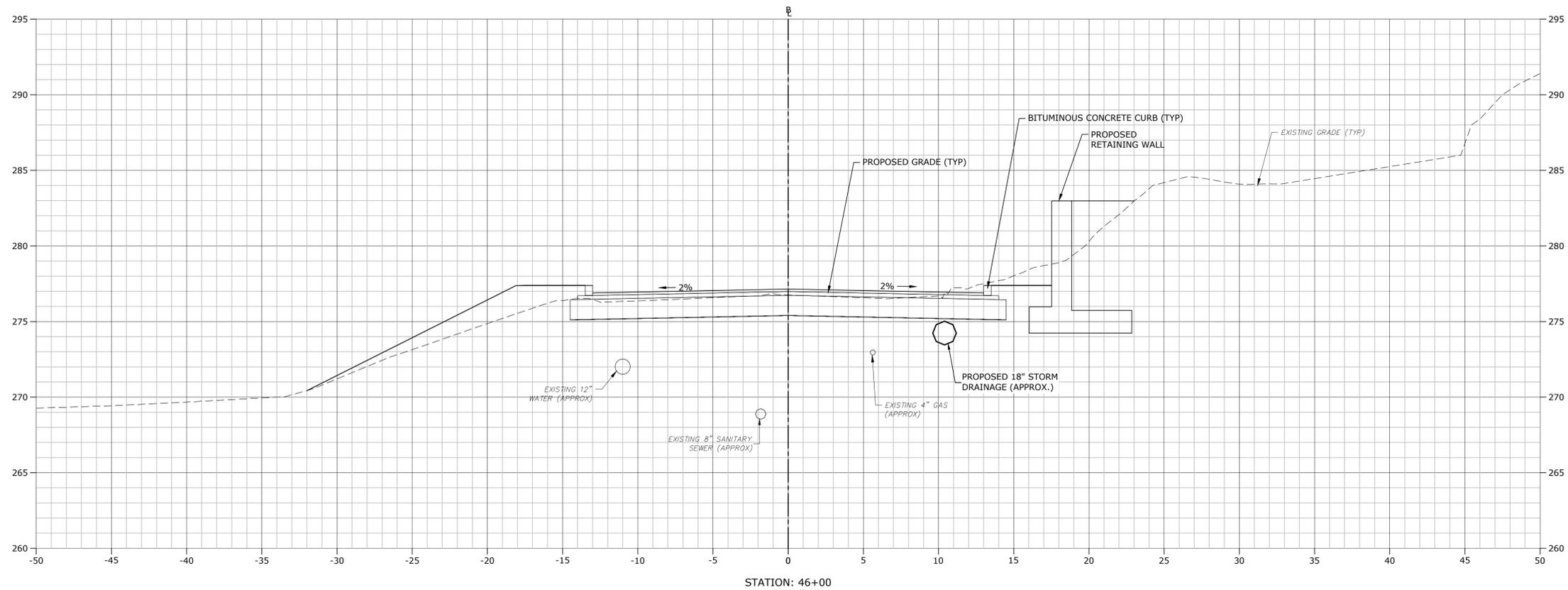
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

STROBEL ROAD CROSS SECTIONS
 STA: 44+50 TO 45+00

SCALE: 1" = 4'

Plot Speed: 2/6/2019 11:27am By: Duhon, J
 Tighe & Bond: J:\T0196\15_Strobel Rd\Drawings\Sheet\T0196-15-C-900-XS.dwg

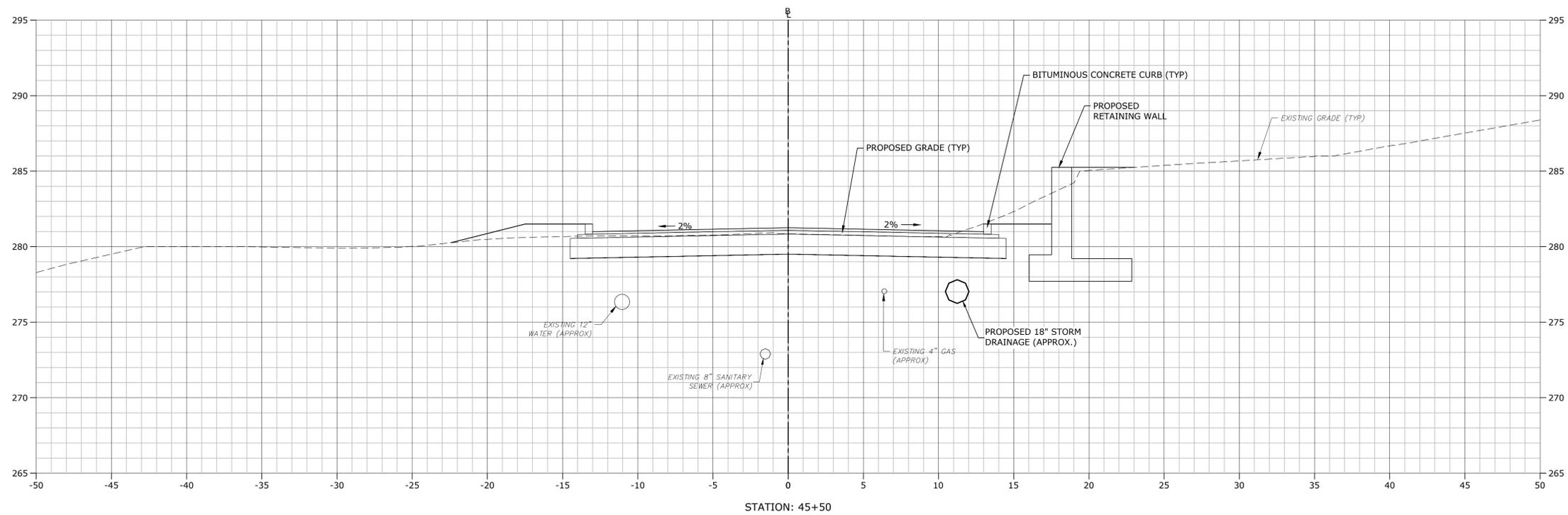


**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
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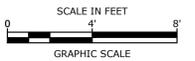
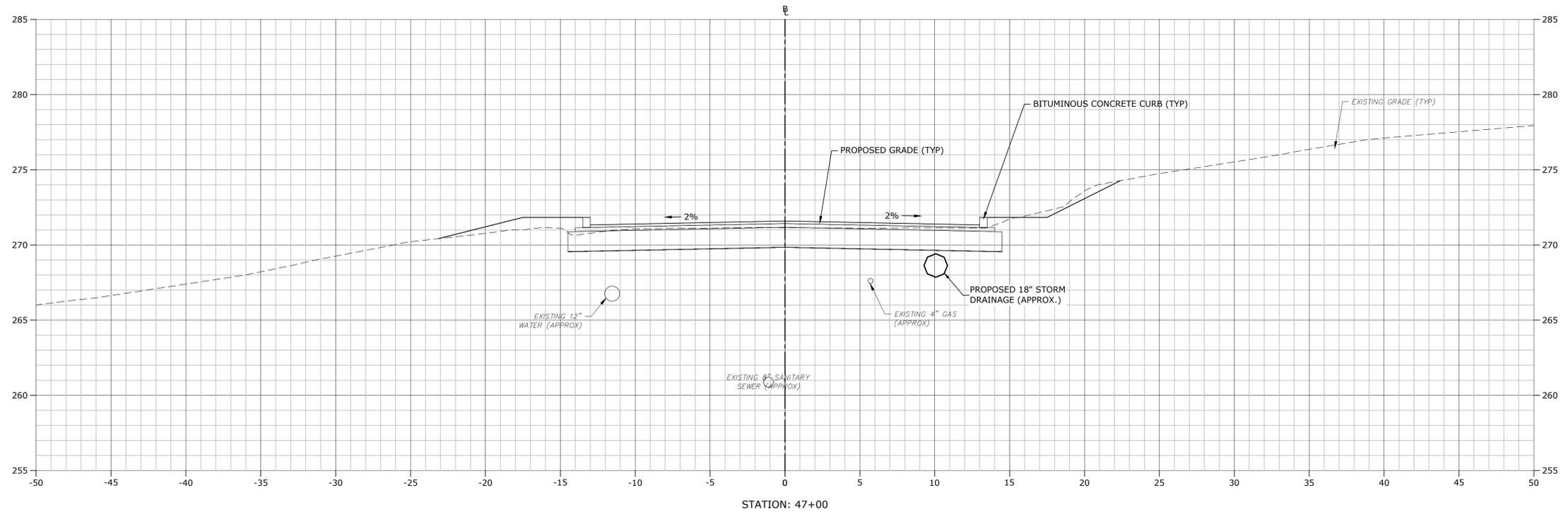


MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 45+50 TO 46+00**

SCALE: 1" = 4'

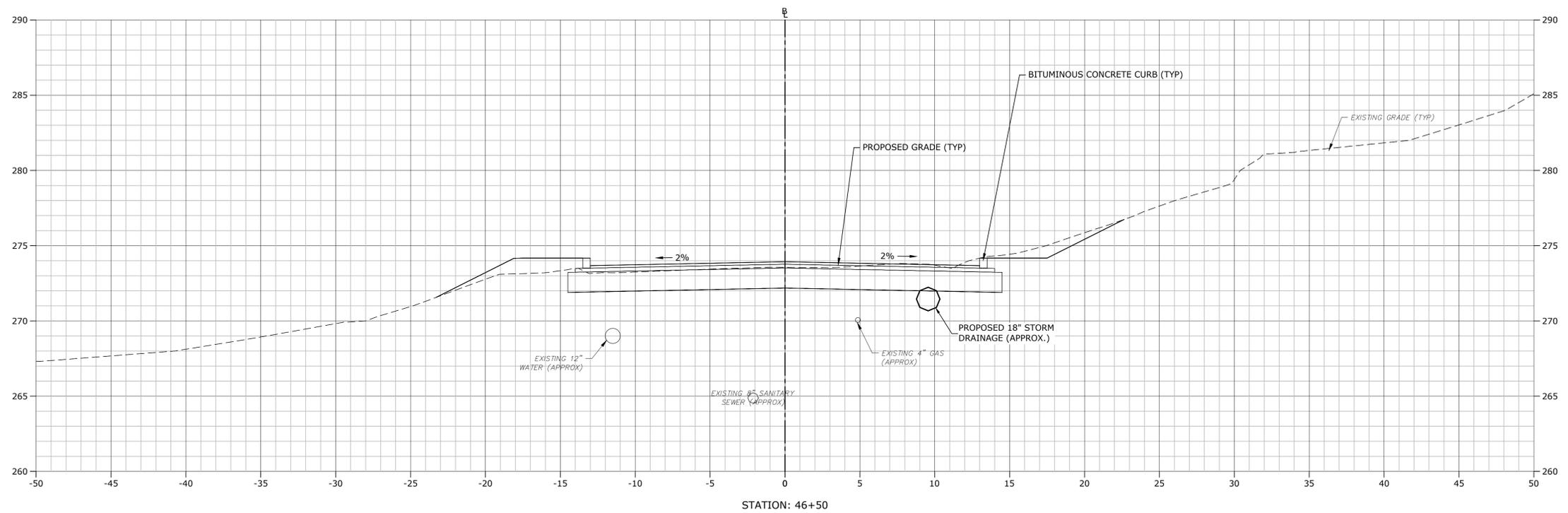


**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
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 ORIGINAL DRAWING
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 THIS SHEET, ADJUST
 SCALES ACCORDINGLY

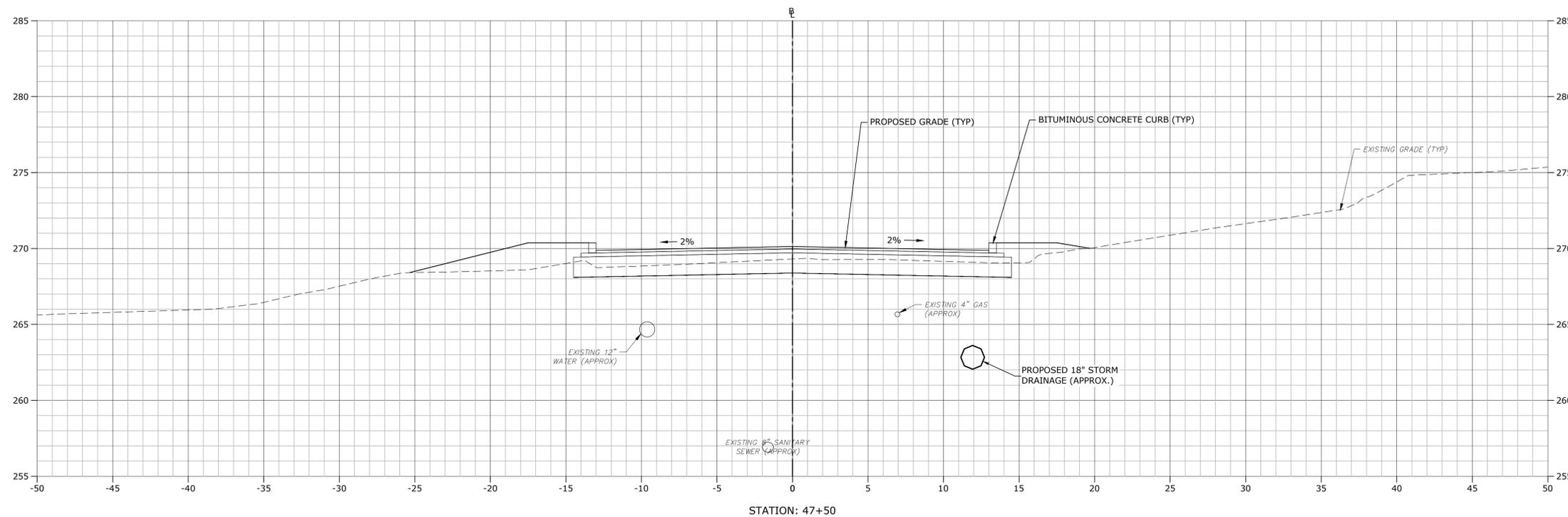
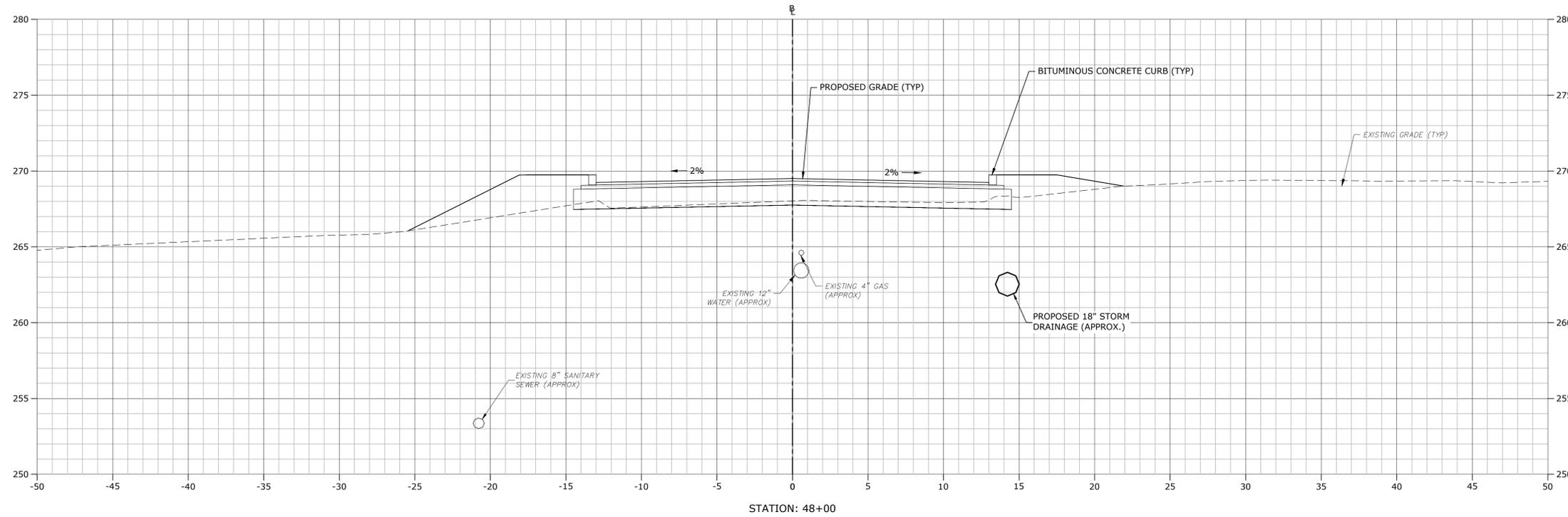


MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 46+50 TO 47+00**

SCALE: 1" = 4'



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

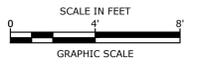
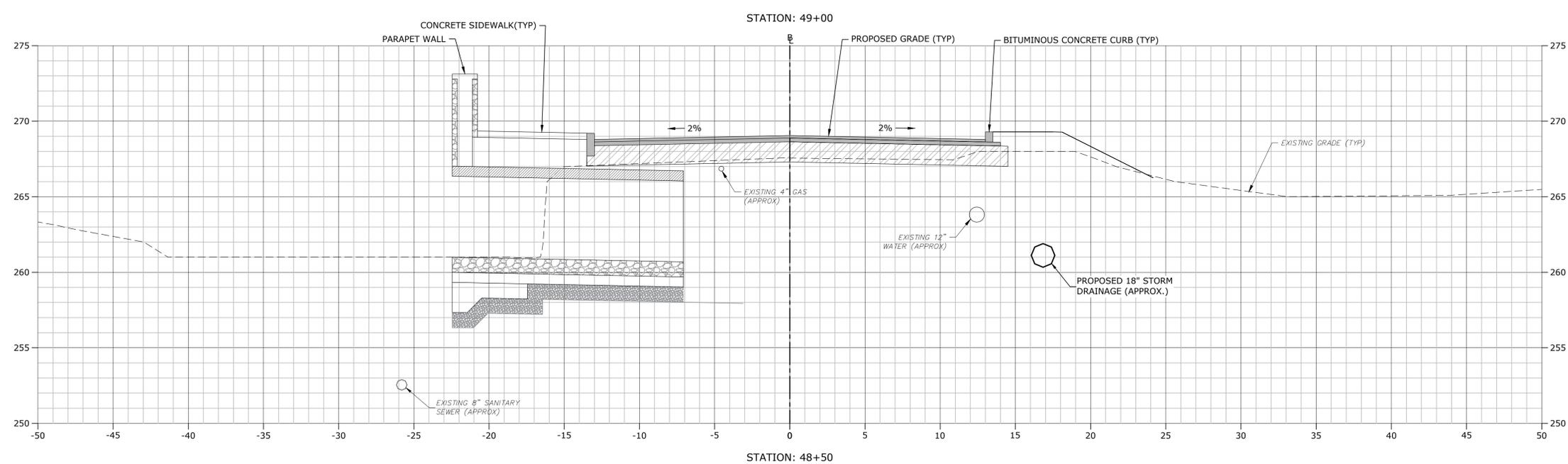
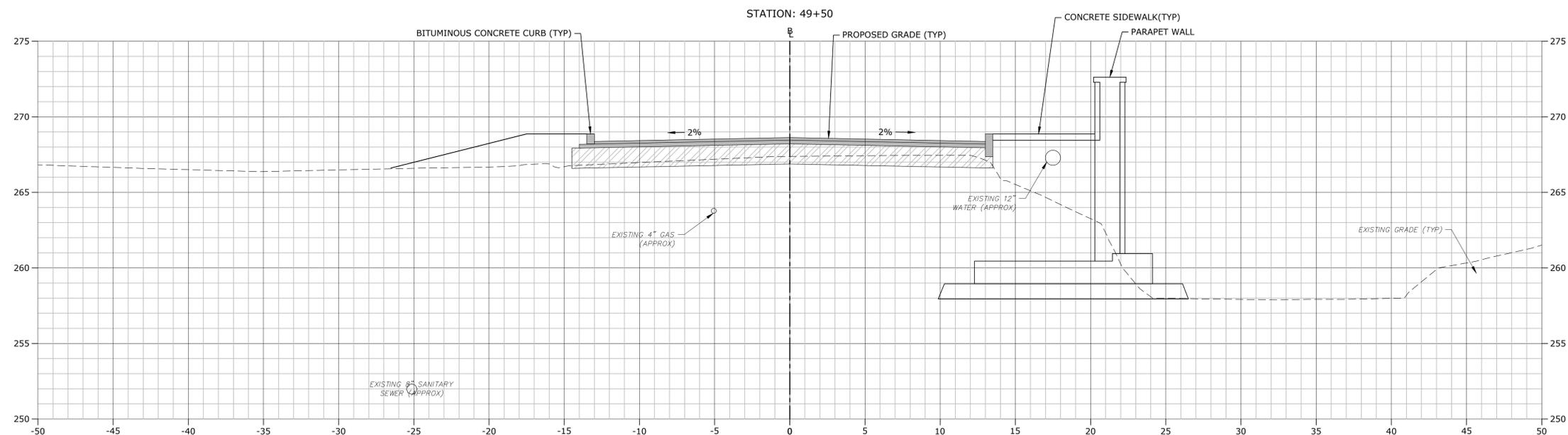
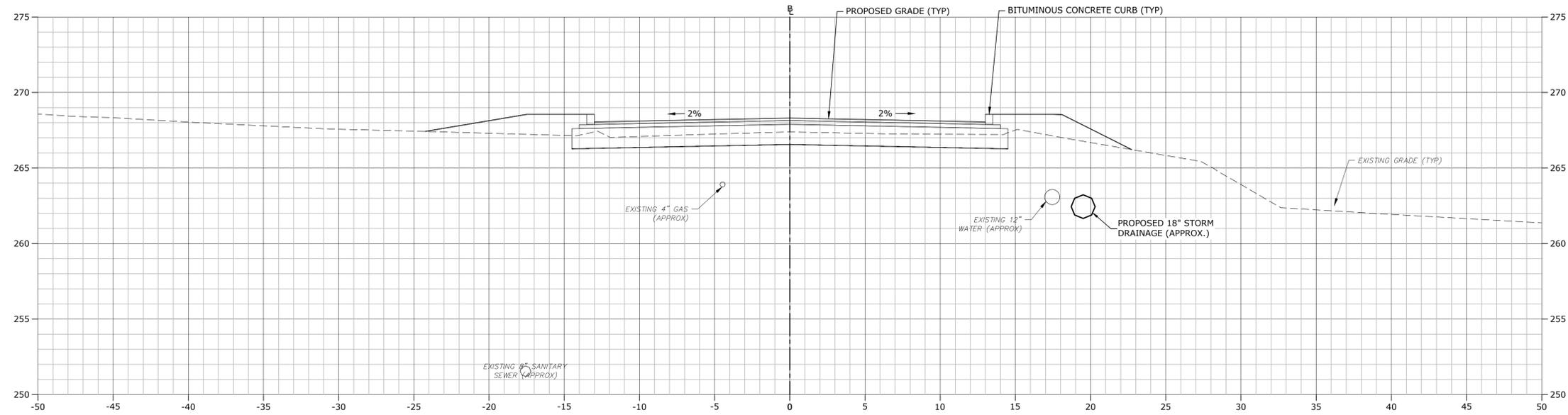
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

STROBEL ROAD CROSS SECTIONS
 STA: 47+50 TO 48+00

SCALE: 1" = 4'

C9.39



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

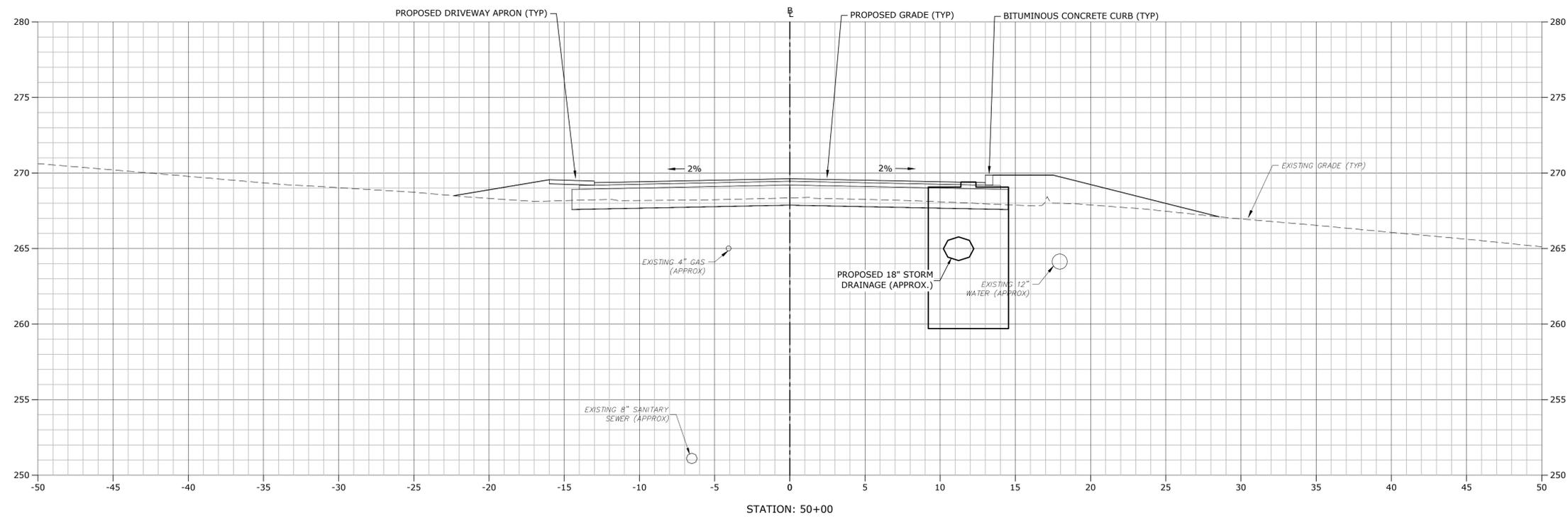
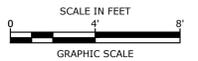
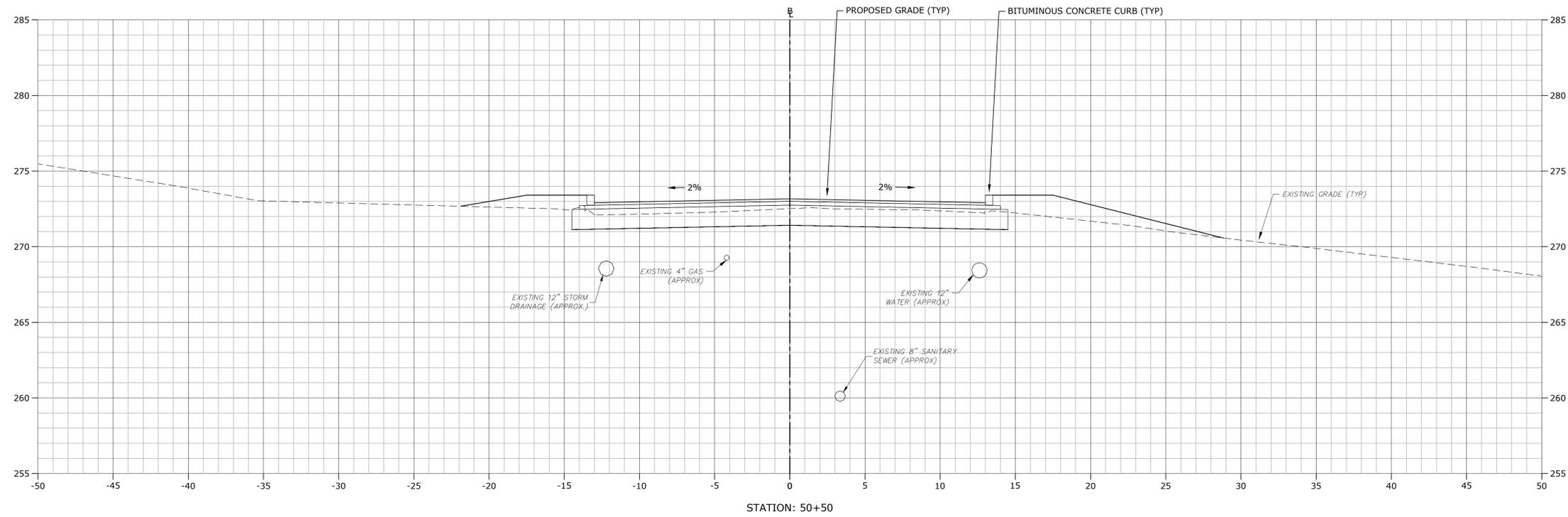
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 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 48+50 TO 49+50**

SCALE: 1" = 4'

C9.40

Job Sheet: 2/18/2019
 Tighe & Bond: T:\T0196\15_Strobel Rd\Drawings\Sheet\T0196-15-C-900-XS.dwg



VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

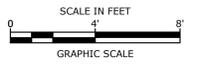
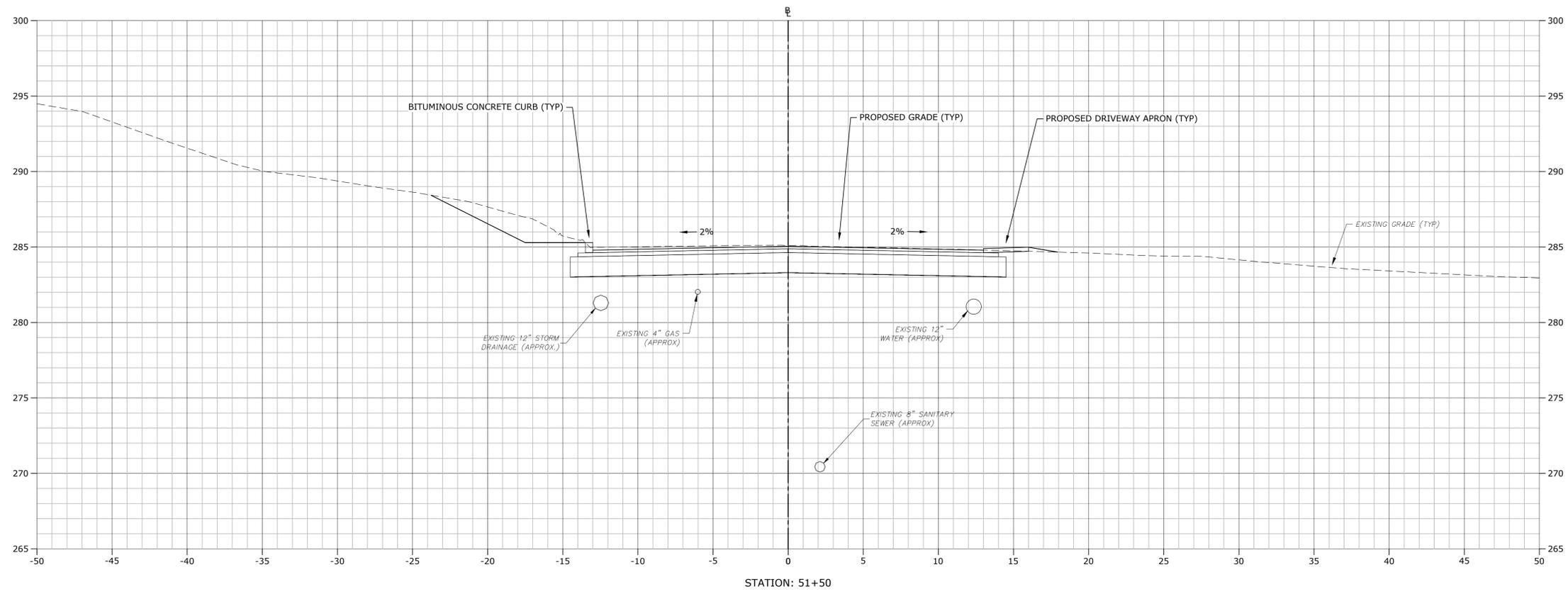
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

STROBEL ROAD CROSS SECTIONS
 STA: 50+00 TO 50+50

SCALE: 1" = 4'

C9.41

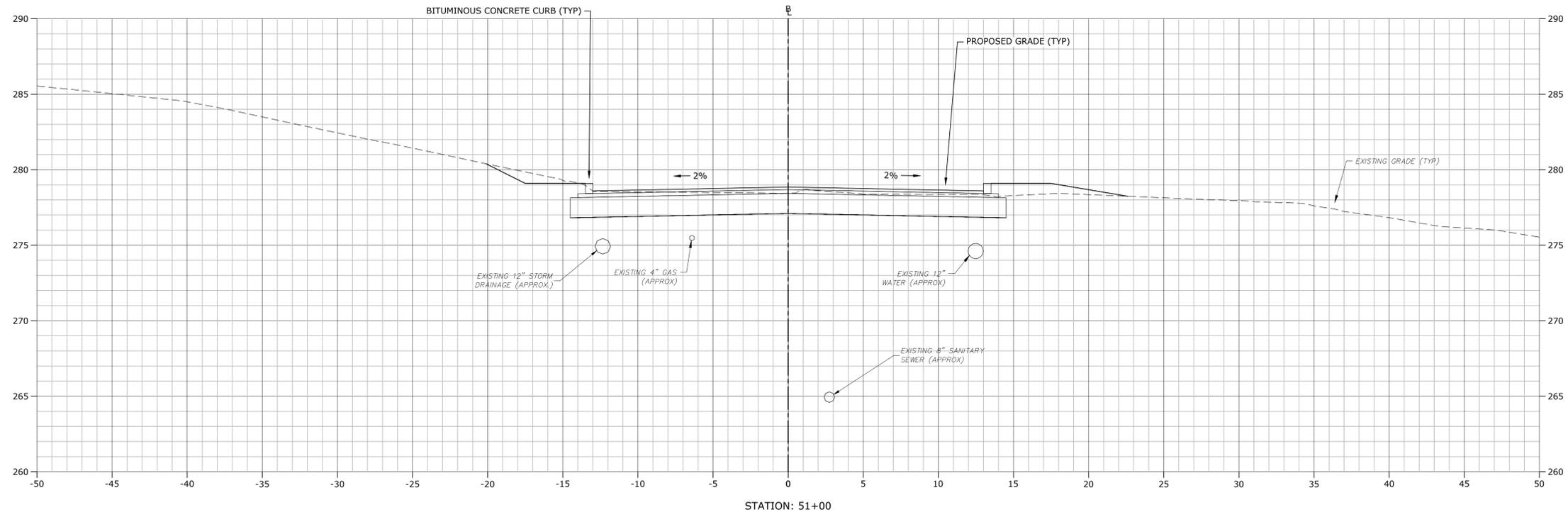


**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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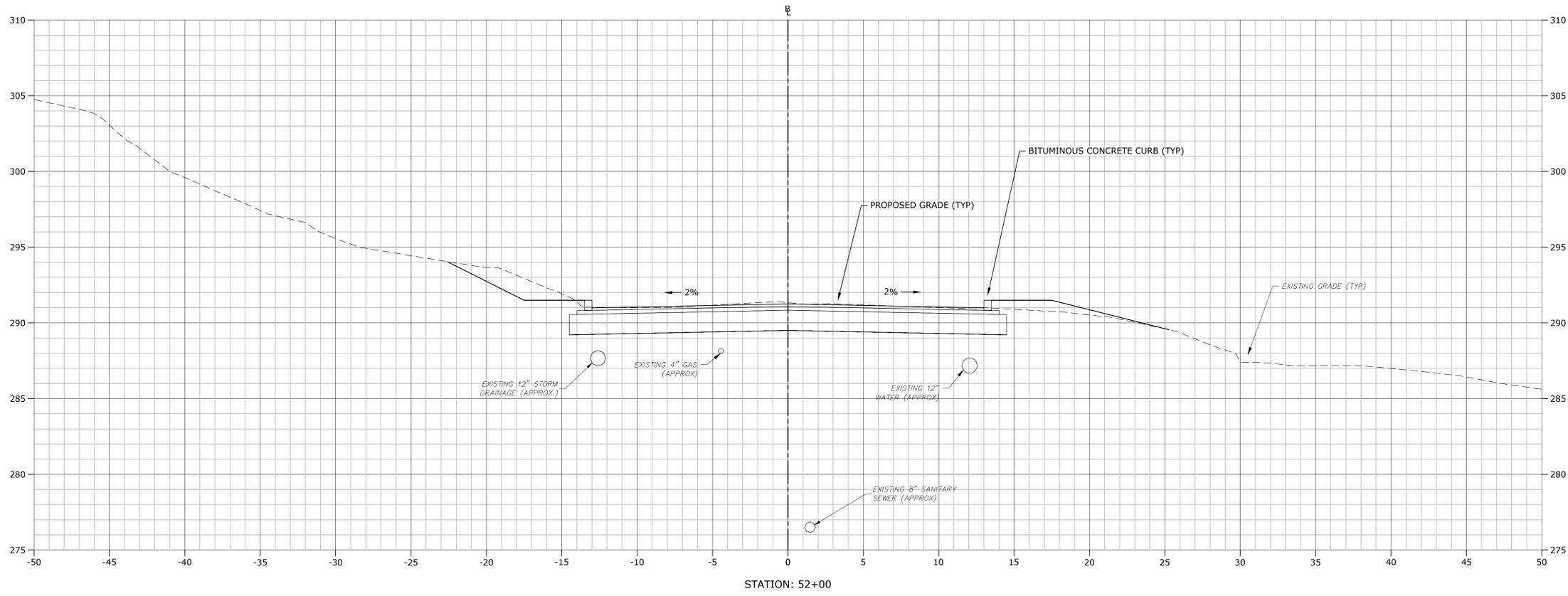
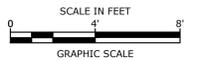
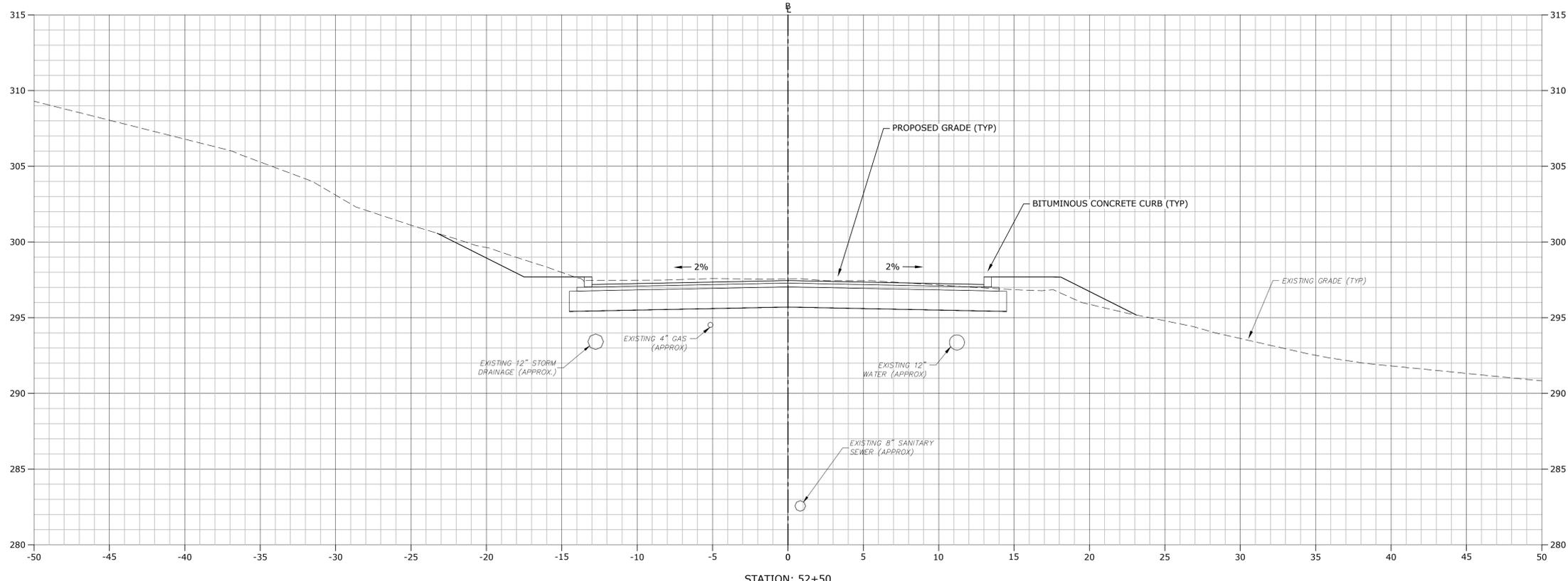
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 51+00 TO 51+50**

SCALE: 1" = 4'

C9.42



VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

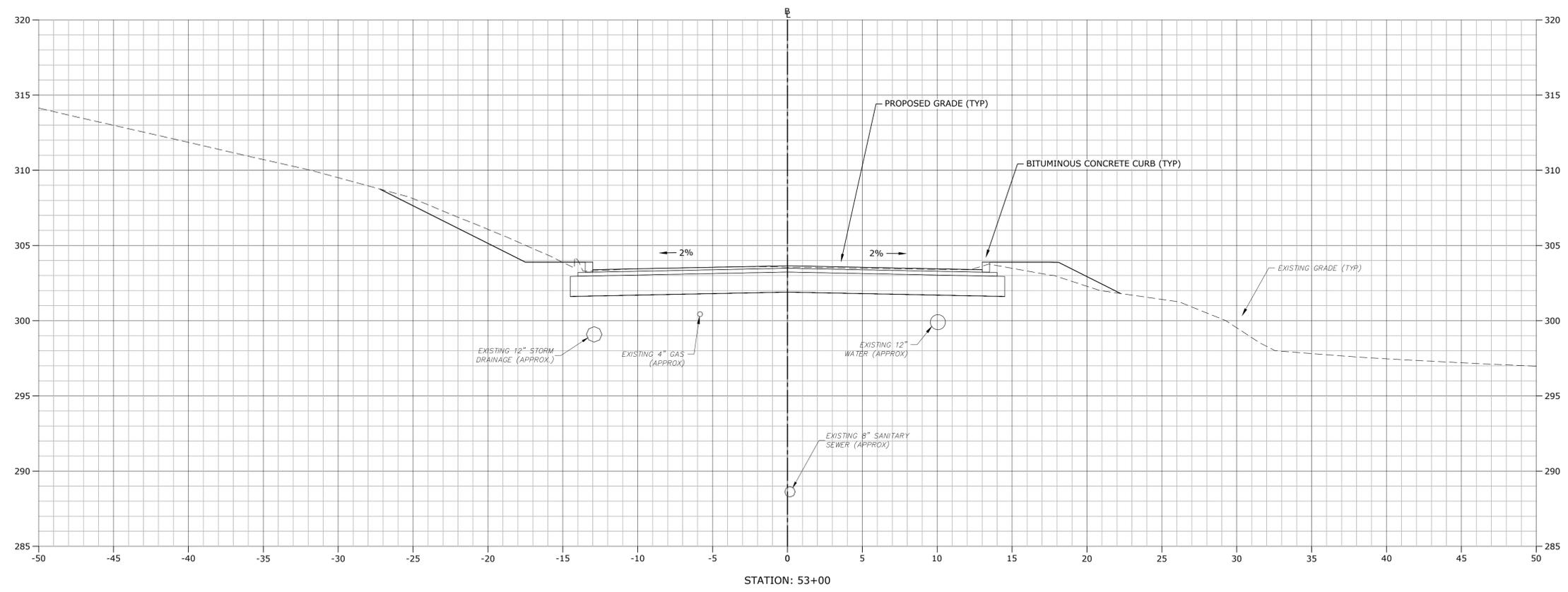
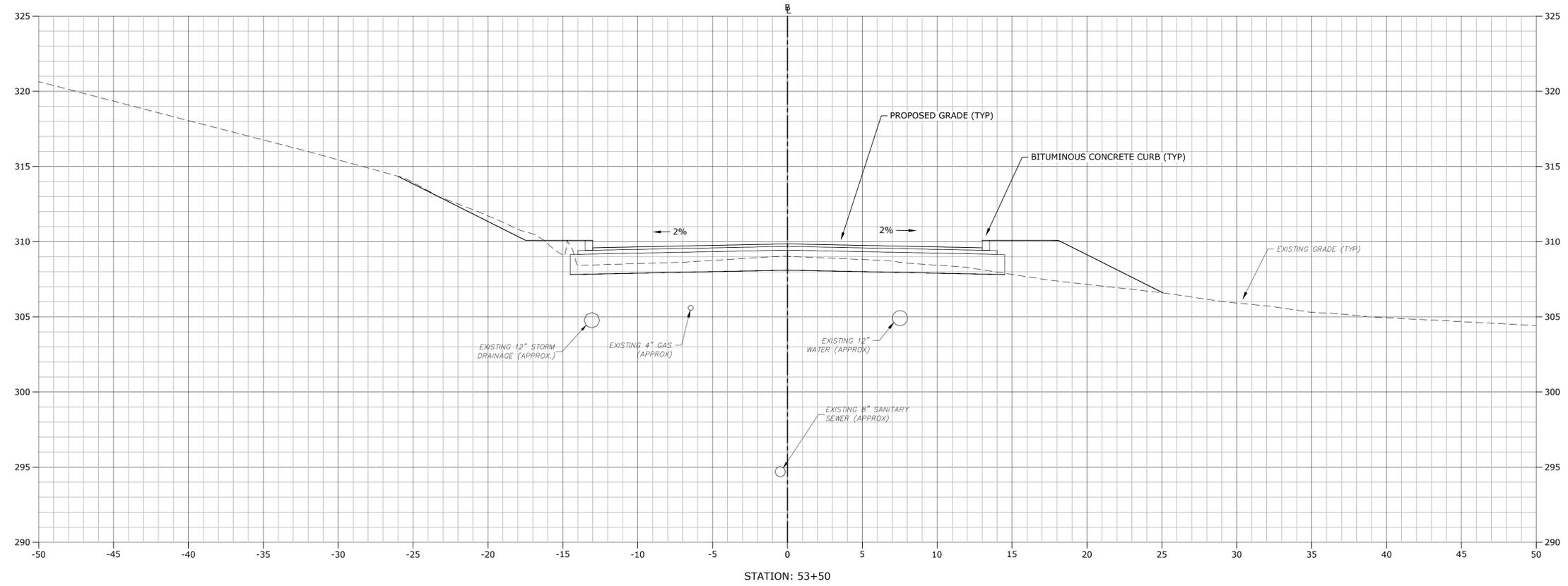
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

STROBEL ROAD CROSS SECTIONS
 STA: 52+00 TO 52+50

SCALE: 1" = 4'

C9.43



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

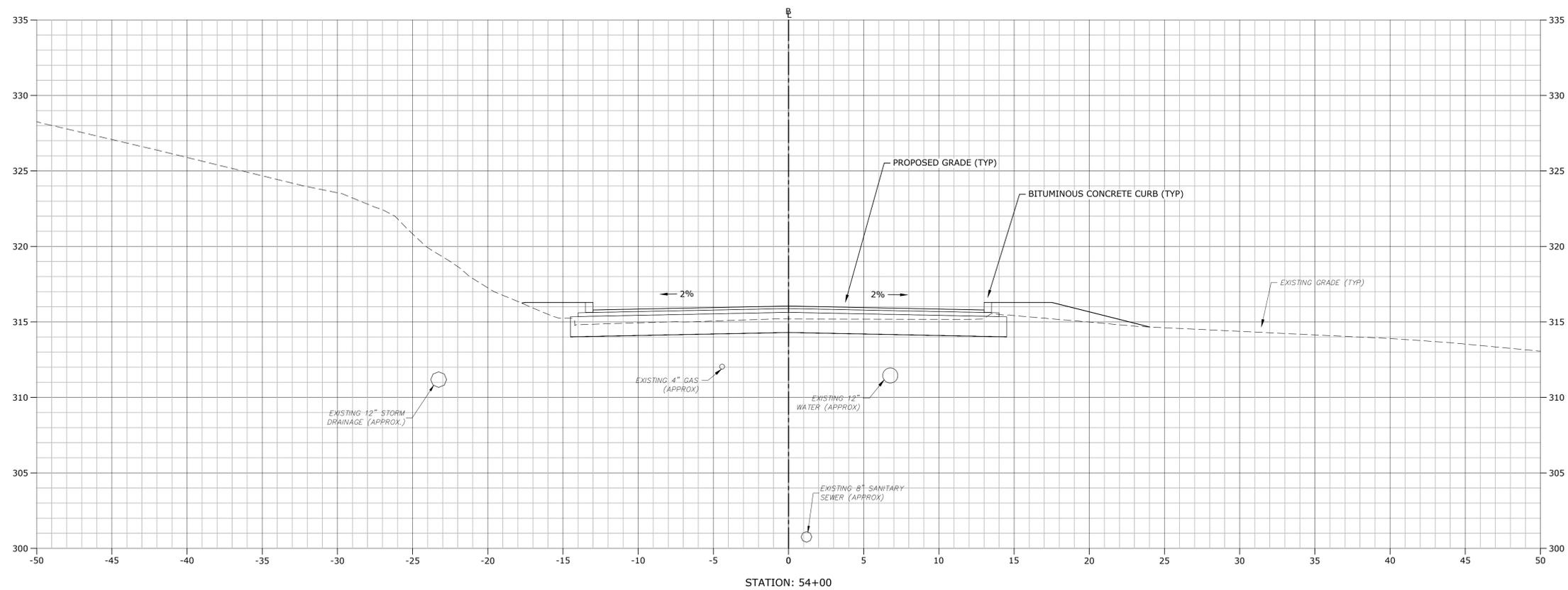
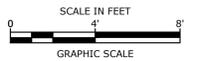
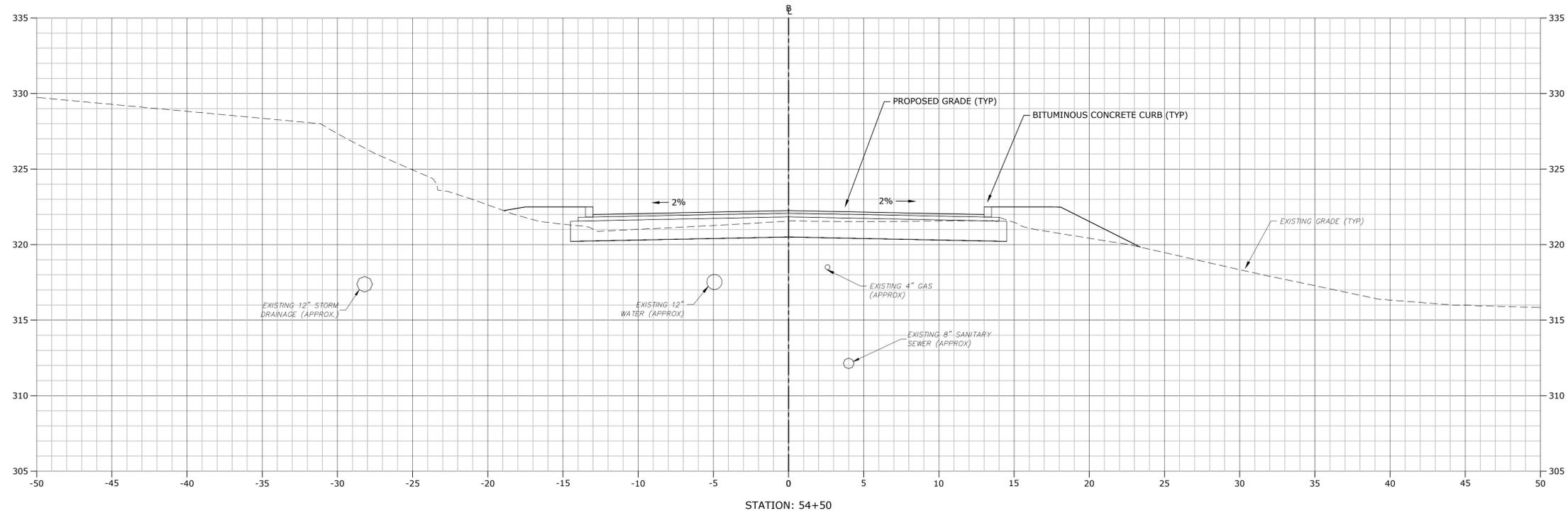
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 53+00 TO 53+50**

SCALE: 1" = 4'

C9.44



VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

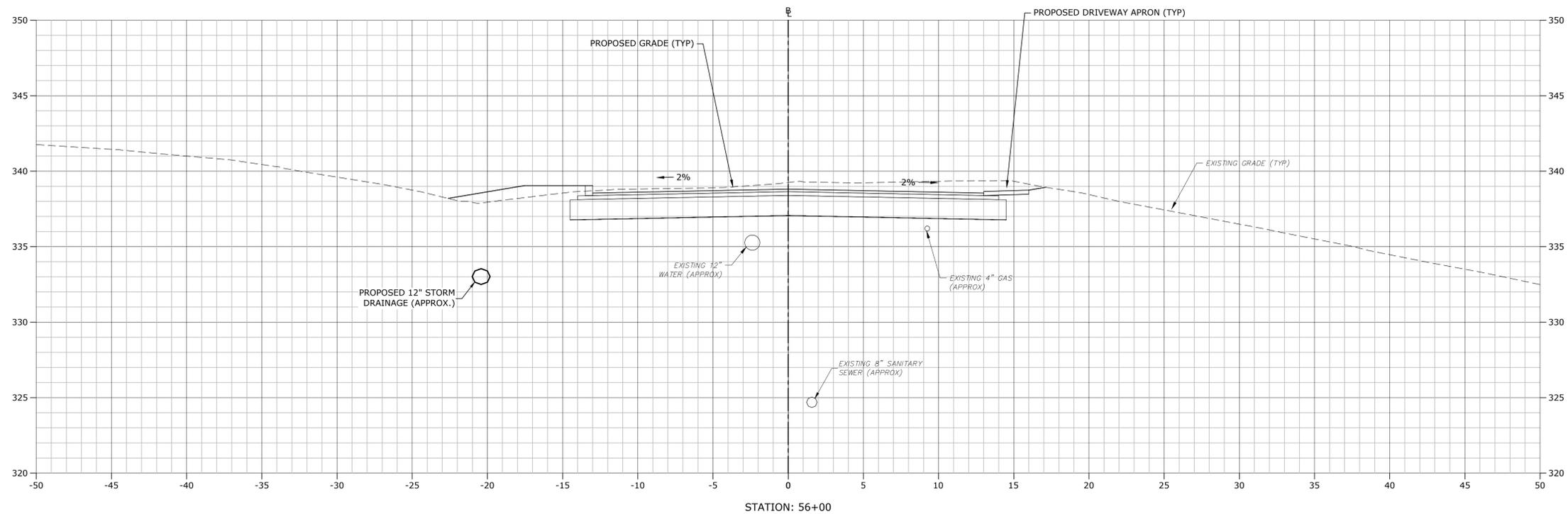
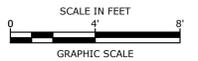
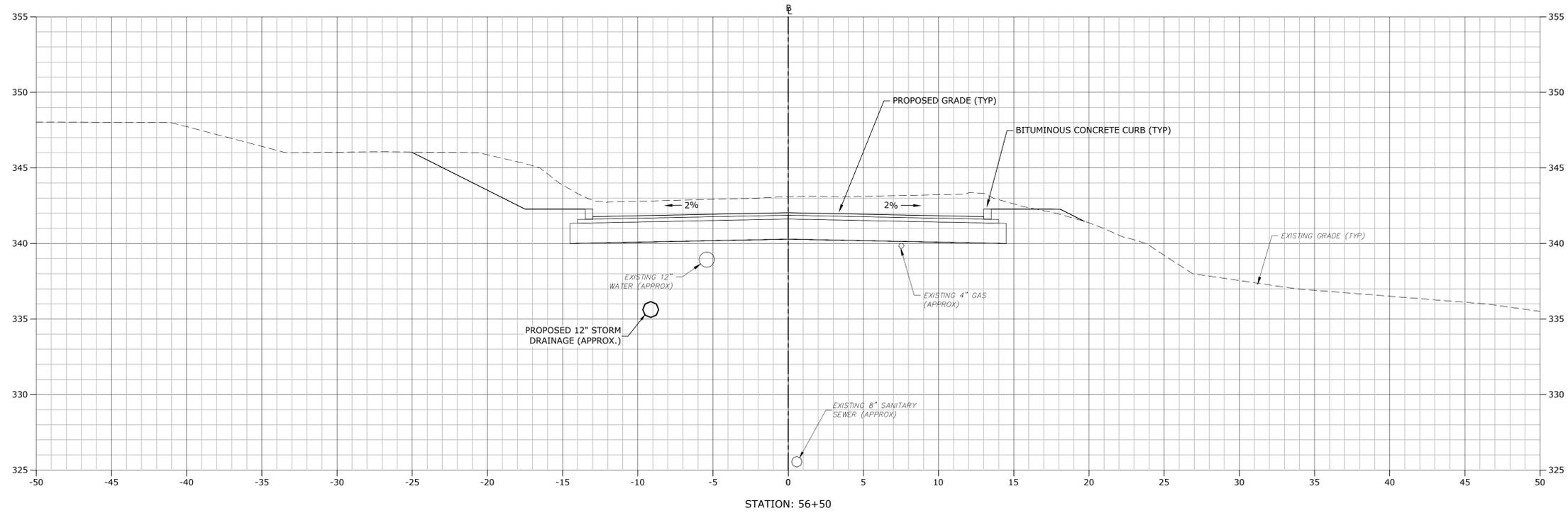
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

STROBEL ROAD CROSS SECTIONS
 STA: 54+00 TO 54+50

SCALE: 1" = 4'

C9.45



VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

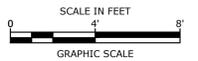
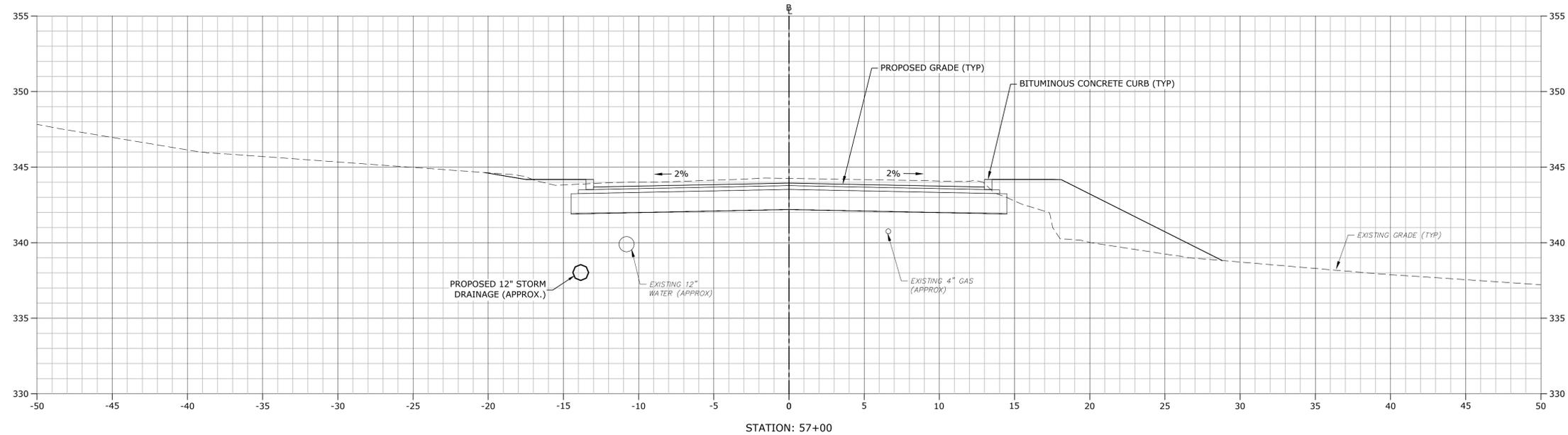
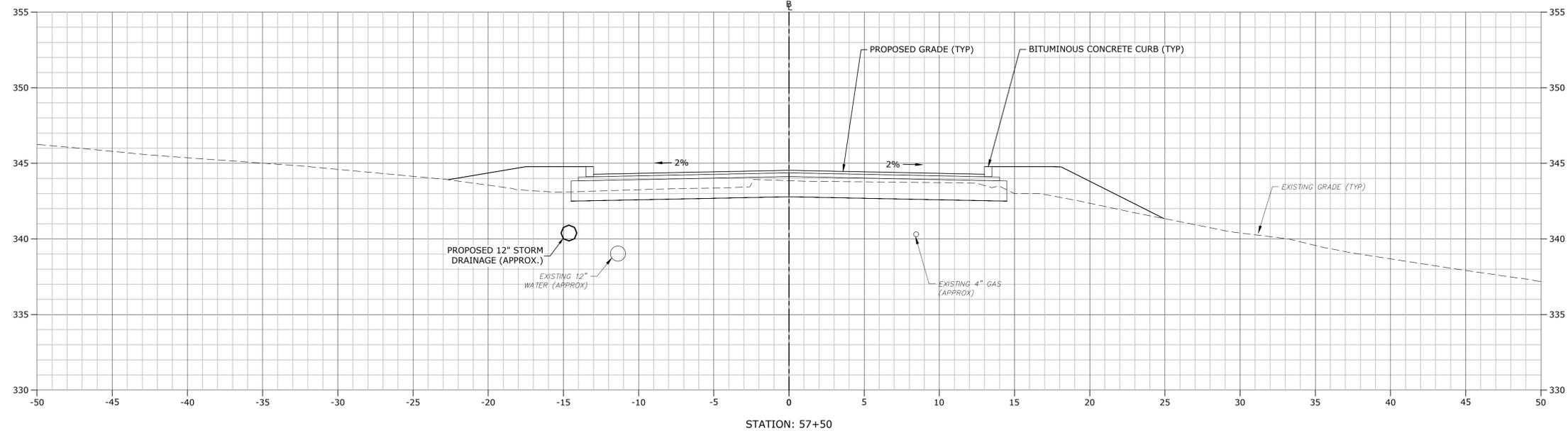
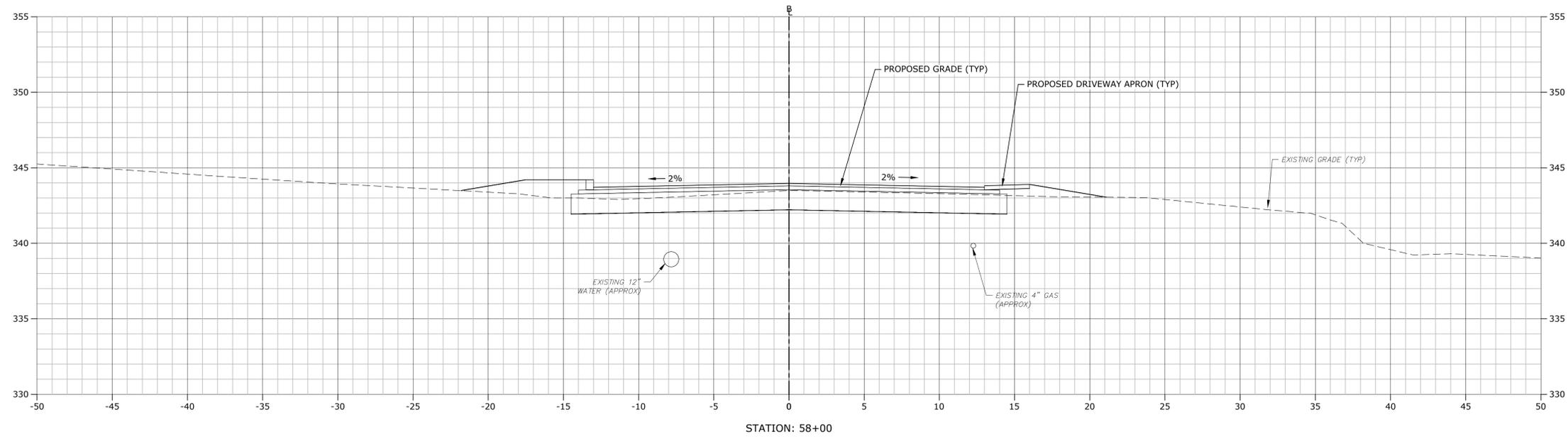
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

STROBEL ROAD CROSS SECTIONS
 STA: 56+00 TO 56+50

SCALE: 1" = 4'

C9.47



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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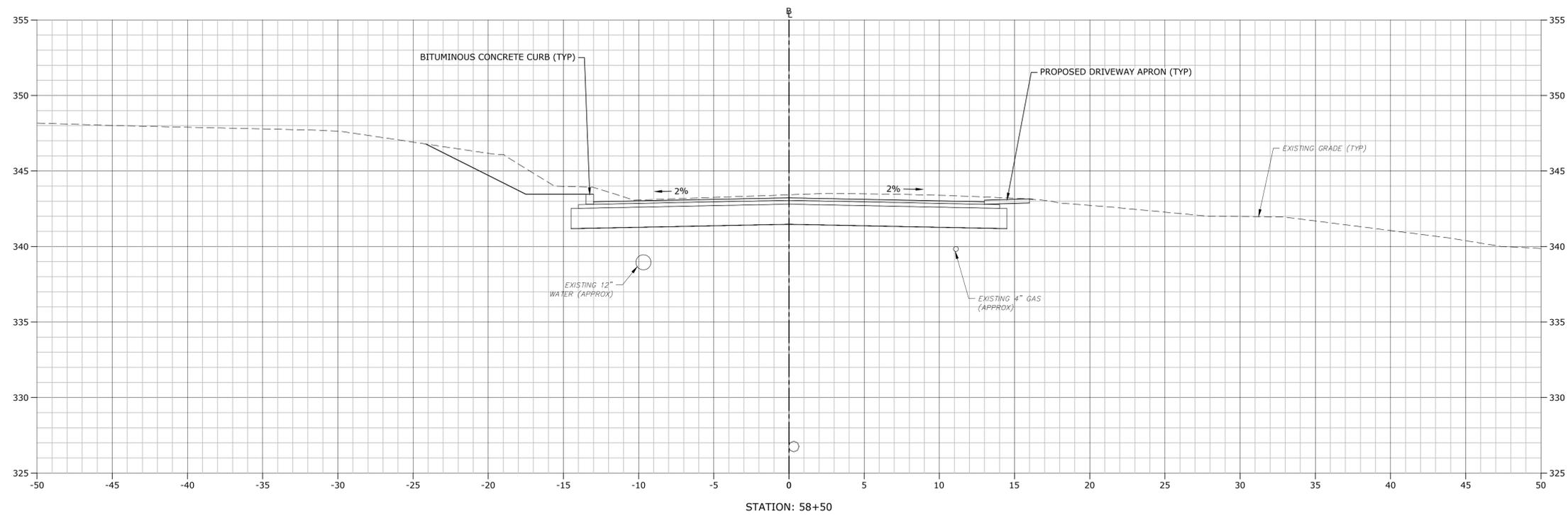
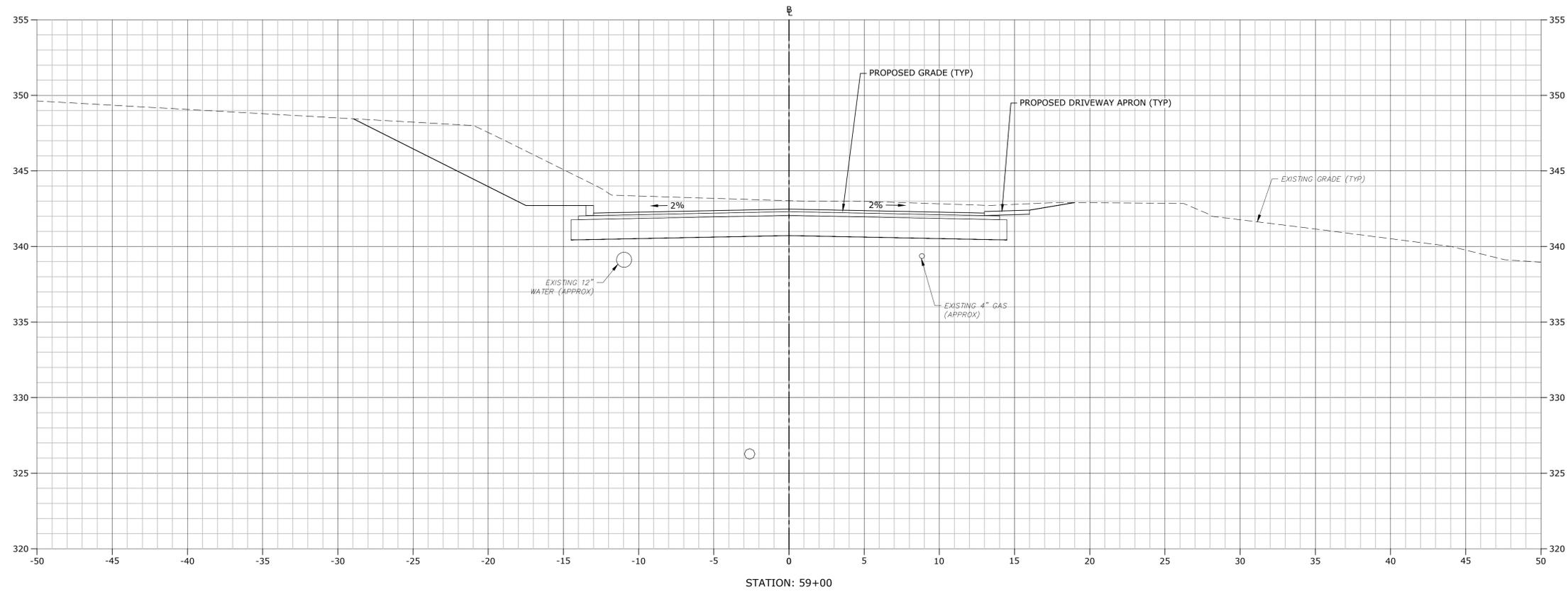
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 57+00 TO 58+00**

SCALE: 1" = 4'

C9.48



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

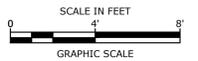
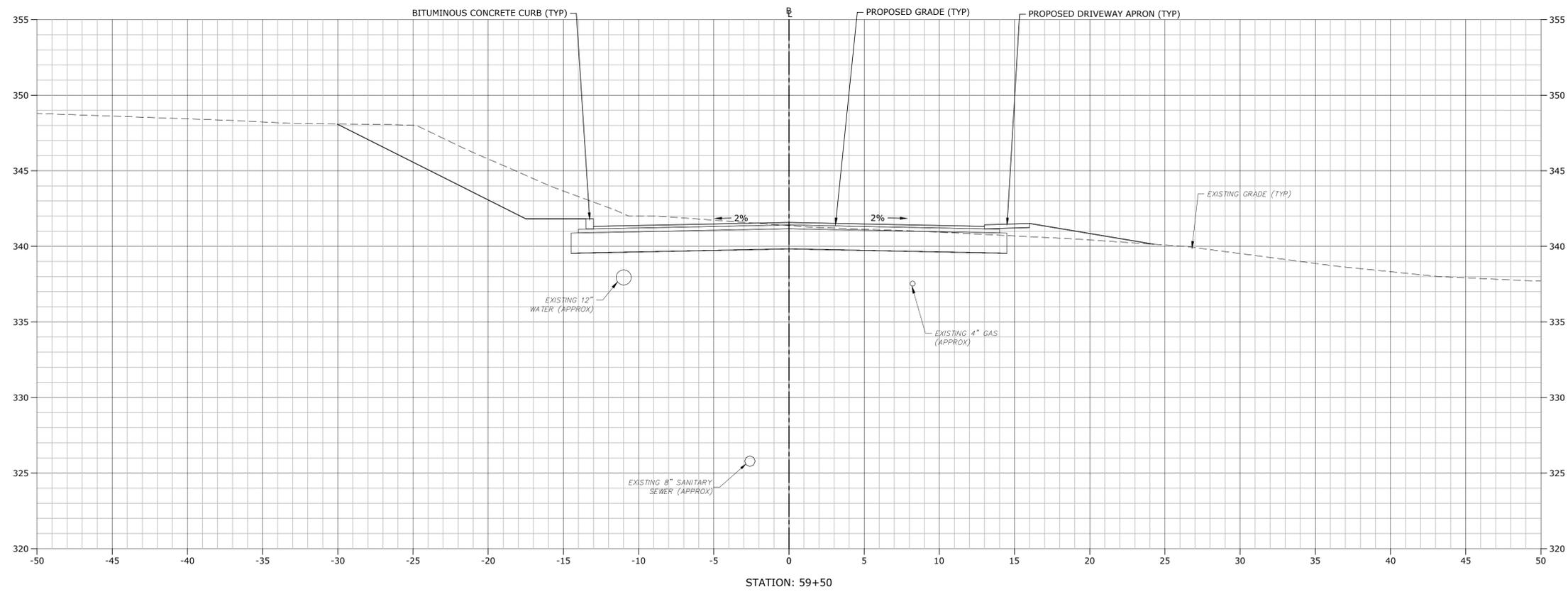
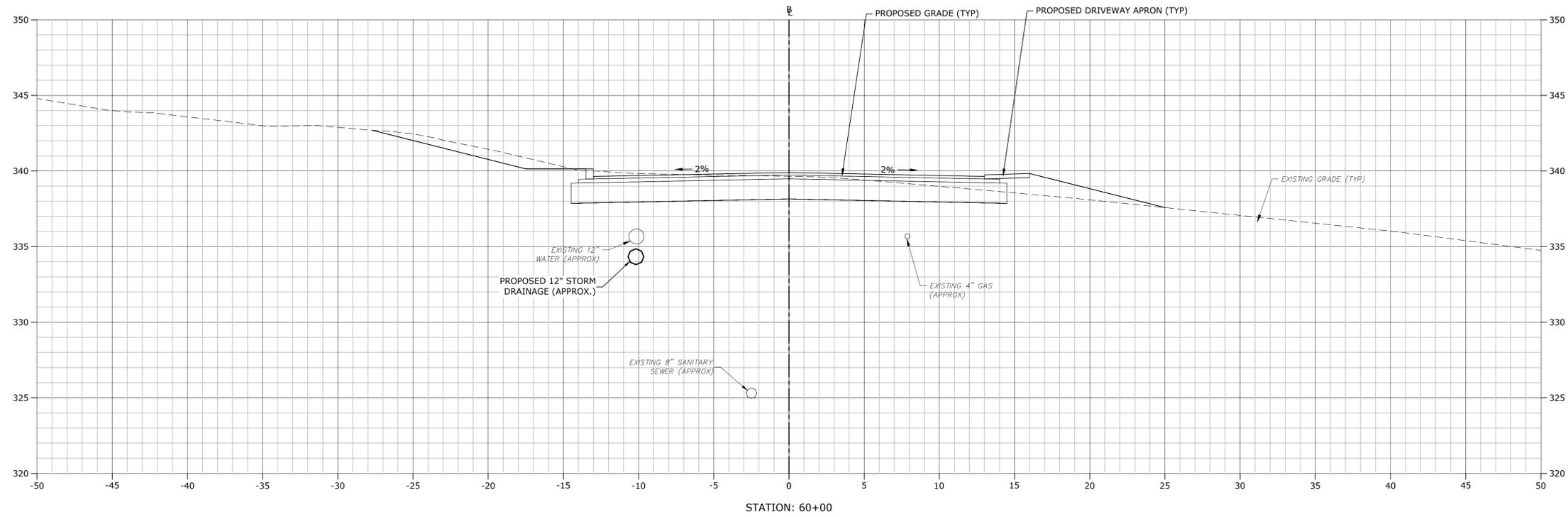
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-15-C-900-XS.dwg	
DRAWN BY:	MDS	
CHECKED:	CRD	
APPROVED:	JWB	

**STROBEL ROAD
 CROSS SECTIONS
 STA: 58+50 TO 59+00**

SCALE: 1" = 4'

C9.49



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

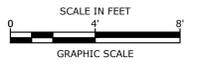
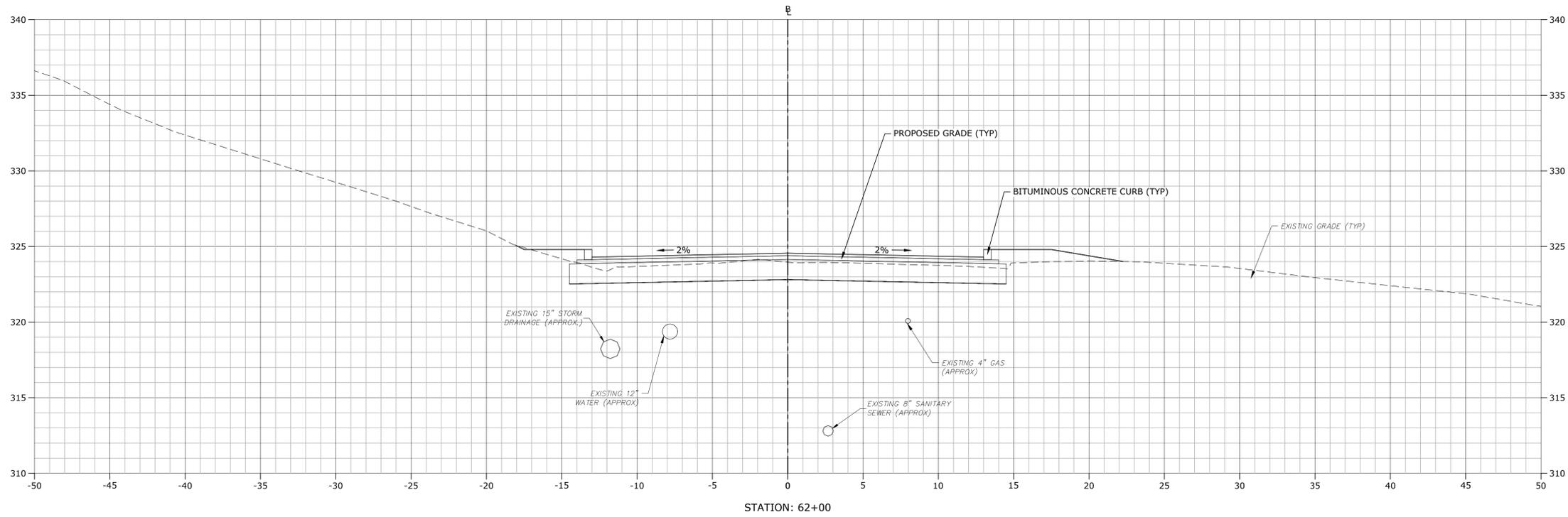
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

STROBEL ROAD CROSS SECTIONS
 STA: 59+50 TO 60+00

SCALE: 1" = 4'

C9.50

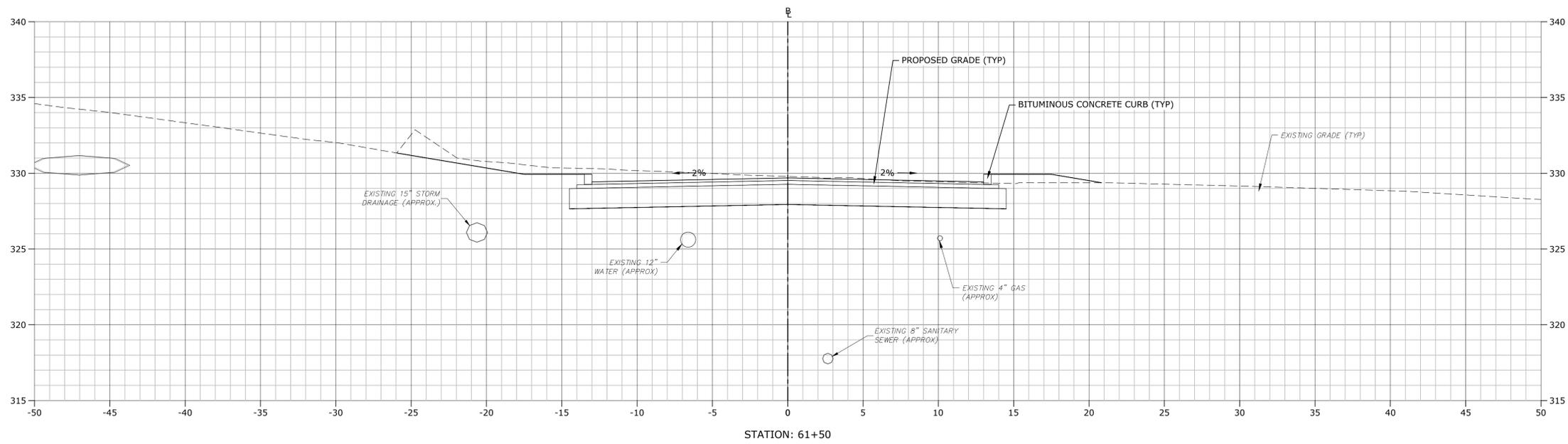


**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY



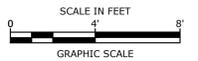
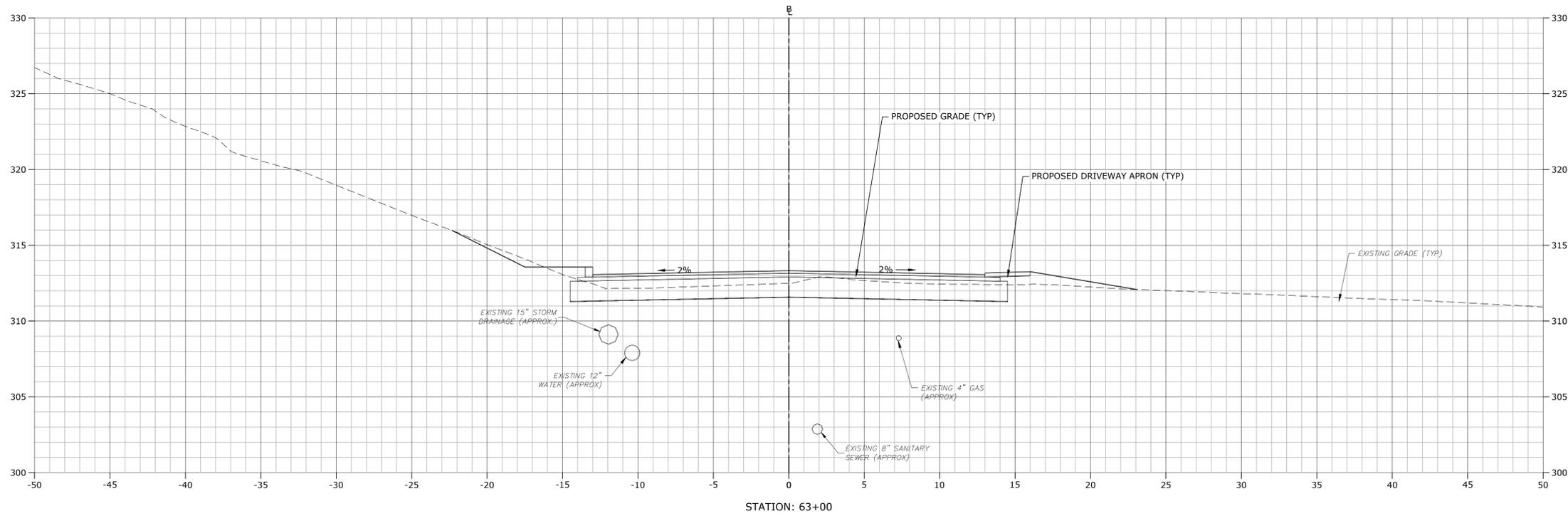
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 61+50 TO 62+00**

SCALE: 1" = 4'

C9.52

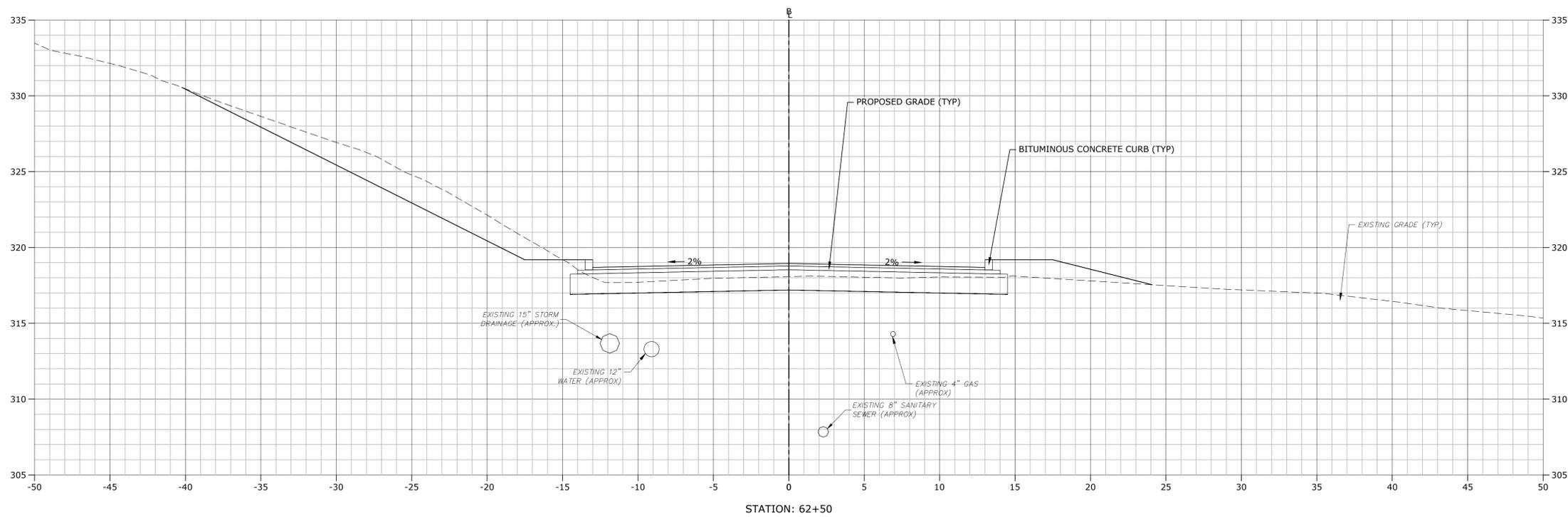


**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY



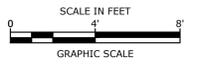
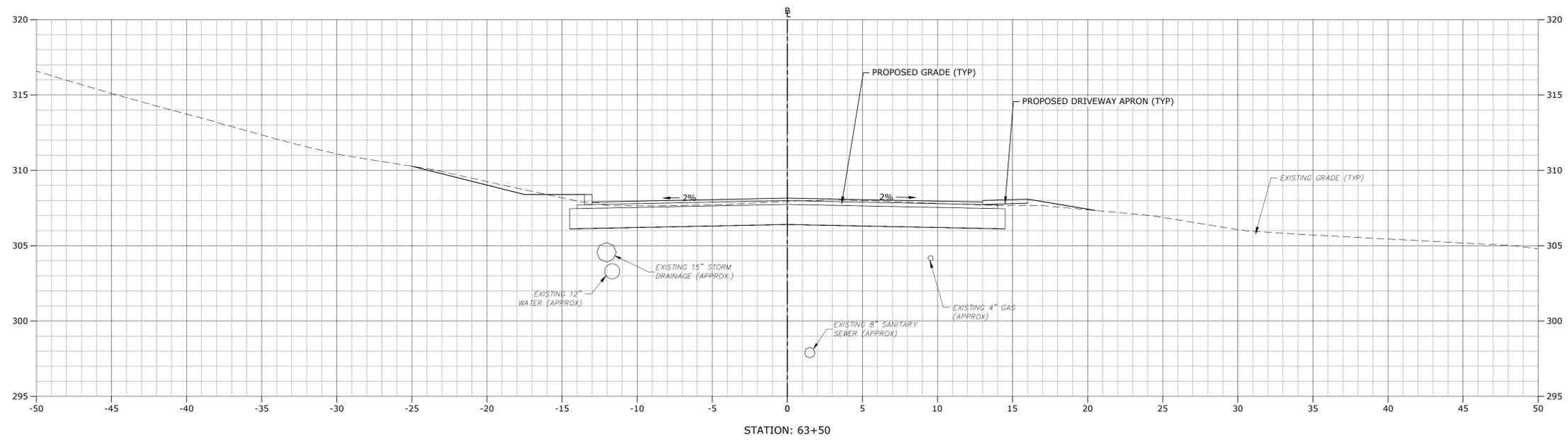
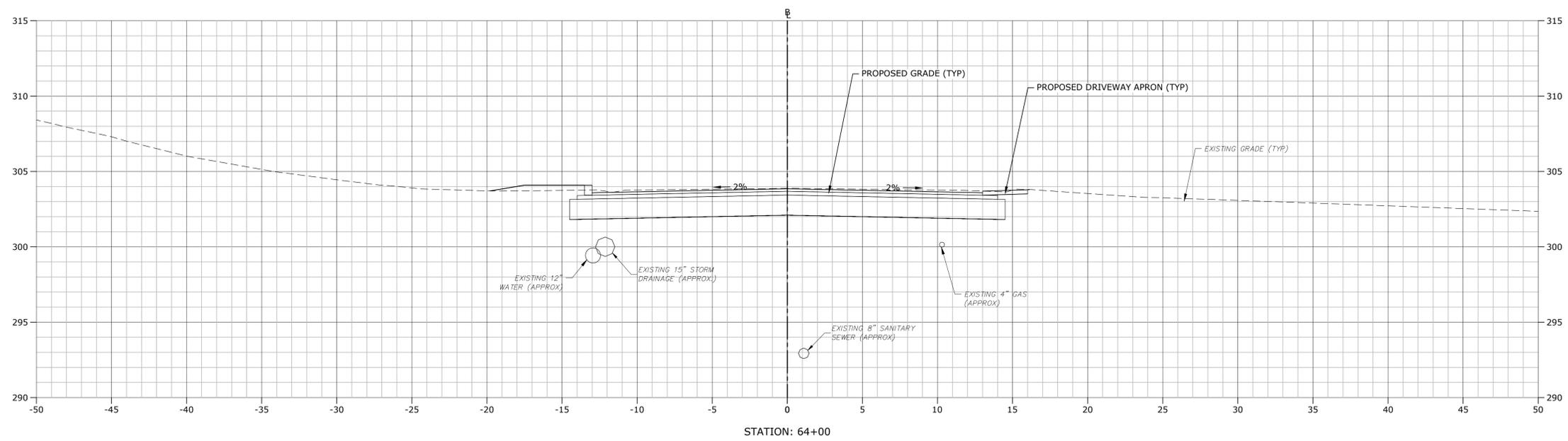
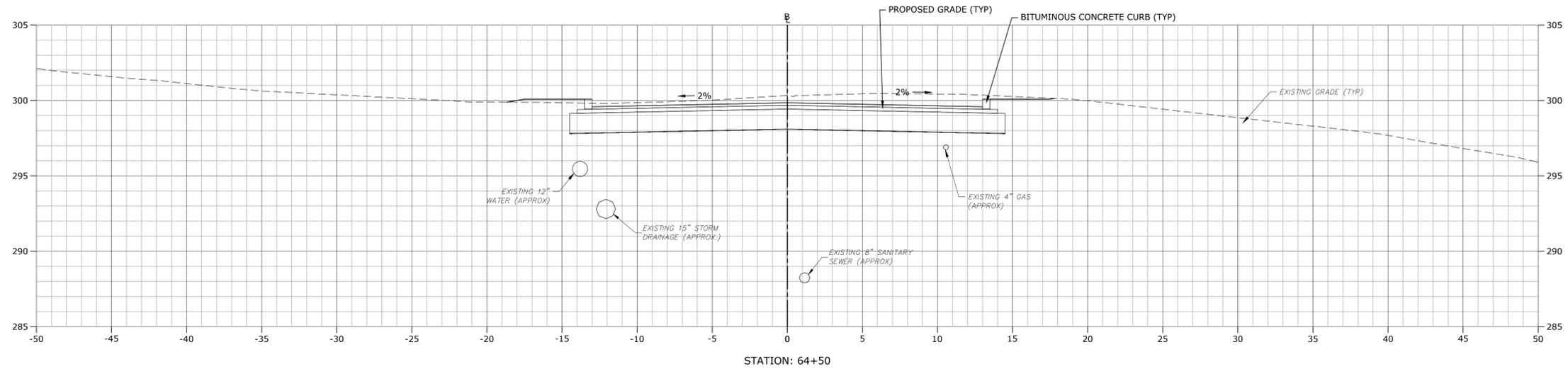
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 62+50 TO 63+00**

SCALE: 1" = 4'

C9.53



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

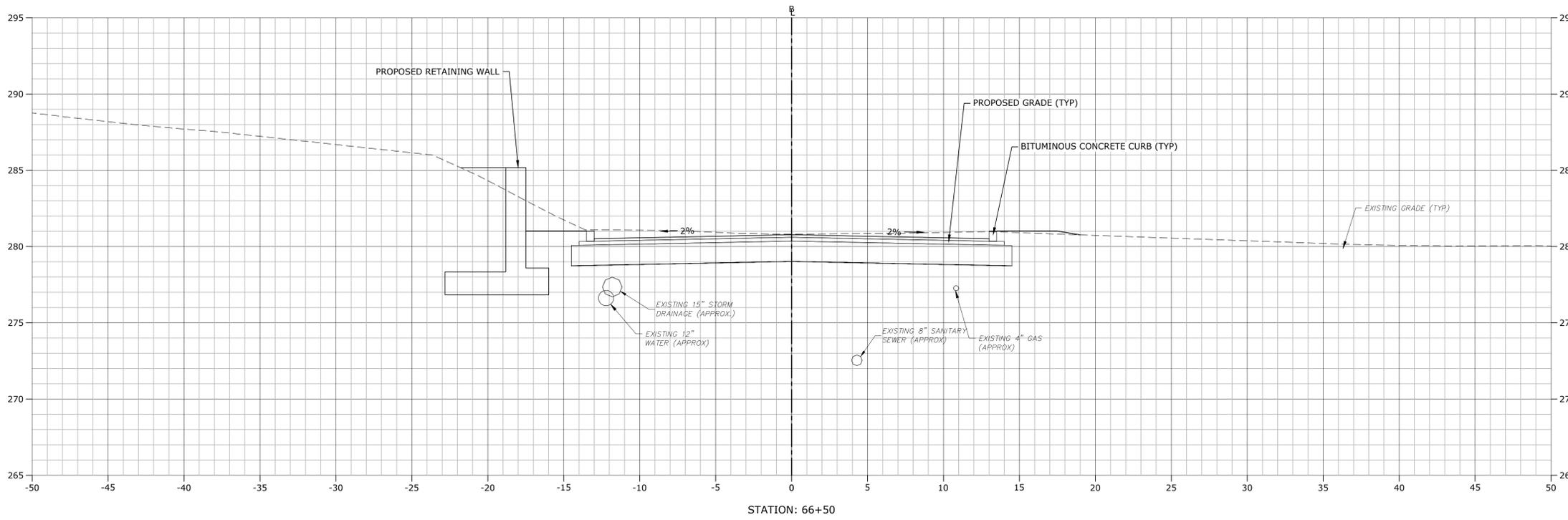
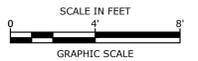
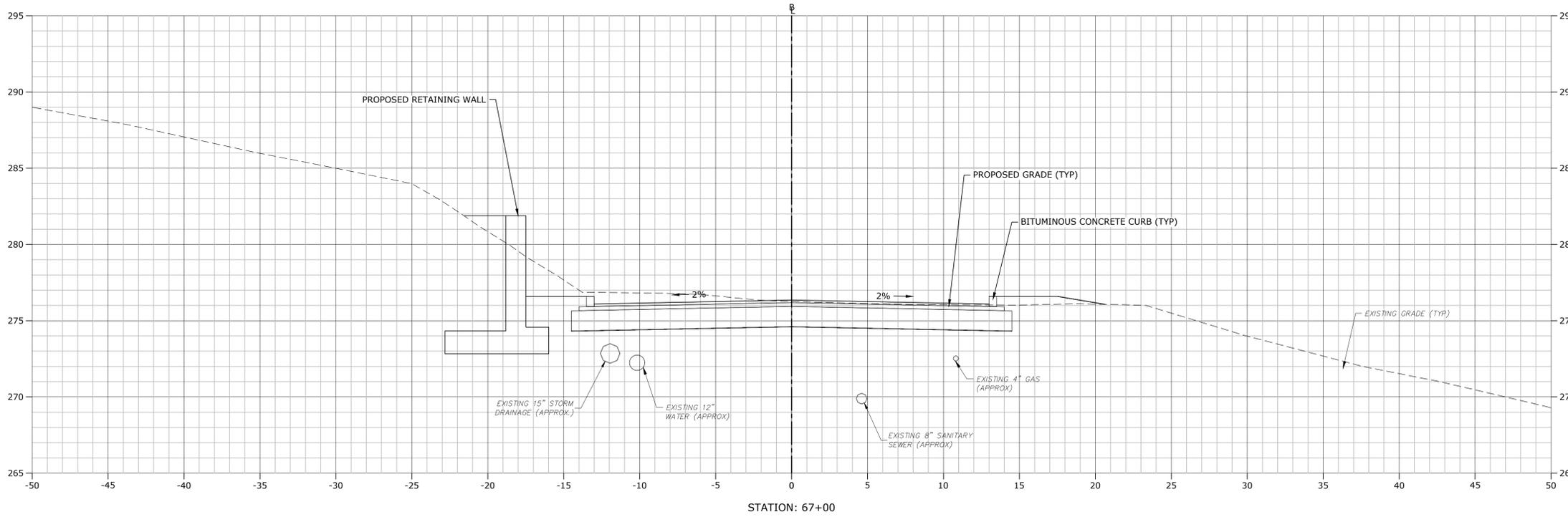
VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

**STROBEL ROAD
 CROSS SECTIONS
 STA: 63+50 TO 64+50**

SCALE: 1" = 4'

C9.54



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

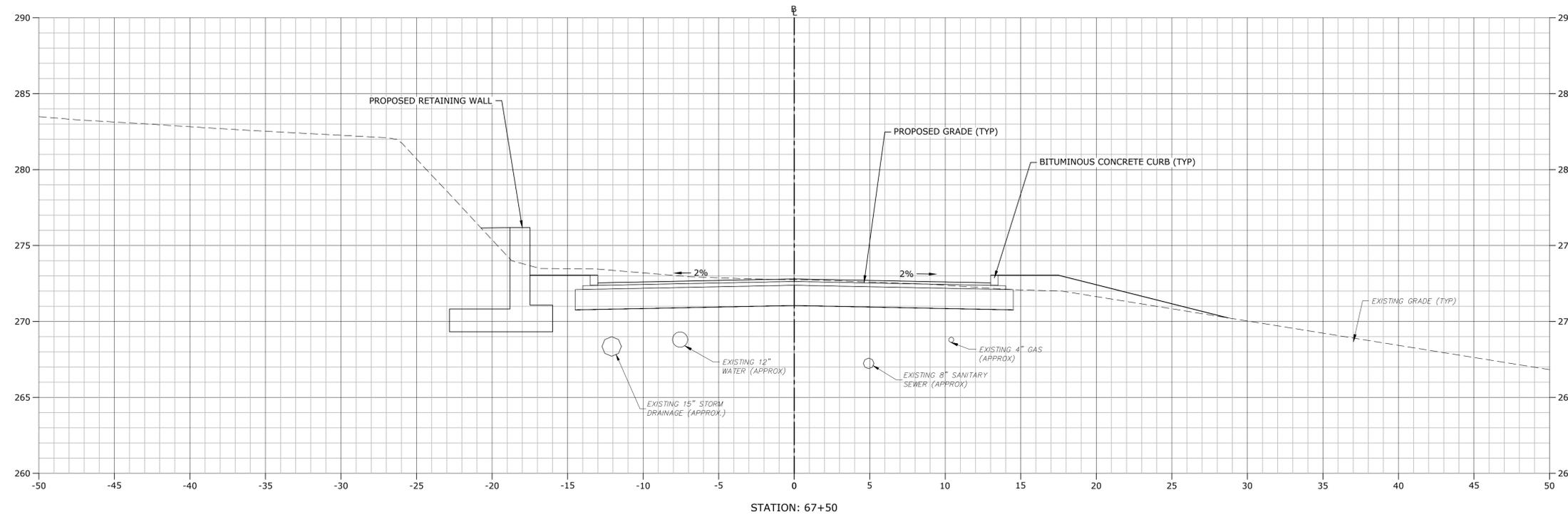
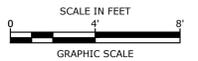
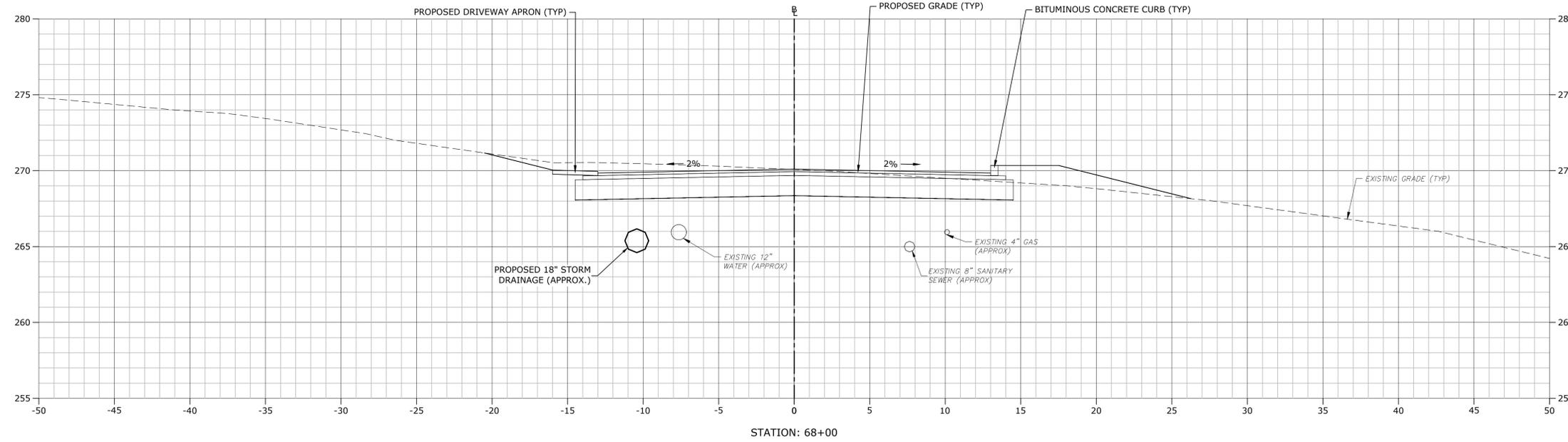
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 66+50 TO 67+00**

SCALE: 1" = 4'

C9.56



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

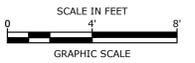
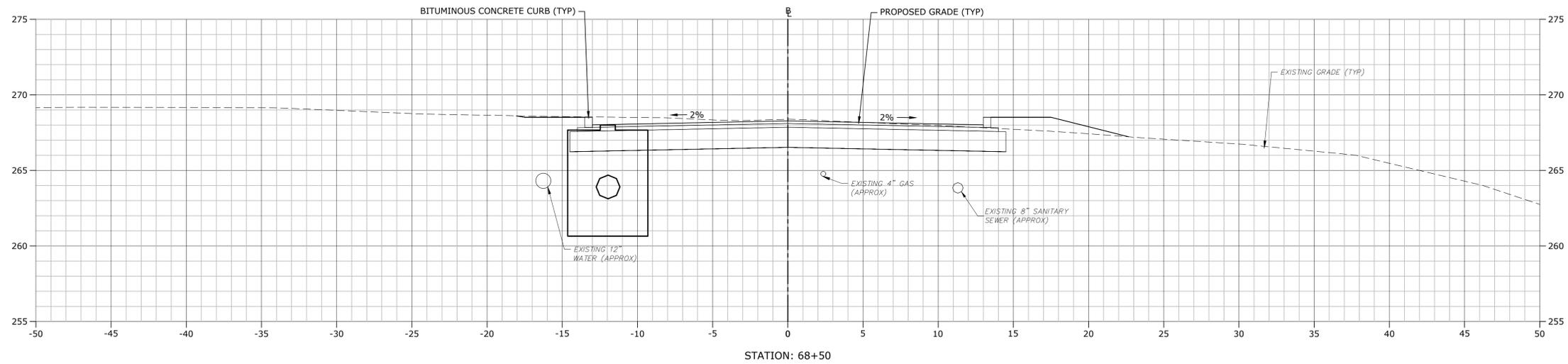
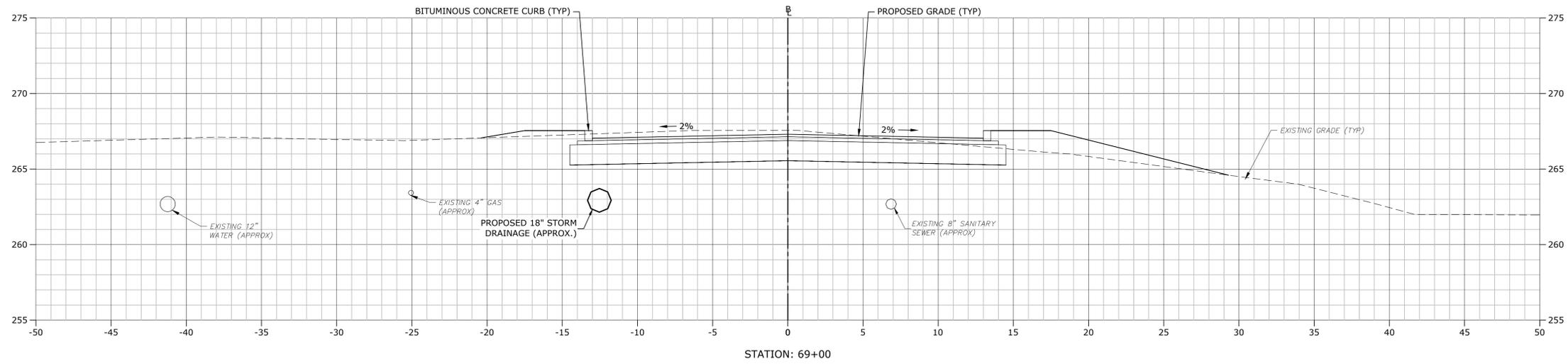
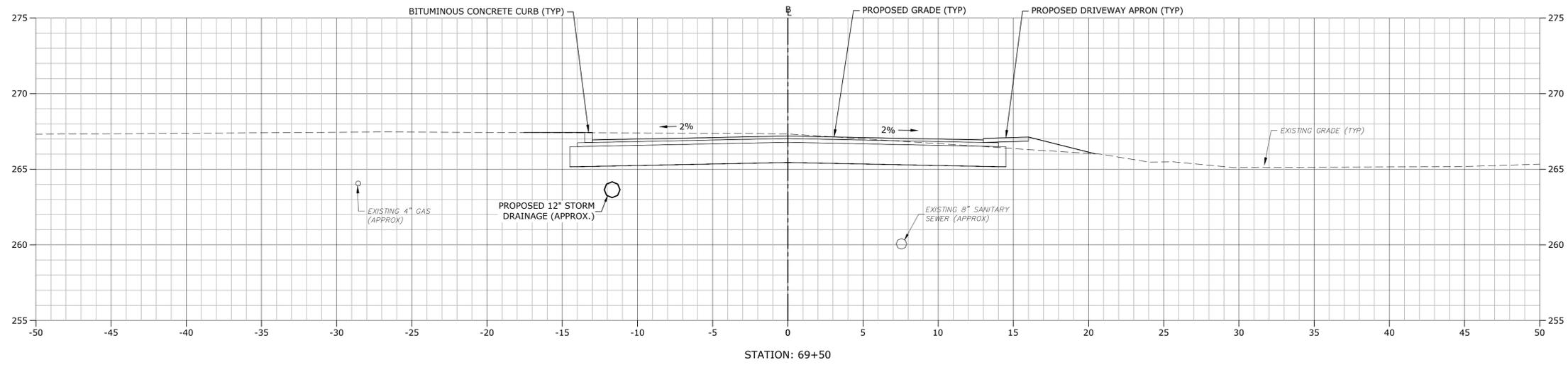
VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED BY: CRD
 APPROVED BY: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 67+50 TO 68+00**

SCALE: 1" = 4'
C9.57



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

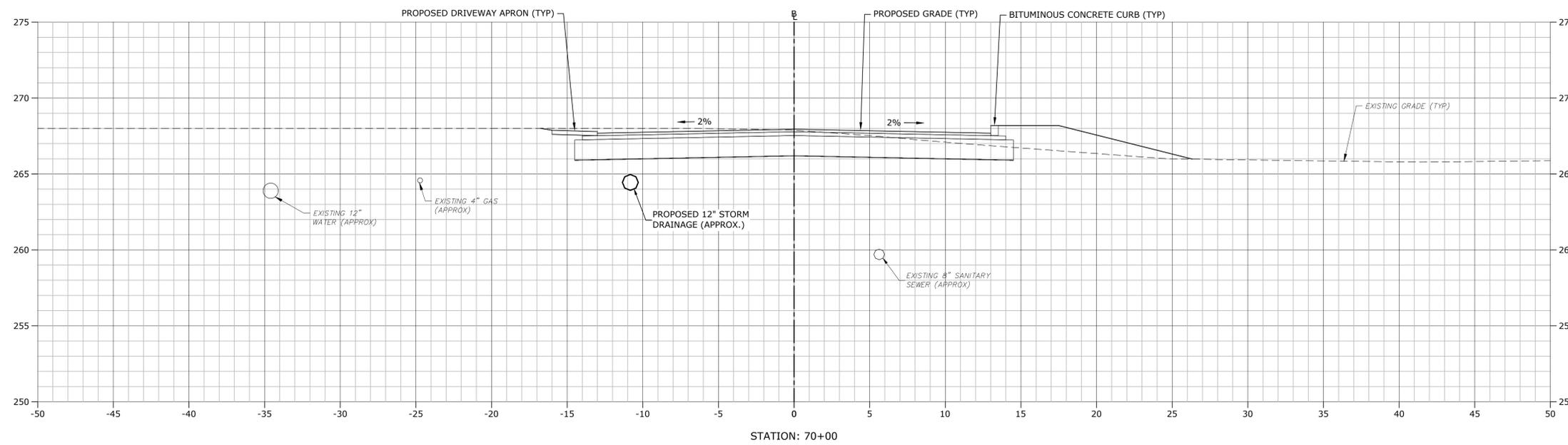
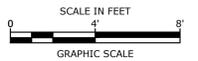
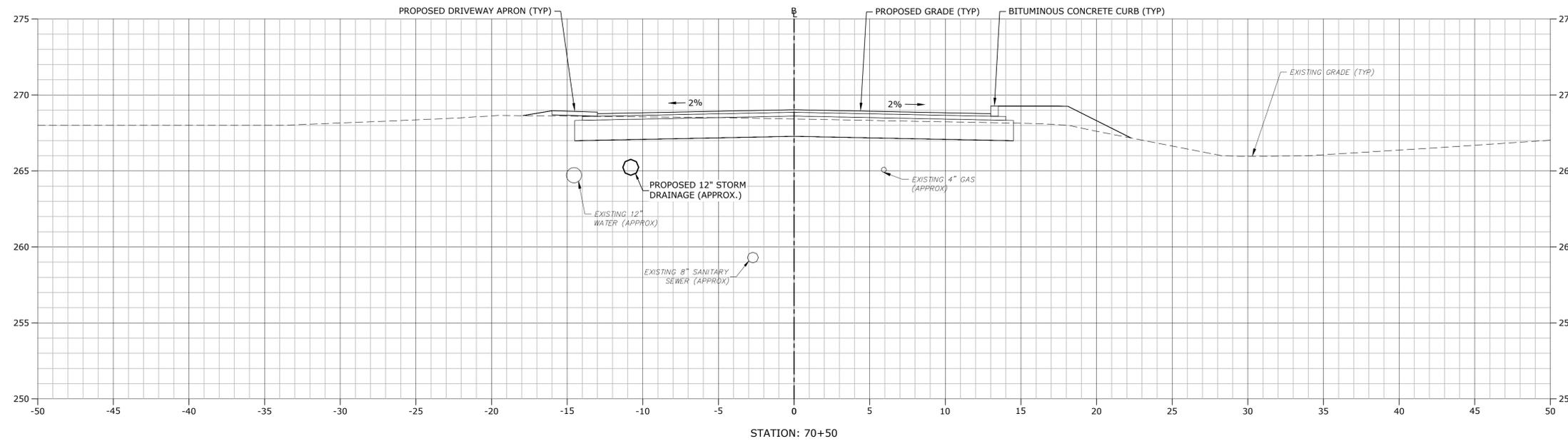
VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-15-C-900-XS.dwg	
DRAWN BY:	MDS	
CHECKED BY:	CRD	
APPROVED BY:	JWB	

**STROBEL ROAD
 CROSS SECTIONS
 STA: 68+50 TO 69+50**

SCALE: 1" = 4'

C9.58



VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

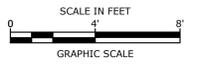
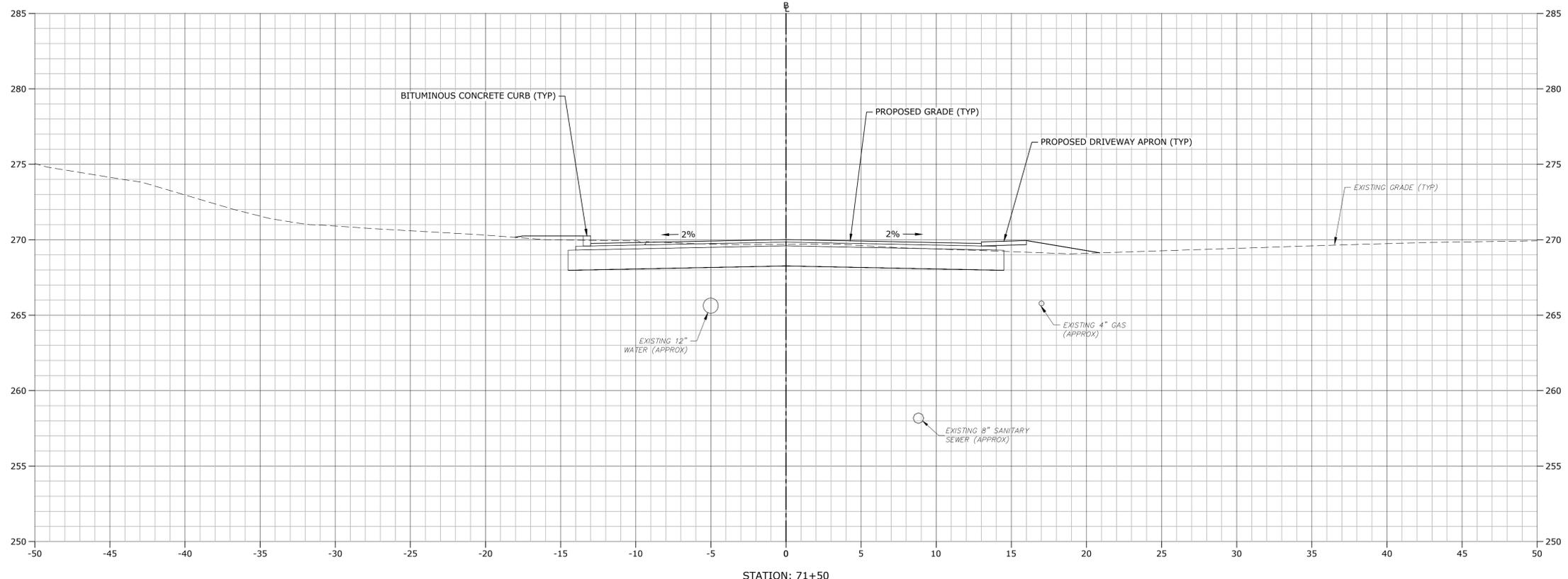
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

STROBEL ROAD CROSS SECTIONS
 STA: 70+00 TO 70+50

SCALE: 1" = 4'

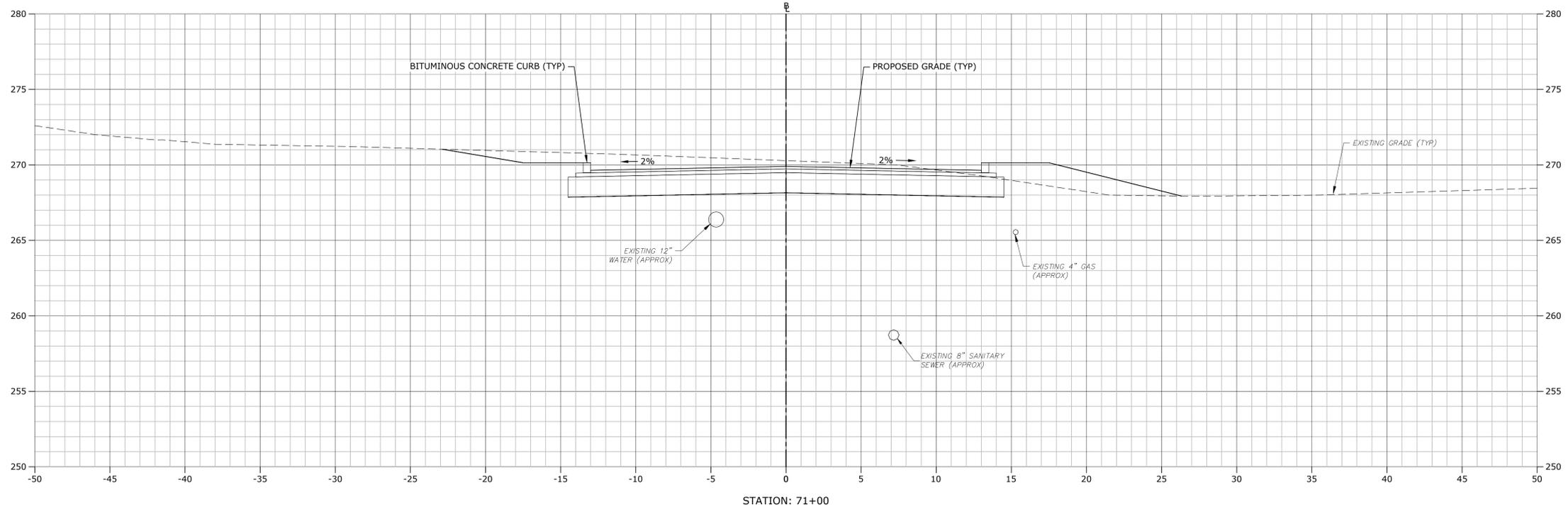
C9.59



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019



VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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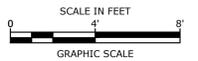
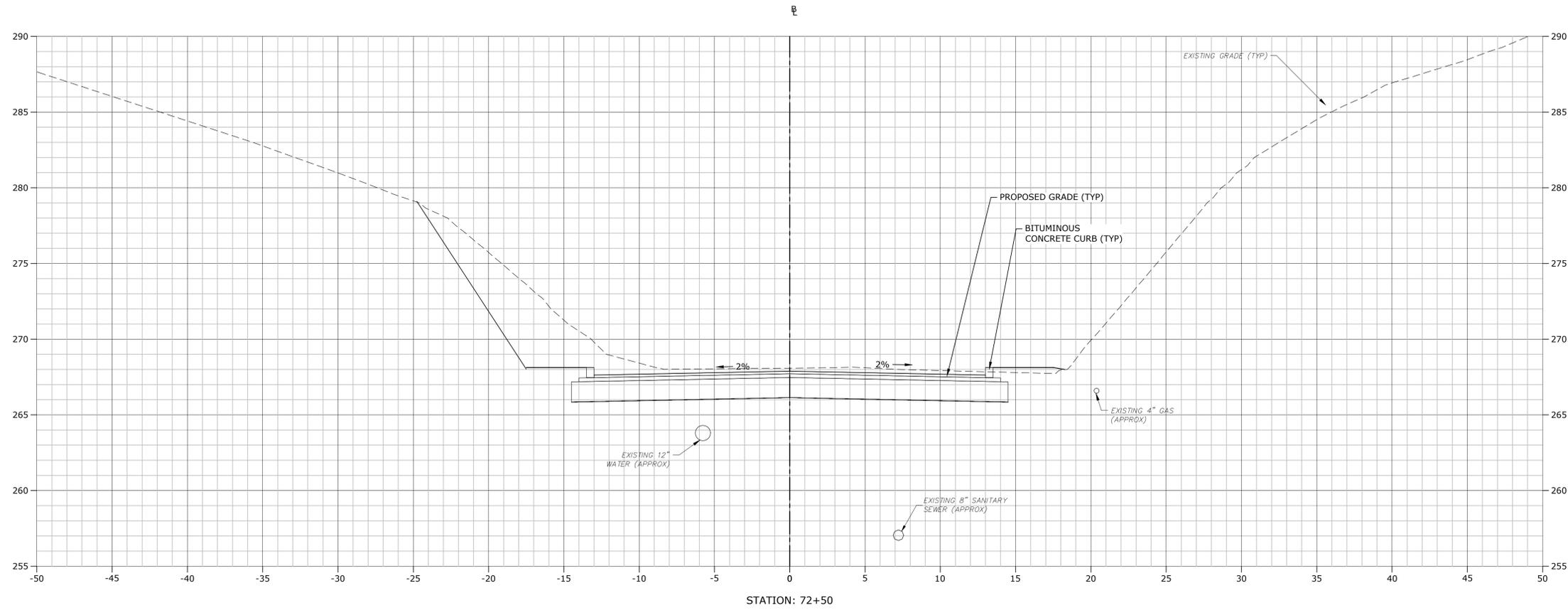
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 71+00 TO 71+50**

SCALE: 1" = 4'

C9.60



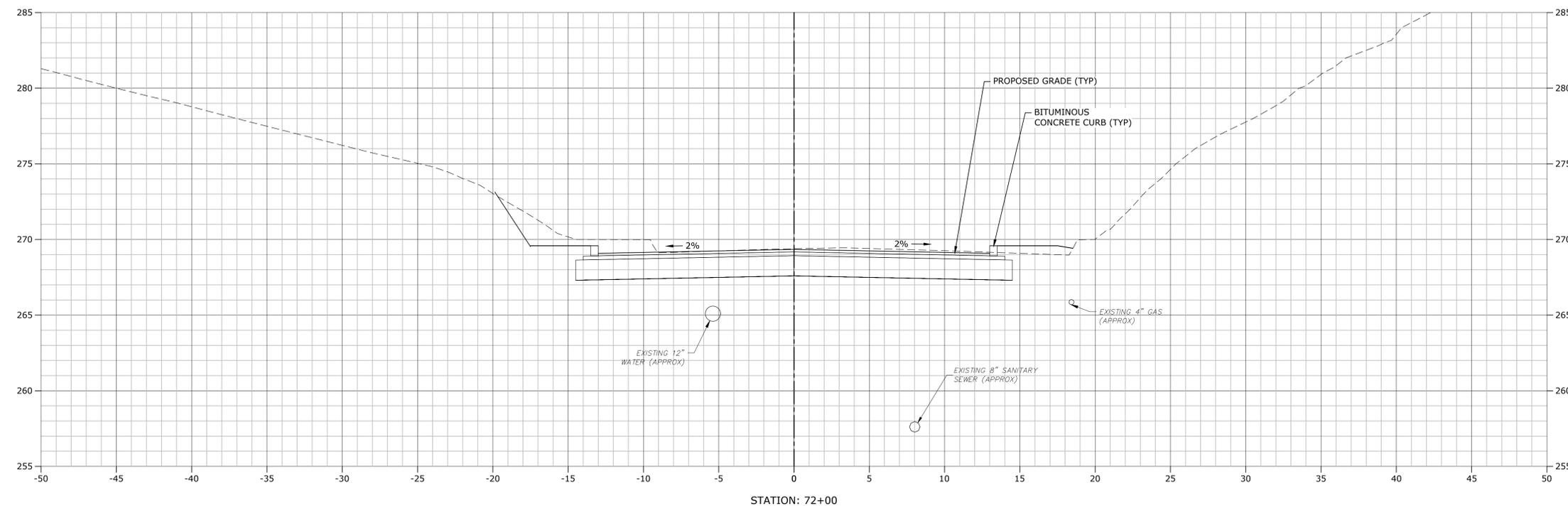
**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

BAR IS 1 INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

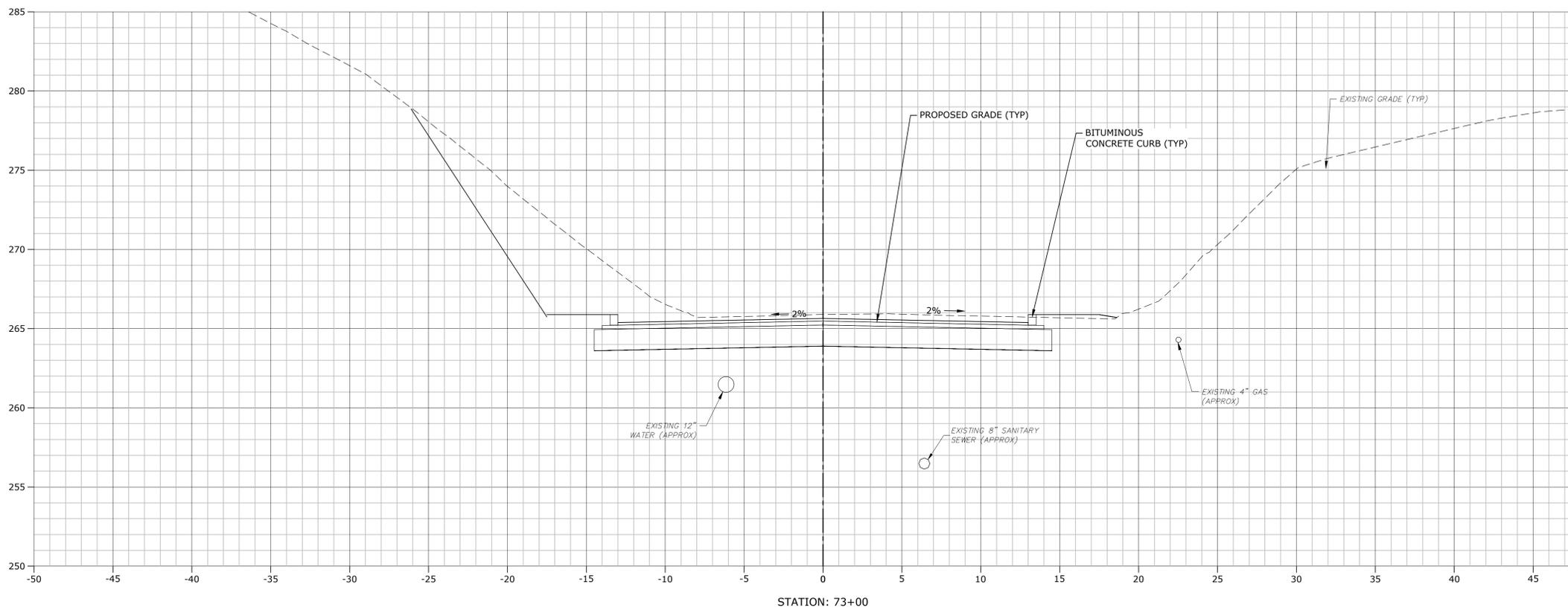
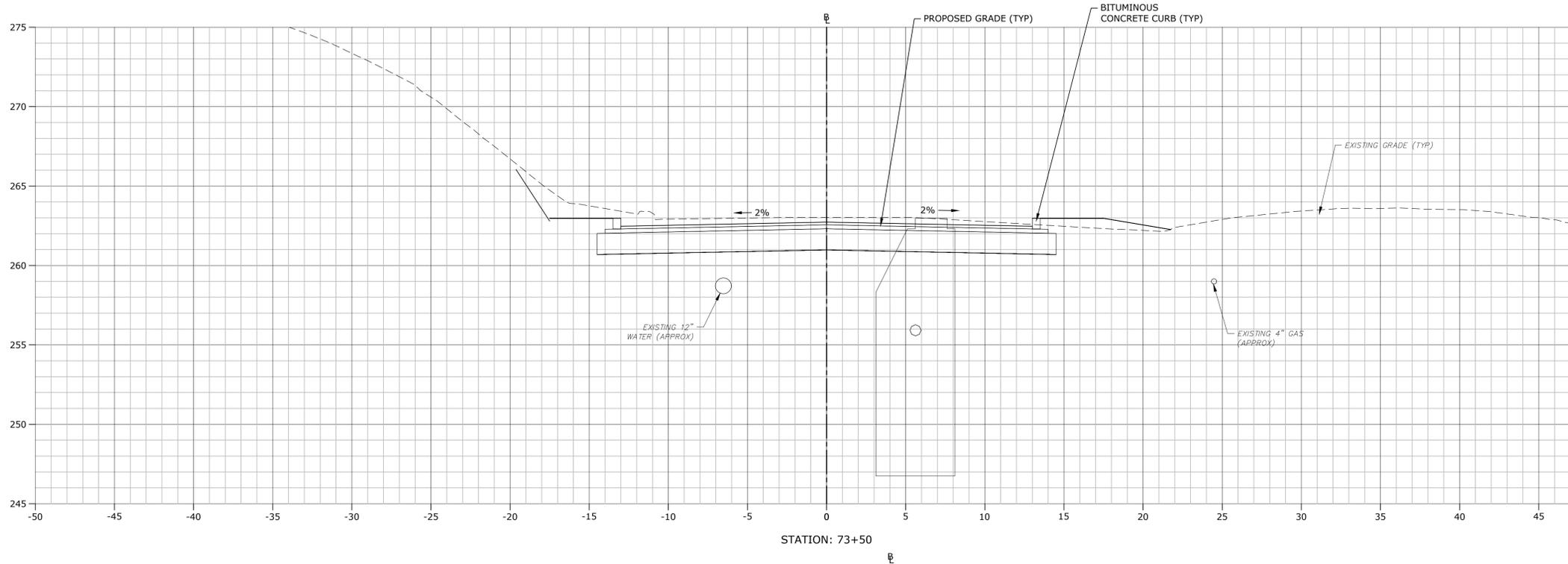


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PROJECT NO:	T0196	
FILE:	T0196-15-C-900-XS.dwg	
DRAWN BY:	MDS	
CHECKED:	CRD	
APPROVED:	JWB	

**STROBEL ROAD
 CROSS SECTIONS
 STA: 72+00 TO 72+50**

SCALE: 1" = 4'

C9.61



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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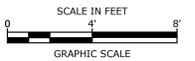
MARK	DATE	DESCRIPTION

PROJECT NO: T0196
 FILE: T0196-15-C-900-XS.dwg
 DRAWN BY: MDS
 CHECKED: CRD
 APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 73+00 TO 73+50**

SCALE: 1" = 4'

C9.62

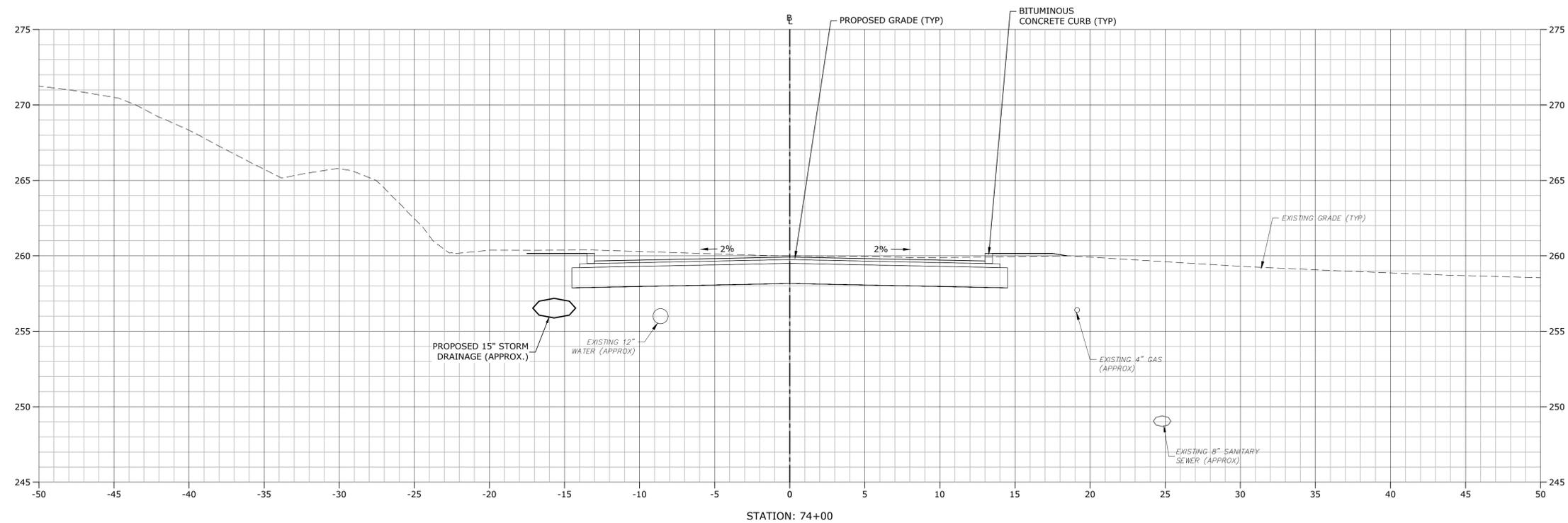


**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON
 ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON
 THIS SHEET, ADJUST
 SCALES ACCORDINGLY



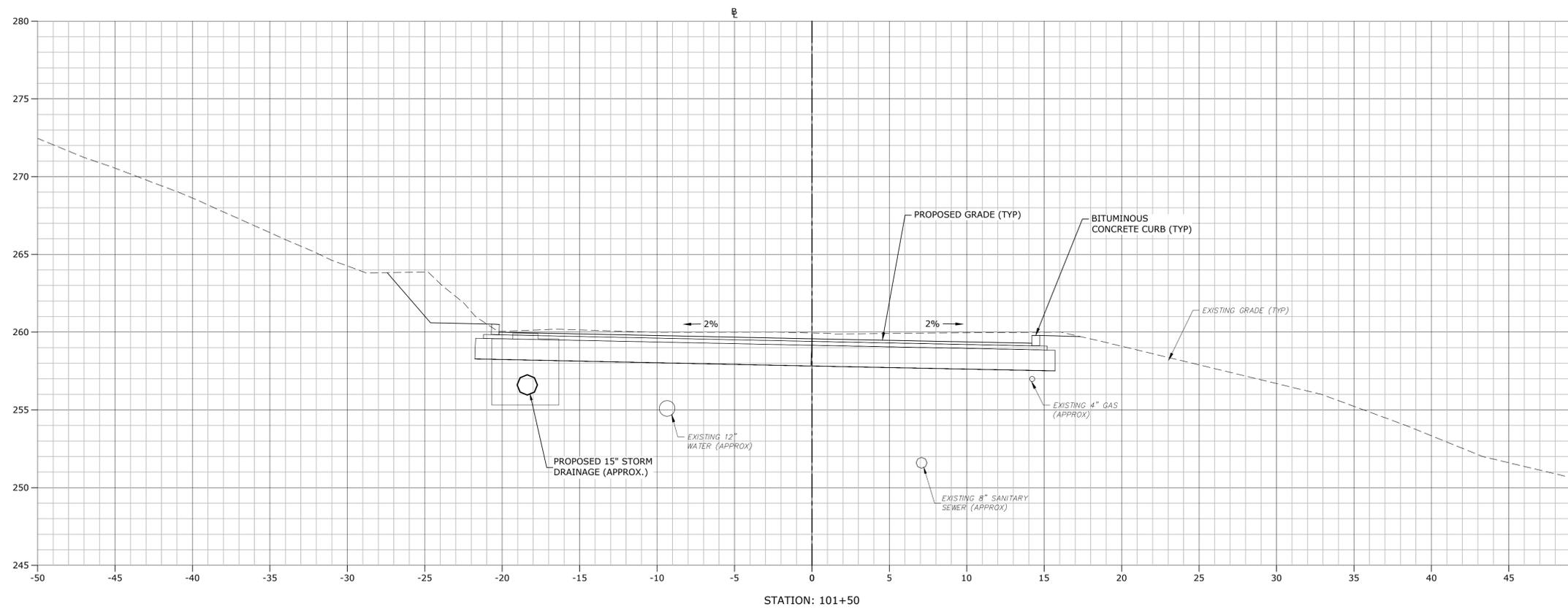
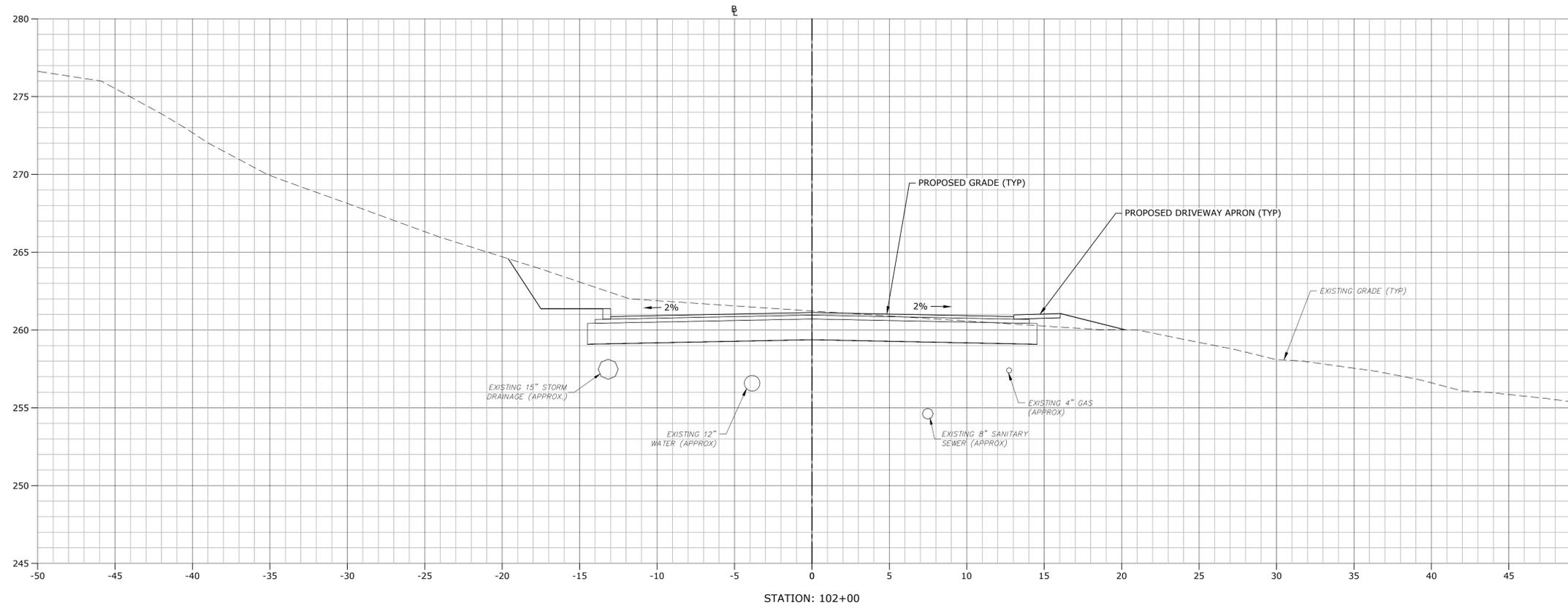
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PROJECT NO:	T0196	
FILE:	T0196-15-C-900-XS.dwg	
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CHECKED:	CRD	
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**STROBEL ROAD
 CROSS SECTIONS
 STA: 73+00 TO 73+50**

SCALE: 1" = 4'

C9.63



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION

PROJECT NO: T0196

FILE: T0196-15-C-900-XS.dwg

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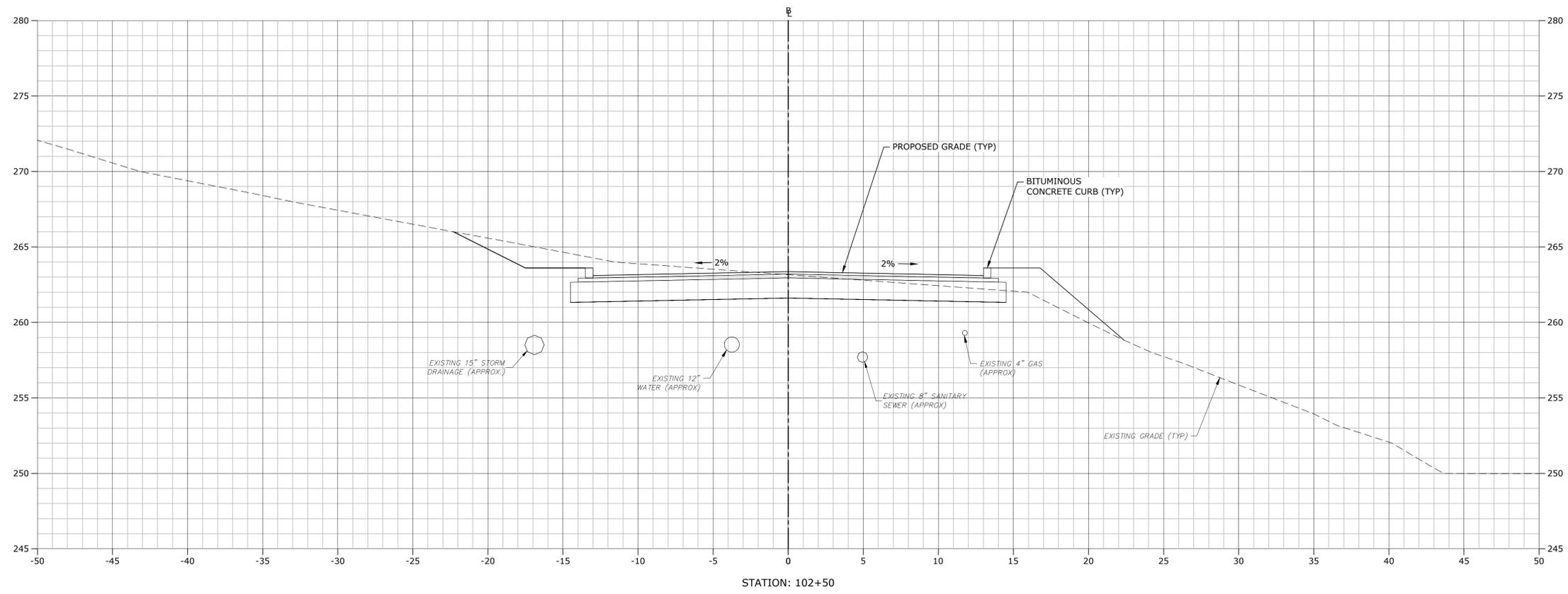
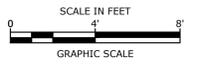
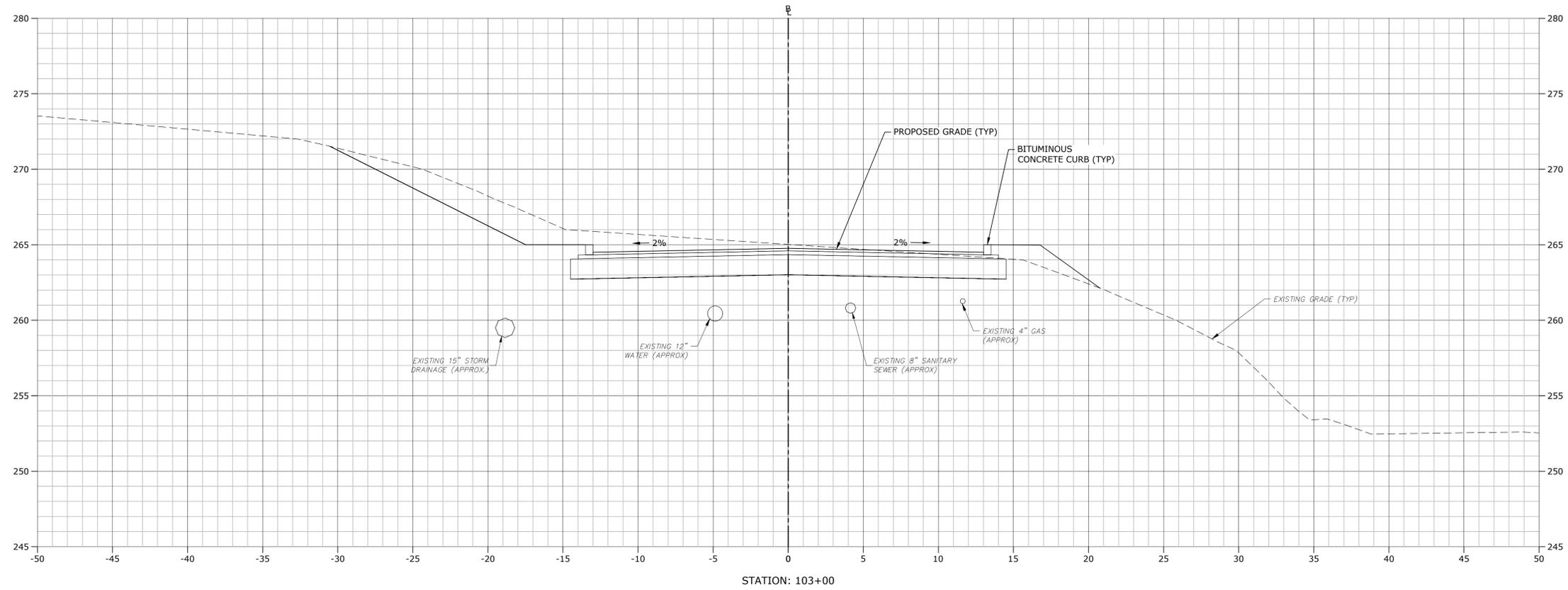
CHECKED: CRD

APPROVED: JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 101+50 TO 102+00**

SCALE: 1" = 4'

C9.64



**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
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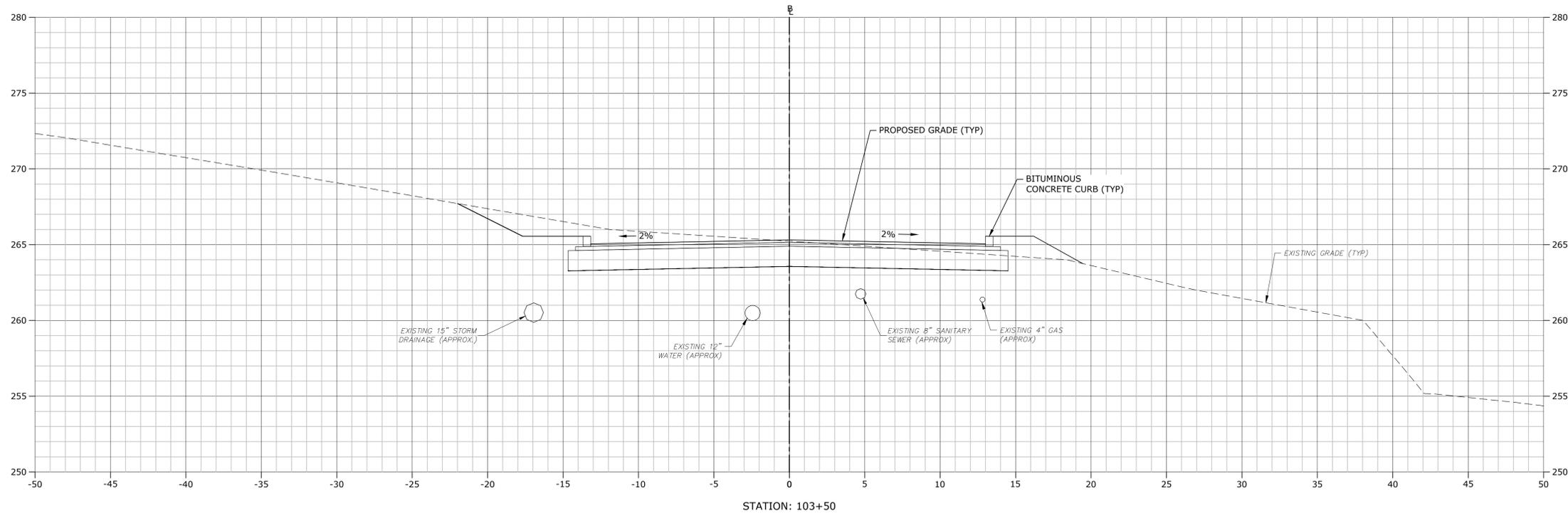
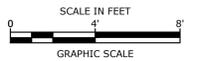
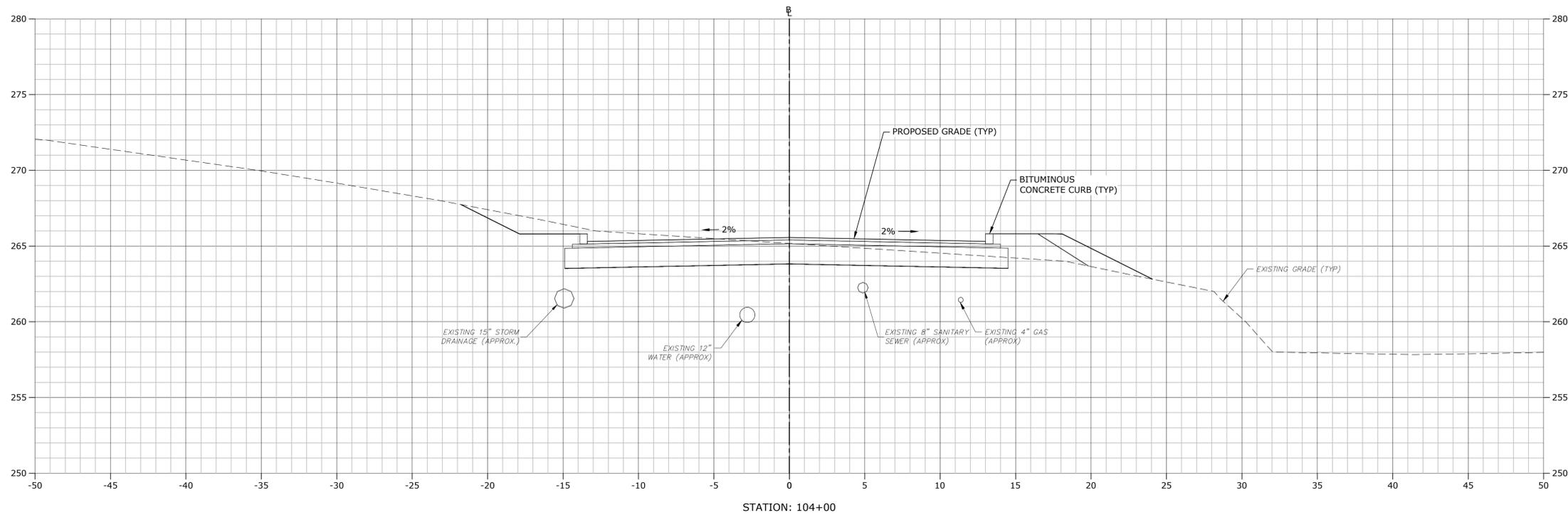
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 102+50 TO 103+00**

SCALE: 1" = 4'

C9.65



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

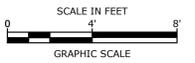
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FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

STROBEL ROAD CROSS SECTIONS
 STA: 103+50 TO 104+00

SCALE: 1" = 4'

C9.66



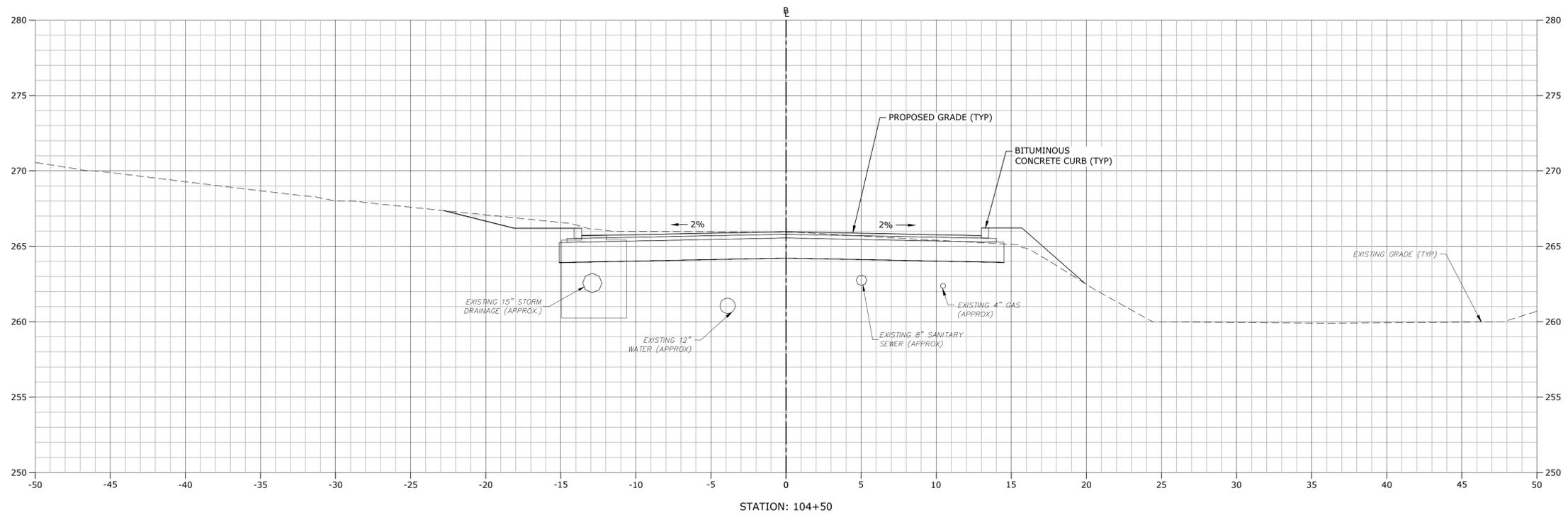
**STROBEL ROAD
 RECONSTRUCTION
 PROJECT**

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY



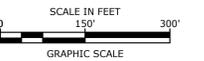
MARK	DATE	DESCRIPTION

PROJECT NO:	T0196
FILE:	T0196-15-C-900-XS.dwg
DRAWN BY:	MDS
CHECKED:	CRD
APPROVED:	JWB

**STROBEL ROAD
 CROSS SECTIONS
 STA: 104+50**

SCALE: 1" = 4'

C9.67



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

VERIFY SCALE

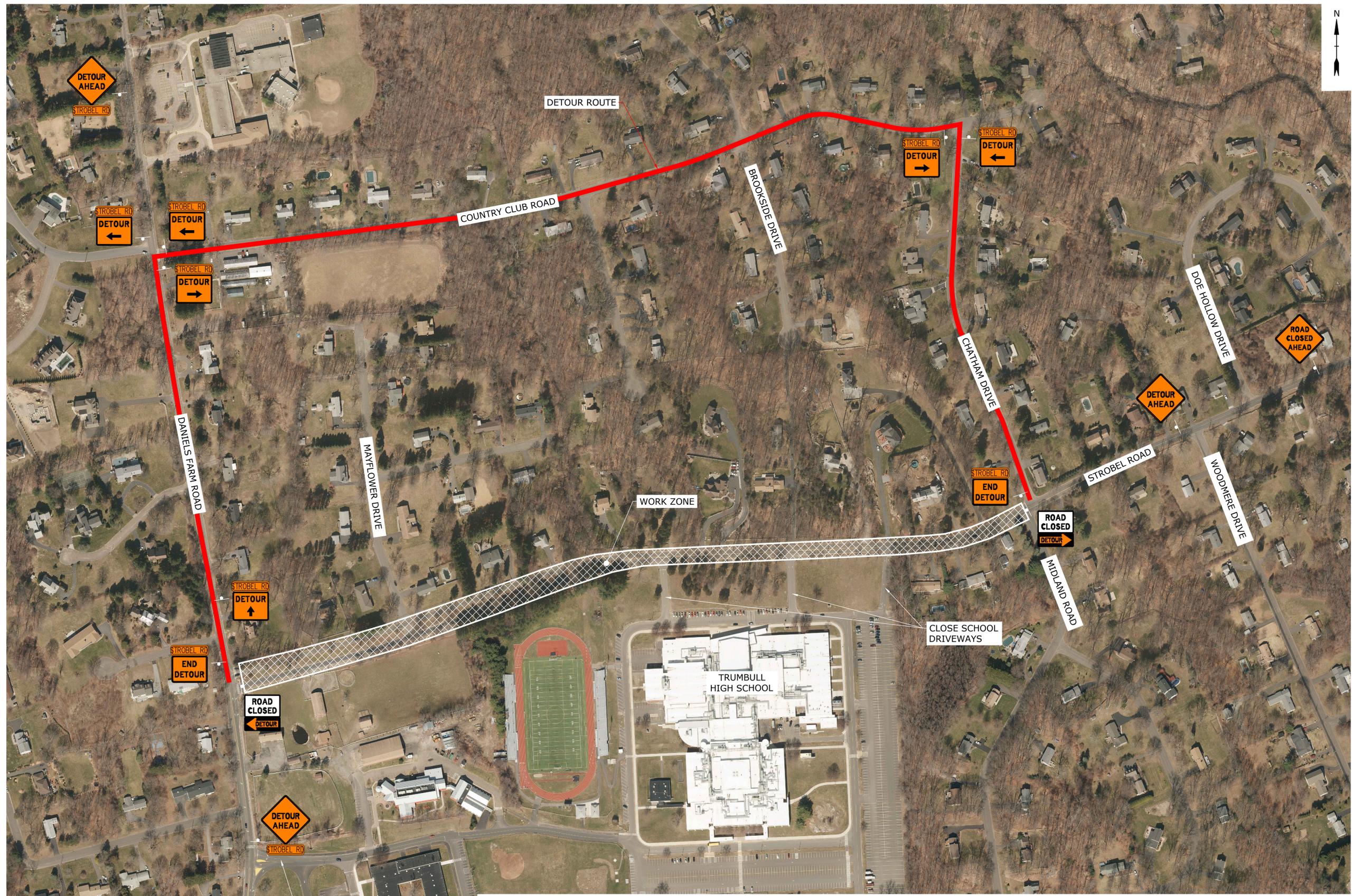
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IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-015-TTC.dwg	
DRAWN BY:	ALW	
CHECKED:	AJM	
APPROVED:	AJM	

STROBEL ROAD PHASE 1 DETOUR PLAN

SCALE: 1" = 150'

TTC-001

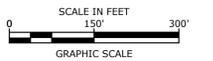


CT DOT SIGN LEGEND

80-9077	80-9080	80-9701	80-9702	80-9708	80-9710	80-9805	80-9810	80-9919

OVERALL TRAFFIC MANAGEMENT NOTES

1. PROVIDE LOCAL ACCESS ONLY THROUGH WORK ZONE AFTER SUBGRADE IS COMPACTED.
2. WORK ZONE IS TO BE REOPENED TO TRAFFIC AT THE END OF EACH DAY FOLLOWING SUBGRADE COMPACTION.
3. COORDINATE THE WORK WITH ADJACENT PROPERTY OWNERS. MAINTAIN ACCESS TO DRIVEWAYS WITHIN THE WORK ZONE AT ALL TIMES. MAKE ACCOMMODATIONS FOR DRIVEWAY ACCESS WITH PROPERTY OWNERS IN ADVANCE OF ROAD CLOSURE.
4. PROVIDE TYPE III BARRICADES AT ALL ROAD CLOSURE POINTS.



STROBEL ROAD RECONSTRUCTION PROJECT

Trumbull, Connecticut

January 18, 2019

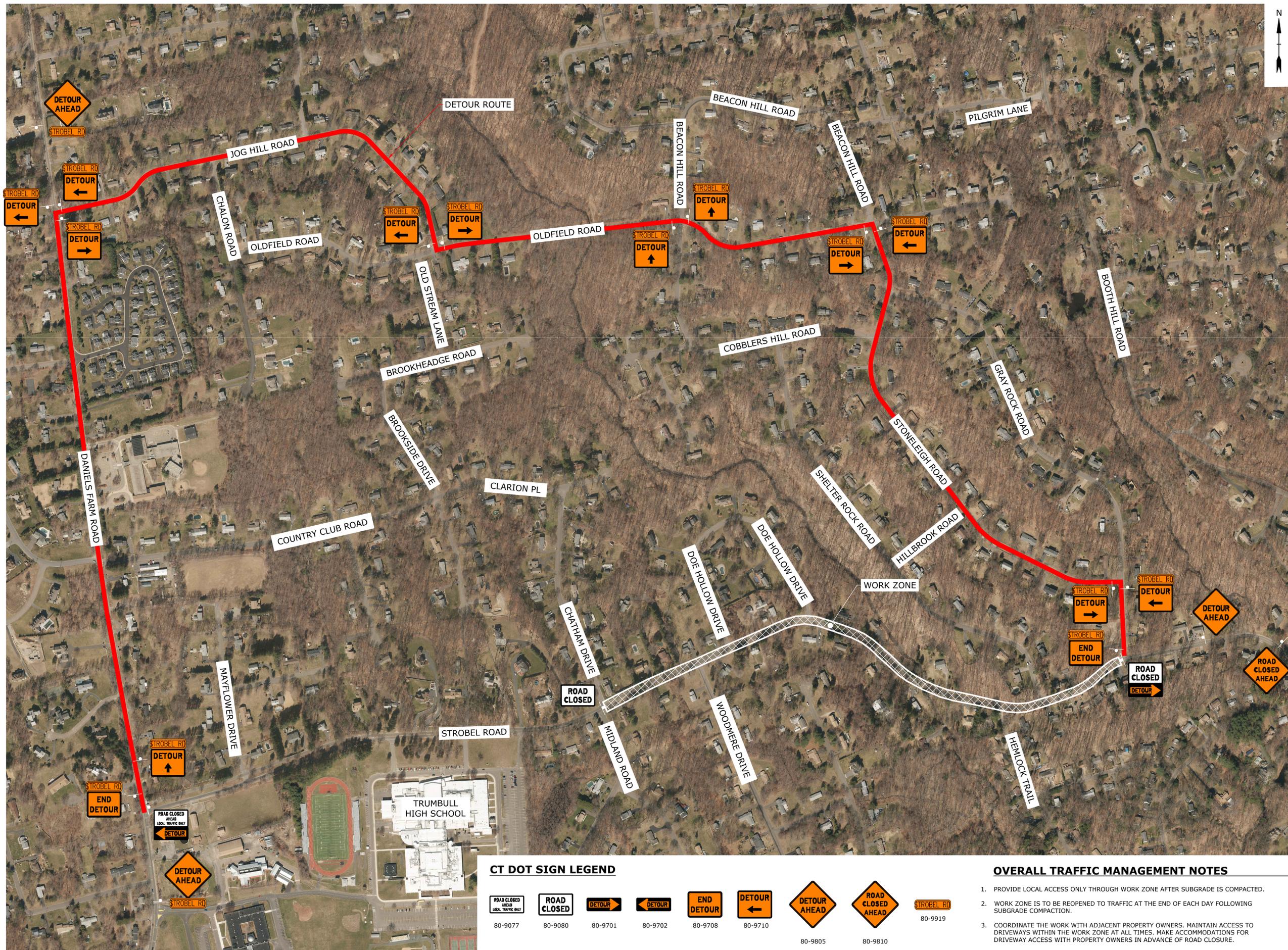
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 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-015-TTC.dwg	
DRAWN BY:	ALW	
CHECKED:	AJM	
APPROVED:	AJM	

STROBEL ROAD PHASE 2 DETOUR PLAN

SCALE: 1" = 250'

TTC-002

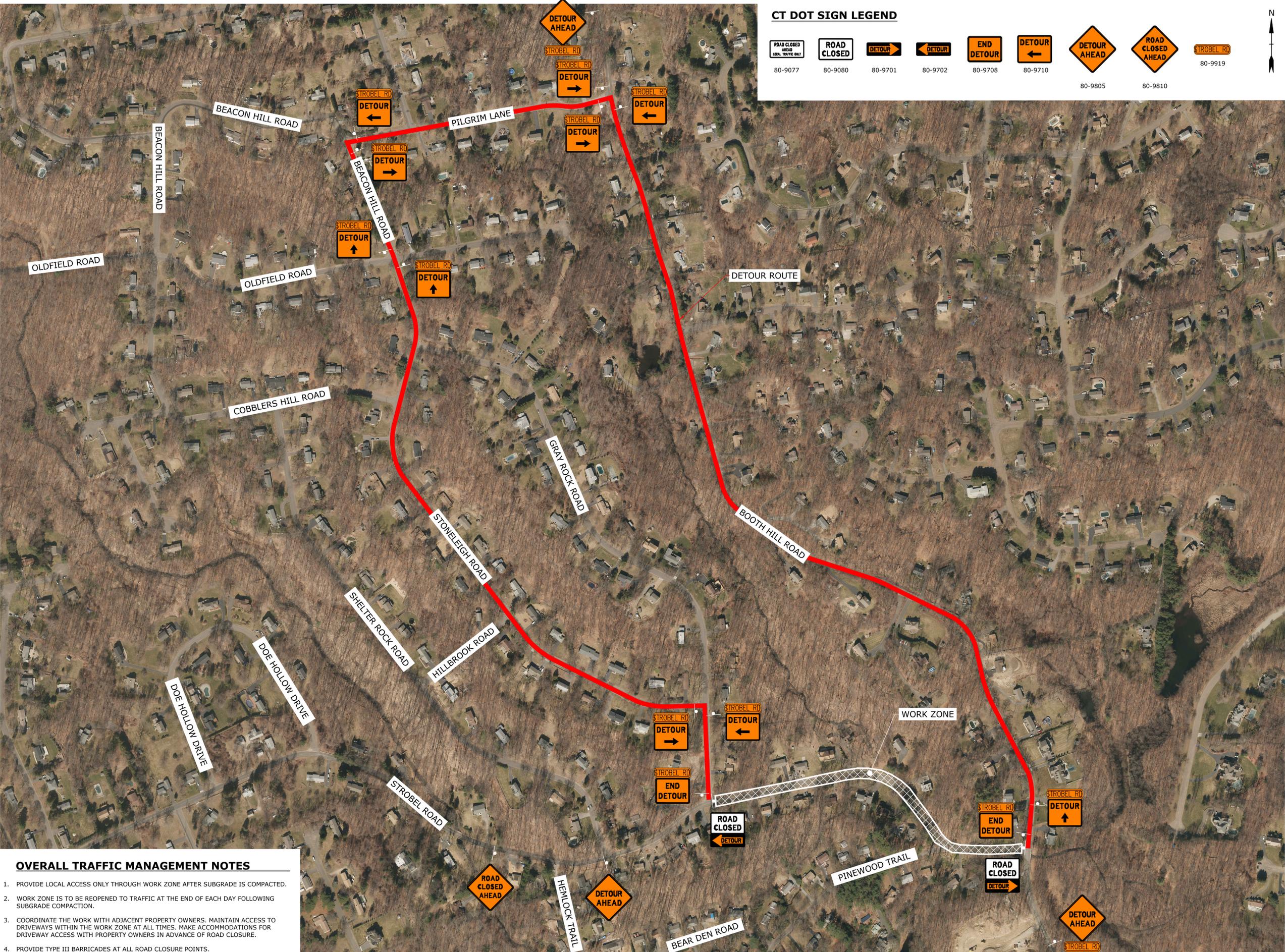


CT DOT SIGN LEGEND



OVERALL TRAFFIC MANAGEMENT NOTES

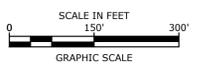
1. PROVIDE LOCAL ACCESS ONLY THROUGH WORK ZONE AFTER SUBGRADE IS COMPACTED.
2. WORK ZONE IS TO BE REOPENED TO TRAFFIC AT THE END OF EACH DAY FOLLOWING SUBGRADE COMPACTATION.
3. COORDINATE THE WORK WITH ADJACENT PROPERTY OWNERS. MAINTAIN ACCESS TO DRIVEWAYS WITHIN THE WORK ZONE AT ALL TIMES. MAKE ACCOMMODATIONS FOR DRIVEWAY ACCESS WITH PROPERTY OWNERS IN ADVANCE OF ROAD CLOSURE.
4. PROVIDE TYPE III BARRICADES AT ALL ROAD CLOSURE POINTS.



CT DOT SIGN LEGEND

80-9077	80-9080	80-9701	80-9702	80-9708	80-9710	80-9805	80-9810	80-9919

Tighe & Bond
 www.tighebond.com
 1000 Bridgeport Avenue
 Suite 320
 Shelton, CT 06484
 (203) 712-1100



STROBEL ROAD RECONSTRUCTION PROJECT
 Trumbull, Connecticut
 January 18, 2019

VERIFY SCALE
 BAR IS 1 INCH ON ORIGINAL DRAWING
 0 1 INCH
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY

- OVERALL TRAFFIC MANAGEMENT NOTES**
1. PROVIDE LOCAL ACCESS ONLY THROUGH WORK ZONE AFTER SUBGRADE IS COMPACTED.
 2. WORK ZONE IS TO BE REOPENED TO TRAFFIC AT THE END OF EACH DAY FOLLOWING SUBGRADE COMPACTION.
 3. COORDINATE THE WORK WITH ADJACENT PROPERTY OWNERS. MAINTAIN ACCESS TO DRIVEWAYS WITHIN THE WORK ZONE AT ALL TIMES. MAKE ACCOMMODATIONS FOR DRIVEWAY ACCESS WITH PROPERTY OWNERS IN ADVANCE OF ROAD CLOSURE.
 4. PROVIDE TYPE III BARRICADES AT ALL ROAD CLOSURE POINTS.

MARK	DATE	DESCRIPTION
PROJECT NO:	T0196	
FILE:	T0196-015-TTC.dwg	
DRAWN BY:	ALW	
CHECKED:	AJM	
APPROVED:	AJM	

STROBEL ROAD PHASE 3 DETOUR PLAN

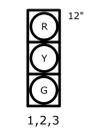
SCALE: 1" = 200'
TTC-003

Last Saved: 4/18/2019 3:30pm By: Dubouque, T
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		MOVEMENT DIAGRAM																																			
NTOR		PHASE 1				PHASE 2				PHASE 3				PHASE 4				PHASE 5				PHASE 6				PHASE 7				PHASE 8							
F A C E #	1	FLASH	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL								
	2	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R								
	3	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R								
MIN.		7	3.0	8.0	7	3.0	8.0	5	3.0	1.0																											
MAX.		30	5.0	12.0	30	5.0	12.0	5	5.0	3.0																											
I N T E R V A L S	MIN GRN	9				9				5																											
	WALK																																				
	PED CLR																																				
	VEH EXT																																				
	MAX 1	15				15				5																											
	MAX 2	10				10				5																											
	YELLOW	4.4				4.9				3.0																											
	RED	10.0				10.0				1.0																											
	ADD INIT																																				
	MAX INIT																																				
TBR																																					
TTR																																					
MIN GAP																																					
MODE	MAX RECALL				MAX RECALL				NON-LOCK				OFF				OFF				OFF				OFF												
INIT START	SEE TECH NOTES				SEE TECH NOTES																																

TECHNICAL NOTES		OFFICE RECORD	
STANDARD OVERLAP SKIP FEATURES APPLY		REV #	TIR # 144-1804-01 SM # N/A SIGNAL REVISED:
BOTTOM OF SIGNAL HOUSING TO BE 17" ABOVE ROADWAY.		TEMPORARY SIGNALIZATION.	
DETECTOR D3 ON TO CALL PHASE 3.			
INITIAL START TO BE PHASE 1 FOR WORK ZONE PHASES 1 AND 3.			
INITIAL START TO BE PHASE 2 FOR WORK ZONE PHASE 2.			

SIGNAL FACES

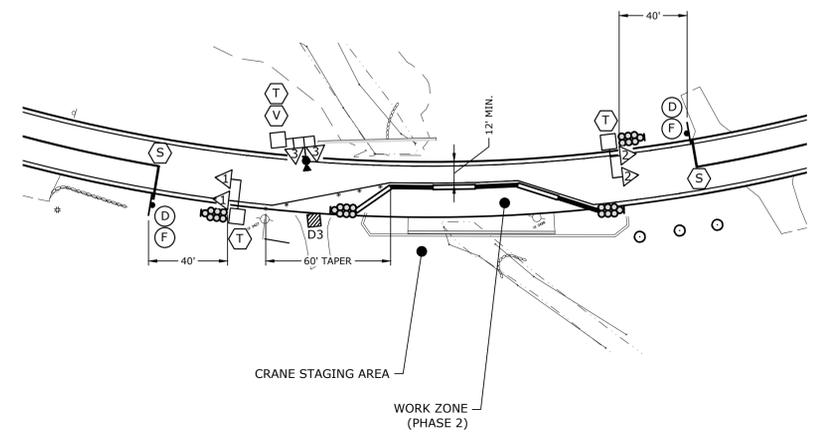


1,2,3
ALL INDICATIONS HAVE LED LAMPS.
ALL FACES HAVE BACKPLATES WITH 2" WIDE YELLOW RETROREFLECTIVE STRIP.

CONSTRUCTION NOTES:

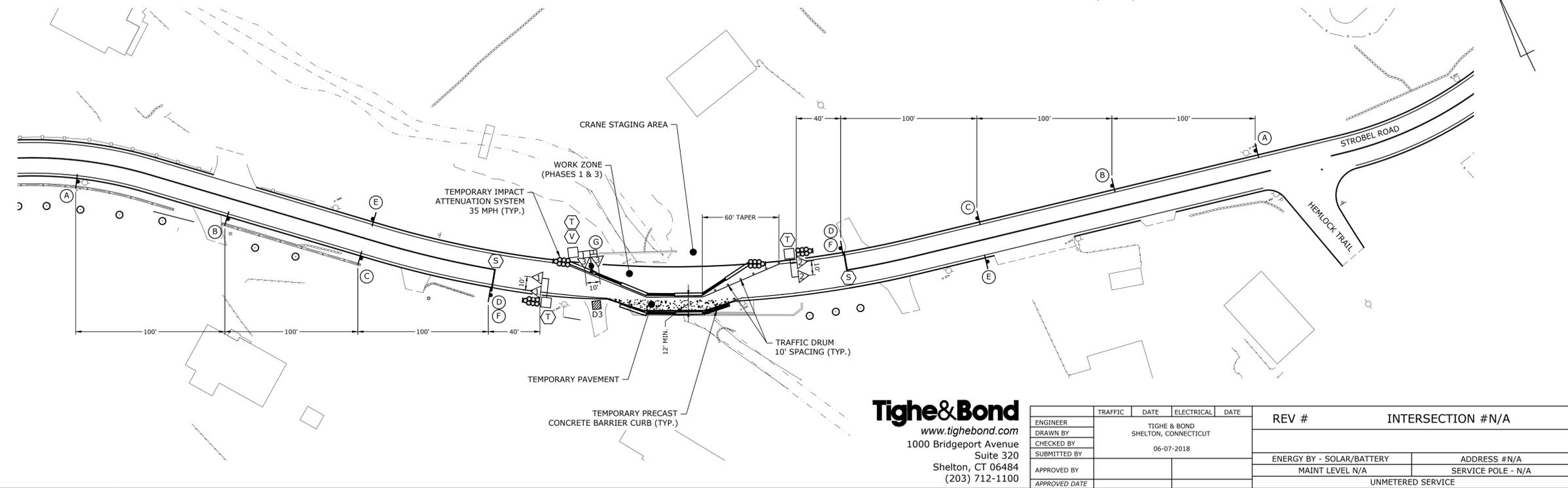
ANY PROPOSED REVISIONS TO THE LOCATION OF THE APPURTENANCES SHOWN ON THE PLAN MUST BE SUBMITTED FOR REVIEW AND APPROVAL BY THE ENGINEER PRIOR TO IMPLEMENTATION.

- (S) INSTALL TEMPORARY 12" WHITE PAINTED STOP BAR
- (T) INSTALL PORTABLE TRAFFIC SIGNAL WITH RED CLEARANCE TIME EXTENDER AND TWO 1-WAY, 3 SECTION SIGNAL HEADS 1, 2, AND 3 MOUNTED OVERHEAD AS SHOWN
- (V) INSTALL VIDEO DETECTION CAMERA



SIGN LEGEND

- (A) INSTALL 80-9603 36"
- (B) INSTALL 80-9933 36"
- (C) INSTALL 80-9052 36"
- (D) INSTALL 31-0802 36"
- (E) INSTALL 80-9612 24"
- (F) INSTALL 31-1774 12"
- (G) INSTALL 31-0817 18"



Tighe & Bond
www.tighebond.com
1000 Bridgeport Avenue
Suite 320
Shelton, CT 06484
(203) 712-1100

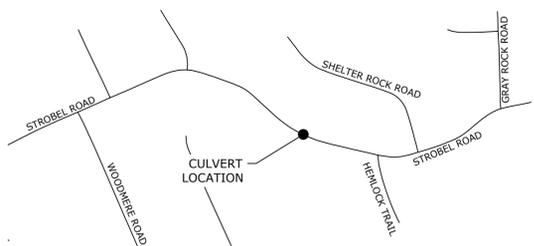
ENGINEER	TRAFFIC	DATE	ELECTRICAL	DATE	REV #	INTERSECTION #N/A	
TIGHE & BOND							
DRAWN BY	SHELTON, CONNECTICUT				ENERGY BY - SOLAR/BATTERY		
CHECKED BY	06-07-2018				ADDRESS #N/A		
SUBMITTED BY					MAINT LEVEL N/A		
APPROVED BY					SERVICE POLE - N/A		
APPROVED DATE					UNMETERED SERVICE		
TOWN: TRUMBULL					PROJECT NO. 290196-15		
					DRAWING NO.		
TOWN OF TRUMBULL PUBLIC WORKS DEPARTMENT					DRAWING TITLE: TEMPORARY TRAFFIC CONTROL SIGNAL PLAN		
					SHEET NO. TTC-004		

Jan 18, 2019 3:01pm Plotted By: Dubucuec
Tighe & Bond, Inc. S:\T0196\15 - Strobels Rd Drawing\Sheet\T0196-15-TTC-TEMP SIGNAL.dwg
Border Version 11/29/13

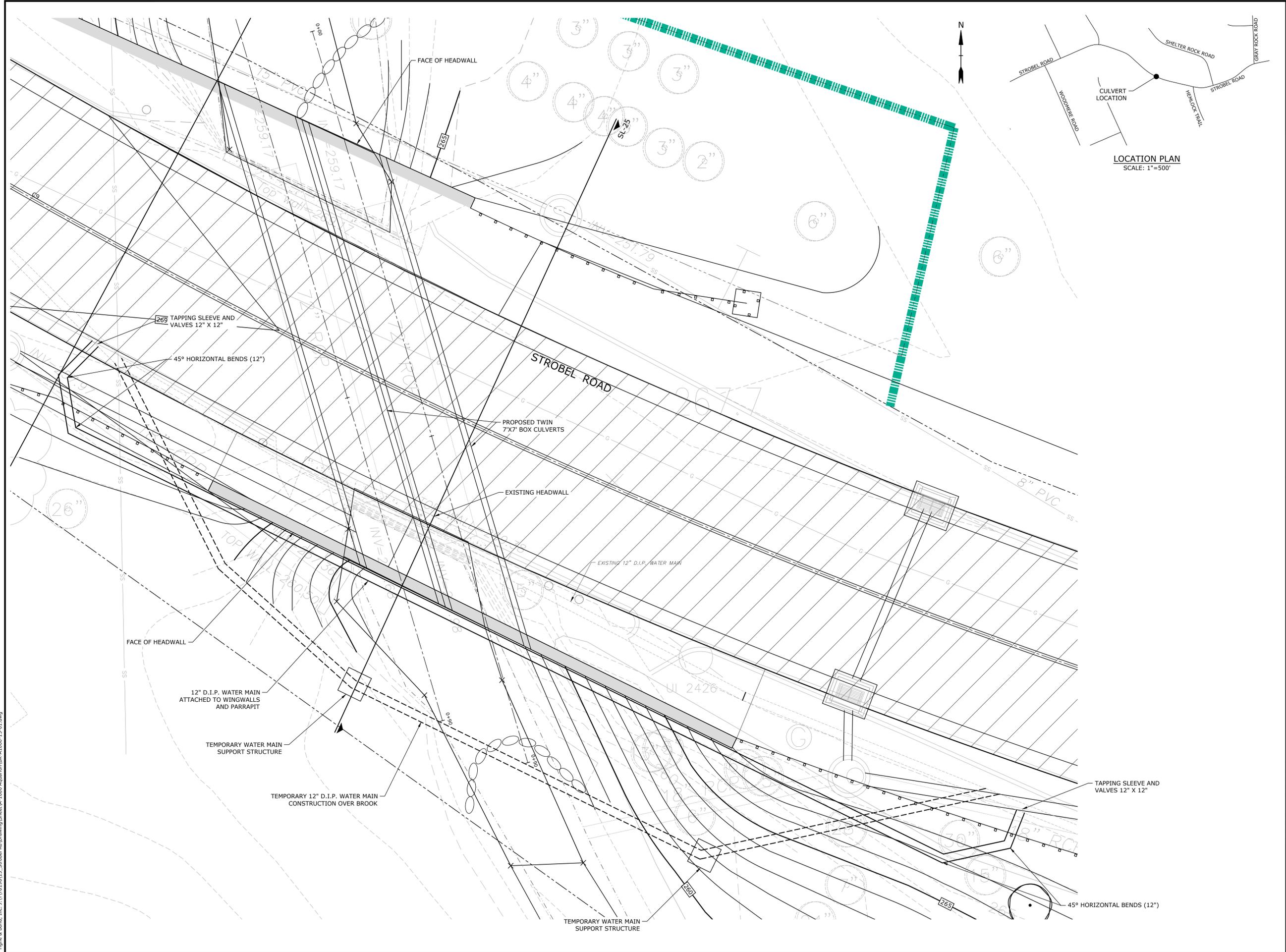
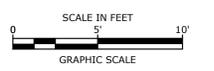
LEGEND:		TRAFFIC SIGNAL FACE		PROPOSED CONTROLLER		VC VIDEO CAMERA CABLE	
R, Y, G	RED, YEL, GRN	TRAFFIC SIGNAL FACE	PEDESTRIAN SIGNAL FACE	PROPOSED CONTROLLER	EXISTING CONTROLLER	CABLE CLOSURE	CABLE CLOSURE
←	RED ARROW	DET. LEADS IN SAW CUT	DET. LEADS IN SAW CUT	LOOP DETECTOR	LOOP DETECTOR	WIRELESS SENSOR	WIRELESS SENSOR
→	GREEN ARROW	PROPOSED RMC (RIGID METAL CONDUIT)	PROPOSED RMC (RIGID METAL CONDUIT)	SYSTEM DETECTOR	SYSTEM DETECTOR	WIRELESS RECEIVER	WIRELESS RECEIVER
W	WALK / PED. CLR	EXISTING UTILITY POLE	EXISTING UTILITY POLE	MAGNETIC DETECTOR	MAGNETIC DETECTOR	WIRELESS TRANSMITTER	WIRELESS TRANSMITTER
D.W.	DON'T WALK	AUXILIARY TERMINATION CABINET	AUXILIARY TERMINATION CABINET	OPTICAL DETECTOR	OPTICAL DETECTOR	GUY WIRE	GUY WIRE
FL	FLASHING	AUXILIARY EQUIPMENT CABINET	AUXILIARY EQUIPMENT CABINET	VIDEO DETECTOR	VIDEO DETECTOR	PROPOSED HANDHOLE	PROPOSED HANDHOLE
		VIDEO DETECTION ZONE	VIDEO DETECTION ZONE	AUDIO DETECTOR	AUDIO DETECTOR	EXISTING HANDHOLE	EXISTING HANDHOLE

DATE PLOTTED : Jan 18, 2019

SCALE 1" = 40'



LOCATION PLAN
 SCALE: 1"=500'



**Aquarion
 Water
 Company**

**Strobel Road
 Water Main
 Project**

Trumbull, Connecticut

April 2018

MARK	DATE	DESCRIPTION

PROJECT NO:	A-1000-15
FILE:	BR-A1000-15-01.dwg
DRAWN BY:	XX
CHECKED:	XX
APPROVED:	XX

CULVERT CROSSING PLAN

SCALE: 1" = 5'

W2.3

Jan 18, 2019 3:02pm Plotted By: DubravkaC
 Tighe & Bond, Inc. \\T1019615_Strobel Rd\Drawing\SheetA-1000-15-01.dwg