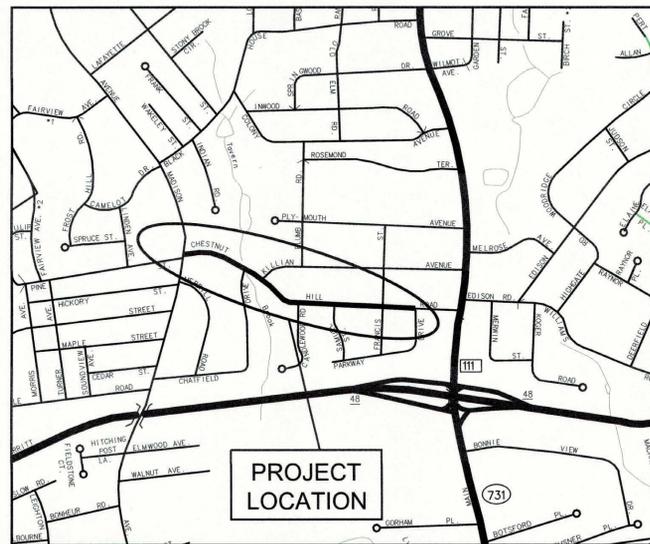


TOWN OF TRUMBULL, CONNECTICUT

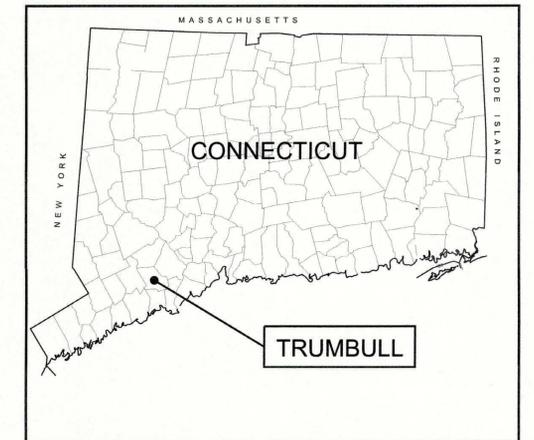
CHESTNUT HILL ROAD

RECONSTRUCTION PROJECT

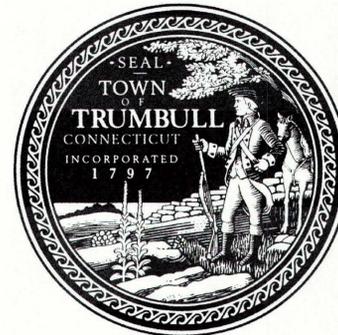
LOTICIP PROJ. #L144-0003



SCALE: 1 IN. = 1,000 FT.



NOT TO SCALE



CHESTNUT HILL ROAD
STA. 0+54 TO STA. 23+20

JANUARY 2019

VICKI A. TESORO
FIRST SELECTMAN

JOHN MARSILIO
DIRECTOR OF PUBLIC
WORKS

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DESIGN SPEED = 30 MPH

2016 CONNECTICUT DEPARTMENT OF
TRANSPORTATION SPECIFICATIONS FORM 817
INCLUDING SUPPLEMENTAL SPECIFICATIONS
DATED JANUARY 2018.

SURVEY INFORMATION DERIVED FROM: TOWN OF
TRUMBULL ENGINEERING DEPARTMENT

VERTICAL DATUM - NAVD88
HORIZONTAL DATUM - ASSUMED

DESIGNED BY: ROY SEELYE PE.
BETA GROUP, INC.
1010 WETHERSFIELD AVENUE
HARTFORD, CONNECTICUT 06114
TEL. 860-513-1503

Roy Seelye
CT. LIC. NO. 16196



CONSTRUCTION DOCUMENTS



www.BETA-Inc.com

GENERAL CONSTRUCTION NOTES:

- ALL CONSTRUCTION MATERIALS AND OPERATIONS SHALL COMPLY WITH THE STANDARD PROVISIONS AND REQUIREMENTS OF THE TOWN OF TRUMBULL OR THE CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, AND INCIDENTAL CONSTRUCTION, FORM 817, AS AMENDED. IN THE CASE OF CONFLICTING SPECIFICATIONS, THE MORE STRINGENT SHALL APPLY.
- THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OF ALL PRODUCTS, MATERIAL SAMPLES, PLAN, OR OTHER INFORMATION TO THE TOWN OR STATE REPRESENTATIVE FOR REVIEW AND APPROVAL PRIOR TO FABRICATION OR DELIVERY TO THE SITE AS SPECIFIED IN THE CONTRACT.
- ALL TESTING, EXCEPT FOR FIELD DENSITY TESTING, MUST BE SUBMITTED TO AND APPROVED BY THE TOWN OF TRUMBULL DEPARTMENT OF PUBLIC WORKS, ENGINEERING DEPARTMENT.
- HOURS OF WORK SHALL BE AS DEFINED IN THE CONTRACT DOCUMENTS. THE TOWN RESERVES THE RIGHT TO ADJUST THESE HOURS IN THE INTEREST OF PUBLIC SAFETY.
- THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS REQUIRED BY GOVERNMENT AGENCIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL OBTAIN ANY TOWN, STATE OF CONNECTICUT OR OTHER PERMITS FOR WORK WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL POST ALL BONDS, PAY ALL FEES, PROVIDE PROOF OF INSURANCE, AND PROVIDE TRAFFIC PROTECTION AS REQUIRED FOR THIS WORK A CONDOT ENCRACHMENT PERMIT IS REQUIRED.
- OSHA REGULATIONS MAKE IT UNLAWFUL TO OPERATE CRANES, BOOMS, HOISTS, ETC. WITHIN 10' OF ANY ELECTRIC LINE UNDER 50KV. IF THE CONTRACTOR MUST OPERATE EQUIPMENT CLOSE TO ELECTRIC LINES, HE MUST CONTACT THE POWER COMPANY TO MAKE ARRANGEMENTS FOR PROPER SAFEGUARDS.
- THE TOWN AND THE ENGINEER IS NOT RESPONSIBLE FOR SITE SAFETY MEASURES TO BE EMPLOYED DURING CONSTRUCTION. THE TOWN AND THE ENGINEER HAS NO CONTRACTUAL DUTY TO CONTROL THE SAFEST METHODS OR MEANS OF THE WORK, JOB SITE RESPONSIBILITIES, SUPERVISION OR TO SUPERVISE SAFETY AND DOES NOT VOLUNTARILY ASSUME ANY SUCH DUTY OR RESPONSIBILITY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING ADJACENT PROPERTY OWNERS OF IMPENDING CONSTRUCTION, AS WELL AS COORDINATING DRIVEWAY AND WALKWAY REMOVAL/REPLACEMENT WORK.
- ACCESS TO PRIVATE PROPERTIES MUST BE MAINTAINED AT ALL TIMES. FOR SECURITY REASONS, ALL PRIVATELY OWNED FENCING THAT IS TO BE REMOVED SHALL BE REPLACED WITHIN 72 HOURS OF REMOVAL. TEMPORARY FENCING SHALL BE INSTALLED AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL VERIFY ALL SITE CONDITIONS IN THE FIELD AND CONTACT THE TOWN IF THERE ARE ANY QUESTIONS OR CONFLICTS REGARDING THE CONSTRUCTION DOCUMENTS AND/OR FIELD CONDITIONS SO THAT APPROPRIATE REVISIONS CAN BE MADE PRIOR TO BIDDING AND CONSTRUCTION OPERATIONS.
- SHOULD ANY UNCHARTED OR INCORRECTLY CHARTED, EXISTING PIPING OR OTHER UTILITY BE UNCOVERED DURING THE CONSTRUCTION, THE CONTRACTOR SHALL CONSULT WITH THE TOWN AND APPLICABLE UTILITY COMPANY IMMEDIATELY FOR DIRECTIONS BEFORE PROCEEDING FURTHER WITH WORK IN THIS AREA.
- ELEVATIONS ARE BASED ON NAVD88 AND ARE AS NOTED ON THE PLAN. COORDINATES ARE ASSUMED.
- ALL DIMENSIONS, ELEVATIONS, AND EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. ANY DISCREPANCIES SHALL BE REPORTED TO THE ENGINEER.
- COORDINATE INFORMATION AND DIMENSIONS ARE DEEMED TO BE RELIABLE, BUT NOT GUARANTEED. THE CONTRACTOR SHALL VERIFY COORDINATES AND DIMENSIONS PRIOR TO CONSTRUCTION.
- PRIOR TO BEGINNING WORK THE CONTRACTOR'S SURVEYOR SHALL TIE OFF ANY EXISTING TOWN OR STATE MONUMENTATION WITHIN THE PROJECT AREA. A MINIMUM OF THREE TIES PER MONUMENT SHALL BE USED. THE SURVEYOR SHALL SUBMIT THIS INFORMATION TO THE ENGINEER AT LEAST TWO WEEKS BEFORE CONSTRUCTION COMMENCES. ANY DISTURBANCE TO THE MONUMENTATION WHICH OCCURS AS A RESULT OF THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED TO THE SATISFACTION OF THE ENGINEER, AT THE CONTRACTOR'S EXPENSE.
- THE TOWN RESERVES THE RIGHT TO ALTER, CHANGE OR MODIFY THE TRAFFIC OR CONSTRUCTION PHASING PLANS IN THE INTEREST OF PUBLIC SAFETY.
- ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE CONSTRUCTION STAGING/PHASING PLAN. NO WORK SHALL BEGIN UNTIL ALL TRAFFIC CONTROL DEVICES ARE IN PLACE.
- TRAFFIC CONTROL SIGNAGE SHALL CONFORM TO THE CONDOT STANDARD DETAIL SHEETS AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. INSTALL NEW OR RELOCATED SIGNS WITH BREAKAWAY POSTS.
- ALL EXISTING PAVEMENT MARKINGS DAMAGED OR REMOVED DURING CONSTRUCTION SHALL BE RESTORED PER TOWN REQUIREMENTS. THE CONTRACTOR SHALL REMOVE CONFLICTING PAVEMENT MARKINGS BY A METHOD APPROVED BY THE TOWN.
- PAVEMENT MARKINGS SHALL BE EPOXY RESIN TYPE IN ACCORDANCE WITH CONDOT SPECIFICATION - FORM 817 UNLESS OTHERWISE NOTED.
- ALL DIMENSIONS ARE REFERENCED TO THE BASELINE UNLESS OTHERWISE NOTED OR SHOWN.
- THE CONTRACTOR SHALL CONFIRM LIMITS OF IMPROVEMENTS PRIOR TO COMMENCING CONSTRUCTION OPERATIONS.
- NO CONSTRUCTION OR DEMOLITION SHALL BEGIN UNTIL APPROVAL OF THE FINAL PLANS IS GRANTED BY ALL GOVERNING AND REGULATORY AGENCIES.
- LINE AND GRADE FOR CONSTRUCTION LAYOUT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL UTILIZE A PROFESSIONAL SURVEYOR LICENSED IN THE STATE OF CONNECTICUT TO PERFORM CONSTRUCTION LAYOUT. THE CONTRACTOR SHALL SUBMIT THE NAME OF THE SURVEYOR TO THE ENGINEER AT LEAST THREE WEEKS PRIOR TO BEGINNING WORK.
- ANY PUBLIC OR PRIVATE PROPERTY THAT IS DISTURBED AS A RESULT OF CONSTRUCTION OPERATIONS SHALL BE RESTORED AS SOON AS POSSIBLE TO THE SATISFACTION OF THE ENGINEER.
- STAGING AREAS AND MATERIAL STOCK PILES SHALL BE LIMITED WITHIN THE SITE AND LOCATED SO AS NOT TO INTERFERE WITH PEDESTRIAN OR VEHICULAR TRAFFIC. THE CONTRACTOR SHALL PROVIDE SAFETY DEVICES (IF ORDERED BY THE TOWN OR STATE) TO PROTECT STOCK PILES OR WORK STAGING AREAS. IF THIS WORK IS SO ORDERED, THERE WILL BE NO ADDITIONAL COMPENSATION MADE TO THE CONTRACTOR.
- EACH LAYER OF MATERIAL USED FOR BACKFILL OR SUBGRADE SHALL BE COMPACTED UNTIL THE DRY DENSITY OF EACH LAYER IS AT LEAST 95% OF THE DRY DENSITY ACHIEVED BY THE AASHTO TEST T180, METHOD D FOR THAT SOIL. MAXIMUM LAYER THICKNESS NOT TO EXCEED 12 INCHES.
- ALTERNATIVE METHODS AND PRODUCTS OTHER THAN THOSE SPECIFIED MAY BE USED IF REVIEWED AND APPROVED BY THE TOWN, STATE OR APPROPRIATE REGULATORY AGENCY PRIOR TO INSTALLATION.
- ALL REGULATORY SIGNS SHALL BE REMOVED AND REPLACED IN EXISTING LOCATIONS UNLESS OTHERWISE NOTED ON THE DRAWINGS OR AS OTHERWISE DIRECTED BY THE ENGINEER OR TOWN REPRESENTATIVE.

- RESET ALL PUBLIC OR PRIVATE MONUMENTS, IRON PIPES OR OTHER TYPES OF PROPERTY LINE AND GEODETIC MARKERS DAMAGED OR DISTURBED BY CONSTRUCTION OPERATIONS. THIS WORK SHALL BE COMPLETED BY A LICENSED LAND SURVEYOR AT THE EXPENSE OF THE CONTRACTOR.
- REPAIR, RESET, RELOCATE OR REPLACE ALL WALKS, DRIVEWAYS, CURBS, WALLS, FENCES, RAILINGS, STONE WALLS, ORNAMENTAL OR UTILITARIAN DOMESTIC ACCESSORIES, ETC. AS NOTED ON THE DRAWINGS OR AS DIRECTED BY THE ENGINEER.
- ALL DISTURBED AREAS NOT OTHERWISE INDICATED IN THE CONTRACT DRAWINGS SHALL BE TOPSOILED AND ESTABLISHED WITH TURF.
- ALL SIGNS CALLED OUT AS "REMOVE EXISTING SIGN" OR "REPLACE EXISTING SIGN" OR EXISTING SIGNS THAT ARE NOT CALLED OUT BUT REQUIRE RELOCATION SHALL BE REPLACED. THIS WORK WILL BE PAID FOR UNDER THE LUMP SUM PAY ITEM "REMOVAL AND REPLACEMENT OF EXISTING SIGNS".
- IF REQUIRED BY FIELD CONDITIONS AND ORDERED BY THE ENGINEER, THE CONTRACTOR WILL DIG TEST PITS TO VERIFY SUBSURFACE CONDITIONS. PAYMENT FOR THIS WORK WILL BE MADE UNDER THE APPLICABLE UTILITY BID ITEM IN THE CONTRACT.
- THE LOCATION OF EXISTING UTILITIES HAS BEEN OBTAINED FROM RECORDED PLANS AND LIMITED FIELD SURVEY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THESE LOCATIONS AND ELEVATIONS AND TAKING ALL NECESSARY STEPS TO SUPPORT AND PROTECT THEM FROM DAMAGE. ANY UTILITY LINE OR EQUIPMENT THAT IS DAMAGED THROUGH THE NEGLIGENCE OF THE CONTRACTOR SHALL BE REPAIRED BY THE CONTROLLING UTILITY COMPANY AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR IS HEREBY REMINDED THAT TITLE 16 CHAPTER 293 OF THE CONNECTICUT GENERAL STATUTES REQUIRES NOTIFICATION TO UTILITY COMPANIES OF PENDING EXCAVATION AT OR NEAR PUBLIC UTILITIES. THE CONTRACTOR SHALL NOTIFY CALL BEFORE YOU DIG , 1-800-922-4455 AT LEAST 48 HOURS PRIOR TO EXCAVATION, BUT NOT MORE THAN 30 DAYS IN ADVANCE.
- RIM OR FRAME ELEVATIONS OF ALL UTILITY STRUCTURES SHALL BE ADJUSTED TO MEET FINISHED PAVED SURFACES. PRIVATE UTILITIES WHO ARE RESPONSIBLE FOR ADJUSTING THEIR OWN STRUCTURES SHALL BE NOTIFIED 3 WEEKS IN ADVANCE BY THE CONTRACTOR. PRIVATE UTILITIES WILL ADJUST THEIR STRUCTURES AT THEIR OWN EXPENSE. THE CONTRACTOR WILL BE RESPONSIBLE TO PROVIDE PRIVATE UTILITIES WITH OFFSETS AND GRADE POINT FOR USE IN ADJUSTING THE STRUCTURES TO PROPOSED GRADE. PUBLIC UTILITIES ARE IDENTIFIED AS TOWN, STATE AND UTILITY STRUCTURES. THE ADJUSTMENT OF PUBLIC STRUCTURES SHALL BE PERFORMED BY THE RESPONSIBLE UTILITY COMPANY. UTILITY STRUCTURE TOPS, FRAMES AND COVERS, VALVE BOXES, ETC. SHALL BE MODIFIED AFTER THE BINDER COURSE OF PAVEMENT IS PLACED AND PRIOR TO PLACEMENT OF THE SURFACE COURSE.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER SHOULD THERE BE ANY CONFLICTS BETWEEN EXISTING UTILITIES AND PROPOSED CONSTRUCTION.
- CATCH BASINS DAMAGED OR FILLED WITH DEBRIS DURING CONSTRUCTION SHALL BE REPLACED OR CLEANED AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL RESTORE ANY DRAINAGE STRUCTURE, PIPE, UTILITY, PAVEMENT, CURBS, SIDEWALKS OR LANDSCAPED AREAS DISTURBED DURING CONSTRUCTION TO THEIR ORIGINAL CONDITION OR BETTER AS APPROVED BY THE ENGINEER, PRIOR TO THE COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.
- EXISTING CONCRETE PIPE AND STRUCTURES TO BE REMOVED THAT ARE NOT COVERED UNDER OTHER CONTRACT ITEMS SHALL BE PAID FOR AS "ROCK-IN-TRENCH EXCAVATION".
- TYPE C CATCH BASINS SHALL BE USED FOR ALL CATCH BASINS UNLESS NOTED OTHERWISE. CONTRACTOR SHALL INSTALL APPROPRIATE CB TOP TO MATCH PROPOSED CURB TYPE.
- ALL SEDIMENTATION AND EROSION CONTROL MEASURES SHALL BE INSPECTED AND MAINTAINED ON A REGULAR BASIS AS OUTLINED HEREIN. INSPECTION AND MAINTENANCE SHALL BE CARRIED OUT THROUGHOUT THE CONSTRUCTION PERIOD AND UNTIL ALL DISTURBED AREAS ARE STABILIZED WITH VEGETATION OR PAVING. THE MINIMUM INSPECTION PERIOD SHALL BE WEEKLY AND/OR AFTER MAJOR STORMS.
- THE CONTRACTOR IS RESPONSIBLE TO ENSURE THAT PROPER STORM DRAINAGE AND SANITARY FLOW ARE MAINTAINED THROUGHOUT CONSTRUCTION.
- ALL EXISTING STRUCTURES AND PIPES THAT ARE NOT REMOVED DUE TO CONSTRUCTION OF NEW STORM SEWER WILL BE PLUGGED AND ABANDONED UNLESS OTHERWISE NOTED ON THE PLANS.
- THE CONTRACTOR IS RESPONSIBLE FOR SUPPORTING EXCAVATIONS, TRENCH WALLS AND PROVIDING TRENCH PROTECTION IN ACCORDANCE WITH OSHA REGULATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE GENERAL COST OF THE CONTRACT. THERE IS NO SEPARATE PAY ITEM FOR THIS WORK.

DEMOLITION NOTES

- THE CONTRACTOR SHALL REMOVE ANY SIDEWALKS, STAIRS, OR RELATED ITEMS NECESSARY TO CONSTRUCT PROPOSED IMPROVEMENTS AND DISPOSE OF THESE ITEMS LEGALLY OFFSITE. PAYMENT FOR THIS WORK WILL BE MADE UNDER THE APPROPRIATE BID ITEM OR IF NOT SO SPECIFIED, UNDER THE ITEM "CLEARING AND GRUBBING".
- THE CONTRACTOR SHALL SECURE ALL PERMITS FOR DEMOLITION AND DISPOSAL OF DEMOLITION MATERIAL TO BE REMOVED FROM WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL POST ALL BONDS AND PAY ANY PERMIT FEE ASSOCIATED WITH THIS WORK. THE COST FOR THIS SHALL BE INCLUDED IN THE GENERAL COST OF THE PROJECT.
- THE CONTRACTOR SHALL PREPARE ALL MANIFEST DOCUMENTS AS REQUIRED PRIOR TO COMMENCEMENT OF DEMOLITION.
- THE CONTRACTOR SHALL PROVIDE DISCONNECT NOTIFICATIONS AS REQUIRED, TO ALL UTILITY COMPANIES AND AGENCIES AT LEAST THREE WEEKS PRIOR TO BEGINNING DEMOLITION.
- BACKFILL DEPRESSIONS AND FILL AND TOPSOIL HOLES WITH APPROVED SOIL MATERIAL. COMPACT, FERTILIZE, SEED AND MULCH DISTURBED AREAS NOT SUBJECT TO FURTHER SITE CONSTRUCTION.
- THE CONTRACTOR SHALL ARRANGE FOR AND COORDINATE ALL WORK TO BE PERFORMED BY UTILITY COMPANIES AND OR THE TOWN. THE CONTRACTOR SHALL REPAIR PAVEMENTS AND COORDINATE WITH ALL UTILITY COMPANIES FOR POWER DISRUPTIONS AND ADJUSTMENTS TO STRUCTURES.
- ALL REGULATORY, WARNING, AND TRAFFIC CONTROL SIGNS, UNLESS NOTED ON THE DRAWINGS AS NEW, SHALL BE REMOVED, AND REPLACED WITH NEW ON BREAK AWAY SUPPORTS. COORDINATE SIGN REPLACEMENT WITH THE TOWN.
- THE CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANIES, TOWN EMERGENCY SERVICES, THE POSTMASTER AND OTHER NECESSARY AGENCIES WHICH ARE AFFECTED BY WORK SHOWN ON THESE PLANS OR REQUIRED IN THE FIELD.
- ALL EXISTING ROAD SIGNING SHALL BE MAINTAINED DURING CONSTRUCTION. TEMPORARY SIGNAGE SHALL BE PLACED IN AREAS WHERE EXISTING SIGNAGE CANNOT REMAIN DUE TO CONSTRUCTION ACTIVITY. ALL SIGNS REMOVED, UNLESS OTHERWISE NOTED AS NEW, SHALL BE STOCKPILED, PROTECTED FROM DAMAGE, AND REINSTALLED AS SOON AS PRACTICABLE, UNLESS OTHERWISE SPECIFIED.

- THE CONTRACTOR SHALL VERIFY ALL EXISTING CONDITIONS, MATERIALS AND DIMENSIONS PRIOR TO COMMENCING DEMOLITION AND REPORT ANY DISCREPANCIES TO THE ENGINEER.
- EXISTING STREET NAME SIGNS, MAILBOXES, UTILITY POLES, ETC. WITHIN STREET RIGHT-OF-WAY AND AS OTHERWISE INDICATED SHALL REMAIN IN PLACE AND BE PROTECTED DURING DEMOLITION ACTIVITIES UNLESS OTHERWISE NOTED.
- SEDIMENT AND EROSION CONTROLS SHALL BE IN PLACE PRIOR TO START OF DEMOLITION OR CLEARING AND GRUBBING OPERATIONS. USE SILT SACKS FOR CATCH BASINS.
- ASBESTOS OR OTHER HAZARDOUS MATERIALS, IF FOUND ON SITE, SHALL BE REMOVED BY A LICENSED HAZARDOUS MATERIAL CONTRACTOR.
- INFORMATION ON EXISTING UTILITIES HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY AND MUNICIPAL RECORD MAPS AND LIMITED FIELD SURVEY AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR OF THEIR PRESENCE AND THE CONTRACTOR IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES INCLUDING SERVICES PRIOR TO DEMOLITION OR CONSTRUCTION.
- THE CONTRACTOR SHALL REPAIR PAVEMENTS IN PUBLIC RIGHT-OF-WAY, AS REQUIRED BY LOCAL GOVERNING AUTHORITIES AND THE STATE, DUE TO DEMOLITION, TEST PIT, AND PIPE REMOVAL ACTIVITIES.

GRADING AND DRAINAGE NOTES

- THE CONTRACTOR SHALL UTILIZE EROSION CONTROL MEASURES ACCEPTABLE TO THE ENGINEER UNTIL THE COMPLETION OF HIS WORK AS REQUIRED OR DEEMED NECESSARY BY THE ENGINEER. THE CONTRACTOR SHALL ASSUME RESPONSIBILITY FOR THE MAINTENANCE OF EROSION AND SEDIMENTATION CONTROLS UNTIL PROJECT COMPLETION. THERE WILL BE NO DIRECT PAYMENT FOR THE MAINTENANCE AND REPAIR OF THE EROSION CONTROL MEASURES AND SUCH COST SHALL BE INCLUDED IN THE OVERALL COST OF THE RESPECTIVE ITEMS.
- UNLESS OTHERWISE NOTED ALL PROPOSED SIDEWALKS, DRIVEWAYS, AND OTHER IMPROVEMENTS SHALL BE INSTALLED TO MATCH EXISTING GRADES AT THE LIMITS OF CONSTRUCTION. GRADING PERFORMED IN ACCORDANCE WITH PROPOSED IMPROVEMENTS SHALL NOT RESULT IN PONDING OF STORM WATER OR NEGATIVELY IMPACT EXISTING DRAINAGE PATTERNS.
- CLEARING LIMITS SHALL BE PHYSICALLY MARKED IN THE FIELD AND APPROVED BY THE TOWN REPRESENTATIVE AND/OR THE ENGINEER PRIOR TO THE START OF WORK ON THE SITE.
- THE CONTRACTOR SHALL COMPACT FILL UNDER ALL WALK AND DRIVE AREAS TO 95% OF THE MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D1557 (MODIFIED PROCTOR TEST) IN MAXIMUM 8" LIFTS.
- MANHOLE RIMS AND CATCH BASIN GRATES SHALL BE SET TO ELEVATIONS SHOWN. RESET ALL EXISTING MANHOLE RIMS AND WATER GATE VALVES FLUSH WITH FINAL GRADE AS NECESSARY. THE CONTRACTOR SHALL COORDINATE THE SETTING OF THESE STRUCTURES WITH THE UTILITY COMPANY. REFER TO GENERAL CONSTRUCTION NOTE #38.
- SIDEWALK CROSS PITCH SHALL BE MAXIMUM 2% UNLESS OTHERWISE NOTED, AS INDICATED BY SPOT ELEVATIONS, AS SHOWN ON THE MISCELLANEOUS DETAILS, AS SHOWN ON TYPICAL CROSS SECTION SHEETS, OR AS SHOWN ON THE CROSS SECTIONS. LONGITUDINAL SLOPE ALONG THE SIDEWALK SHALL BE MAXIMUM 5% (OR MATCH ROAD GRADES AS REQUIRED).
- WHEN A NEW WALK MEETS AN EXISTING STEP OR WALL, MAINTAIN THE EXISTING RISER HEIGHT OR WALL HEIGHT UNLESS OTHERWISE NOTED AND AS INDICATED. NOTIFY THE TOWN REPRESENTATIVE OF ANY DISCREPANCIES.
- CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANY THE NEED FOR TEMPORARY SUPPORT OR RELOCATION OF UTILITY POLES RESULTING FROM EXCAVATIONS RELATED TO THIS PROJECT.
- PROPOSED GRADES TO SMOOTHLY MEET EXISTING GRADES AT ALL LIMITS OF CONSTRUCTION.
- CONTRACTOR SHALL VERIFY THE CONDITION OF ALL DRAINAGE STRUCTURES PRIOR TO THE START OF CONSTRUCTION. CATCH BASINS MAY BE INSPECTED BY THE TOWN AT THE END OF CONSTRUCTION TO ENSURE SATISFACTORY CONDITION.
- EXISTING PAVEMENT SHALL BE SAWCUT WHERE DRAINAGE PIPE OR STRUCTURES ARE TO BE INSTALLED.
- ALL PIPES SHALL BE LAID IN STRAIGHT ALIGNMENTS AND EVEN GRADES USING A PIPE LASER OR OTHER ACCURATE METHOD.
- MINIMUM CLEARANCE, AS REQUIRED BY EACH UTILITY, BETWEEN WATER, GAS, ELECTRICAL, AND TELEPHONE LINES AND SEWERS SHALL BE PROVIDED UNLESS OTHERWISE NOTED. 6" MINIMUM CLEARANCE SHALL BE MAINTAINED BETWEEN STORM AND SANITARY SEWERS WITH A CONCRETE ENCASEMENT.
- CONTRACTOR TO FIELD VERIFY ALL ELEVATIONS PRIOR TO FINAL INSTALLATION. BRING ANY DISCREPANCIES TO THE ENGINEER'S IMMEDIATE ATTENTION.
- ALL SPOT ELEVATIONS AT BUILDING WALLS ARE EXISTING ELEVATIONS UNLESS OTHERWISE NOTED. BRING ANY DISCREPANCIES TO THE ENGINEER'S IMMEDIATE ATTENTION.
- CONTRACTOR IS RESPONSIBLE FOR THE SUPPORT OF UTILITIES IN A METHOD ACCEPTABLE TO THE OWNER OF THAT UTILITY FOR WORK RELATED TO THE INSTALLATION OF DRAINAGE STRUCTURES AND PIPING. THIS WORK WILL NOT BE MEASURED FOR PAYMENT.

UTILITY NOTES

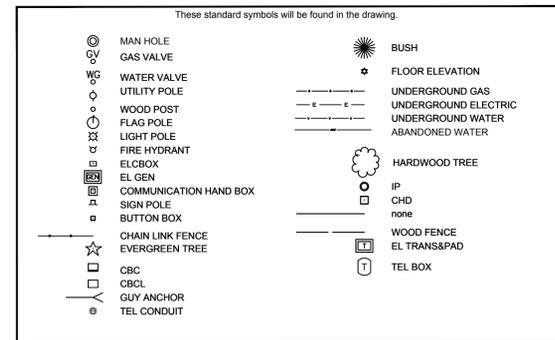
- INFORMATION ON EXISTING UTILITIES HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY AND MUNICIPAL RECORD MAPS AND LIMITED FIELD SURVEY AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES INCLUDING SERVICES PRIOR TO CONSTRUCTION. CONTACT "CALL BEFORE YOU DIG" AT 1-800-922-4455 48 HOURS MINIMUM PRIOR TO BEGINNING CONSTRUCTION OR DEMOLITION AND VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS. THE CONTRACTOR SHALL VISIT THE SITE AND VERIFY THE ELEVATION AND LOCATION OF ALL UTILITIES BY VARIOUS MEANS PRIOR TO BEGINNING ANY EXCAVATION. TEST PITS SHALL BE DUG AT ALL LOCATIONS WHERE PROPOSED UTILITIES INCLUDING STREET LIGHTING AND TRAFFIC CONTROL CONDUIT CROSS EXISTING UTILITIES, AND THE HORIZONTAL AND VERTICAL LOCATIONS OF THE UTILITIES SHALL BE DETERMINED. THE CONTRACTOR SHALL CONTACT THE ENGINEER IN THE EVENT OF ANY UNFORESEEN CONFLICTS BETWEEN EXISTING AND PROPOSED UTILITIES SO THAT AN APPROPRIATE MODIFICATION MAY BE MADE. UTILITY CONNECTIONS ARE SUBJECT TO UTILITY COMPANY AND TOWN APPROVAL.
- PROPOSED STORM DRAINAGE, SANITARY SEWER, AND WATER MAINS TO BE CONSTRUCTED BY OTHERS MAY BE SHOWN ON THE PLANS FOR INFORMATION ONLY AND ARE SUBJECT TO CHANGE. CONTRACTOR TO VERIFY THE LOCATION AND ELEVATIONS OF ALL FACILITIES PRIOR TO CONSTRUCTION.
- PROPER COORDINATION WITH THE RESPECTIVE UTILITY COMPANY SHALL BE PERFORMED BY THE CONTRACTOR TO INSURE THAT ALL UTILITY COMPANIES, TOWN AND CONDOT STANDARDS FOR MATERIALS AND CONSTRUCTION METHODS ARE MET. REFER TO THE DRAINAGE AND UTILITIES PLAN FOR ADDITIONAL INFO.
- ALL UTILITY CONSTRUCTION IS SUBJECT TO INSPECTION PRIOR TO APPROVAL FOR BACKFILL IN ACCORDANCE WITH THE APPROPRIATE UTILITY COMPANY, TOWN AND CONDOT REQUIREMENTS.
- ALL DISTURBANCE CAUSED TO TOWN, STATE OR PRIVATE PROPERTY DUE TO CONSTRUCTION SHALL BE RESTORED TO ITS PREVIOUS CONDITION OR BETTER, TO THE SATISFACTION OF THE TOWN AUTHORITY.
- THE CONTRACTOR SHALL ARRANGE FOR WORK TO BE PERFORMED BY UTILITY COMPANIES AND THE TOWN. THE CONTRACTOR SHALL COORDINATE THIS WORK AND SECURE ANY PERMITS AND PAY ANY UTILITY COMPANY OR TOWN FEES.
- THE CONTRACTOR SHALL COORDINATE AND CONDUCT, AS DETERMINED BY THE APPROPRIATE UTILITY, THE RESETTling OF MANHOLES, HANDHOLES, CURB BOXES AND VALVE BOXES TO THE NEW SIDEWALK OR ROADWAY GRADE.
- THE CONTRACTOR SHALL ARRANGE FOR AND COORDINATE WITH THE RESPECTIVE UTILITY COMPANIES FOR SERVICE INSTALLATIONS AND CONNECTIONS. THE CONTRACTOR SHALL COORDINATE WORK TO BE PERFORMED BY THE VARIOUS UTILITY COMPANIES AND SHALL PAY ALL FEES FOR CONNECTIONS, DISCONNECTION, RELOCATIONS, INSPECTIONS AND DEMOLITION.

EROSION CONTROL NOTES

- THESE GUIDELINES SHALL APPLY TO ALL WORK CONSISTING OF ANY AND ALL TEMPORARY AND/OR PERMANENT MEASURES TO CONTROL WATER POLLUTION AND SOIL EROSION AS MAY BE REQUIRED, DURING THE CONSTRUCTION OF THE PROJECT.
- IN GENERAL, ALL CONSTRUCTION ACTIVITIES SHALL PROCEED IN SUCH A MANNER SO AS NOT TO POLLUTE ANY WETLANDS, WATERCOURSE, WATER BODY AND CONDUIT CARRYING WATER, ETC. THE CONTRACTOR SHALL LIMIT, AS POSSIBLE, THE SURFACE AREA OF EARTH MATERIALS EXPOSED BY CONSTRUCTION METHODS, AND IMMEDIATELY PROVIDE TEMPORARY AND PERMANENT POLLUTION CONTROL MEASURES TO PREVENT CONTAMINATION OF ADJACENT WETLANDS, WATERCOURSES AND WATER BODIES, AND TO PREVENT, INsofar AS POSSIBLE, EROSION ON THE SITE.
- CONSTRUCTION METHODS, IN GENERAL SHALL BE IN ACCORDANCE WITH THE PROVISIONS SET FORTH IN THE SPECIFICATIONS.
- ANY ADDITIONAL EROSION/SEDIMENT CONTROL MEASURES DEEMED NECESSARY BY THE OWNER'S REPRESENTATIVE SHALL BE IMPLEMENTED BY THE CONTRACTOR. ADDITIONALLY, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR AND REPLACEMENT OF ALL EROSION/SEDIMENT CONTROLS.
- STAKED HAY BALES - HAY OR STRAW BALES EROSION CHECKS ARE TO BE INSTALLED AT THE BASE OF SLOPES INDICATED ON THE PLANS. HAY BALES SHALL ALSO BE PLACED AT CATCH BASINS WHERE SEDIMENT MAY ENTER THE BASIN OR AS DIRECTED BY THE OWNER'S REPRESENTATIVE. HAY BALE EROSION CHECKS SHALL BE PLACED IMMEDIATELY AFTER A CUT SLOPE HAS BEEN GRADED AND BEFORE A FILL SLOPE HAS BEEN CREATED. DEPOSITS OF SEDIMENT AND SILT ARE TO BE PERIODICALLY REMOVED FROM THE UPSTREAM SIDES OF THE EROSION CHECKS. THIS MATERIAL IS TO BE SPREAD AND STABILIZED IN AREAS NOT SUBJECT TO EROSION, OR IN AREAS WHICH ARE NOT TO BE PAVED OR BUILT ON. HAY OR STRAW BALES ARE TO BE REPLACED AS NECESSARY TO PROVIDE PROPER FILTERING ACTION. EROSION CHECKS ARE TO REMAIN IN PLACE AND BE MAINTAINED TO INSURE EFFICIENT SILTATION CONTROL UNTIL ALL AREAS ABOVE THE EROSION CHECKS ARE STABILIZED AND VEGETATION IS ESTABLISHED.
- FILTER FENCE - SYNTHETIC FILTER BARRIER FENCE MAY BE USED IN PLACE OF HAY BALES SPECIFIED AT ALL LOCATIONS AS INDICATED ON THE PLANS TO INTERCEPT SILT AND SEDIMENT BEFORE IT REACHES THE DRAINAGE WAYS OR WETLANDS. DEPOSITS OF SEDIMENT AND SILT ARE TO BE PERIODICALLY REMOVED FROM THE UPSTREAM SIDES OF THE FENCE. THIS MATERIAL IS TO BE SPREAD AND STABILIZED IN AREAS NOT SUBJECT TO EROSION, OR IN AREAS WHICH ARE NOT TO BE PAVED OR BUILT ON. THE FENCE IS TO BE REPLACED AS NECESSARY TO PROVIDE PROPER FILTERING ACTION. THE FENCE IS TO REMAIN IN PLACE AND BE MAINTAINED TO INSURE EFFICIENT SILTATION CONTROL UNTIL ALL AREAS ABOVE THE FENCE ARE STABILIZED AND VEGETATION IS ESTABLISHED.
- ALL TOPSOIL STOCKPILES ARE TO BE MULCHED WITH EROSION CONTROLS INSTALLED AROUND THEM IMMEDIATELY AFTER THEY ARE CREATED.
- IN ALL AREAS, REMOVAL OF VEGETATION, AND DISTURBANCE OF THE SOIL, IS TO BE KEPT TO A MINIMUM WHILE ALLOWING PROPER DEVELOPMENT OF THE SITE.
- DEWATERING SYSTEMS SHALL BE PUMPED INTO A FRAC TANK. DISCHARGE TO GROUND, STORM DRAINAGE WASTE OR WATERCOURSES IS PROHIBITED.
- PLACE SILT FENCE AT LIMITS OF DISTURBANCE EXCEPT AT DRIVEWAYS

THE TOPOGRAPHIC AND BOUNDARY SURVEY FOR THIS PROJECT WAS PREPARED BY THE TOWN OF TRUMBULL. BETA GROUP, INC. MAKES NO WARRANTY WITH REGARD TO THE ACCURACY OR COMPLETENESS OF THE SURVEY. USE OF THIS SURVEY IS AT THE RISK OF THE CONTRACTOR. THE CONTRACTOR ACKNOWLEDGES THAT BETA GROUP, INC. WILL NOT BE HELD RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF THE SURVEY. IN THE EVENT THAT A CONFLICT ARISES BETWEEN THE DRAWINGS AND FIELD CONDITIONS, THE CONTRACTOR SHALL NOTIFY THE TOWN IMMEDIATELY. ADJUSTMENTS TO THE PLANS MAY BE MADE AFTER APPROVAL BY THE TOWN.

EXISTING LEGEND



1/17/2019 8:59 AM H:\5000\6568 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\6568-GENERAL NOTES.DWG

				DRAWN BY:	REGISTERED PROFESSIONAL	PREPARED BY	SUBCONSULTANT	SCALE	TITLE	BETA JOB NO.	
				J.D.					CHESTNUT HILL ROAD RECONSTRUCTION	5658	
				DESIGNED BY:					TRUMBULL, CT	ISSUE DATE	
				R.S.					GENERAL NOTES	1/16/2019	
				CHECKED BY:						SHEET NO.	
				J.B.						GN-1	
NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS							
 www.BETA-inc.com											
								NONE		2 OF 47	

UNLESS OTHERWISE NOTED OR CHANGED BY REPRODUCTION



1/17/2019 9:00 AM H:\5000\668 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\668-INDEX.DWG

NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

DRAWN BY:
JD

DESIGNED BY:
RS

CHECKED BY:
JB

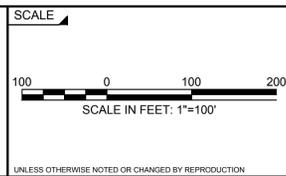
REGISTERED PROFESSIONAL

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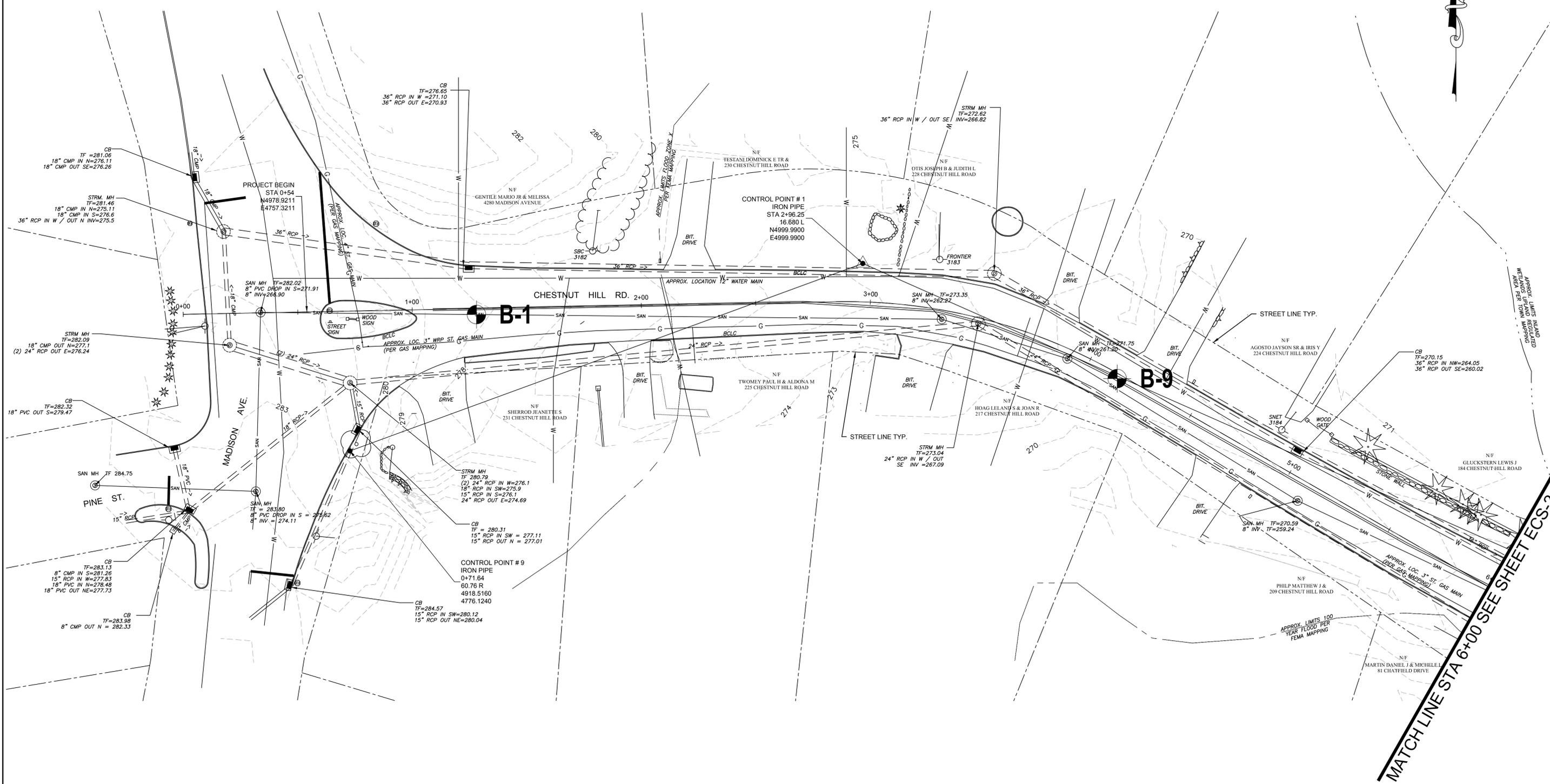
TITLE

**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
INDEX PLAN**

BETA JOB NO. 5658

ISSUE DATE 1/16/2019

SHEET NO. **IND-1**
3 OF 47



MATCH LINE STA 6+00 SEE SHEET ECS-2

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B-#
APPROXIMATE LOCATION OF BORING
REFER TO GEOTECHNICAL REPORT PREPARED
BY GEI FOR BORING LOGS

1/17/2019 9:00 AM H:\5000\6568 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\6568-EXISTING.CONDITIONS.DWG

NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

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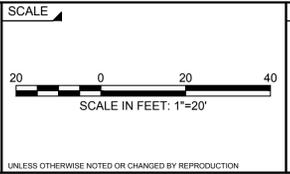
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TITLE

CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
EXISTING CONDITIONS PLAN

BETA JOB NO. 5658

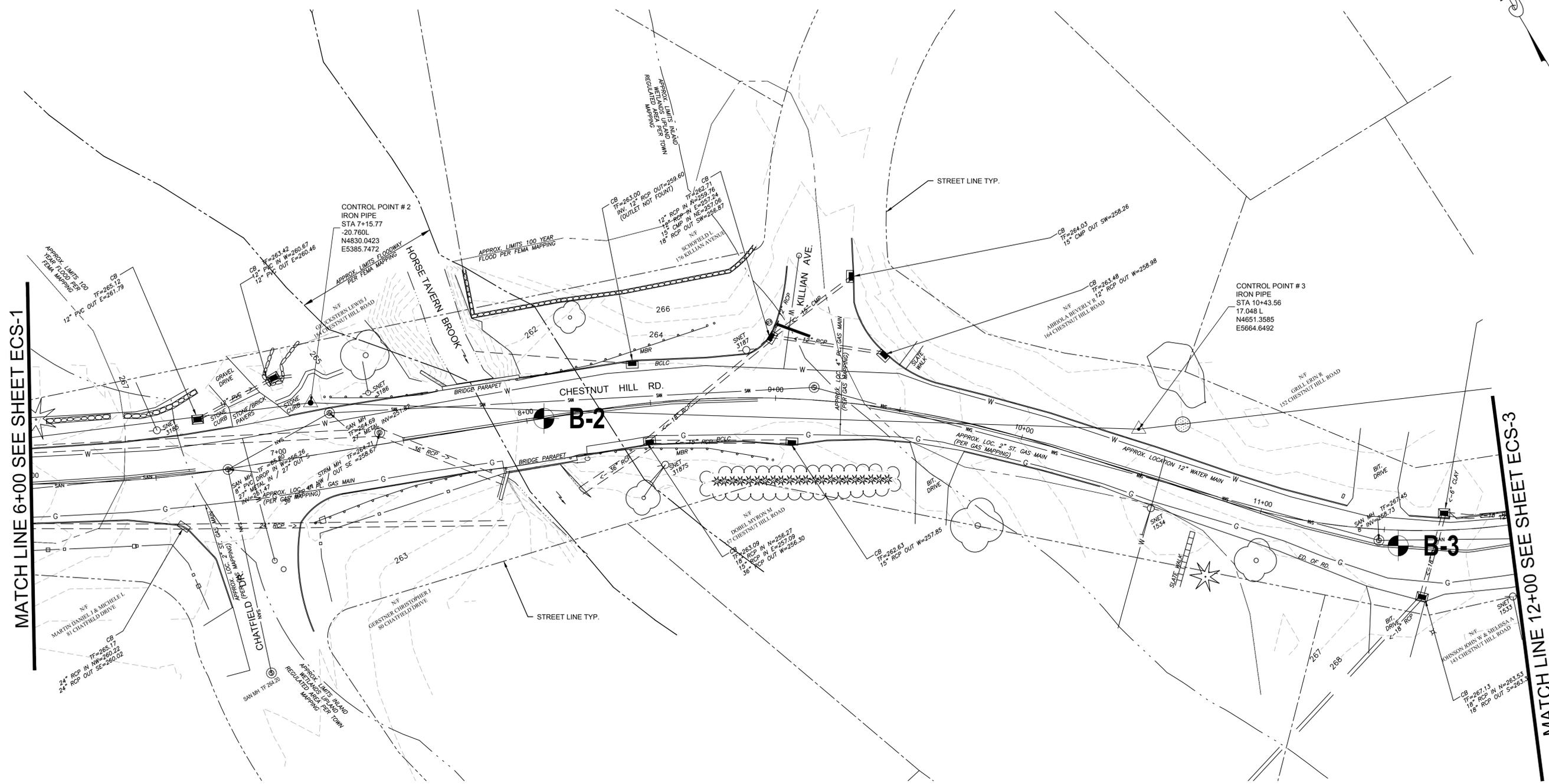
ISSUE DATE 1/16/2019

SHEET NO. ECS-1
4 OF 47



MATCH LINE 6+00 SEE SHEET ECS-1

MATCH LINE 12+00 SEE SHEET ECS-3



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B#
 APPROXIMATE LOCATION OF BORING
 REFER TO GEOTECHNICAL REPORT PREPARED
 BY GEI FOR BORING LOGS

NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

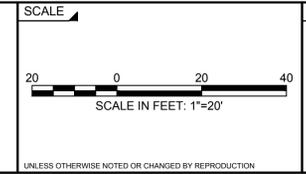
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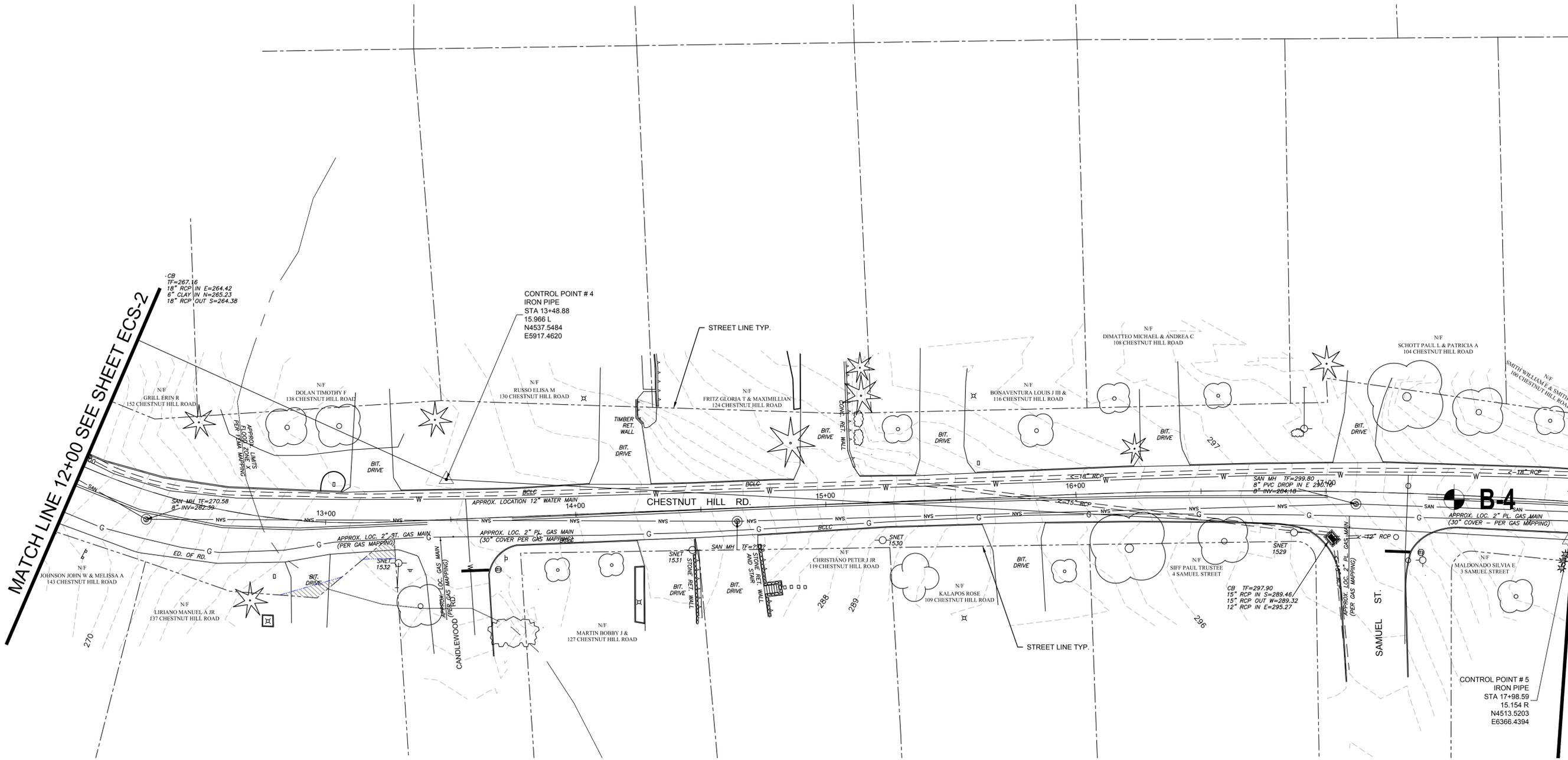
TITLE

CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
 EXISTING CONDITIONS PLAN

BETA JOB NO. 5658
 ISSUE DATE 1/16/2019
 SHEET NO. ECS-2
 5 OF 47

1/17/2019 9:00 AM H:\5000\5658 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\6668-EXISTINGCONDITIONS.DWG

1/17/2019 9:00 AM H:\5000\6568 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\6568-EXISTING\CONDITIONS.DWG



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B-#
 APPROXIMATE LOCATION OF BORING
 REFER TO GEOTECHNICAL REPORT PREPARED
 BY GEI FOR BORING LOGS

NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

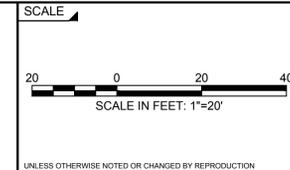
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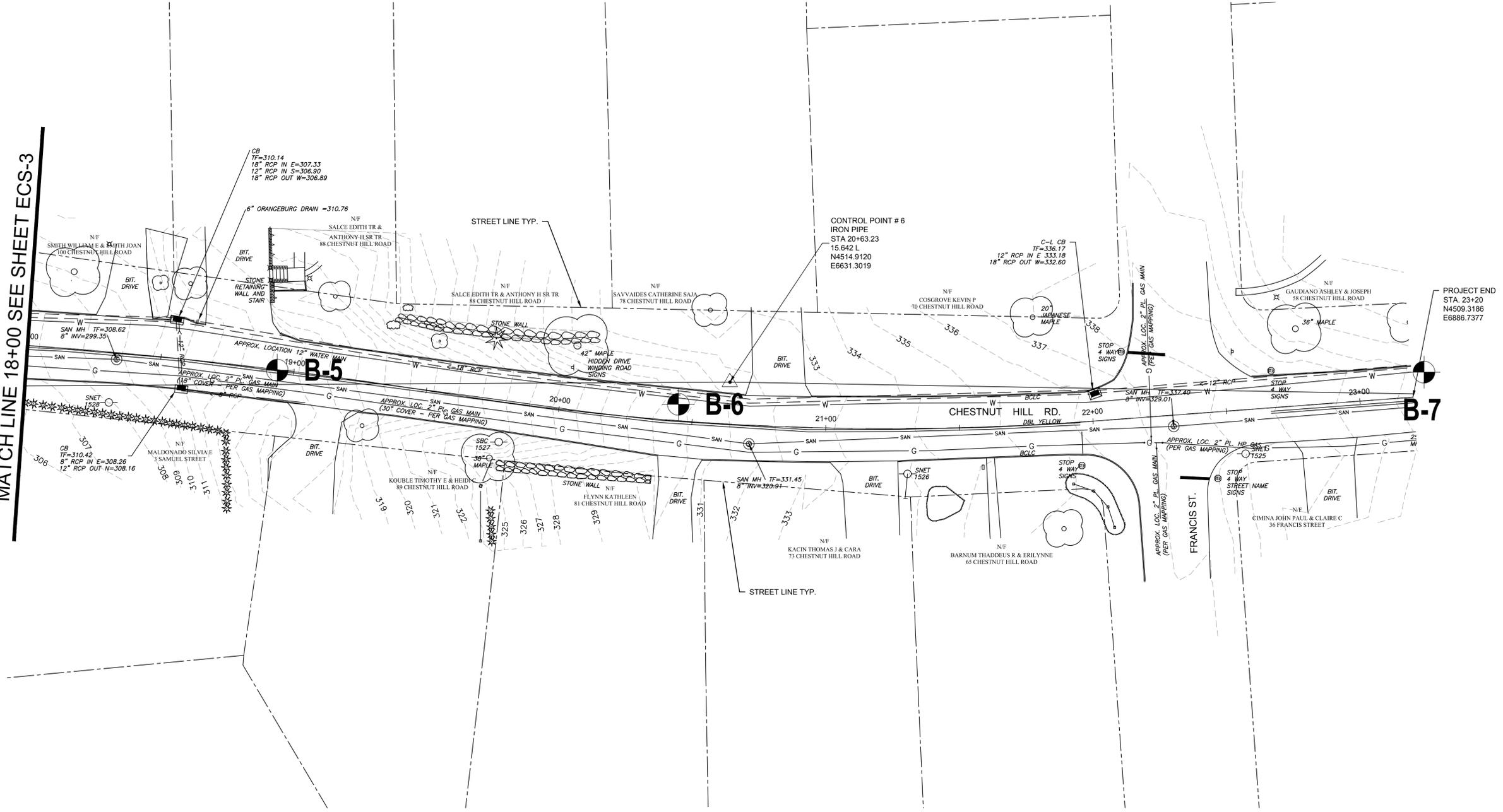
TITLE

CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
 EXISTING CONDITIONS PLAN

BETA JOB NO. 5658
 ISSUE DATE 1/16/2019
 SHEET NO. ECS-3
 6 OF 47



MATCH LINE 18+00 SEE SHEET ECS-3



PROJECT END
STA. 23+20
N4509.3186
E6886.7377

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B-#
APPROXIMATE LOCATION OF BORING
REFER TO GEOTECHNICAL REPORT PREPARED
BY GEI FOR BORING LOGS

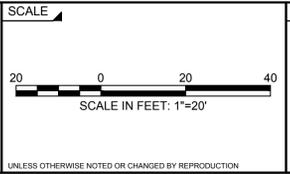
NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

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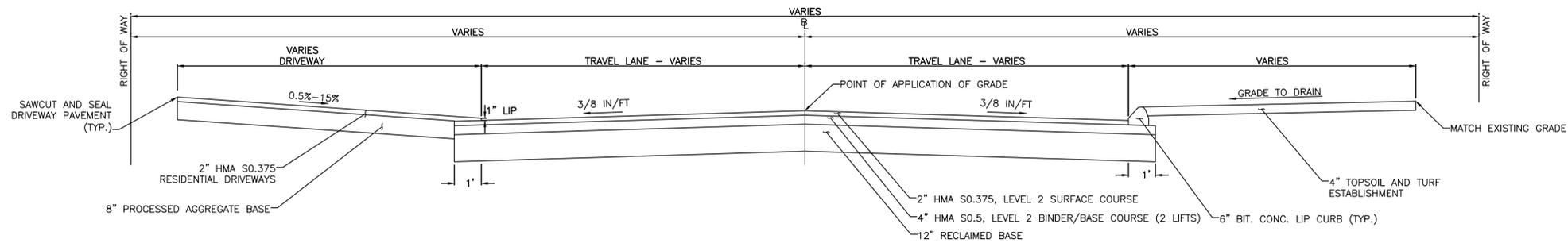


TITLE

**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
EXISTING CONDITIONS PLAN**

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. **ECS-4**
7 OF 47

1/17/2019 9:00 AM H:\5000\5658 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\5658-EXISTINGCONDITIONS.DWG



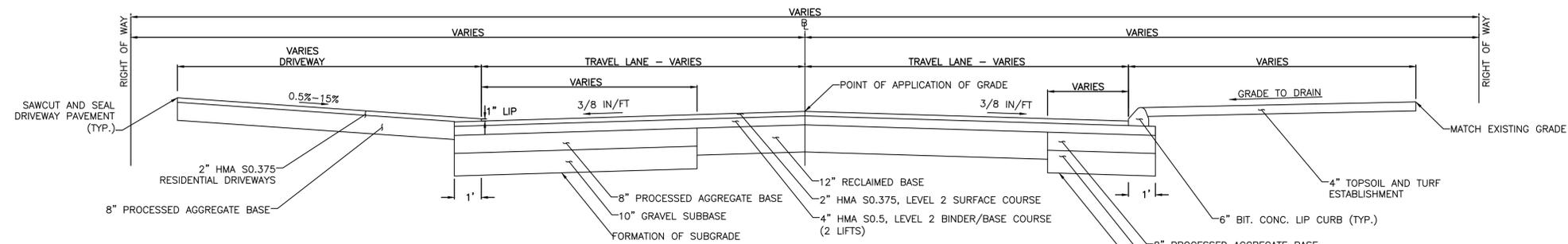
PARTIAL DEPTH RECONSTRUCTION - RECLAIM

STA: 0+54 TO STA: 7+68*
 STA: 8+17 TO STA: 10+81
 STA: 12+93 TO STA: 14+00
 STA: 17+00 TO STA: 19+50
 STA: 22+00 TO STA: 23+20

NOT TO SCALE

NOTE: DEPTH OF RECLAMATION BELOW EXISTING GRADE = ±15".

* STA: 0+59 TO 1+02 - BITUMINOUS ISLAND NOT SHOWN. REFER TO PLANS AND CROSS SECTIONS FOR LOCATION AND ADDITIONAL INFORMATION.



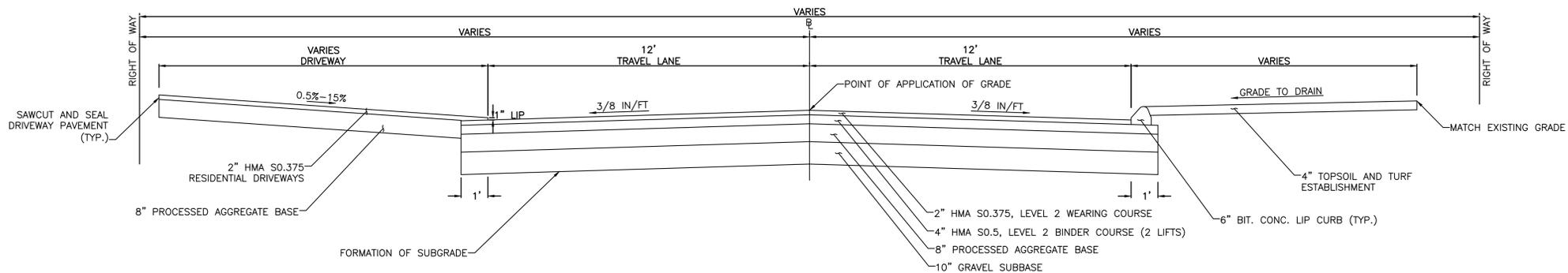
**COMBINATION SECTION
 PARTIAL DEPTH RECONSTRUCTION - RECLAIM
 WITH FULL DEPTH RECONSTRUCTION**

STA: 10+81 TO STA: 12+93
 STA: 14+00 TO STA: 17+00

NOT TO SCALE

NOTES:

1. DEPTH OF RECLAMATION BELOW EXISTING GRADE = ±15".
2. FOR AREAS WHERE ROADWAY IS WIDENED OR HORIZONTAL ALIGNMENT HAS BEEN MODIFIED THE LIMITS OF THE EXISTING ROADWAY.



FULL DEPTH RECONSTRUCTION

STA: 19+50 TO STA: 22+00

NOT TO SCALE

1/17/2019 9:01 AM H:\5000\6568 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\6568-TYPICAL SECTION.DWG

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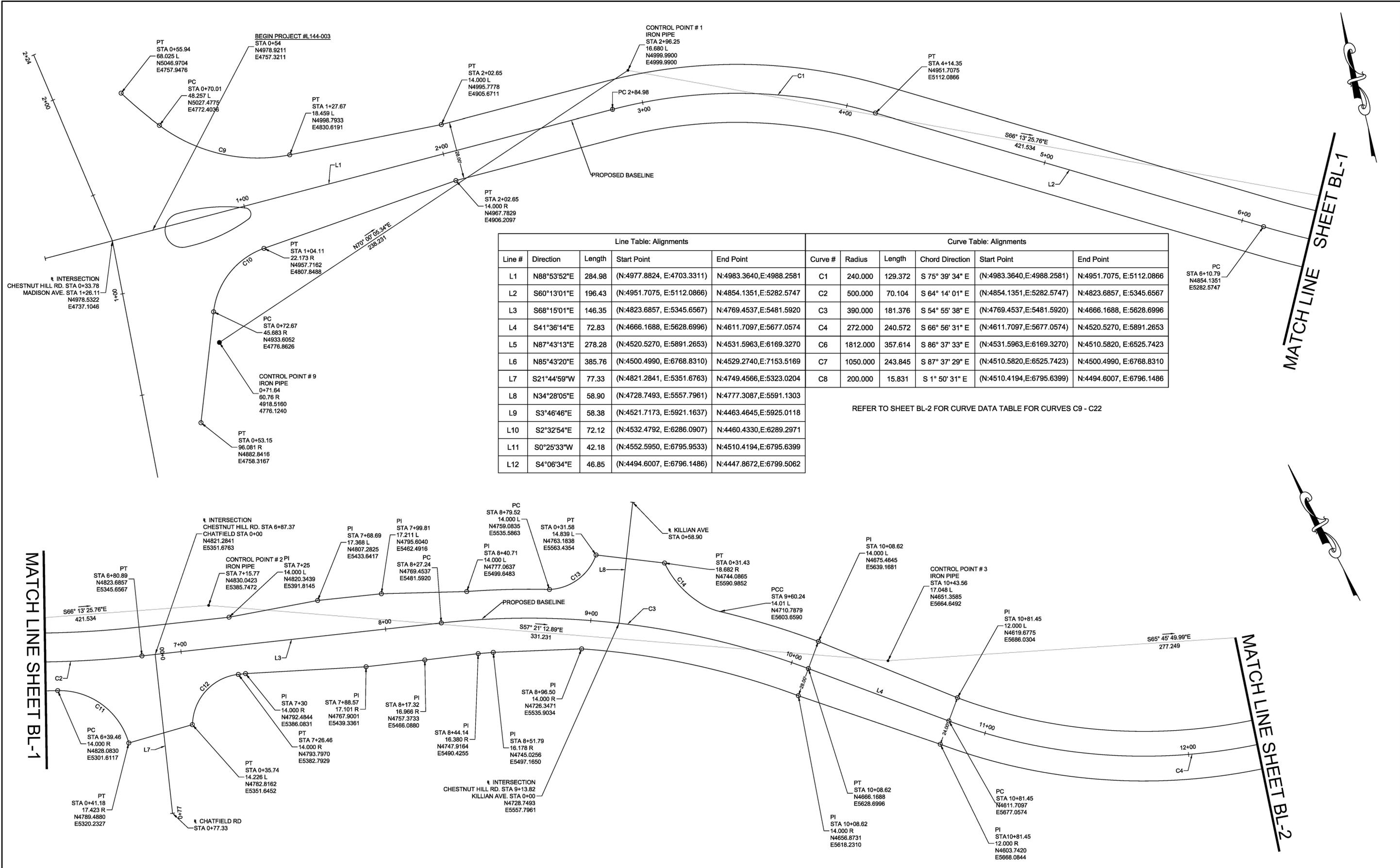
SCALE
AS SHOWN

TITLE
**CHESTNUT HILL ROAD RECONSTRUCTION
 TRUMBULL, CT
 TYPICAL SECTIONS**

BETA JOB NO. 5658
 ISSUE DATE 1/16/2019
 SHEET NO. TS
 8 OF 47

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1/17/2019 9:01 AM H:\5000\6568 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\CIVIL\BL-1\6568 BASELINE.DWG



Line Table: Alignments					Curve Table: Alignments					
Line #	Direction	Length	Start Point	End Point	Curve #	Radius	Length	Chord Direction	Start Point	End Point
L1	N88°53'52"E	284.98	(N:4977.8824, E:4703.3311)	N:4983.3640, E:4988.2581	C1	240.000	129.372	S 75° 39' 34" E	(N:4983.3640, E:4988.2581)	N:4951.7075, E:5112.0866
L2	S60°13'01"E	196.43	(N:4951.7075, E:5112.0866)	N:4854.1351, E:5282.5747	C2	500.000	70.104	S 64° 14' 01" E	(N:4854.1351, E:5282.5747)	N:4823.6857, E:5345.6567
L3	S68°15'01"E	146.35	(N:4823.6857, E:5345.6567)	N:4769.4537, E:5481.5920	C3	390.000	181.376	S 54° 55' 38" E	(N:4769.4537, E:5481.5920)	N:4666.1688, E:5628.6996
L4	S41°36'14"E	72.83	(N:4666.1688, E:5628.6996)	N:4611.7097, E:5677.0574	C4	272.000	240.572	S 66° 56' 31" E	(N:4611.7097, E:5677.0574)	N:4520.5270, E:5891.2653
L5	N87°43'13"E	278.28	(N:4520.5270, E:5891.2653)	N:4531.5963, E:6169.3270	C6	1812.000	357.614	S 86° 37' 33" E	(N:4531.5963, E:6169.3270)	N:4510.5820, E:6525.7423
L6	N85°43'20"E	385.76	(N:4500.4990, E:6768.8310)	N:4529.2740, E:7153.5169	C7	1050.000	243.845	S 87° 37' 29" E	(N:4510.5820, E:6525.7423)	N:4500.4990, E:6768.8310
L7	S21°44'59"W	77.33	(N:4821.2841, E:5351.6763)	N:4749.4566, E:5323.0204	C8	200.000	15.831	S 1° 50' 31" E	(N:4510.4194, E:6795.6399)	N:4494.6007, E:6796.1486
L8	N34°28'05"E	58.90	(N:4728.7493, E:5557.7961)	N:4777.3087, E:5591.1303						
L9	S3°46'46"E	58.38	(N:4521.7173, E:5921.1637)	N:4463.4645, E:5925.0118						
L10	S2°32'54"E	72.12	(N:4532.4792, E:6286.0907)	N:4460.4330, E:6289.2971						
L11	S0°25'33"W	42.18	(N:4552.5950, E:6795.9533)	N:4510.4194, E:6795.6399						
L12	S4°06'34"E	46.85	(N:4494.6007, E:6796.1486)	N:4447.8672, E:6799.5062						

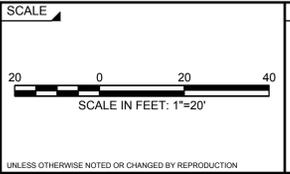
REFER TO SHEET BL-2 FOR CURVE DATA TABLE FOR CURVES C9 - C22

NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

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M.M.
DESIGNED BY:
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CHECKED BY:
J.B.

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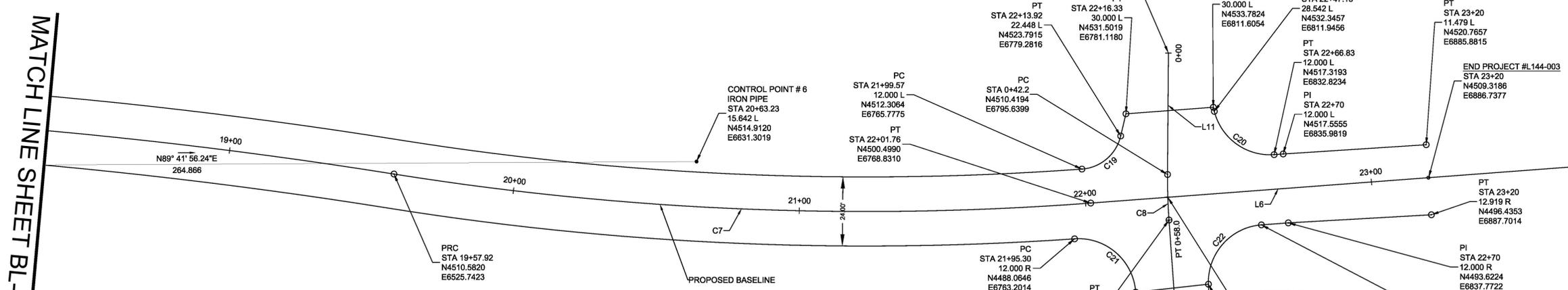
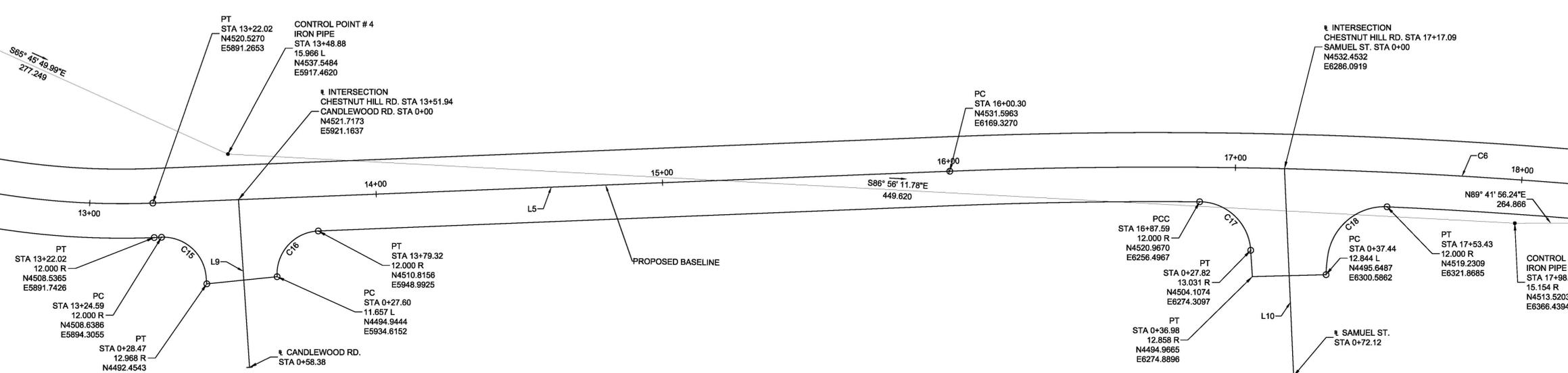


TITLE
CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
BASELINE & LAYOUT PLAN

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. BL-1
9 OF 47

MATCH LINE SHEET BL-1

MATCH LINE SHEET BL-2



Curve Table				
CURVE #	LENGTH	RADIUS	DELTA	TANGENT
C9	66.82	80.00	047.86	35.50
C10	20.78	37.00	032.18	10.67
C11	46.12	35.00	075.49	27.10
C12	34.05	24.99	078.07	20.26
C13	29.88	25.13	068.11	16.99
C14	24.96	46.65	030.66	12.79
C15	22.64	15.00	086.48	14.11
C16	23.85	15.00	091.10	15.29
C17	26.98	18.00	085.89	16.75
C18	29.09	20.00	083.34	17.80
C19	18.97	15.00	072.44	10.99
C20	27.94	20.00	080.06	16.80
C21	31.37	20.00	089.86	19.95
C22	30.58	20.00	087.61	19.18

REFER TO SHEET BL-1 FOR CURVE DATA TABLE FOR CURVES C3-C4, C6-C8 AND ALIGNMENT LINE DATA

1/17/2019 9:01 AM H:\5000\6568 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\CIVIL\3D\6568 BASELINE.DWG

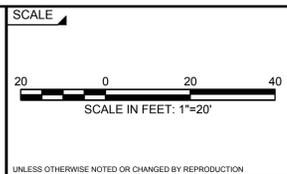
NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

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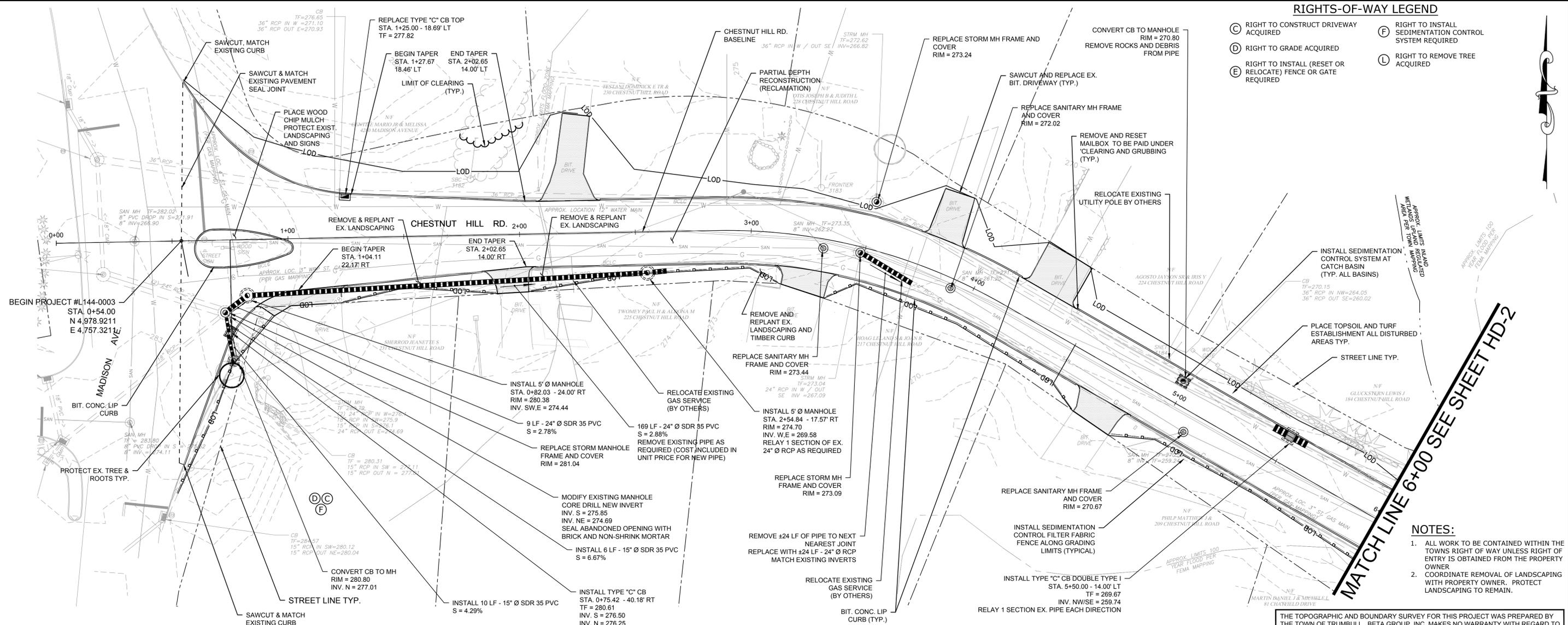


TITLE
CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
BASELINE & LAYOUT PLAN

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. BL-2
10 OF 47

RIGHTS-OF-WAY LEGEND

- (C) RIGHT TO CONSTRUCT DRIVEWAY ACQUIRED
- (D) RIGHT TO GRADE ACQUIRED
- (E) RIGHT TO INSTALL (RESET OR RELOCATE) FENCE OR GATE REQUIRED
- (F) RIGHT TO INSTALL SEDIMENTATION CONTROL SYSTEM REQUIRED
- (L) RIGHT TO REMOVE TREE ACQUIRED

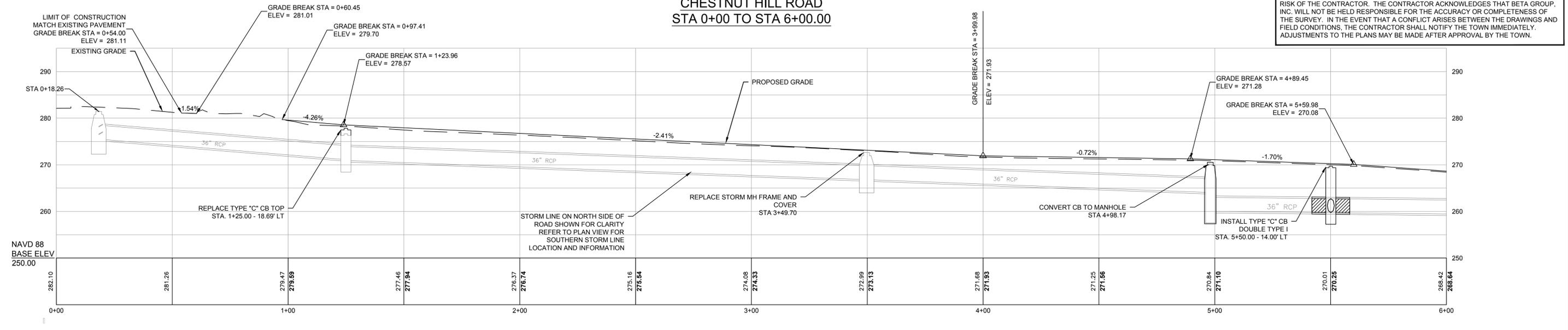


MATCH LINE 6+00 SEE SHEET HD-2

- NOTES:**
- ALL WORK TO BE CONTAINED WITHIN THE TOWNS RIGHT OF WAY UNLESS RIGHT OF ENTRY IS OBTAINED FROM THE PROPERTY OWNER.
 - COORDINATE REMOVAL OF LANDSCAPING WITH PROPERTY OWNER. PROTECT LANDSCAPING TO REMAIN.

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**CHESTNUT HILL ROAD
STA 0+00 TO STA 6+00.00**



1/17/2019 9:02 AM H:\50005658 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\6658-PLAN&PROFILE.DWG

NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

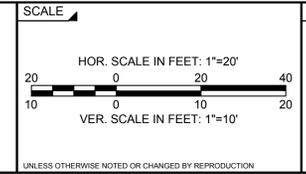
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DESIGNED BY:	R.S.
CHECKED BY:	J.B.

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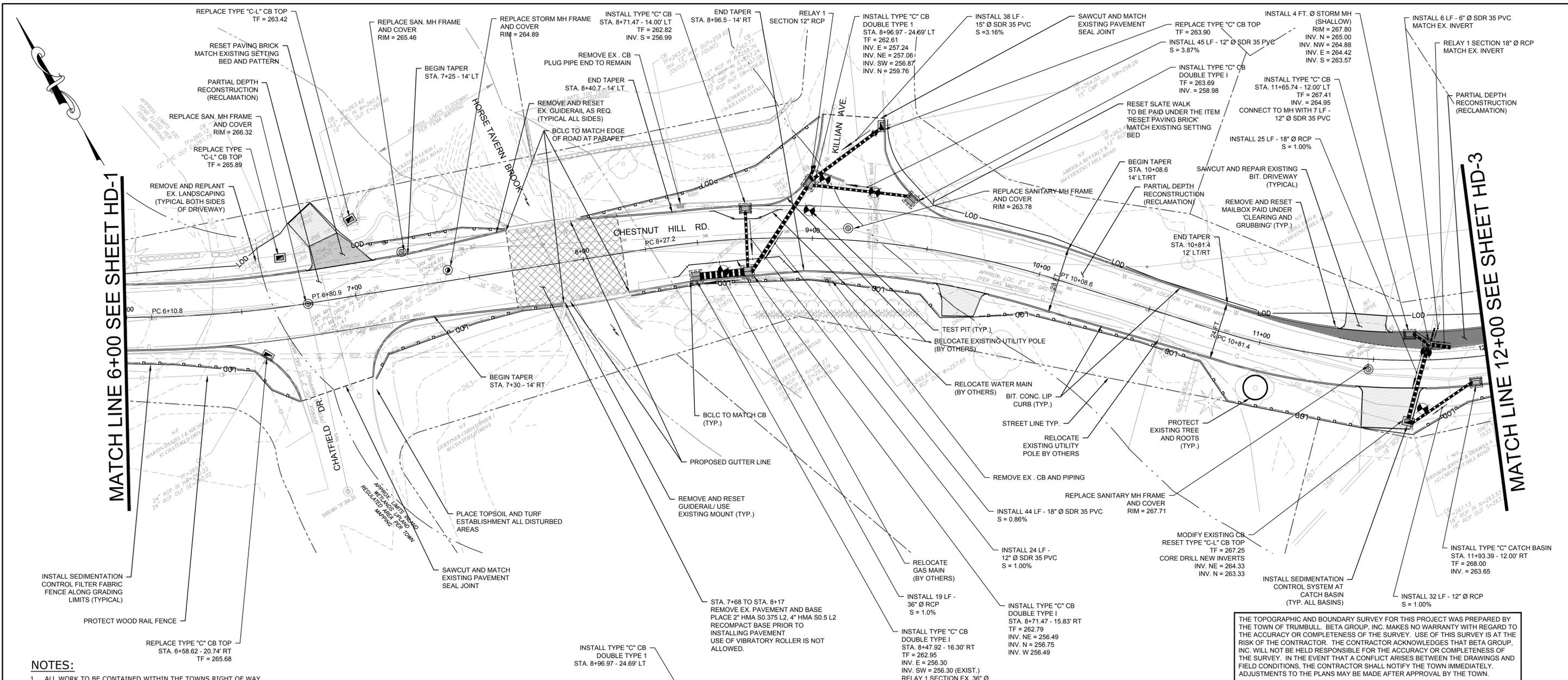
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**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
PLAN & PROFILE**

BETA JOB NO.	5658
ISSUE DATE	1/16/2019
SHEET NO.	HD-1 11 OF 47

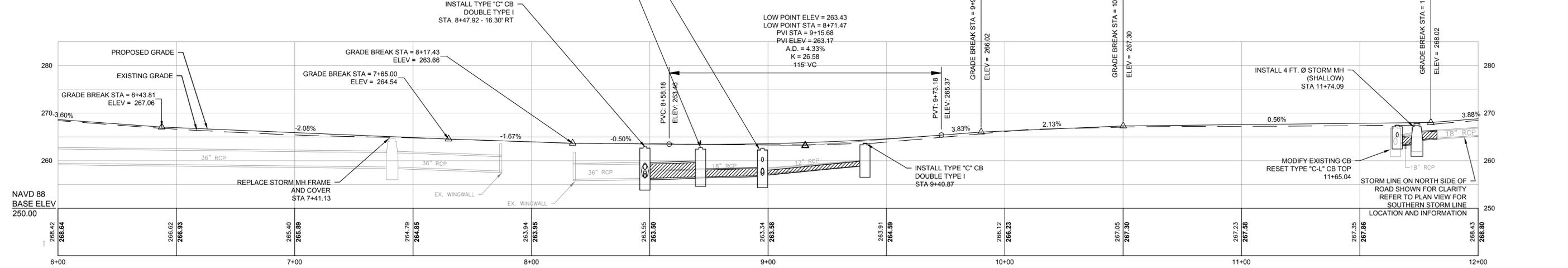
1/17/2019 9:02 AM H:\5000\5658 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\6658-PLAN&PROFILE.DWG



- NOTES:**
1. ALL WORK TO BE CONTAINED WITHIN THE TOWNS RIGHT OF WAY UNLESS RIGHT OF ENTRY IS OBTAINED FROM THE PROPERTY OWNER
 2. COORDINATE REMOVAL OF LANDSCAPING WITH PROPERTY OWNER. PROTECT LANDSCAPING TO REMAIN.

THE TOPOGRAPHIC AND BOUNDARY SURVEY FOR THIS PROJECT WAS PREPARED BY THE TOWN OF TRUMBULL. BETA GROUP, INC. MAKES NO WARRANTY WITH REGARD TO THE ACCURACY OR COMPLETENESS OF THE SURVEY. USE OF THIS SURVEY IS AT THE RISK OF THE CONTRACTOR. THE CONTRACTOR ACKNOWLEDGES THAT BETA GROUP, INC. WILL NOT BE HELD RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF THE SURVEY. IN THE EVENT THAT A CONFLICT ARISES BETWEEN THE DRAWINGS AND FIELD CONDITIONS, THE CONTRACTOR SHALL NOTIFY THE TOWN IMMEDIATELY. ADJUSTMENTS TO THE PLANS MAY BE MADE AFTER APPROVAL BY THE TOWN.

**CHESTNUT HILL ROAD
STA 6+00.00 TO STA 12+00.00**



NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

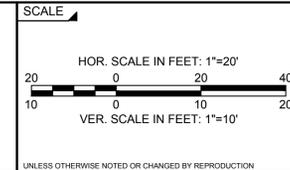
DRAWN BY:
J.D.

DESIGNED BY:
R.S.

CHECKED BY:
J.B.

REGISTERED PROFESSIONAL
PREPARED BY
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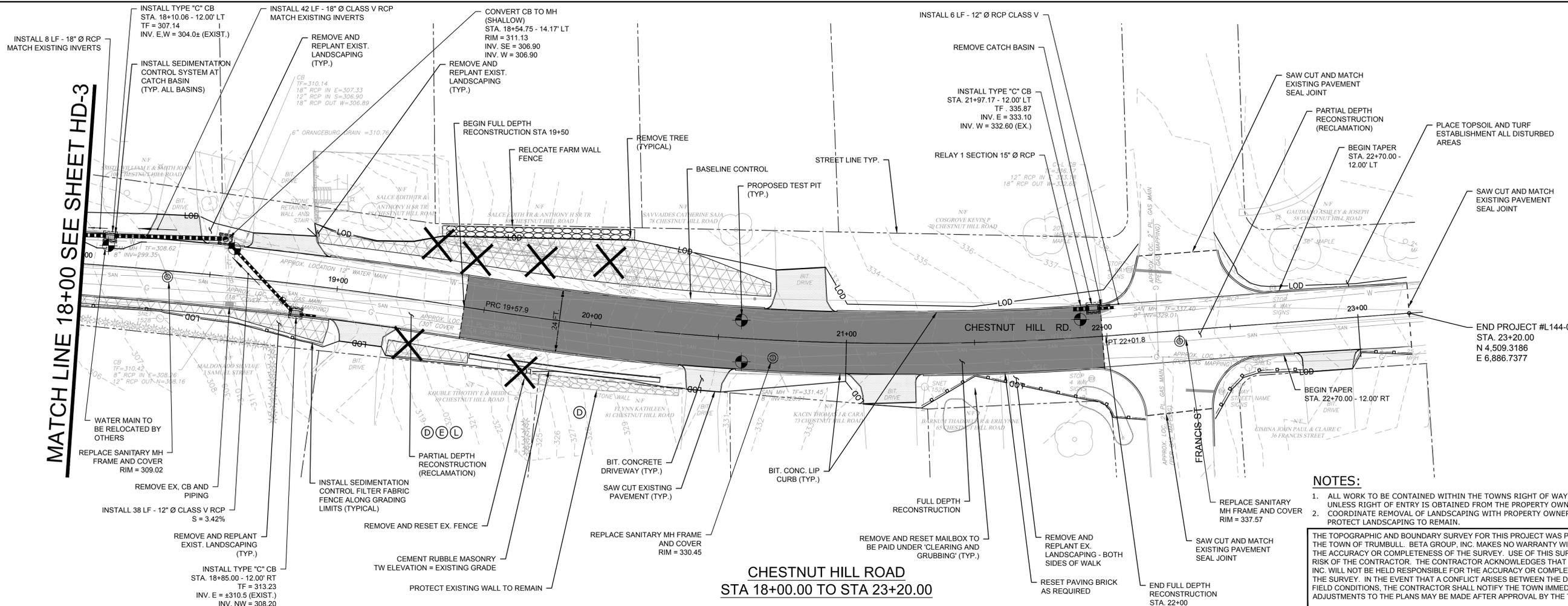


**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
PLAN & PROFILE**

BETA JOB NO. 5658

ISSUE DATE 1/16/2019

SHEET NO. **HD-2**
12 OF 47



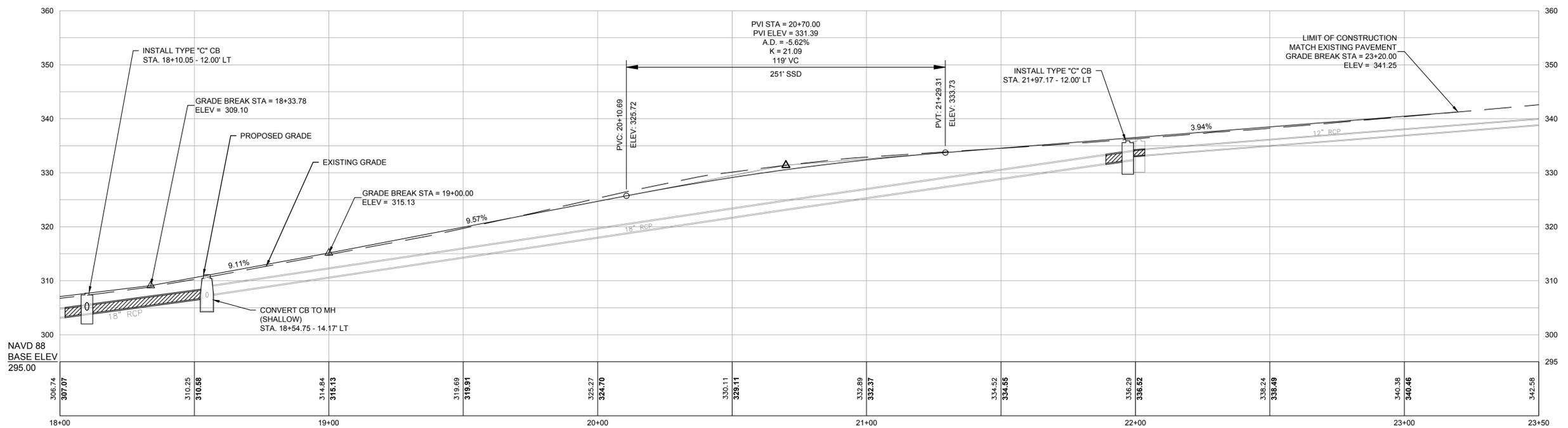
MATCH LINE 18+00 SEE SHEET HD-3

NOTES:

1. ALL WORK TO BE CONTAINED WITHIN THE TOWNS RIGHT OF WAY UNLESS RIGHT OF ENTRY IS OBTAINED FROM THE PROPERTY OWNER
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**CHESTNUT HILL ROAD
STA 18+00.00 TO STA 23+20.00**



1/17/2019 9:02 AM H:\5000\5658 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\6658-PLAN&PROFILE.DWG

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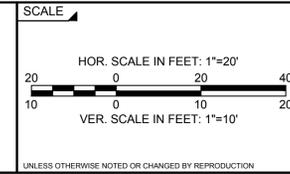
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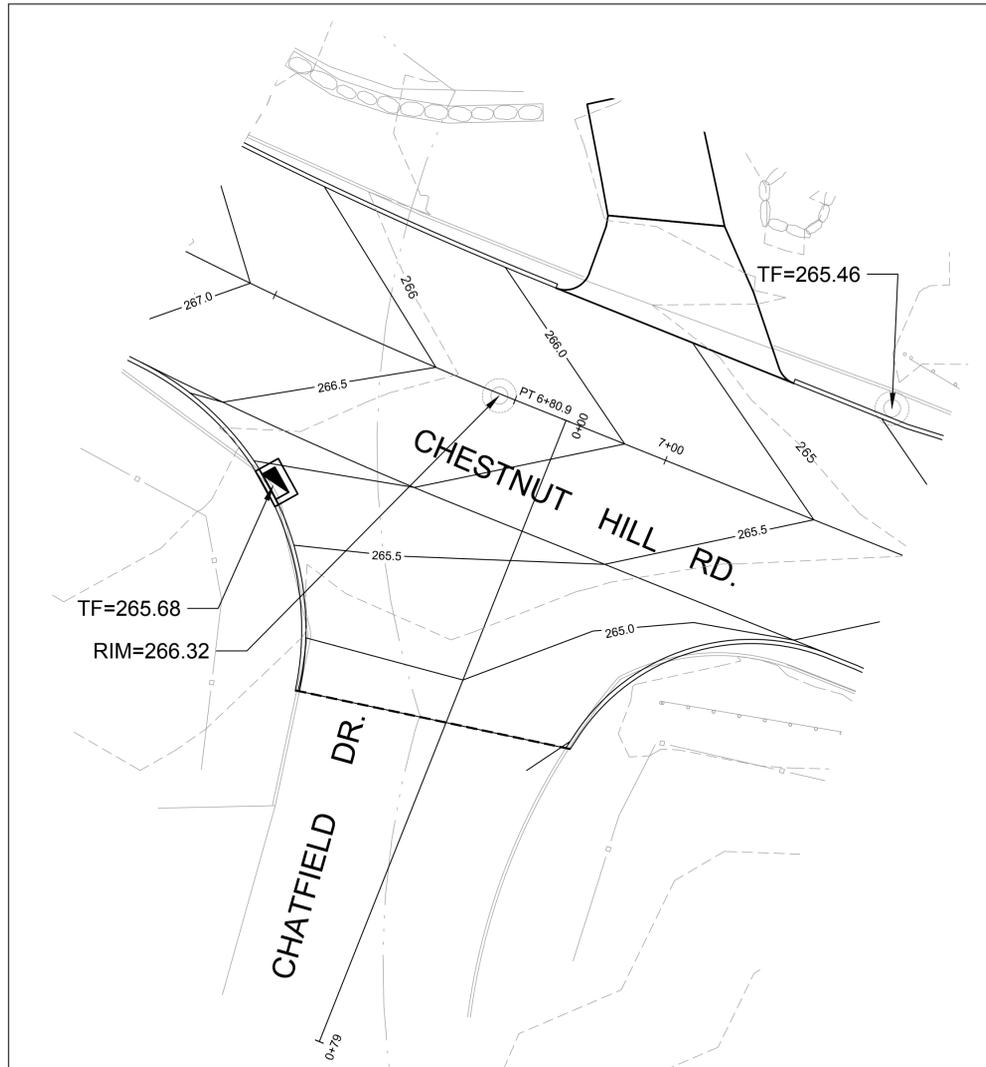
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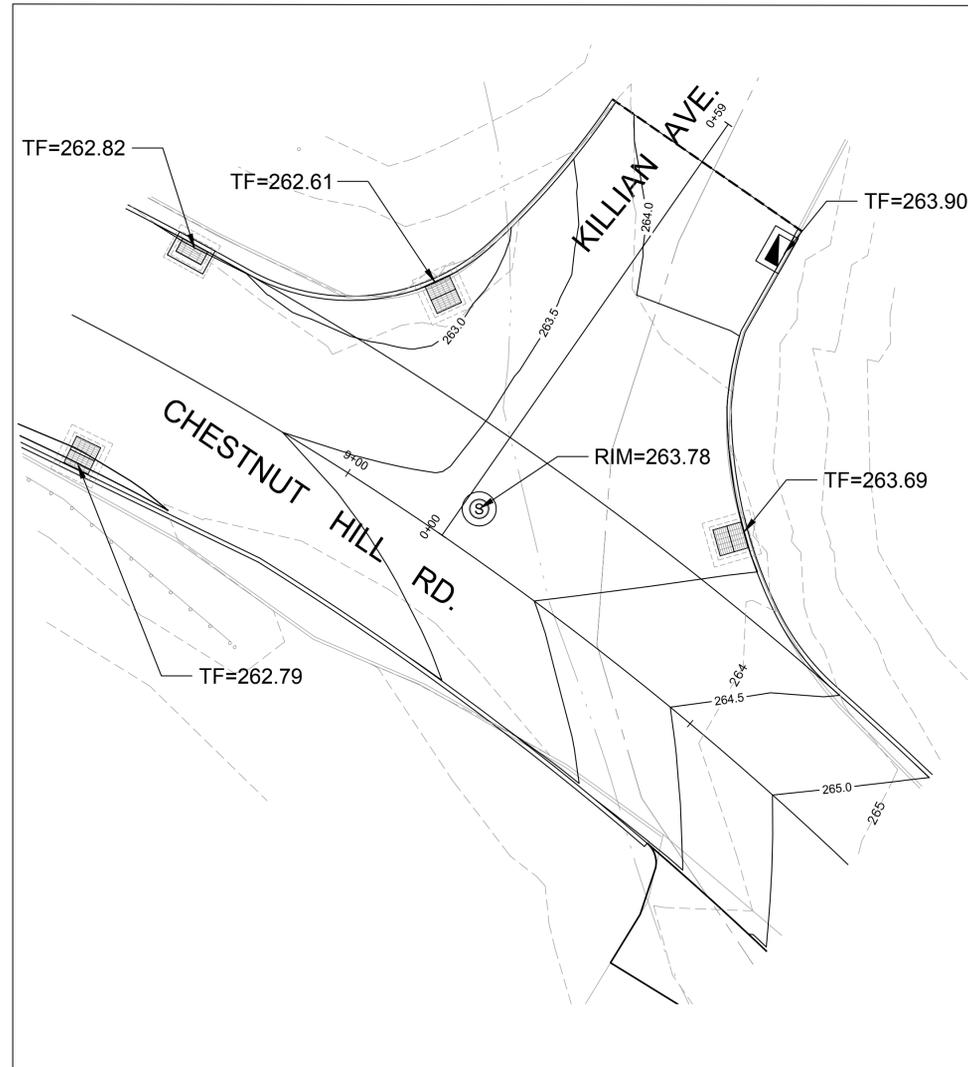


**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
PLAN & PROFILE**

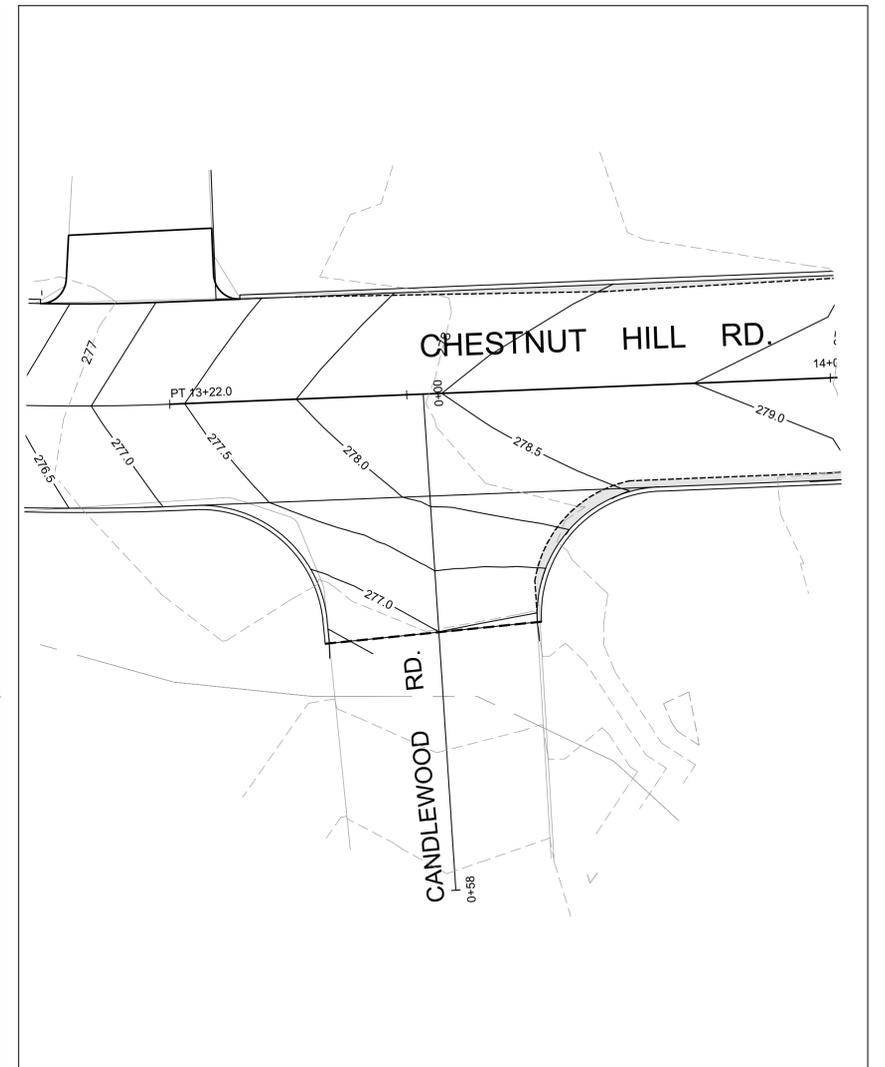
BETA JOB NO.	5658
ISSUE DATE	1/16/2019
SHEET NO.	HD-4 14 OF 47



CHESTNUT HILL ROAD
&
CHATFIELD DRIVE



CHESTNUT HILL ROAD
&
KILLIAN AVENUE



CHESTNUT HILL ROAD
&
CANDLEWOOD ROAD

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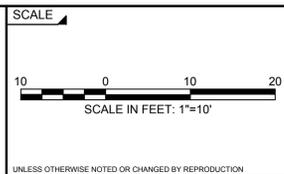
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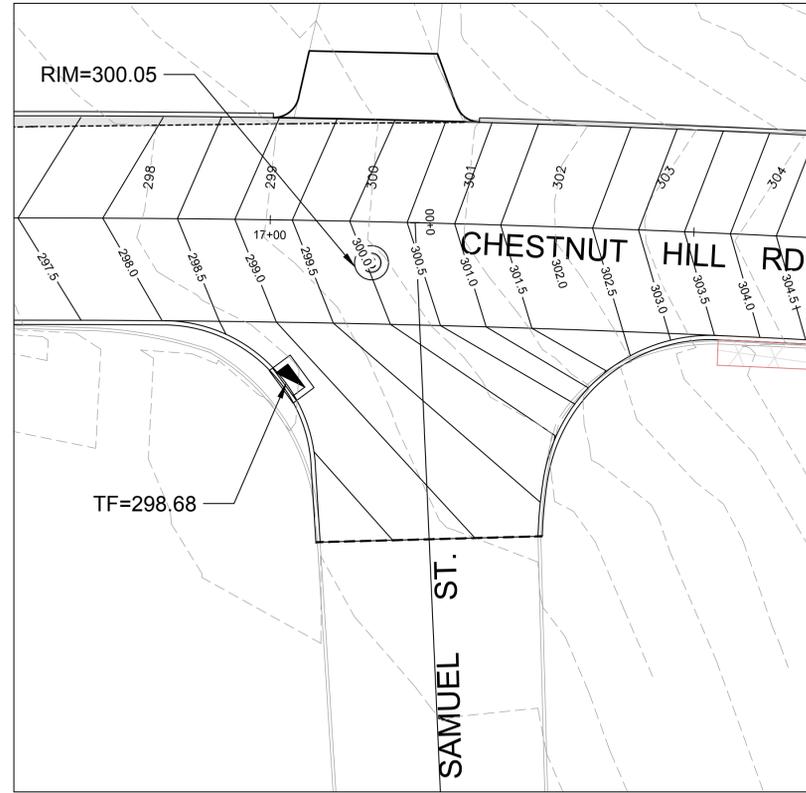
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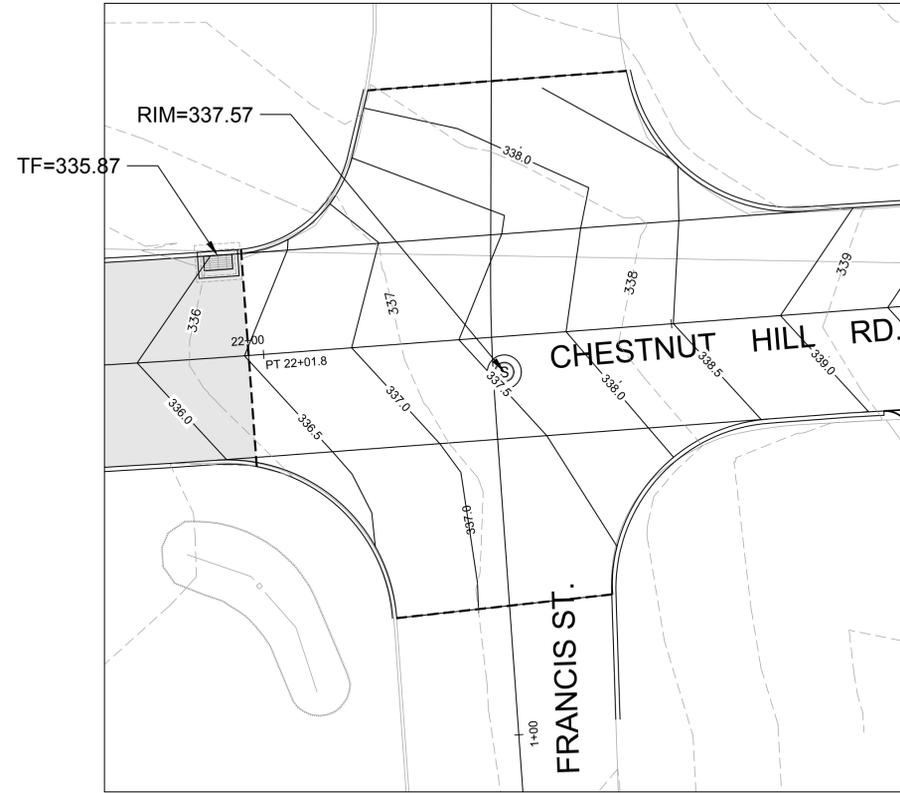
TITLE
CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
INTERSECTION GRADING
PLAN

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. GP-1
15 OF 47

1/17/2019 9:03 AM H:\56006568 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\CIVIL\DESIGN\GP-1.DWG



CHESTNUT HILL ROAD
&
SAMUEL STREET



CHESTNUT HILL ROAD
&
FRANCIS STREET

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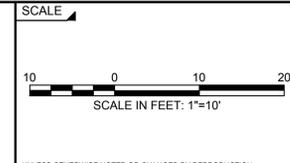
NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

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J.B.

REGISTERED PROFESSIONAL

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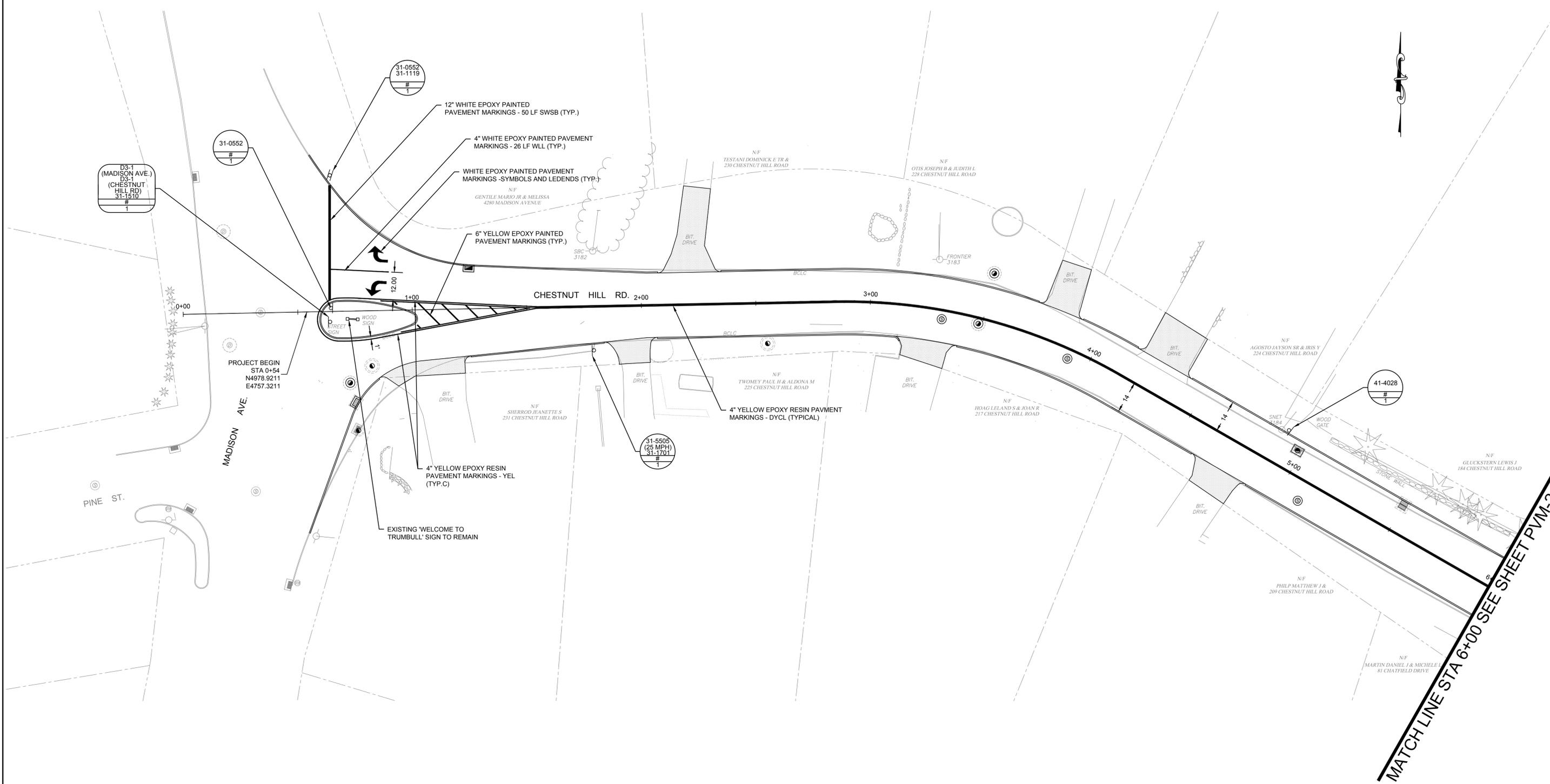


TITLE
CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
INTERSECTION GRADING
PLAN

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. GP-2
16 OF 47

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1/17/2019 9:03 AM H:\5000\668 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\668-PAVEMENT\MARKINGS.DWG



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SEE SHEET PVM-4 FOR PAVEMENT MARKINGS AND SIGN NOTES.
SEE SHEET PVM-4 FOR SIGN LEGEND

MATCH LINE STA 6+00 SEE SHEET PVM-2

NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

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DESIGNED BY:
R.S.

CHECKED BY:
J.B.

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SCALE



SCALE IN FEET: 1"=20'

UNLESS OTHERWISE NOTED OR CHANGED BY REPRODUCTION

TITLE

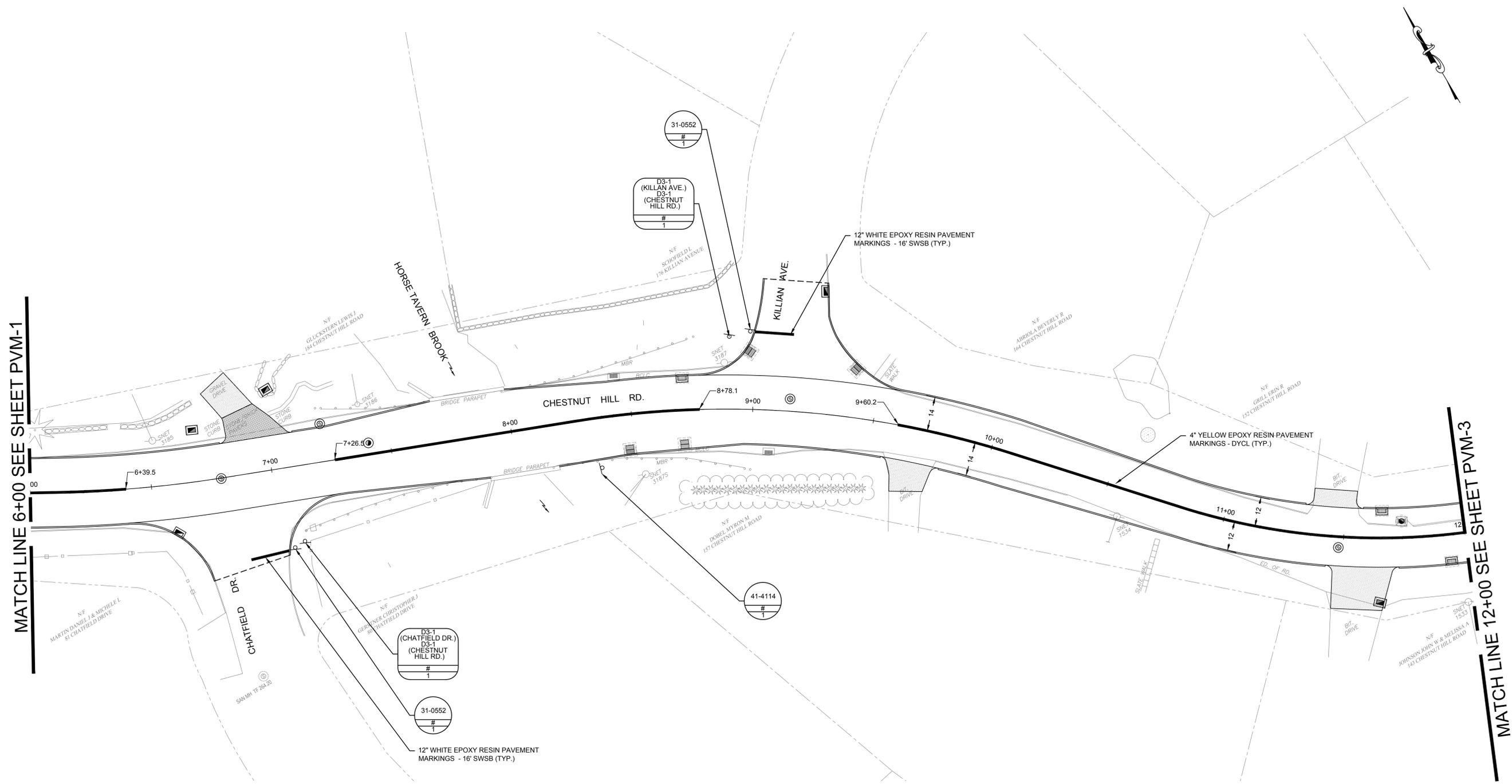
CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
PAVEMENT MARKINGS AND
SIGNAGE PLANS

BETA JOB NO. 5658

ISSUE DATE 1/16/2019

SHEET NO. PVM-1
17 OF 47

1/17/2019 9:03 AM H:\5000\6568 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\6568-PAVEMENT\MARKINGS.DWG



MATCH LINE 6+00 SEE SHEET PVM-1

MATCH LINE 12+00 SEE SHEET PVM-3

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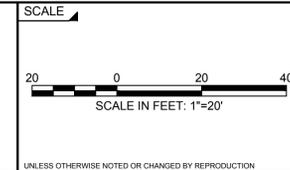
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J.B.

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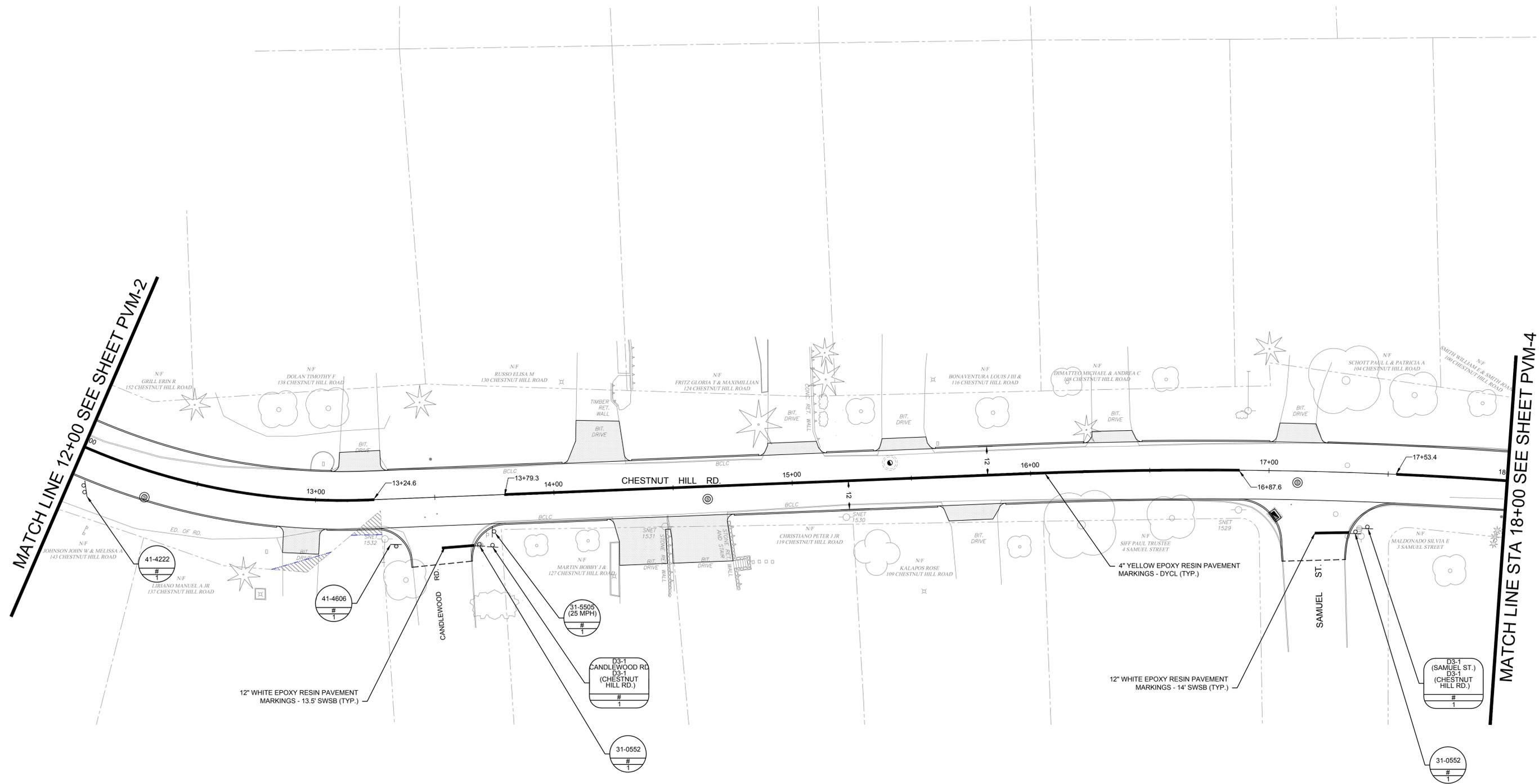
**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
PAVEMENT MARKINGS AND
SIGNAGE PLANS**

BETA JOB NO. 5658

ISSUE DATE 1/16/2019

SHEET NO. PVM-2
18 OF 47

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SEE SHEET PVM-4 FOR PAVEMENT MARKINGS AND SIGN NOTES.
SEE SHEET PVM-4 FOR SIGN LEGEND

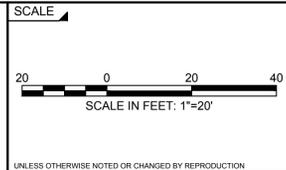
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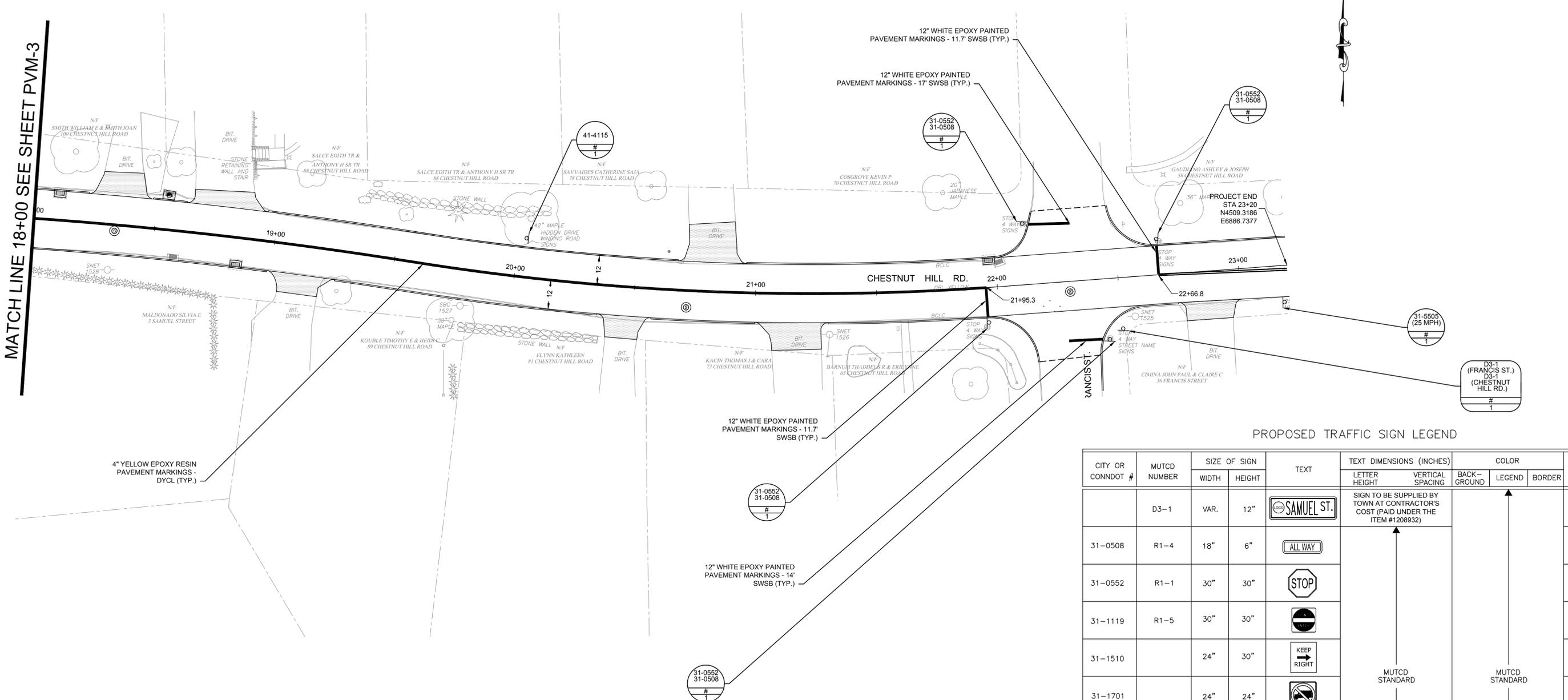
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TITLE
**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
PAVEMENT MARKINGS AND
SIGNAGE PLANS**

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. PVM-3
19 OF 47

MATCH LINE 18+00 SEE SHEET PVM-3

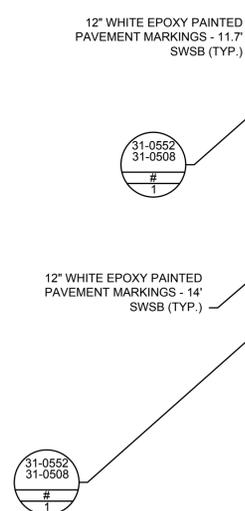


PROPOSED TRAFFIC SIGN LEGEND

CITY OR CONNDOT #	MUTCD NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			COLOR		MOUNTING REQUIREMENTS
		WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	BACK-GROUND	LEGEND	BORDER	
	D3-1	VAR.	12"		SIGN TO BE SUPPLIED BY TOWN AT CONTRACTOR'S COST (PAID UNDER THE ITEM #1208932)					1 Posts
31-0508	R1-4	18"	6"							1 Posts
31-0552	R1-1	30"	30"							1 Posts
31-1119	R1-5	30"	30"							1 Posts
31-1510		24"	30"							1 Posts
31-1701		24"	24"							1 Posts
31-1749		24"	30"							1 Posts
31-5505	R2-1	24"	30"							1 Posts
41-4028	W1-2	30"	30"							1 Posts
41-4114	W1-5	30"	30"							1 Posts
41-4115	W1-5	30"	30"							1 Posts
41-4222	W1-6	48"	24"							2 Posts
41-4606	W14-1	30"	30"							1 Posts

PAVEMENT MARKING AND SIGN NOTES:

- REMOVE ALL EXISTING TRAFFIC SIGNS WITHIN THE PROJECT LIMITS UNLESS OTHERWISE NOTED.
- SIGNS SHALL BE MEASURED FOR PAYMENT UNDER SIGN FACE SHEET ALUMINUM, ITEM #1208931 & #1208932. NEW SIGNS SHALL BE INSTALLED IN THE LOCATIONS SHOWN ON THE DRAWINGS AND SHALL BE PLACED IN ACCORDANCE WITH MUTCD REQUIREMENTS WITH RESPECT TO HEIGHT AND OFFSET FROM CURB.
- ALL FINAL PAVEMENT MARKINGS SHALL BE EPOXY PAINT IN ACCORDANCE WITH CONNDOT FORM 817 SECTION 12.10.02 AND ITEM #1210105 & #1210106.
- MATCH EXISTING PAVEMENT MARKINGS WHERE NEW MARKINGS ARE TO MEET EXISTING MARKINGS AT LIMITS OF CONSTRUCTION.
- STREET NAME SIGNS SHALL BE 12" WIDE WITH 6" LETTERING IN ACCORDANCE WITH MUTCD REQUIREMENTS.
- ALL SIGN POST SHALL BE BREAKAWAY TYPE.
- MOUNTED BRACKETS SHALL BE PROVIDED FOR D3-1 STREET NAME SIGNS AND PAID FOR UNDER ITEM #1208932.



PROPOSED PAVEMENT MARKINGS LEGEND

ABBREVIATION	DESCRIPTION
WLL	WHITE LANE LINE
DYCL	DOUBLE YELLOW CENTER LINE
YEL	YELLOW EDGE LINE
SWSB	SINGLE WHITE STOP BAR

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1/17/2019 9:04 AM H:\5000\668 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\668-PAVEMENT\MARKINGS.DWG

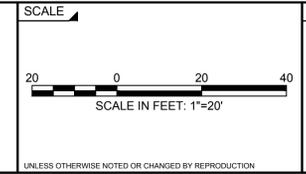
NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

DRAWN BY:
J.D.
DESIGNED BY:
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REGISTERED PROFESSIONAL
PREPARED BY

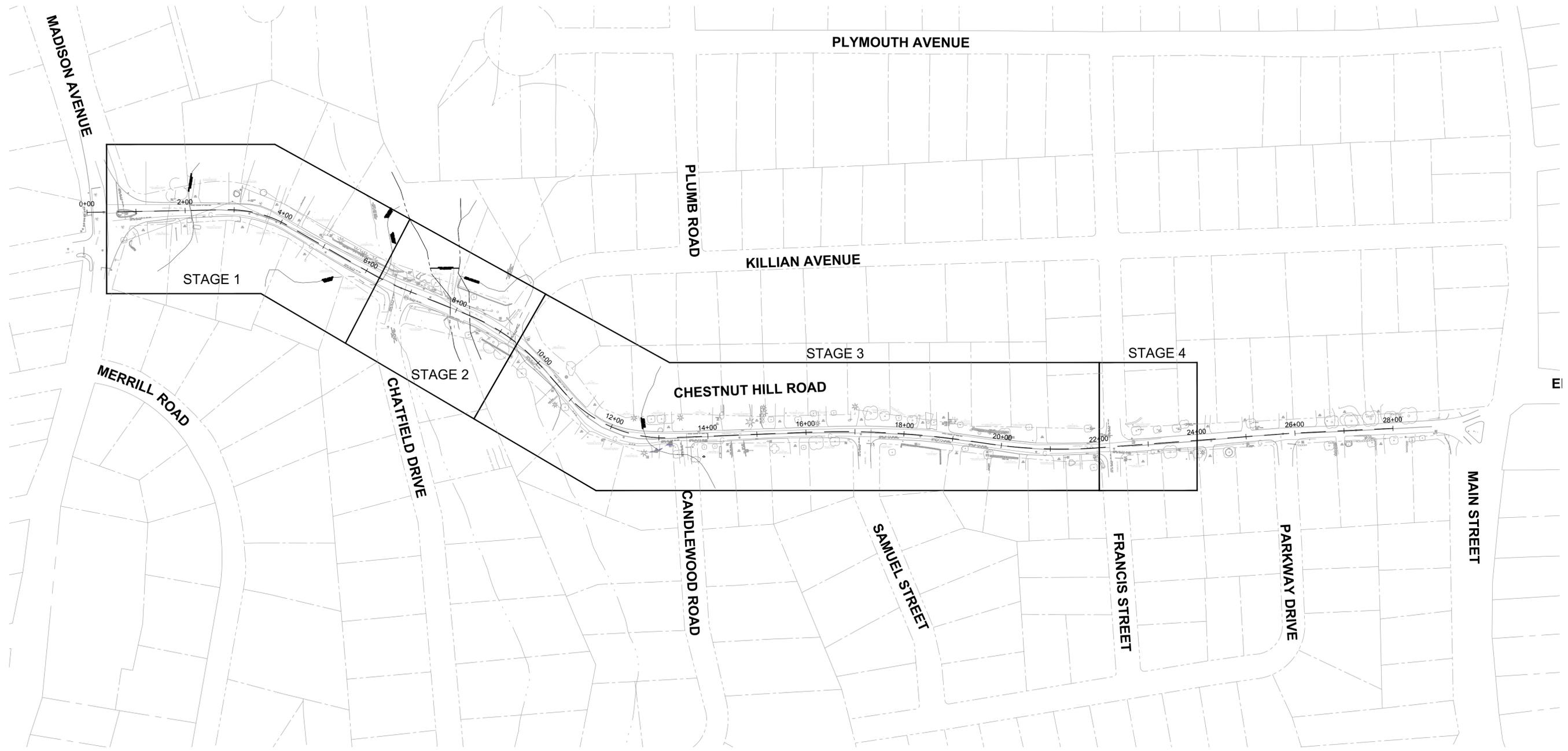


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TITLE
**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
PAVEMENT MARKINGS AND
SIGNAGE PLANS**

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. **PVM-4**
20 OF 47



STAGES OF CONSTRUCTION:
 STAGE 1 - CHESTNUT HILL RD STA. 00+40 TO STA. 06+30
 STAGE 2 - CHESTNUT HILL RD STA. 06+30 TO STA. 09+40
 STAGE 3 - CHESTNUT HILL RD STA. 09+40 TO STA. 22+00
 STAGE 4 - CHESTNUT HILL RD STA. 22+00 TO STA. 24+00

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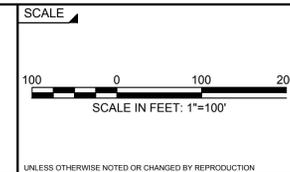
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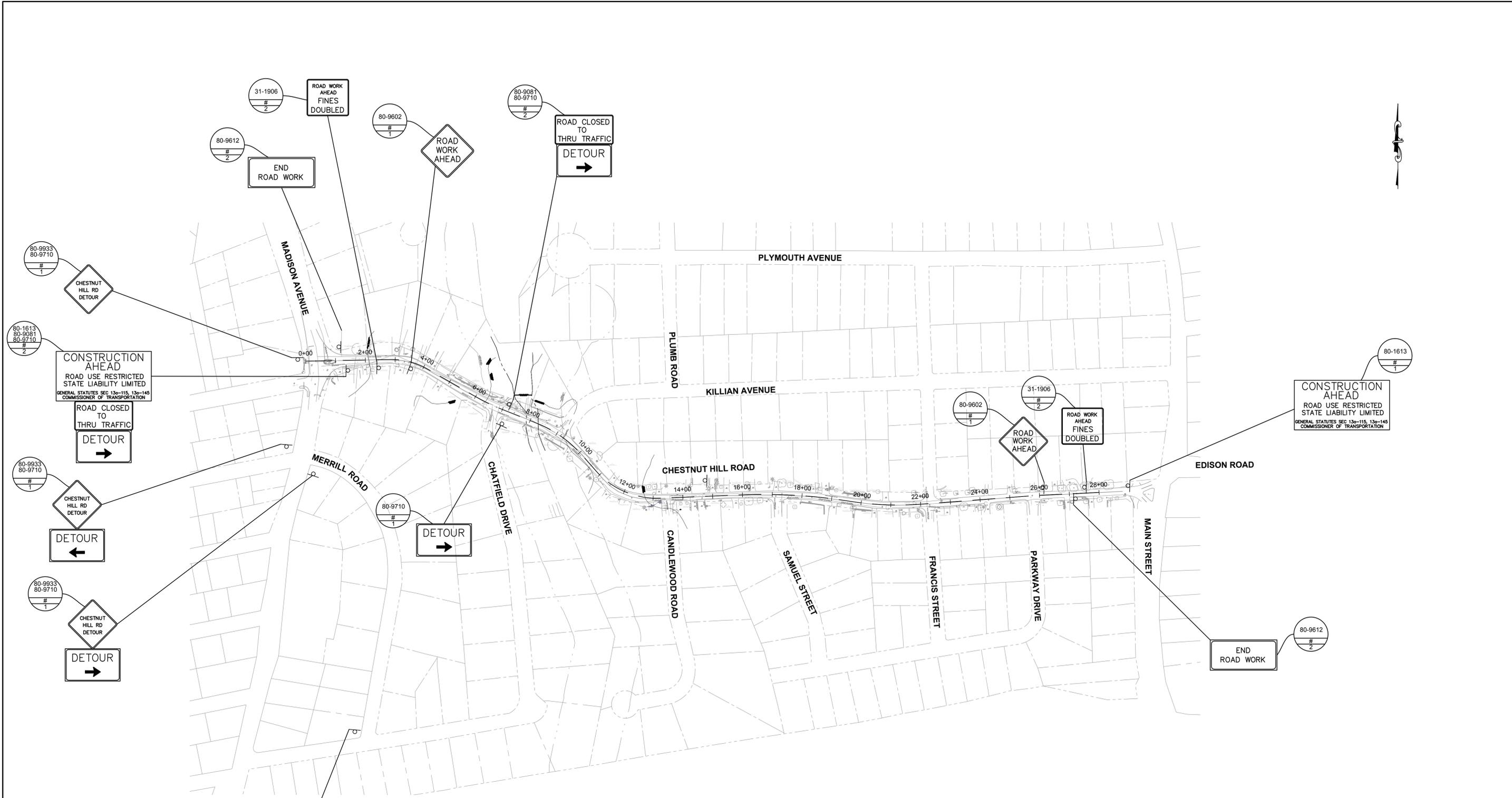
CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
 MAINTENANCE AND PROTECTION OF
 TRAFFIC STAGING PLAN

BETA JOB NO. 5658

ISSUE DATE 1/16/2019

SHEET NO. MPT-1
21 OF 47

1/17/2019 9:04 AM H:\5000\6568 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\6568-DETOUR PLAN.DWG



LEGEND:

-  APPROXIMATE LOCATION OF UNIFORMED FLAGGER
-  TRAFFIC DRUM
-  TYPE III BARRICADE WITH BARRICADE WARNING LIGHT HIGH INTENSITY
-  CTDOT SIGN # / MUTCD SIGN #
PROJECT SIGN ID # / OF SUPPORTS

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:

1. MAINTAIN ALTERNATING ONE-WAY TRAFFIC OPERATION WITHIN THE STAGE 2 AREA DURING CONSTRUCTION
2. COVER OR REMOVE FLAGGER AND "ONE LANE ROAD AHEAD" SIGNS DURING HOURS WHEN WORK IS NOT BEING CONDUCTED
3. ADDITIONAL TRAFFIC DRUMS TO BE PLACED AT THE CHESTNUT HILL RD./ MADISON AVE. AND CHESTNUT HILL RD./ KILLIAN AVE. INTERSECTIONS DURING CONSTRUCTION HOURS. REFER TO SPECIFICATION FOR TRAFFIC DRUM PLACEMENT FOR ALTERNATING ONE-WAY TRAFFIC OPERATIONS.

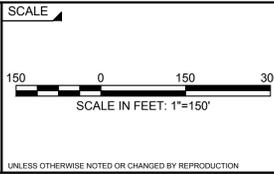
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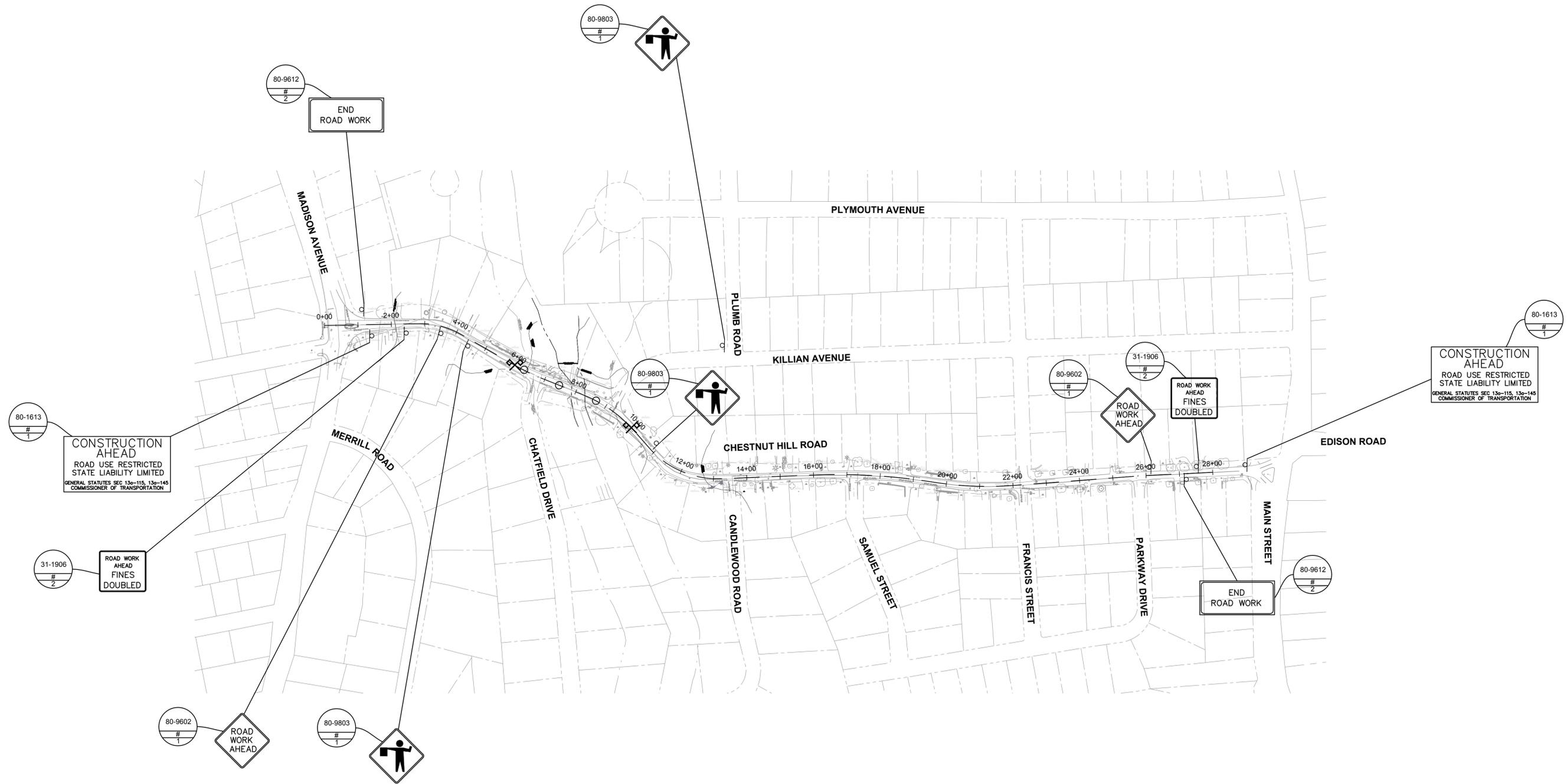
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TITLE
**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
MAINTENANCE AND PROTECTION
OF TRAFFIC STAGING PLAN
STAGE 1**

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. MPT-2
22 OF 47

1/17/2019 9:05 AM H:\5000\6568 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\6568-DETOUR PLAN.DWG



LEGEND:

-  APPROXIMATE LOCATION OF UNIFORMED FLAGGER
-  TRAFFIC DRUM
-  TYPE III BARRICADE WITH BARRICADE WARNING LIGHT HIGH INTENSITY
-  CTDOT SIGN #/ MUTCD SIGN #
PROJECT SIGN ID
OF SUPPORTS

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:

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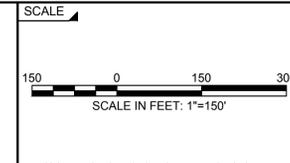
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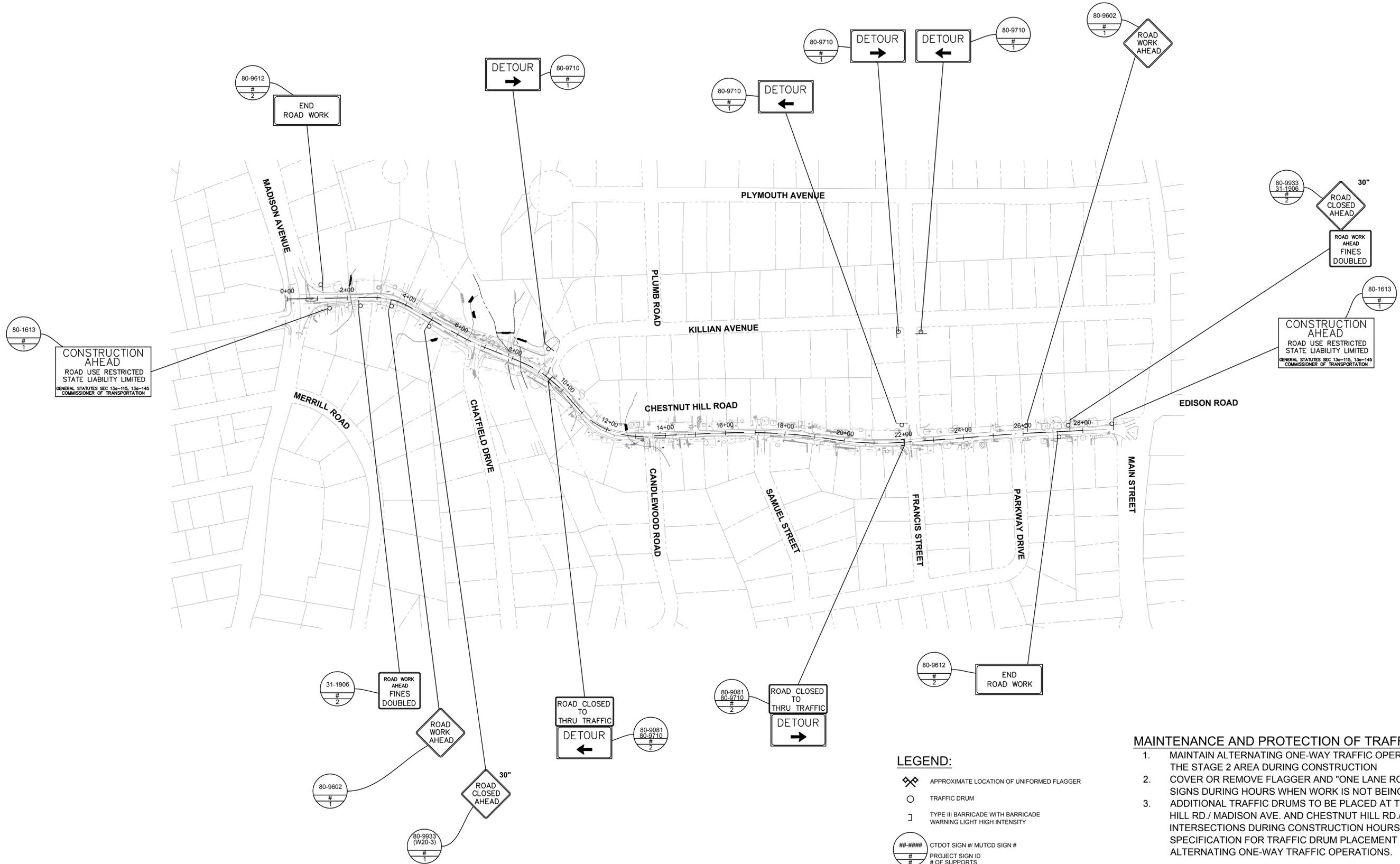
**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
MAINTENANCE AND PROTECTION
OF TRAFFIC STAGING PLAN
STAGE 2**

BETA JOB NO. 5658

ISSUE DATE 1/16/2019

SHEET NO. MPT-3
23 OF 47

1/17/2019 9:05 AM H:\5000\6568 TRUMBULL CHESTNUT HILL ROAD\DRAWINGS\PLANS\SET\6568-DETOUR PLAN.DWG



CONSTRUCTION AHEAD
ROAD USE RESTRICTED
STATE LIABILITY LIMITED
GENERAL STATUTES SEC 13a-115, 13a-145
COMMISSIONER OF TRANSPORTATION

CONSTRUCTION AHEAD
ROAD USE RESTRICTED
STATE LIABILITY LIMITED
GENERAL STATUTES SEC 13a-115, 13a-145
COMMISSIONER OF TRANSPORTATION

LEGEND:

- APPROXIMATE LOCATION OF UNIFORMED FLAGGER
- TRAFFIC DRUM
- TYPE III BARRICADE WITH BARRICADE WARNING LIGHT HIGH INTENSITY
- CTDOT SIGN # / MUTCD SIGN #
PROJECT SIGN ID # / # OF SUPPORTS

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:

1. MAINTAIN ALTERNATING ONE-WAY TRAFFIC OPERATION WITHIN THE STAGE 2 AREA DURING CONSTRUCTION
2. COVER OR REMOVE FLAGGER AND "ONE LANE ROAD AHEAD" SIGNS DURING HOURS WHEN WORK IS NOT BEING CONDUCTED
3. ADDITIONAL TRAFFIC DRUMS TO BE PLACED AT THE CHESTNUT HILL RD./ MADISON AVE. AND CHESTNUT HILL RD./ KILLIAN AVE. INTERSECTIONS DURING CONSTRUCTION HOURS. REFER TO SPECIFICATION FOR TRAFFIC DRUM PLACEMENT FOR ALTERNATING ONE-WAY TRAFFIC OPERATIONS.

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J.D.

DESIGNED BY:
J.R.

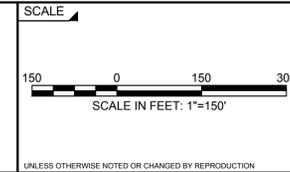
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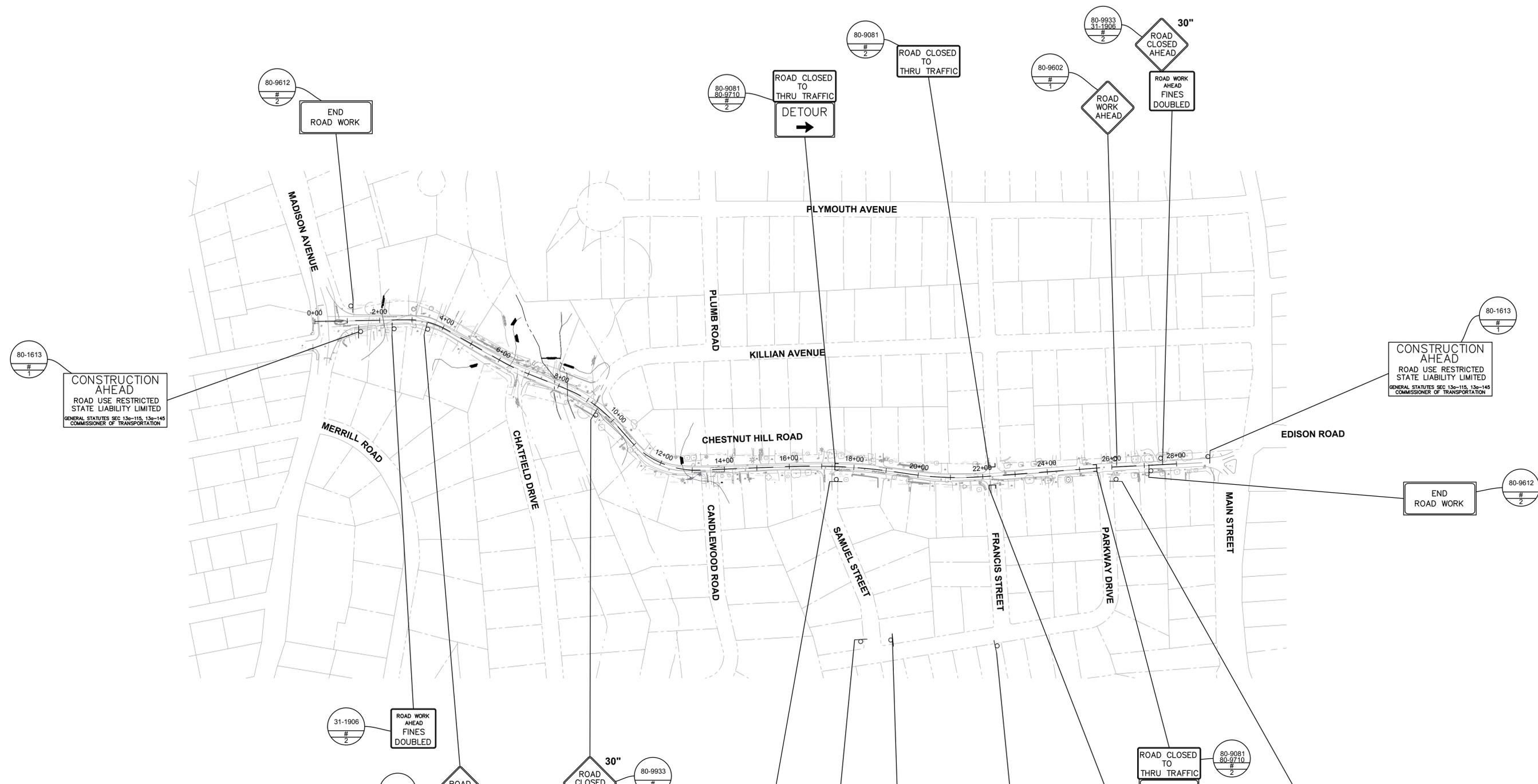
CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
MAINTENANCE AND PROTECTION
OF TRAFFIC STAGING PLAN
STAGE 3

BETA JOB NO. 5658

ISSUE DATE 1/16/2019

SHEET NO. **MPT-4**
24 OF 47

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MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:

1. MAINTAIN ALTERNATING ONE-WAY TRAFFIC OPERATION WITHIN THE STAGE 2 AREA DURING CONSTRUCTION
2. COVER OR REMOVE FLAGGER AND "ONE LANE ROAD AHEAD" SIGNS DURING HOURS WHEN WORK IS NOT BEING CONDUCTED
3. ADDITIONAL TRAFFIC DRUMS TO BE PLACED AT THE CHESTNUT HILL RD./ MADISON AVE. AND CHESTNUT HILL RD./ KILLIAN AVE. INTERSECTIONS DURING CONSTRUCTION HOURS. REFER TO SPECIFICATION FOR TRAFFIC DRUM PLACEMENT FOR ALTERNATING ONE-WAY TRAFFIC OPERATIONS.

LEGEND:

- APPROXIMATE LOCATION OF UNIFORMED FLAGGER
- TRAFFIC DRUM
- TYPE III BARRICADE WITH BARRICADE WARNING LIGHT HIGH INTENSITY
- CTDOT SIGN #/ MUTCD SIGN #
- PROJECT SIGN ID # OF SUPPORTS

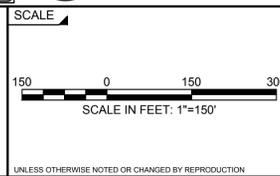
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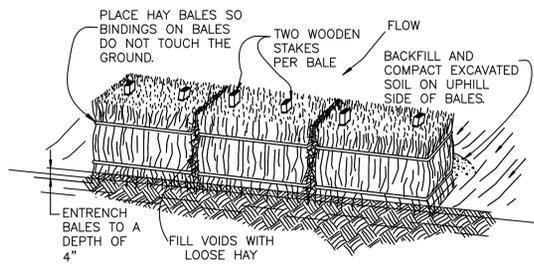


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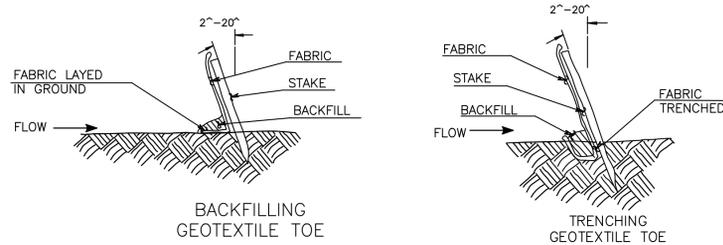
**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
MAINTENANCE AND PROTECTION
OF TRAFFIC STAGING PLAN
STAGE 4**

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. MPT-5
25 OF 47



HAY BALE SYSTEM

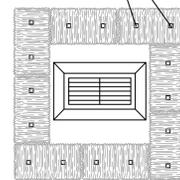
- NOTES:
- HAY BALES SHALL NOT BE USED IN A WATERCOURSE.
 - HAY BALES SHALL BE ENTRENCHED 4" AND TIGHTLY BUTTED TOGETHER. REMOVE HEAVY BRUSH AND FILL ALL VOIDS WITH LOOSE HAY.
 - WOOD STAKES SHALL HAVE A MINIMUM CROSS-SECTION SIZE OF AT LEAST 1" x 1" AND A MINIMUM LENGTH OF 4'.
 - CLEAN OUT ACCUMULATED SEDIMENT WHEN ONE-HALF (1/2) OF THE ORIGINAL HEIGHT OF THE HAY BALE FENCE, AS INSTALLED, BECOMES FILLED WITH SEDIMENT OR AS DIRECTED BY THE ENGINEER.
 - NOT TO BE USED IN THE VICINITY OF URBAN AND RESIDENTIAL AREAS.



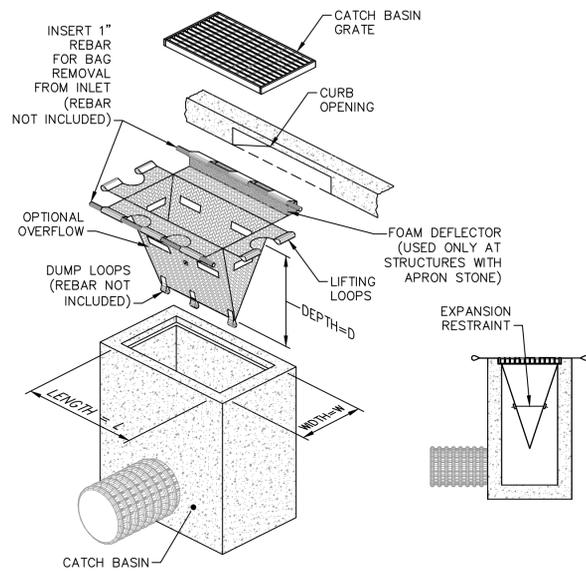
GEOTEXTILE FENCE SYSTEM

- NOTES:
- GEOTEXTILE FENCE SHOULD BE PLACED SO THE FENCE LEANS TOWARD THE SOURCE OF SEDIMENT.
 - MAXIMUM SPACING FOR WOODEN STAKES OR STEEL POSTS IS 10.0'.
 - WOOD STAKES SHALL HAVE A MINIMUM CROSS-SECTION SIZE OF 1.5" x 1.5" AND A MINIMUM LENGTH OF 4 FT. STEEL POSTS SHALL BE AT LEAST 0.5 LB. PER FOOT WITH A MINIMUM LENGTH OF 4 FT. 4. WOODEN STAKES OR STEEL POSTS SHALL BE DRIVEN TO A MINIMUM OF 1 FT. INTO THE GROUND.
 - 6" OF GEOTEXTILE SHALL BE BURIED BY BACKFILLING OR TRENCHING AND AT LEAST 2.5 FT. IN HEIGHT OF GEOTEXTILE SHALL BE EXPOSED.
 - FABRIC SHALL BE JOINED ONLY AT A SUPPORT POST WITH A MINIMUM OF 6" OVERLAP AND SECURELY SEALED.
 - UPON RE-ESTABLISHMENT OF GROUND COVER IN DISTURBED AREAS AND WHEN DIRECTED BY THE ENGINEER, OR UPON FINAL INSPECTION FENCE AND ANY SEDIMENT SHALL BE REMOVED. AT NO TIME WILL THE FENCE REMAIN IN PLACE AFTER PROJECT COMPLETION.
 - GEOTEXTILE FENCE SHALL NOT BE USED IN A WATER COURSE.
 - ONLY GEOTEXTILE FROM THE DEPARTMENTS APPROVED PRODUCT LIST SHALL BE USED.
 - BACKFILLING OF GEOTEXTILE SHALL ONLY BE USED WHEN GROUND IS FROZEN OR WHERE OTHER OBSTRUCTIONS ARE ENCOUNTERED THAT PROHIBIT TRENCHING, IE. STUMPS OR ROCKS.
 - CLEAN OUT ACCUMULATED SEDIMENT WHEN ONE-HALF (1/2) OF THE ORIGINAL HEIGHT OF THE GEOTEXTILE FENCE, AS INSTALLED, BECOMES FILLED WITH SEDIMENT OR AS DIRECTED BY THE ENGINEER.

2-1"x1"x3" STAKES IN EACH BALE DRIVEN 18" (MIN.) INTO EARTH



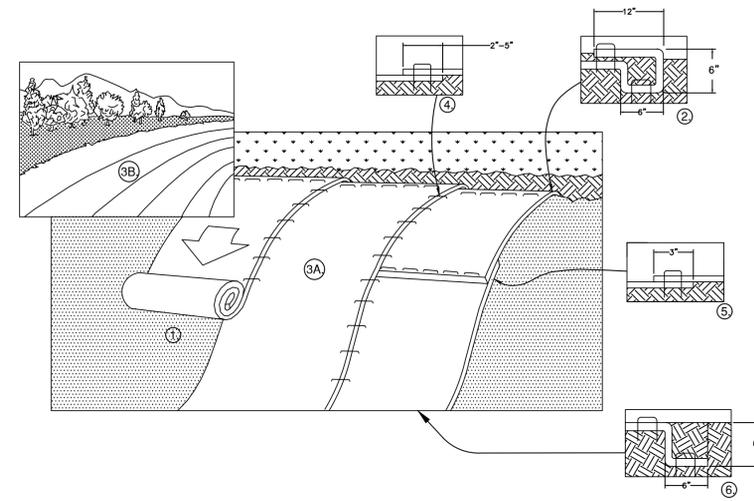
SEDIMENTATION CONTROL BALE PLACEMENT AT CATCH BASIN
N.T.S.



NOTES

- CONTRACTOR SHALL MAINTAIN ALL SILT SACK SEDIMENT CONTROL DEVICE INSTALLATIONS AND REPLACE WHEN SATURATED WITH SEDIMENT BASED ON MANUFACTURER RECOMMENDATIONS. SILT SACK SHALL BE INSPECTED AFTER EVERY RAIN EVENT BUT NO LONGER THAN EVERY TWO WEEKS.
- SILT SACK SHALL BE DOUBLE NEEDLE SEWN POLYPROPYLENE GEOTEXTILE FABRIC AND RATED FOR HIGH FLOW UNLESS OTHERWISE DIRECTED.
- CONTRACTOR SHALL PROVIDE CURB OPENING DEFLECTOR AT ALL CATCH BASINS AS NECESSARY.
- SILT SACKS SHALL BE INSTALLED IN ALL DRAINAGE STRUCTURE INLETS (CATCH BASINS, DROP INLETS, ETC.) WITHIN 50 FT. OF AND IMMEDIATELY DOWN GRADE FROM ALL WORK AREAS.

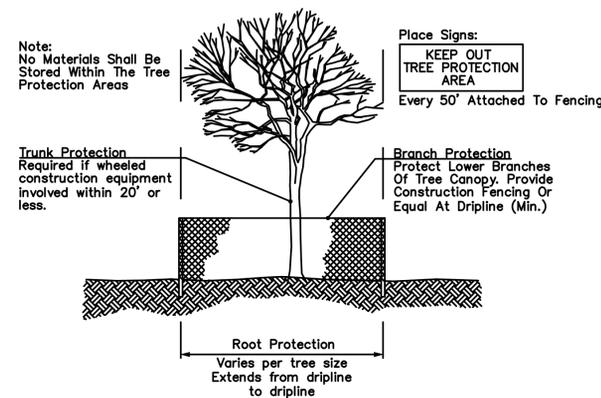
INLET SEDIMENT CONTROL DEVICE (SILT SACK)
(WITH OPTIONAL CURB DEFLECTOR)
NOT TO SCALE



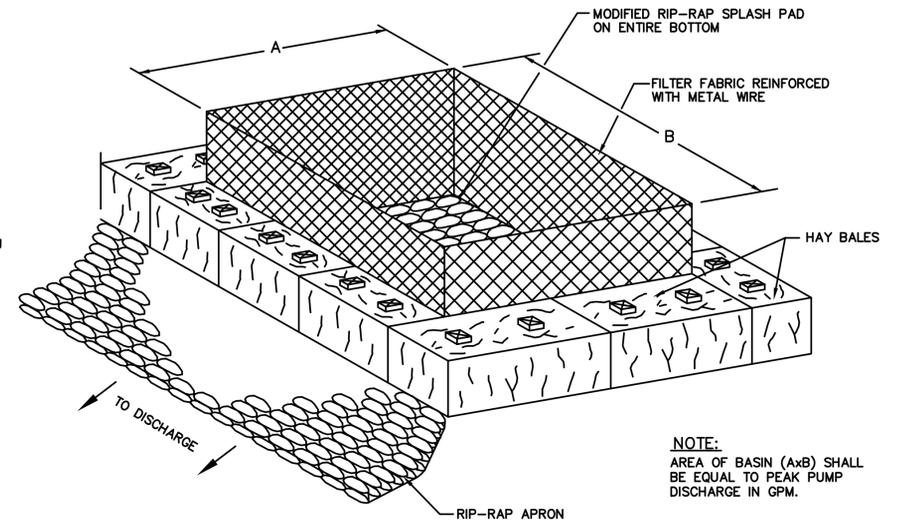
- PREPARE SOIL BEFORE INSTALLING ROLLED EROSION CONTROL MAT (RECM's), INCLUDING ANY NECESSARY APPLICATION OF LIME, FERTILIZER, AND SEED.
- BEGIN AT THE TOP OF THE SLOPE BY ANCHORING THE RECM's IN A 6" DEEP X 6" WIDE TRENCH WITH APPROXIMATELY 12" OF RECM's EXTENDED BEYOND THE UP-SLOPE PORTION OF THE TRENCH. ANCHOR THE RECM's WITH A ROW OF STAPLES/STAKES APPROXIMATELY 12" APART IN THE BOTTOM OF THE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING. APPLY SEED TO COMPACTED SOIL AND FOLD REMAINING 12" PORTION OF RECM's BACK OVER SEED AND COMPACTED SOIL. SECURE RECM's OVER COMPACTED SOIL WITH A ROW OF STAPLES/STAKES SPACED APPROXIMATELY 12" APART ACROSS THE WIDTH OF THE RECM's.
- ROLL THE RECM's (A.) DOWN OR (B.) HORIZONTALLY ACROSS THE SLOPE. RECM's WILL UNROLL WITH APPROPRIATE SIDE AGAINST THE SOIL SURFACE. ALL RECM's MUST BE SECURELY FASTENED TO SOIL SURFACE BY PLACING STAPLES/STAKES IN APPROPRIATE LOCATIONS AS SHOWN IN THE STAPLE PATTERN GUIDE.
- THE EDGES OF PARALLEL RECM's MUST BE STAPLED WITH APPROXIMATELY 2" - 5" OVERLAP DEPENDING ON RECM's TYPE.
- CONSECUTIVE RECM's SPICED DOWN THE SLOPE MUST BE PLACED END OVER END (SHINGLE STYLE) WITH AN APPROXIMATE 3" OVERLAP. STAPLE THROUGH OVERLAPPED AREA, APPROXIMATELY 12" APART ACROSS ENTIRE RECM's WIDTH.
- ANCHOR BOTTOM OF RECM IN

NOTE:
*IN LOOSE SOIL CONDITIONS, THE USE OF STAPLE OR STAKE LENGTHS GREATER THAN 6" MAY BE NECESSARY TO PROPERLY SECURE THE RECP's OR AS DIRECTED BY THE ENGINEER

EROSION CONTROL MAT
N.T.S.



TREE PROTECTION
N.T.S.



TEMPORARY SEDIMENT BASIN FOR DEWATERING DISCHARGE
N.T.S.

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CHECKED BY:	J.B.

REGISTERED PROFESSIONAL	PREPARED BY
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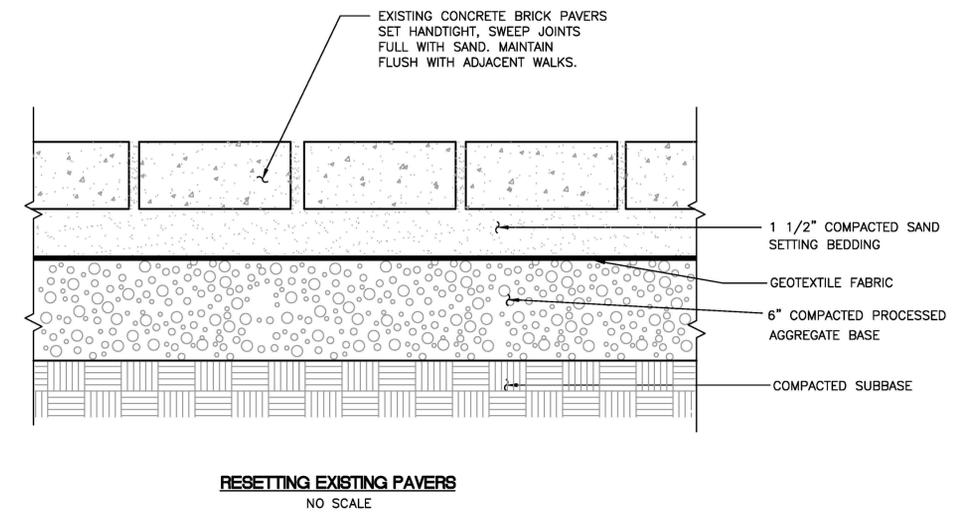
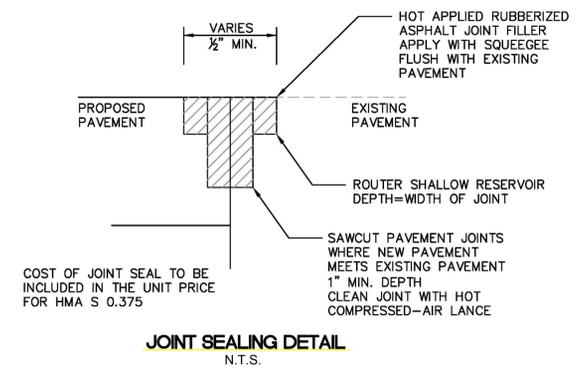
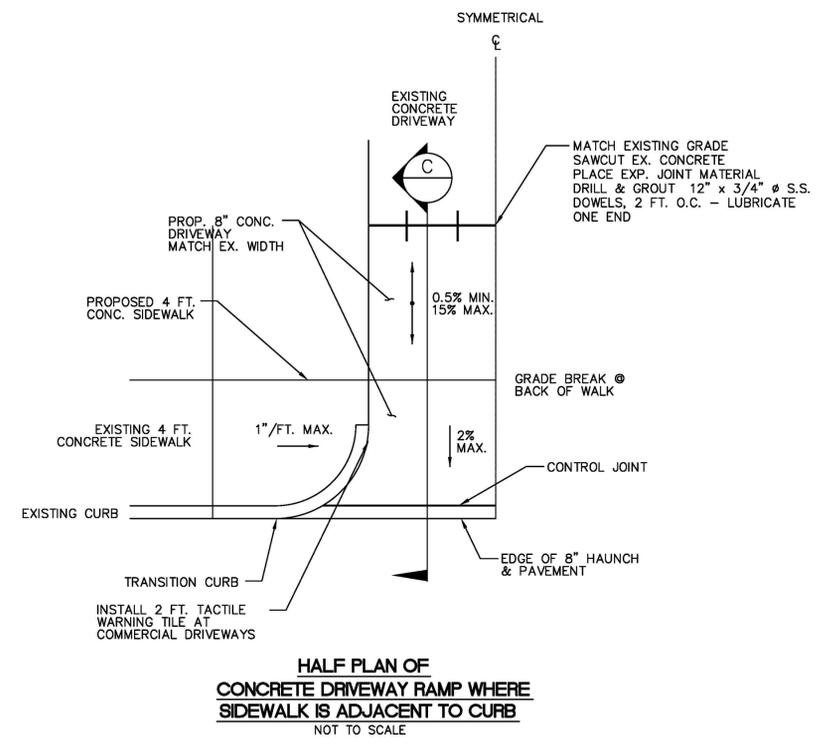
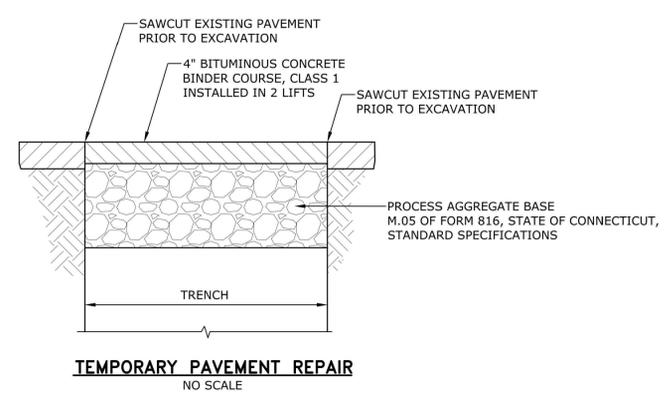
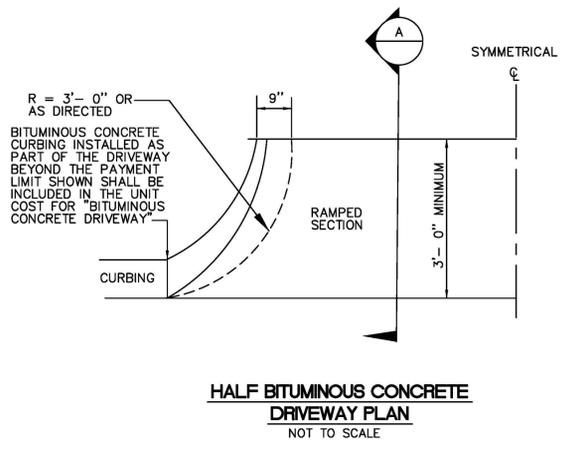
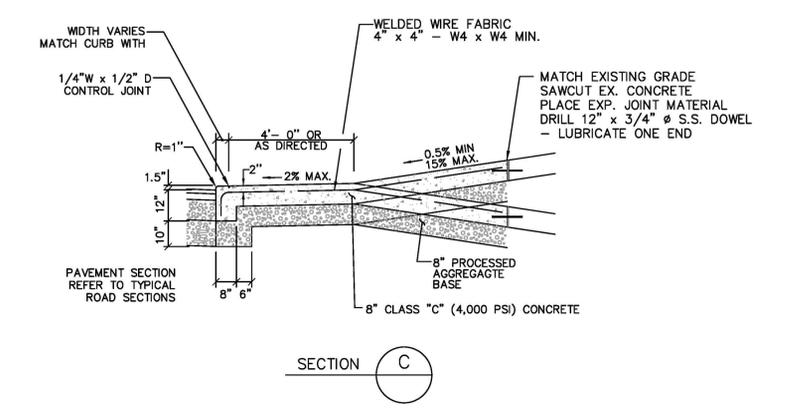
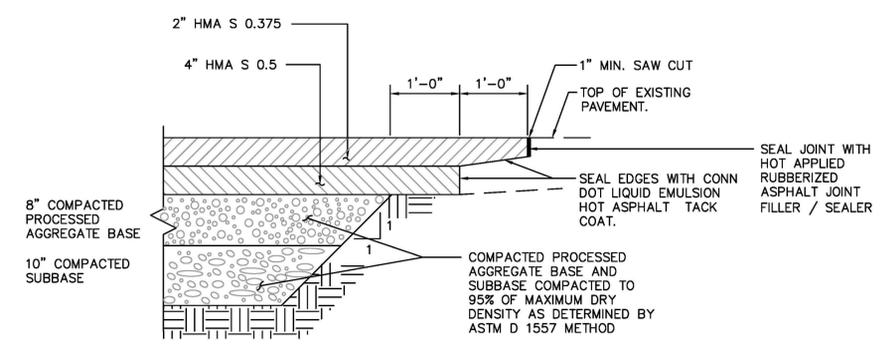
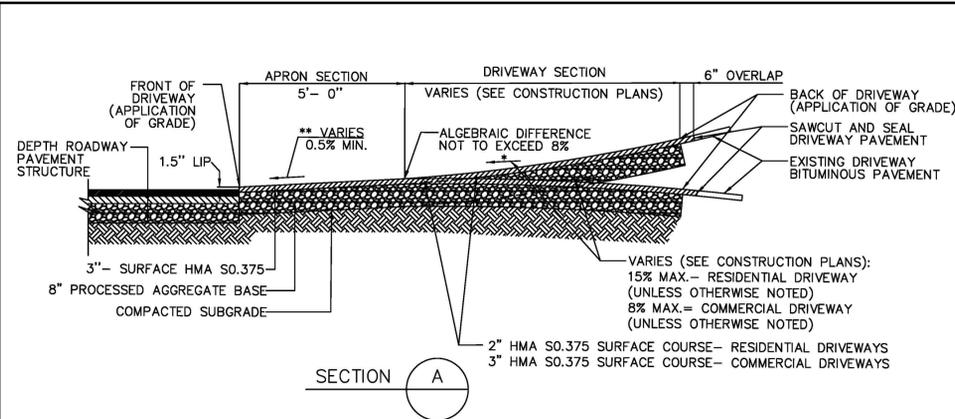
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	AS SHOWN

TITLE	

CHESTNUT HILL ROAD RECONSTRUCTION	
TRUMBULL, CT	
SPECIAL DETAILS	
EROSION & SEDIMENTATION CONTROL	

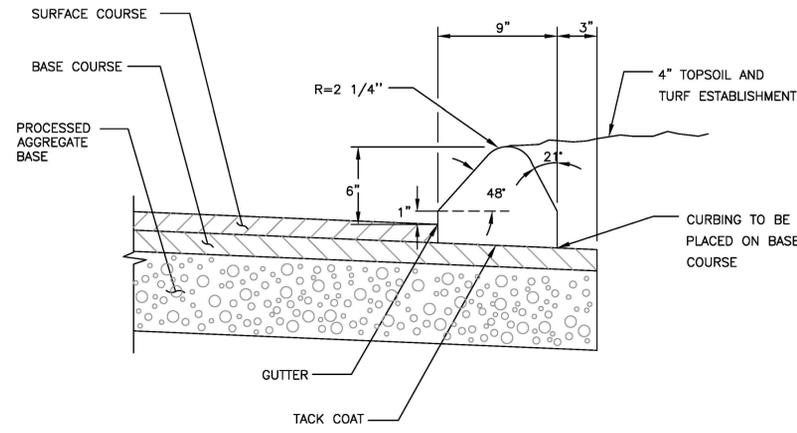
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ISSUE DATE	1/16/2019
SHEET NO.	SPD-1
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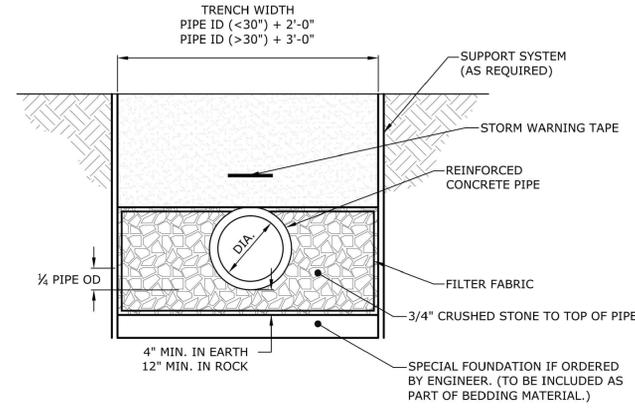


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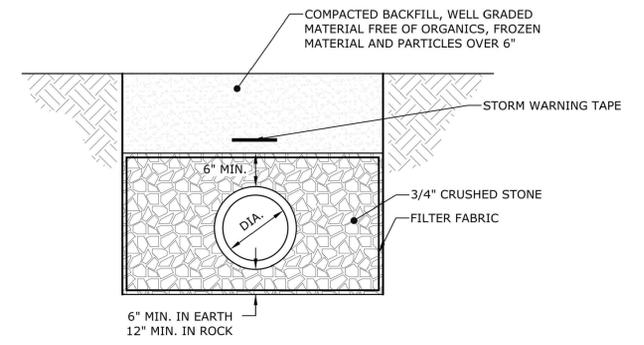
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				DESIGNED BY: R.S.		 www.BETA-Inc.com				ISSUE DATE: 1/16/2019	
				CHECKED BY: J.B.						SHEET NO. SPD-2	
NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS							27 OF 47



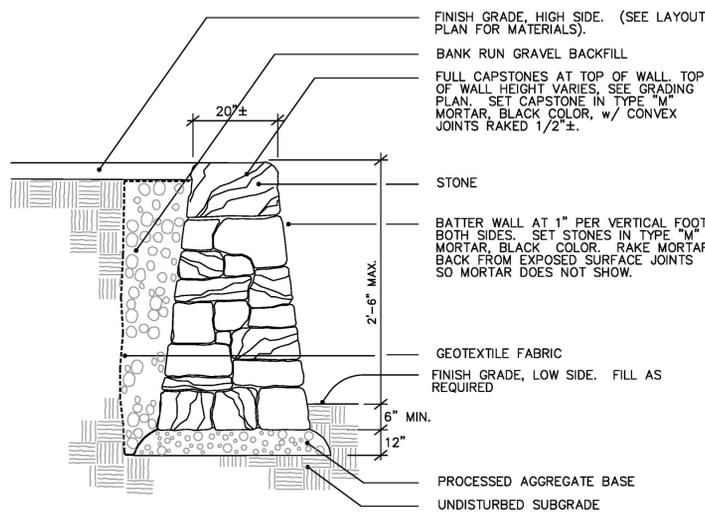
6" BITUMINOUS CONCRETE LIP CURBING (BCLC)
N.T.S.



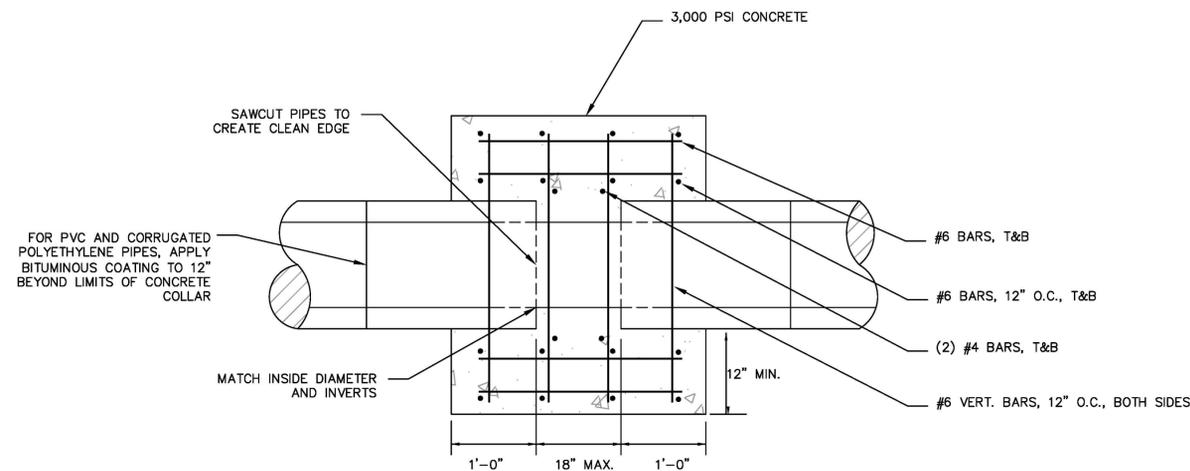
CIRCULAR R.C.P. TRENCH BEDDING
NO SCALE



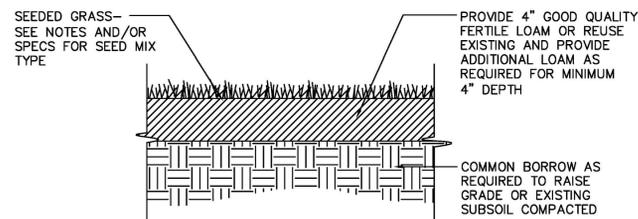
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NO SCALE



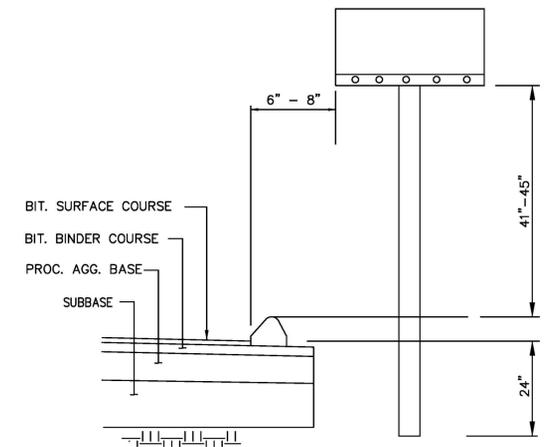
CEMENT RUBBLE MASONRY (STONE RETAINING WALL) DETAIL
NOT TO SCALE



CONCRETE COLLAR FOR JOINING PIPE
NOT TO SCALE



LOAM AND SEED DETAIL
NOT TO SCALE



NOTE: COST OF RESETTING AND/OR RELOCATING EXISTING MAILBOXES AND PAPER TUBES SHALL BE INCLUDED IN THE ITEM "CLEARING AND GRUBBING"

MAILBOX AND PAPER TUBE SETTING LOCATION
NOT TO SCALE

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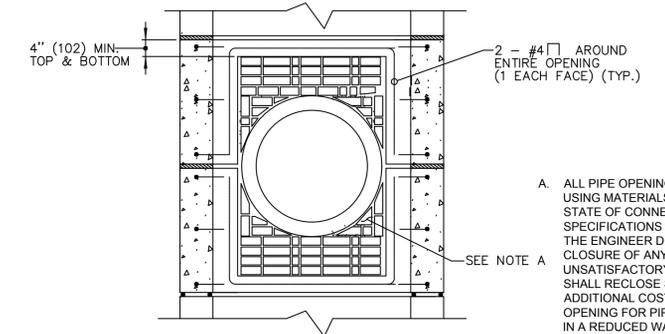
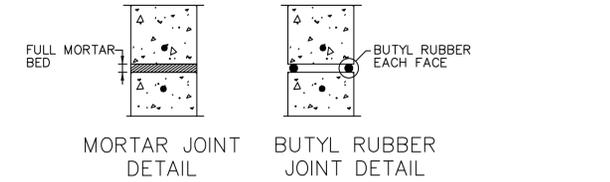
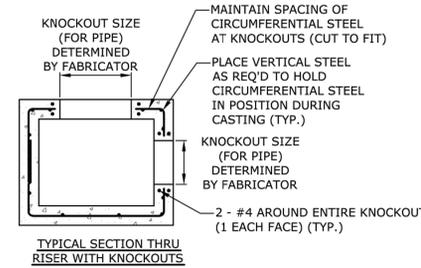
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**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
SPECIAL DETAILS
MISCELLANEOUS**

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. SPD-3
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GENERAL NOTES:

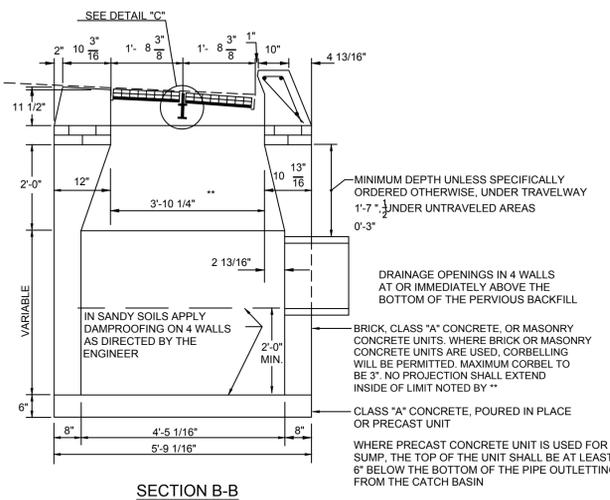
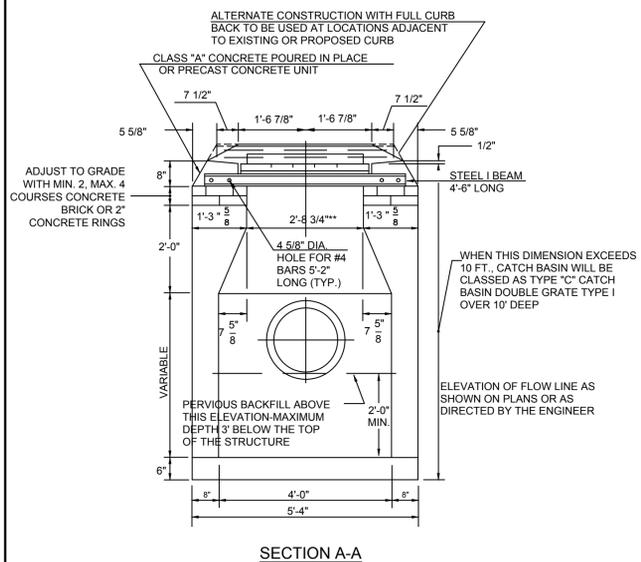
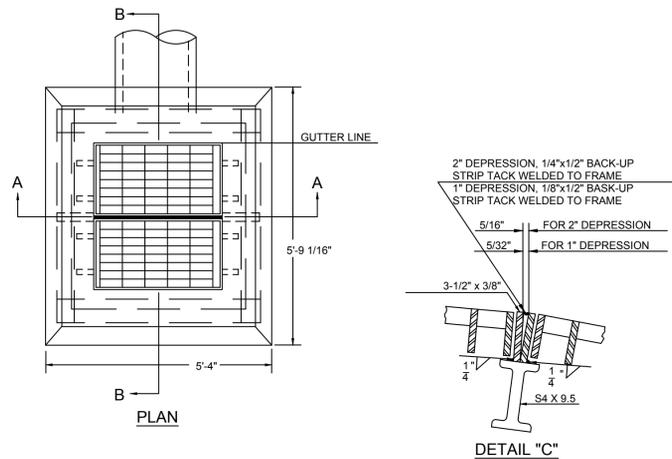
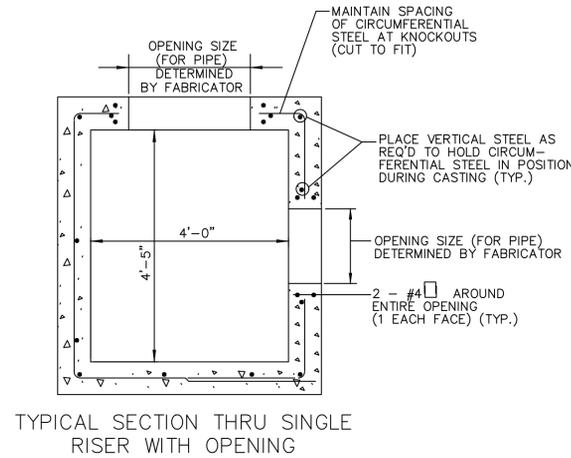
1. REINFORCEMENT SHALL CONFORM TO ASTM A615, GRADE 60.
2. DETAILS ON THIS SHEET SHOW STANDARD REINFORCEMENT. WELDED WIRE FABRIC WITH AN AREA EQUAL TO OR GREATER THAN THE REINFORCING SHOWN MAY BE SUBSTITUTED.
3. ALL LAP SPLICES, DEVELOPMENT LENGTHS, BENDS FOR REINFORCEMENT, AND WELDED WIRE FABRIC SHALL CONFORM TO AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES.
4. ALL REINFORCEMENT SHALL HAVE A MINIMUM CLEAR COVER OF 2", EXCEPT FOR BENEATH BOTTOM REINFORCEMENT IN TOP SLABS, WHERE THE MINIMUM MAY BE 1 1/2".
5. MINIMUM CONCRETE COMPRESSIVE STRENGTH FC' = 4000 PSI SHALL BE OBTAINED BEFORE SHIPPING.
6. BASES AND RISERS AT A DEPTH OF 20' AND GREATER SHALL BE DESIGNED BY THE CONTRACTOR AND WORKING DRAWINGS SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW.
7. SEE STANDARD DRAWING 507-K FOR CATCH BASIN FRAMES AND GRATES.
8. FOR DOT MAINTENANCE PERSONNEL, RISERS MAY BE PREFABRICATED WITH PIPE OPENINGS IN ALL FOUR WALLS. ADEQUATE REINFORCING AROUND PIPE OPENINGS TO CONFORMING TO THESE PLANS SHALL BE PROVIDED. ANY RISERS USED WHERE A PIPE OPENING IS TO REMAIN IN PLACE MUST BE FORMED UP WITH BRICK AS DIRECTED BY THE ENGINEER.
9. RISERS SHALL NEVER HAVE CORNER PIPE ENTRIES. WHERE THE ALIGNMENT OF THE PIPE WITH RESPECT TO THE CORNER OF THE CATCH BASIN CANNOT BE CHANGED, A ROUND STRUCTURE CONFORMING TO ASTM C478 SHALL BE USED. REINFORCING FOR THE ROUND TOP SLAB WITH A RECTANGULAR OPENING SHALL CONFORM TO DETAILS SHOWN HERE.
10. ALL PIPE OPENINGS SHALL BE CLOSED USING MATERIALS WHICH CONFORM TO STATE OF CONNECTICUT STANDARD SPECIFICATIONS SECTION M.08.02. IF THE ENGINEER DETERMINES THAT THE CLOSURE OF ANY PIPE OPENING IS UNSATISFACTORY, THE CONTRACTOR SHALL RECLOSE SAID OPENING AT NO ADDITIONAL COST TO THE STATE. OPENING FOR PIPE SHALL NOT RESULT IN A REDUCED WALL THICKNESS.
11. THE LATEST STATE OF CONNECTICUT STANDARD SPECIFICATIONS AND SUPPLEMENTALS SHALL GOVERN.
12. FOR ADDITIONAL DETAILS, SEE OTHER CATCH BASIN SHEETS.
13. WALL THICKNESS OF ALL CB'S OVER 10' DEEP SHALL BE INCREASED TO 12" THICK, INSIDE DIMENSION SHALL REMAIN THE SAME. (THE 12" THICKNESS WILL START AFTER THE FIRST 10')
14. BUTYL RUBBER JOINT SEAL SHALL CONFORM TO AASHTO M-198 AND MORTAR SHALL CONFORM TO THE LATEST STATE OF CONNECTICUT STANDARD SPECIFICATIONS MATERIAL SECTION M11.04.
15. SHRINKAGE AND TEMPERATURE REINFORCEMENT SHALL BE PROVIDED IN THE TOPS OF SLABS. THE TOTAL AREA OF REINFORCEMENT PROVIDED SHALL BE AT LEAST 0.125 IN²/FT IN EACH DIRECTION. THE MAXIMUM SPACING OF THIS REINFORCEMENT SHALL NOT EXCEED 18 INCHES.
16. THE DETAILS SHOWN IN THE PLAN VIEW FOR THE PRECAST CONCRETE ROUND STRUCTURES SHALL ALSO BE USED FOR CONVERTING MANHOLES TO CATCH BASINS.
17. SET CATCH BASIN TOPS ON 6" MINIMUM BRICK OR BLOCK LEVELING COURSE.
18. AFFIX PLAQUE TO ALL CATCH BASINS STATING "DUMP NO WASTE-DRAINS TO LONG ISLAND SOUND". COST FOR PLAQUE SHALL BE INCLUDED IN THE CONTRACT UNIT COST FOR CATCH BASINS.



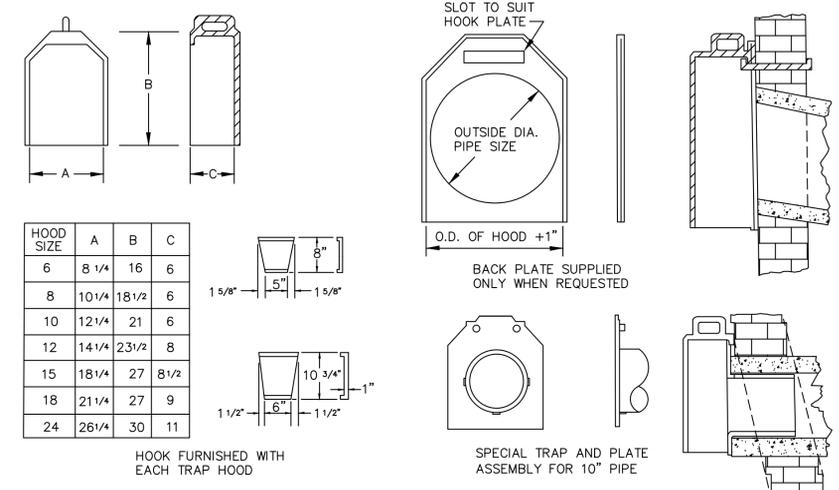
A. ALL PIPE OPENINGS SHALL BE CLOSED USING MATERIALS WHICH CONFORM TO STATE OF CONNECTICUT STANDARD SPECIFICATIONS SECTION M.08.02. IF THE ENGINEER DETERMINES THAT THE CLOSURE OF ANY PIPE OPENING IS UNSATISFACTORY, THE CONTRACTOR SHALL RECLOSE SAID OPENING AT NO ADDITIONAL COST TO THE TOWN. OPENING FOR PIPE SHALL NOT RESULT IN A REDUCED WALL THICKNESS.

FRAME AND GRATE NOTES:

1. FRAME AND GRATE SHALL BE CT DOT TYPE A STEEL.
2. ALL STEEL, EXCEPT REINFORCING BARS, SHALL BE GALVANIZED IN CONFORMANCE WITH SECTION M06.03 OF CT DOT STANDARD SPECIFICATIONS.
3. ALL BARS SHALL HAVE A MINIMUM 2" (51) COVER.



TYPE 'C' CATCH BASIN DOUBLE GRATE - TYPE I
N.T.S.



HOOD SIZE	A	B	C
6	8 1/4"	16	6
8	10 1/4"	18 1/2"	6
10	12 1/4"	21	6
12	14 1/4"	23 1/2"	8
15	18 1/4"	27	8 1/2"
18	21 1/4"	27	9
24	26 1/4"	30	11

CATCH BASIN TRAP HOODS
N.T.S.

NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

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DESIGNED BY:	R.S.
CHECKED BY:	J.B.

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	AS SHOWN	CHESTNUT HILL ROAD RECONSTRUCTION TRUMBULL, CT SPECIAL DETAILS STORM DRAINAGE

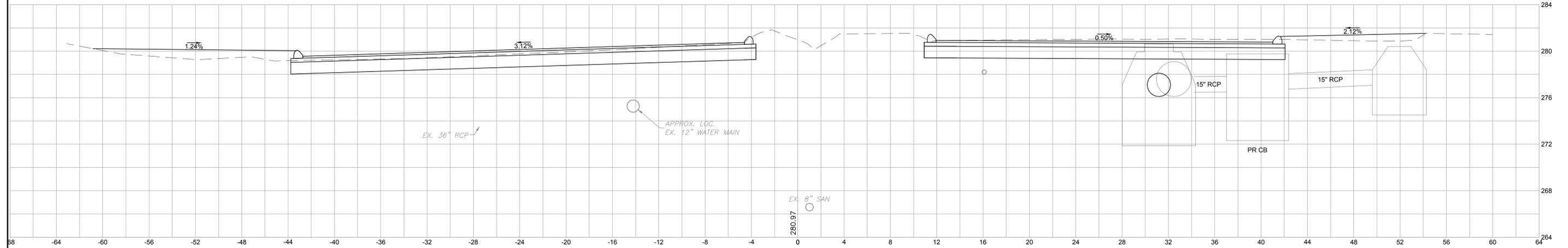
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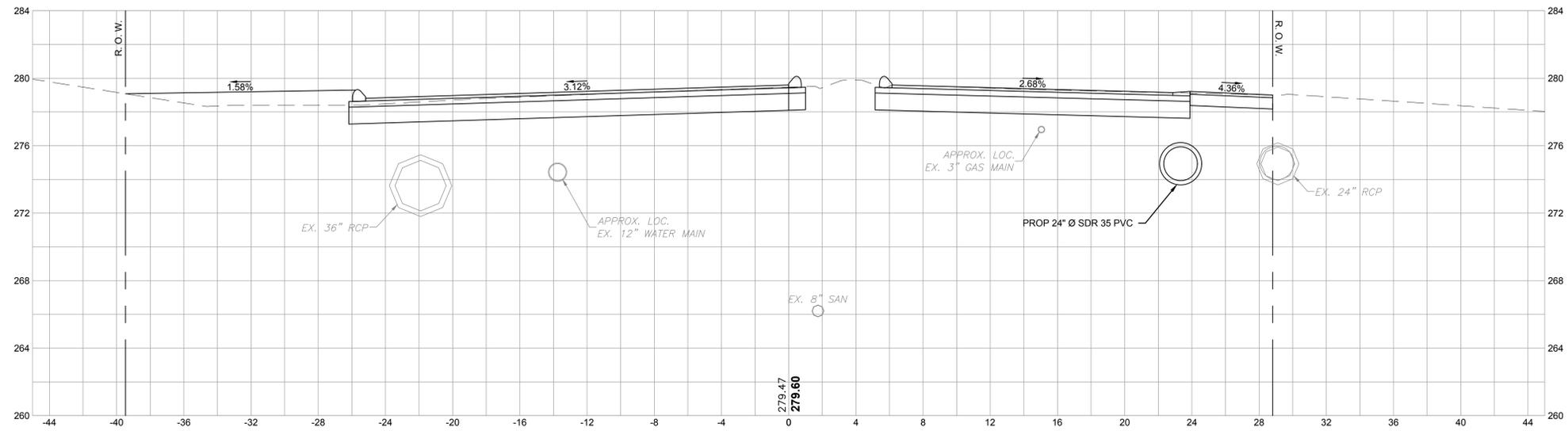
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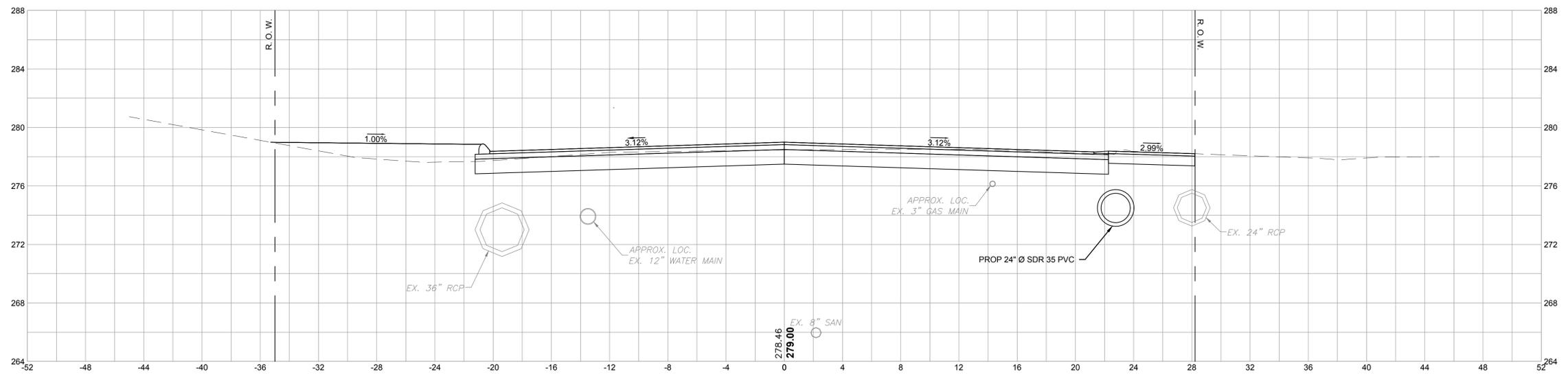
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1+00



1+15



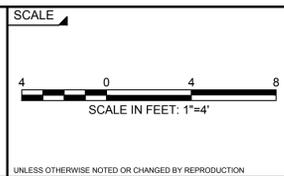
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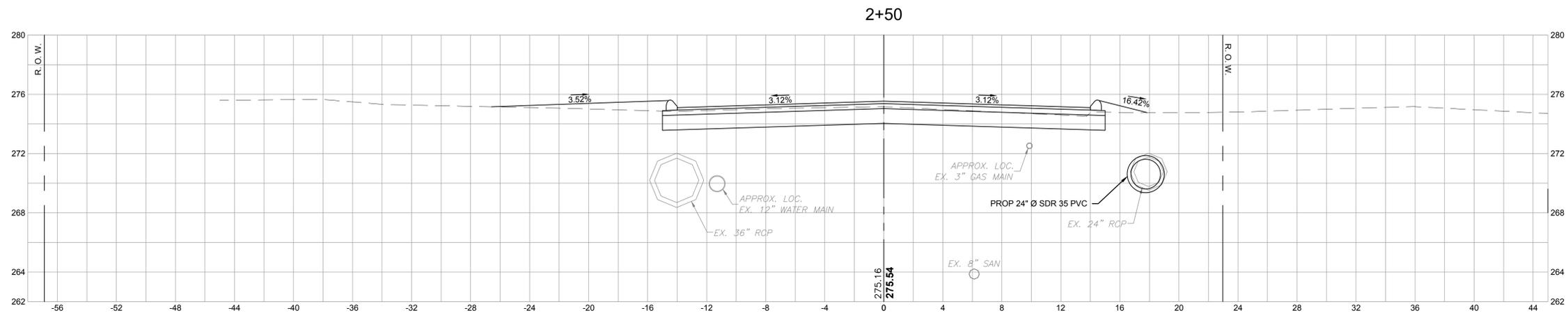
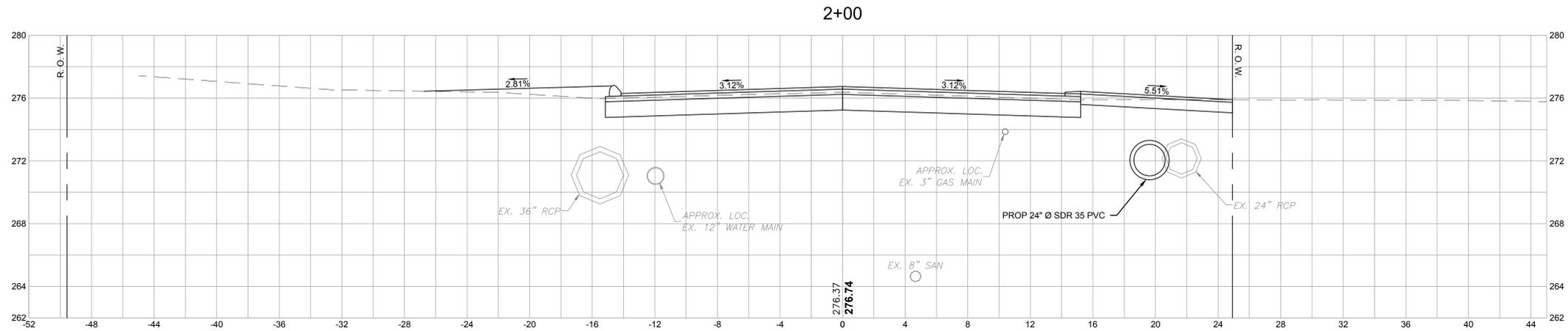
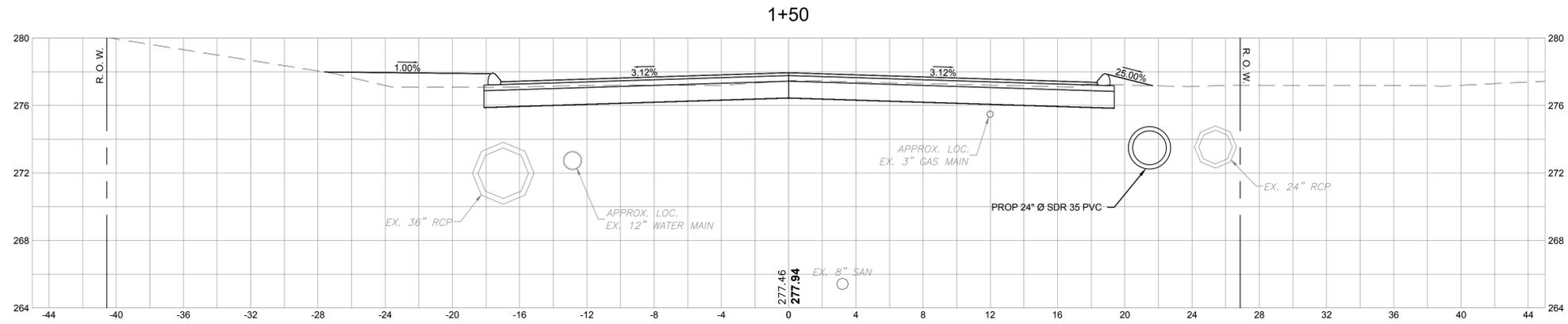
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TITLE
CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
CROSS SECTIONS
PLANS

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. XS-1
31 OF 47

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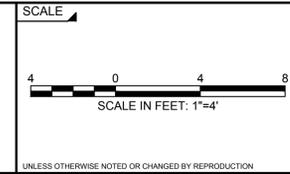
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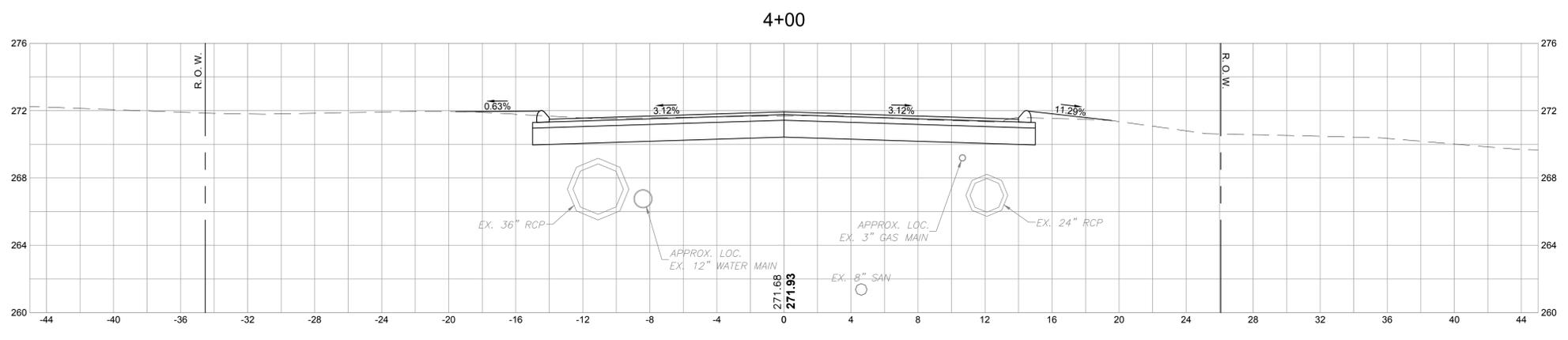
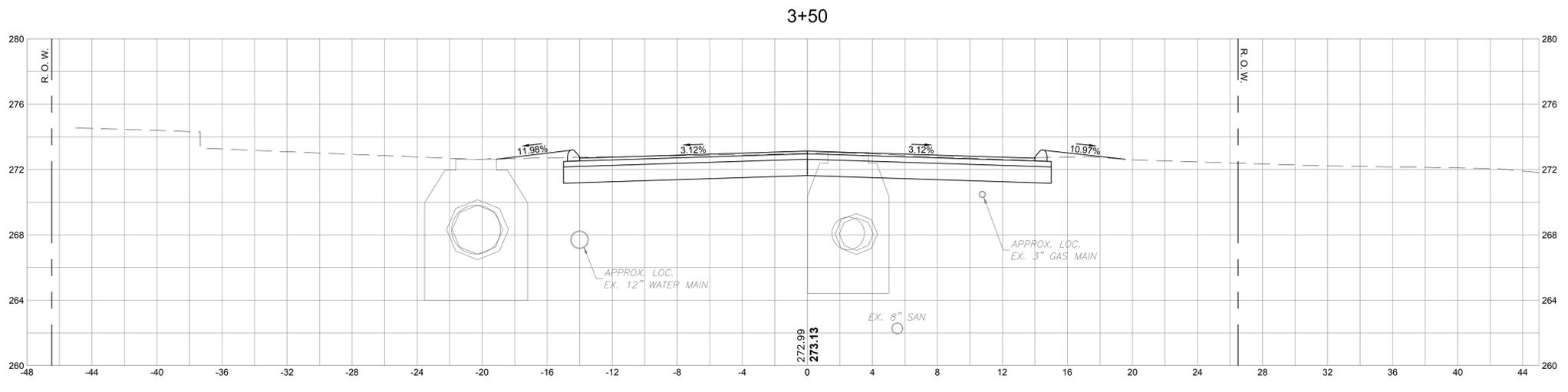
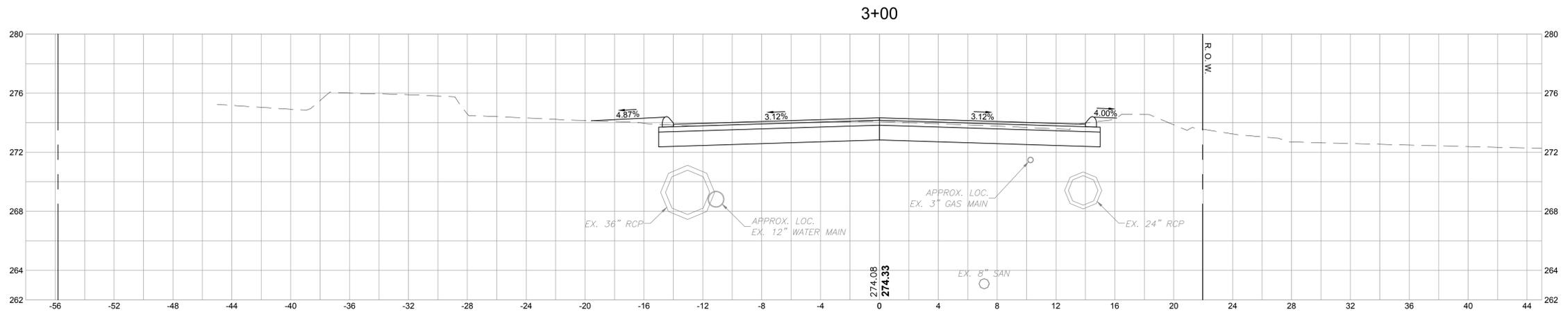
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**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
CROSS SECTIONS
PLANS**

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. XS-2
32 OF 47

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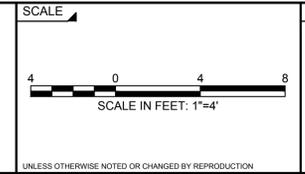
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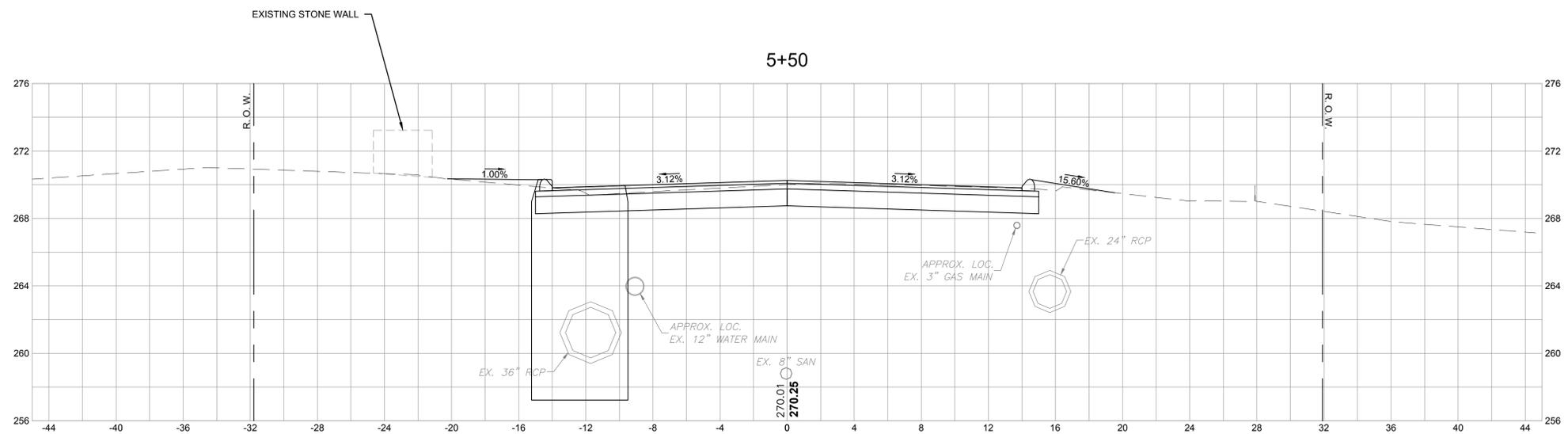
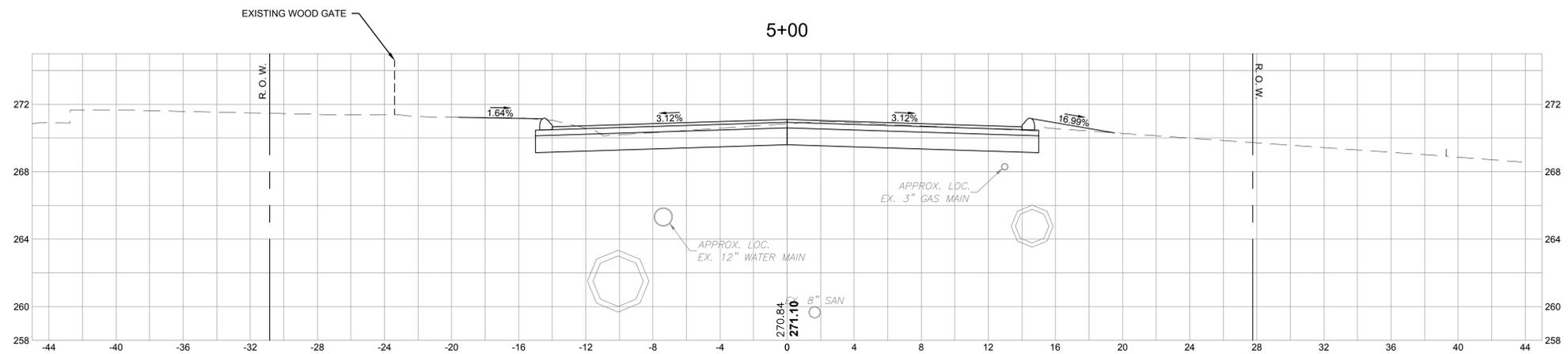
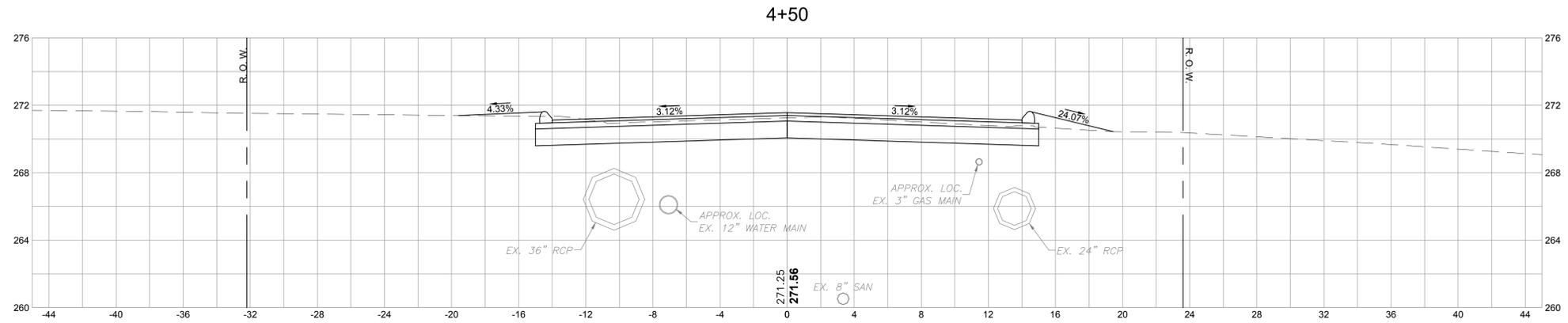
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CHESTNUT HILL ROAD RECONSTRUCTION TRUMBULL, CT

CROSS SECTIONS PLANS

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. XS-3
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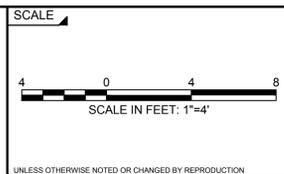
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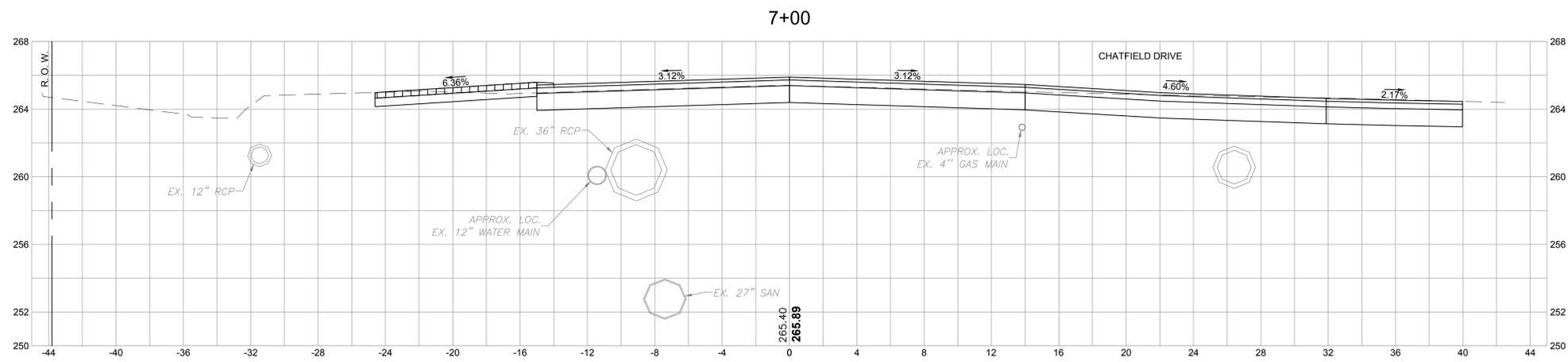
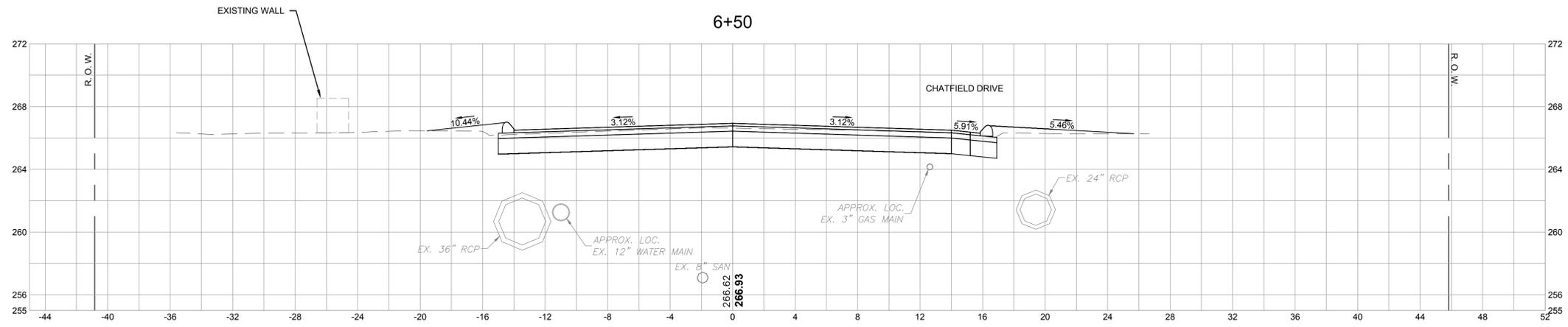
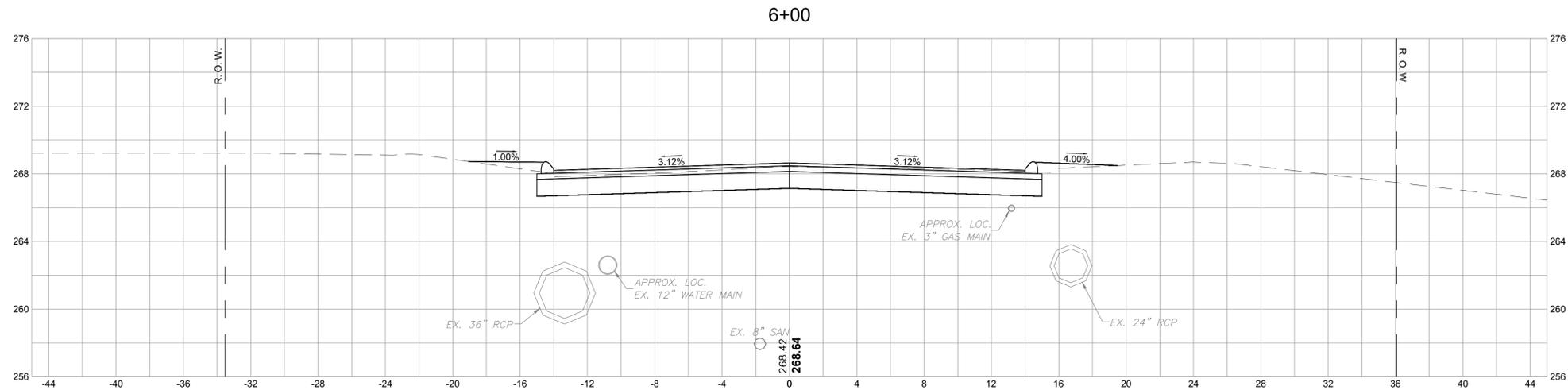


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TRUMBULL, CT
CROSS SECTIONS
PLANS**

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. XS-4
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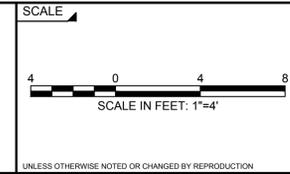
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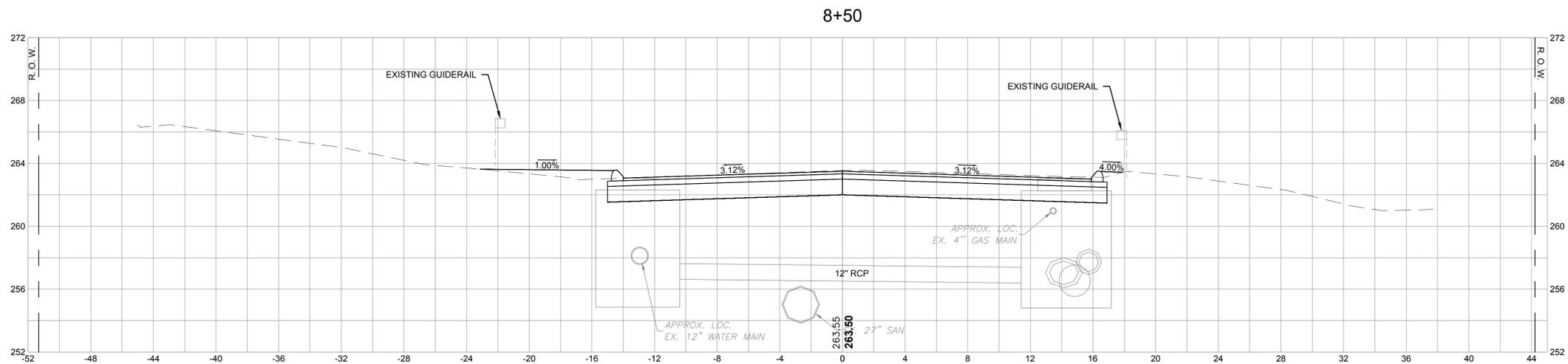
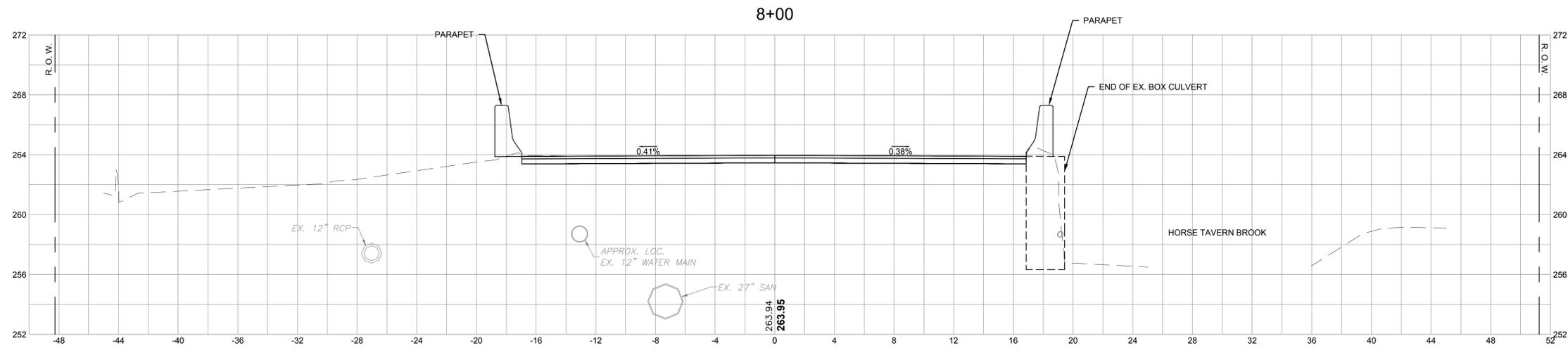
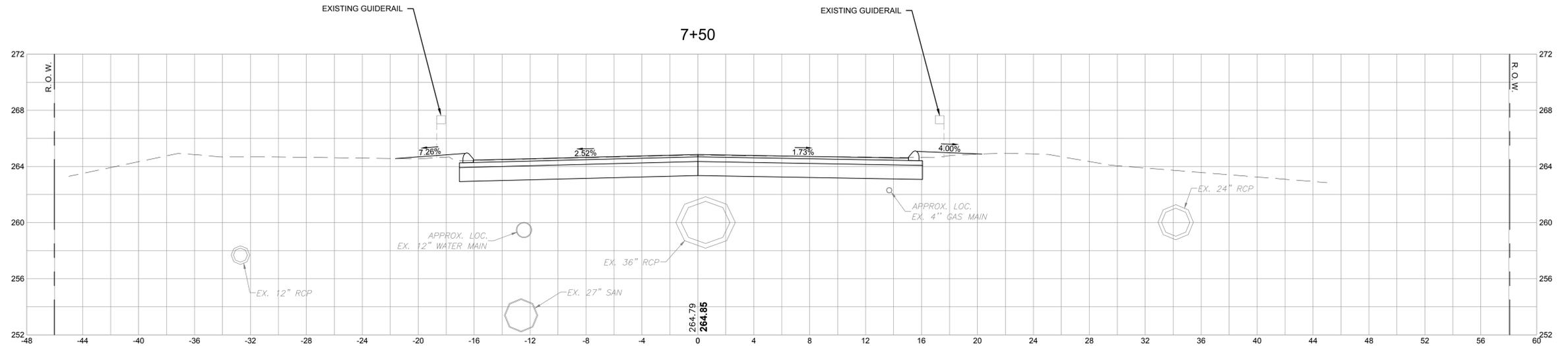
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TRUMBULL, CT
CROSS SECTIONS
PLANS**

BETA JOB NO. 5658
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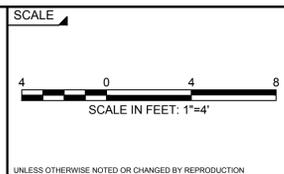
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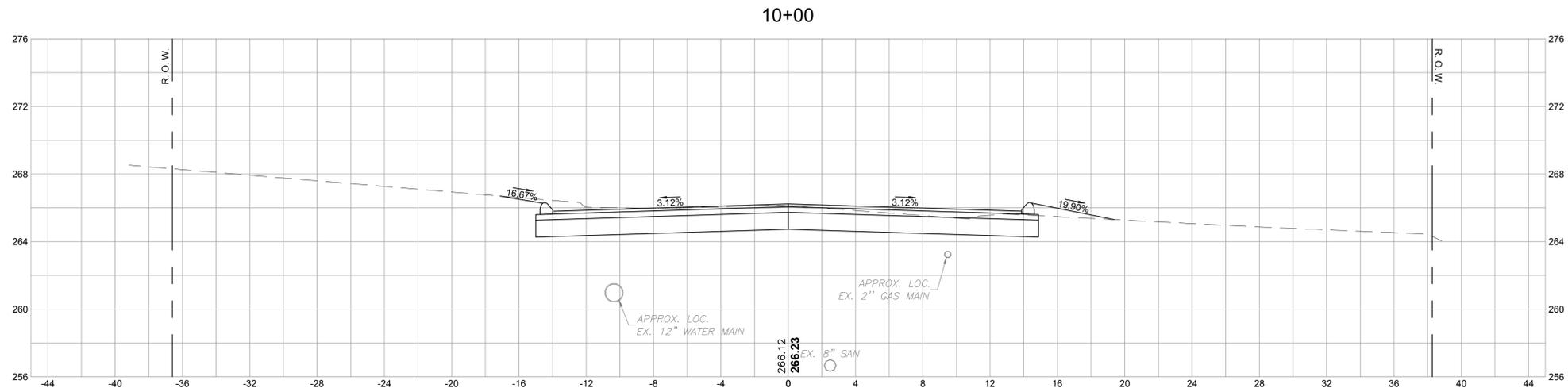
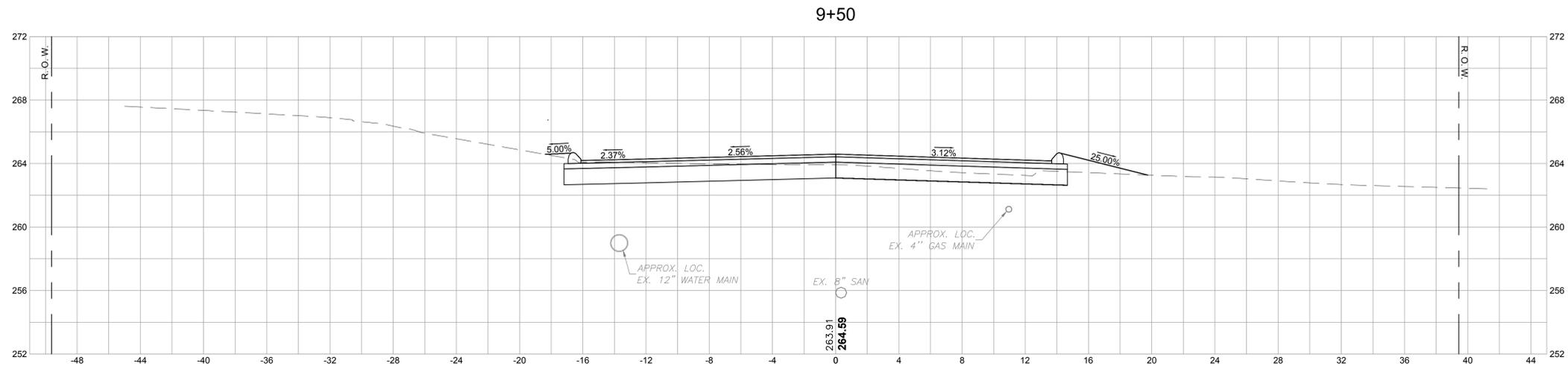
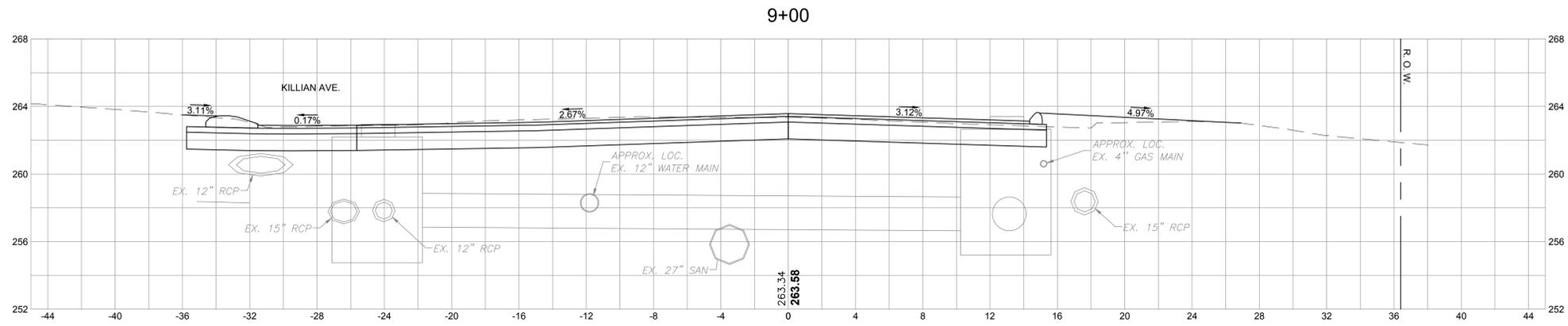


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TRUMBULL, CT
CROSS SECTIONS
PLANS**

BETA JOB NO. 5658
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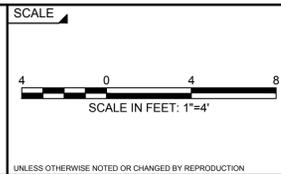
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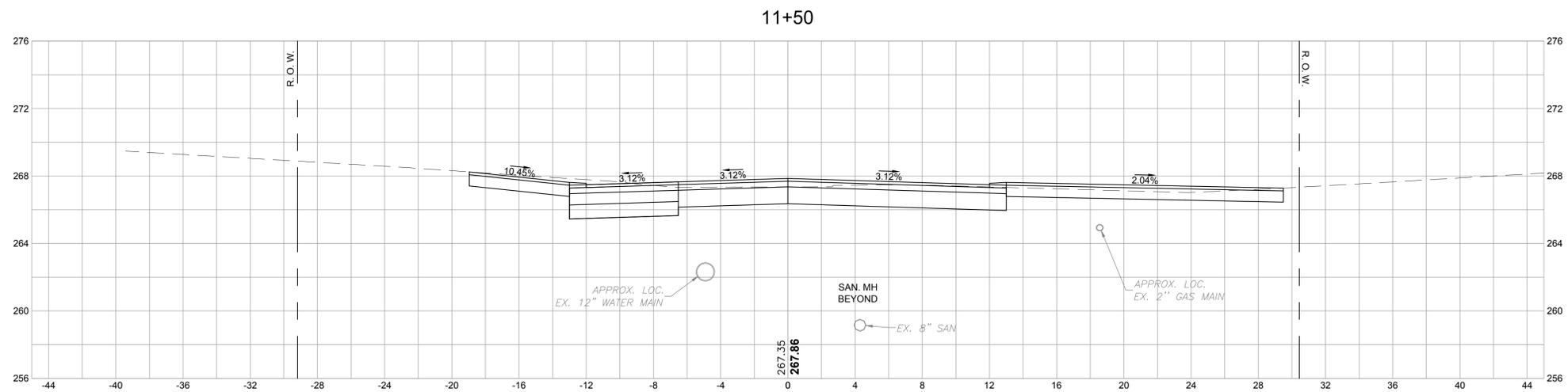
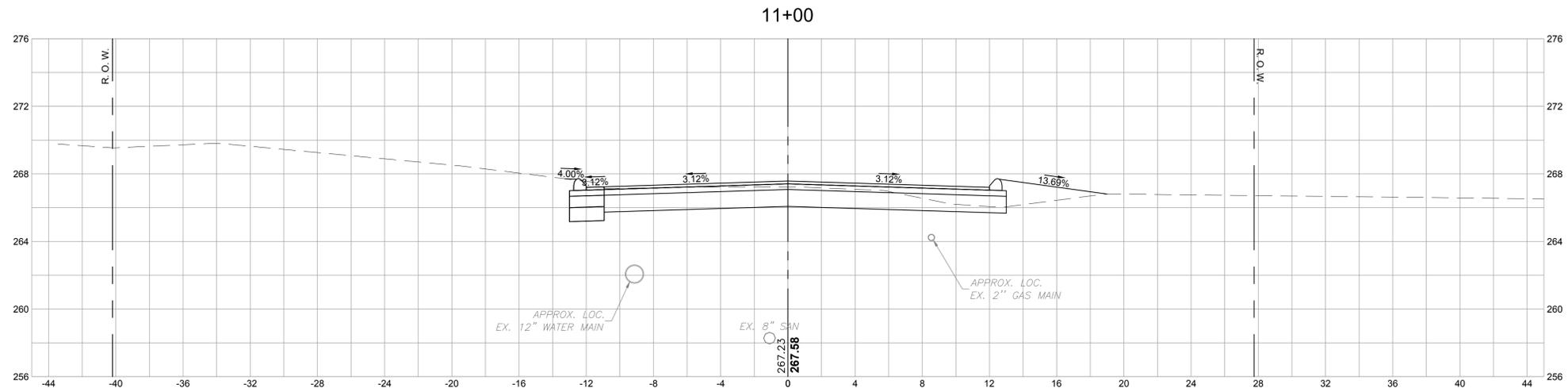
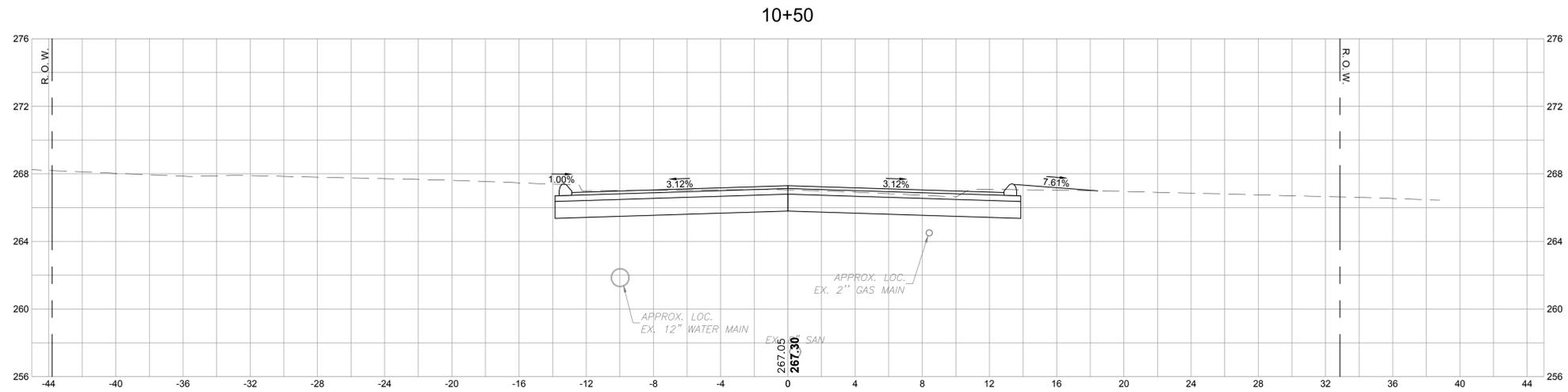


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**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
CROSS SECTIONS
PLANS**

BETA JOB NO. 5658
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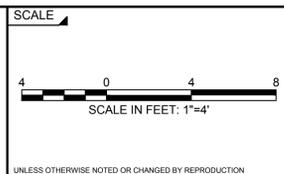
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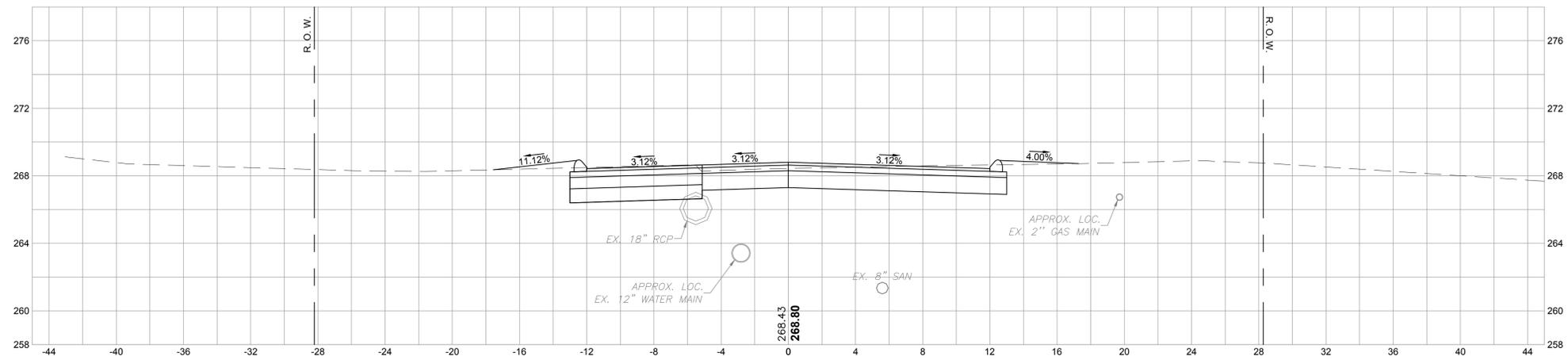
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**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
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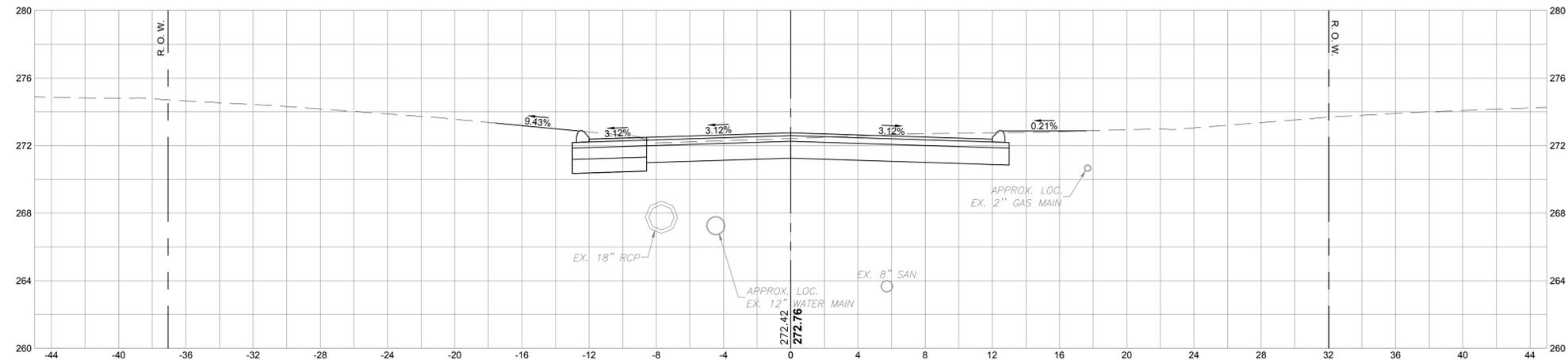
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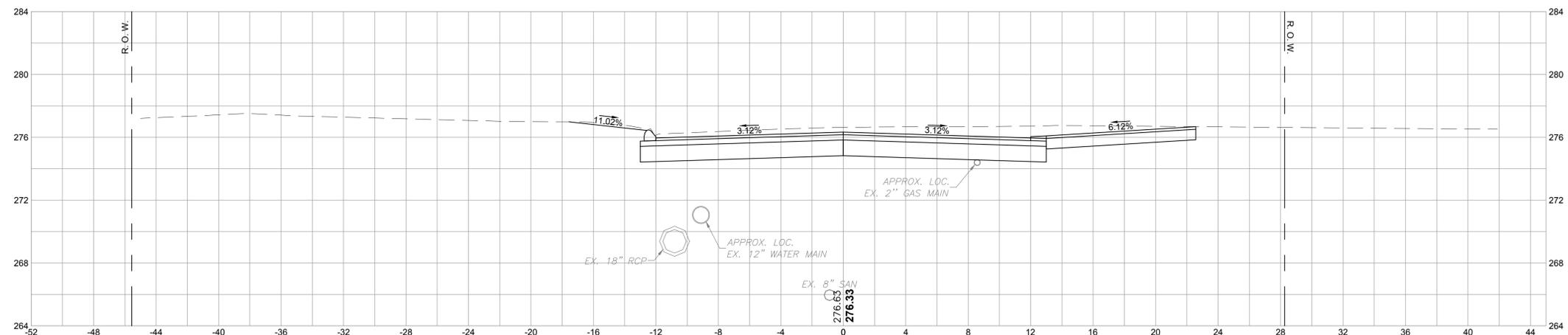
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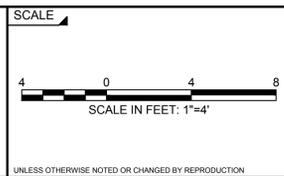
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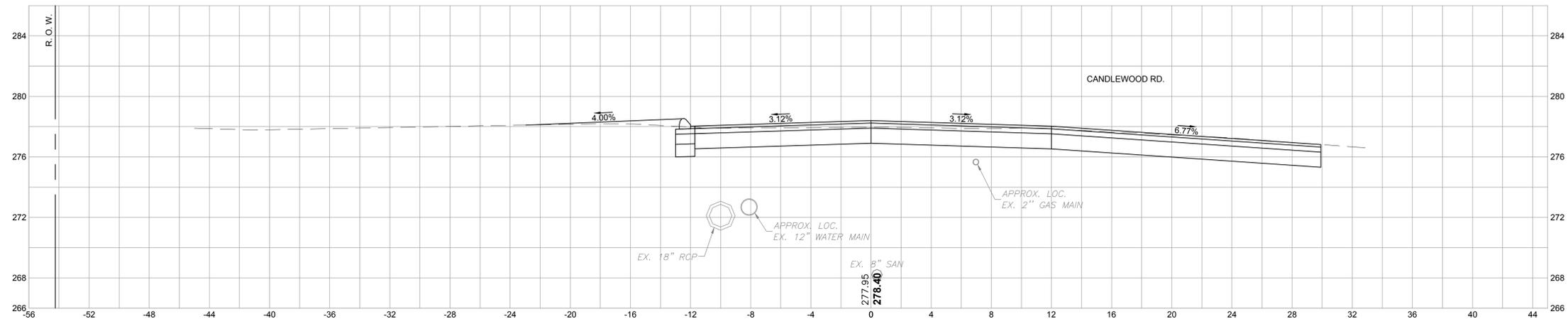
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TRUMBULL, CT
CROSS SECTIONS
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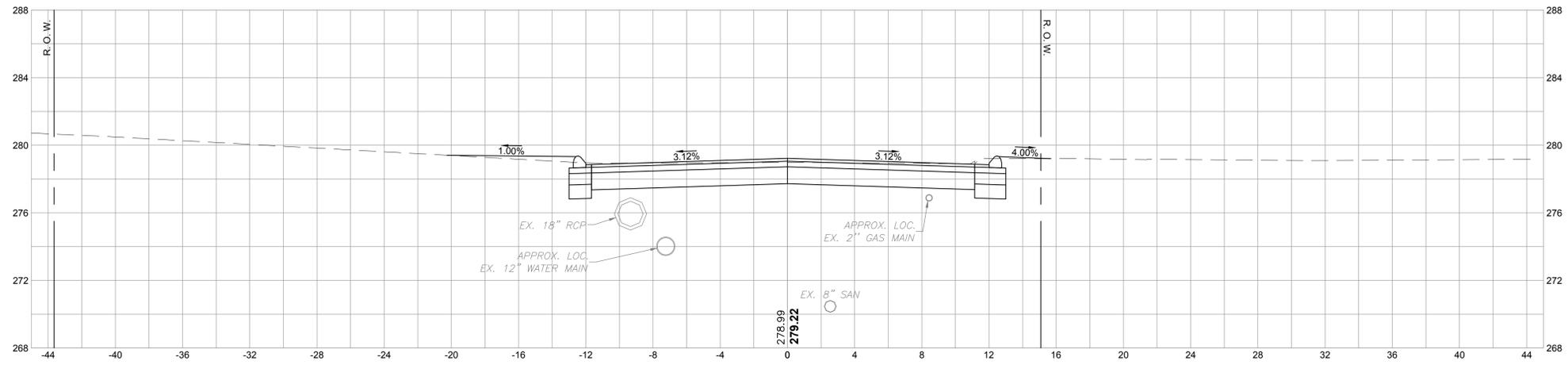
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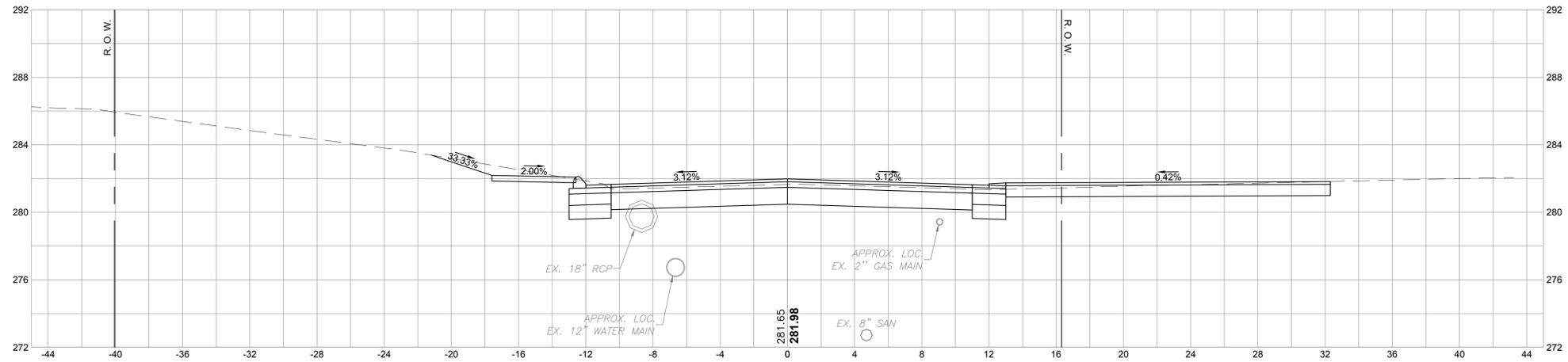
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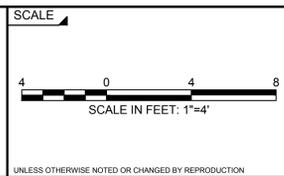
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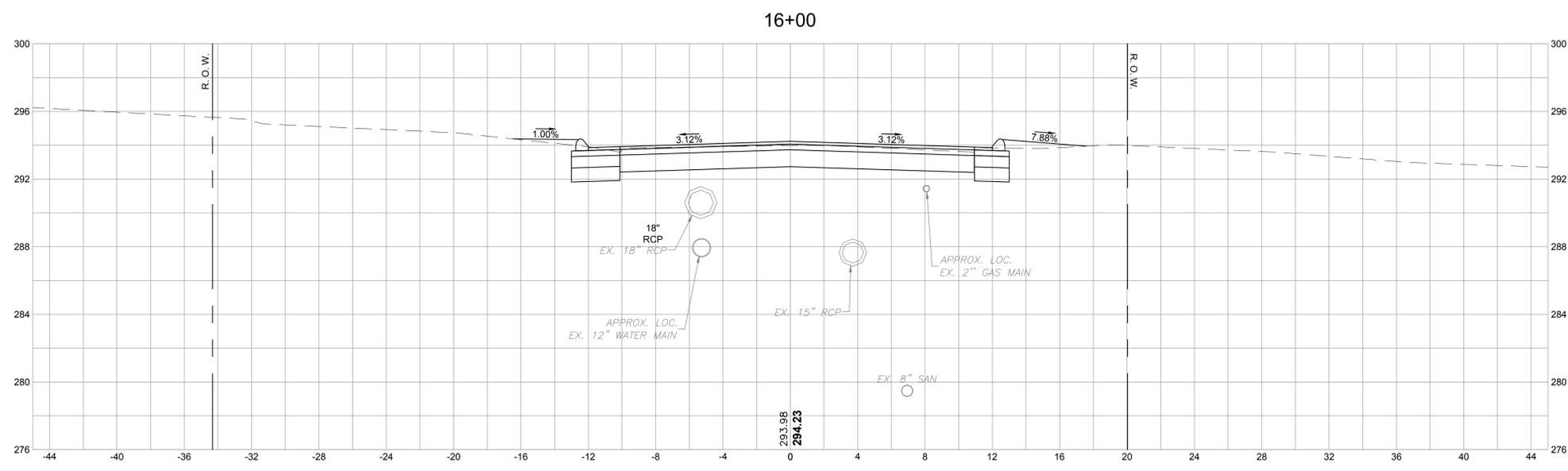
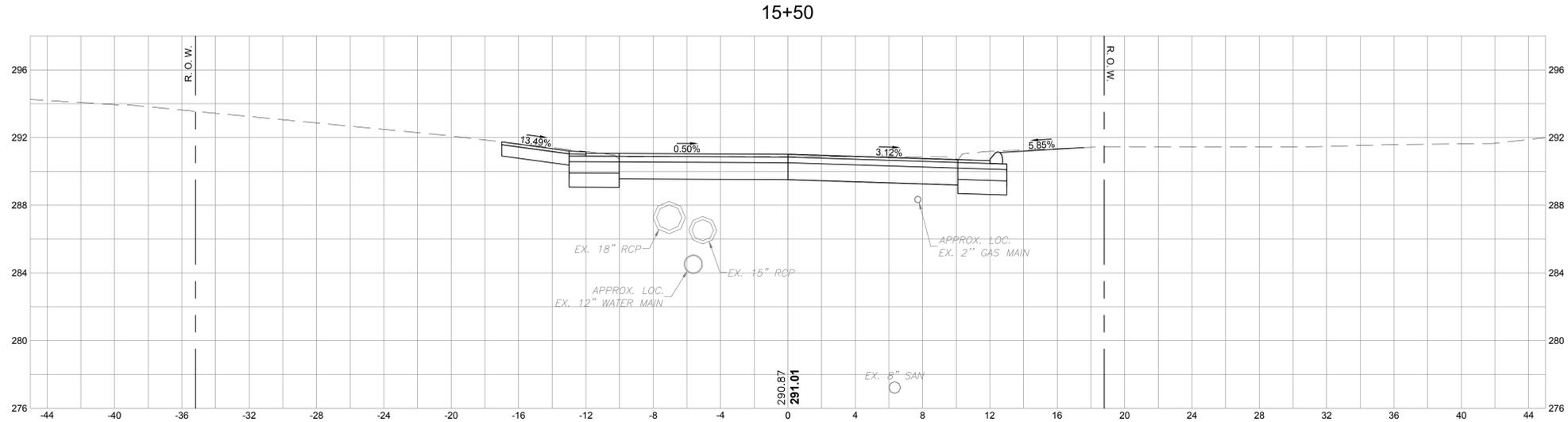
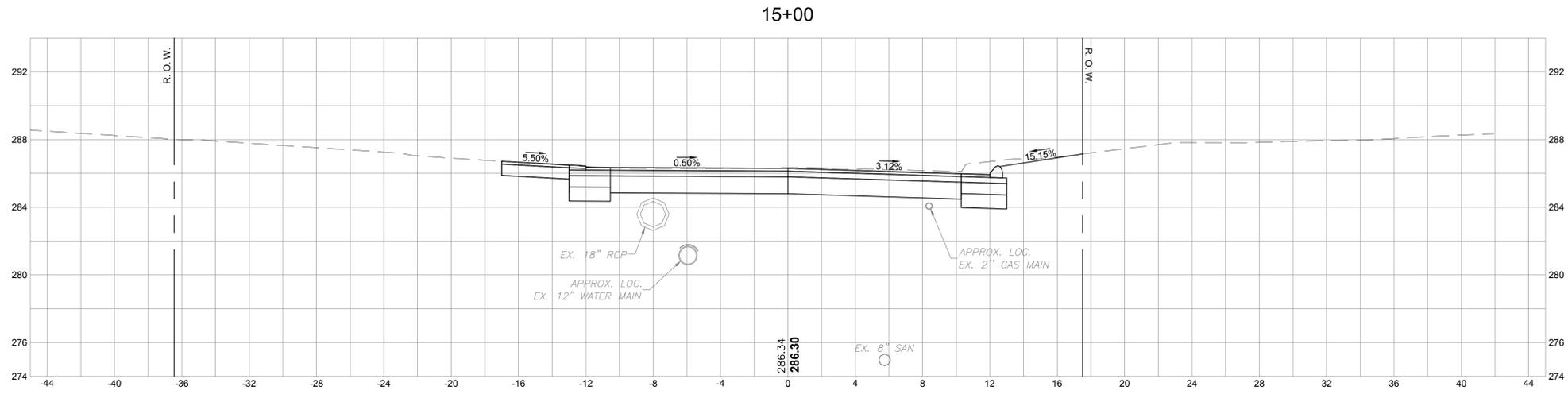
CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
CROSS SECTIONS
PLANS

BETA JOB NO. 5658

ISSUE DATE 1/16/2019

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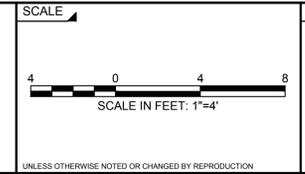
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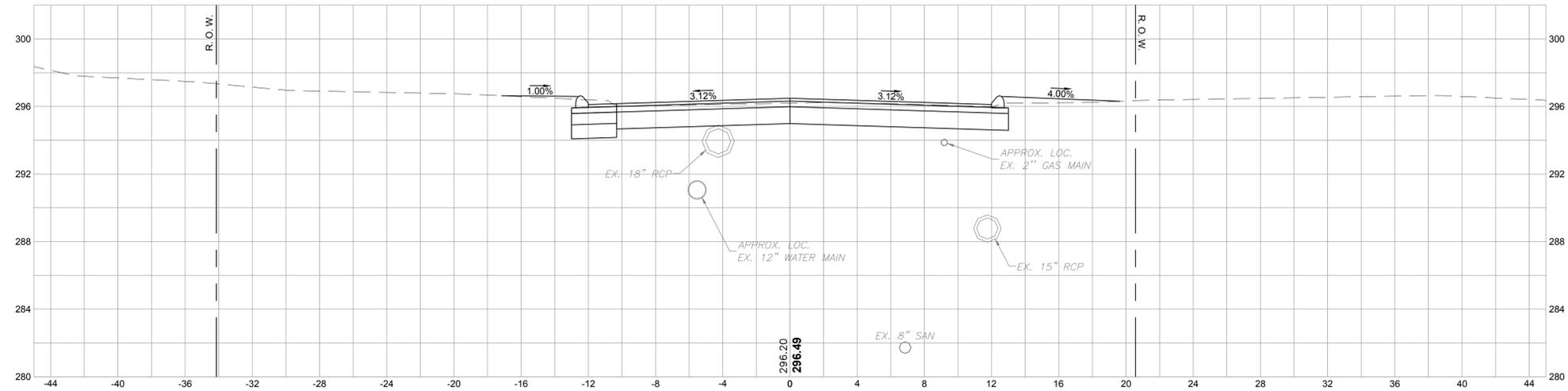


CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
CROSS SECTIONS
PLANS

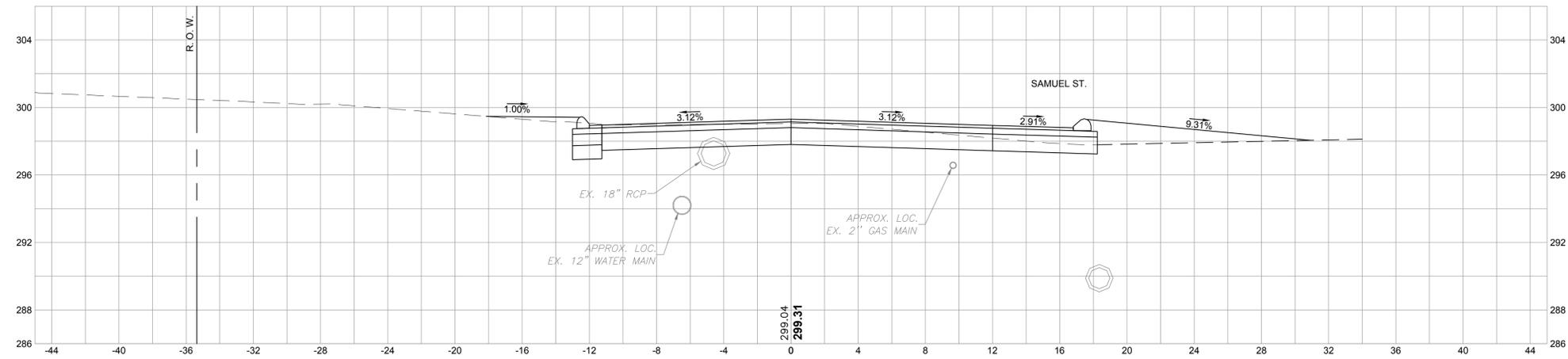
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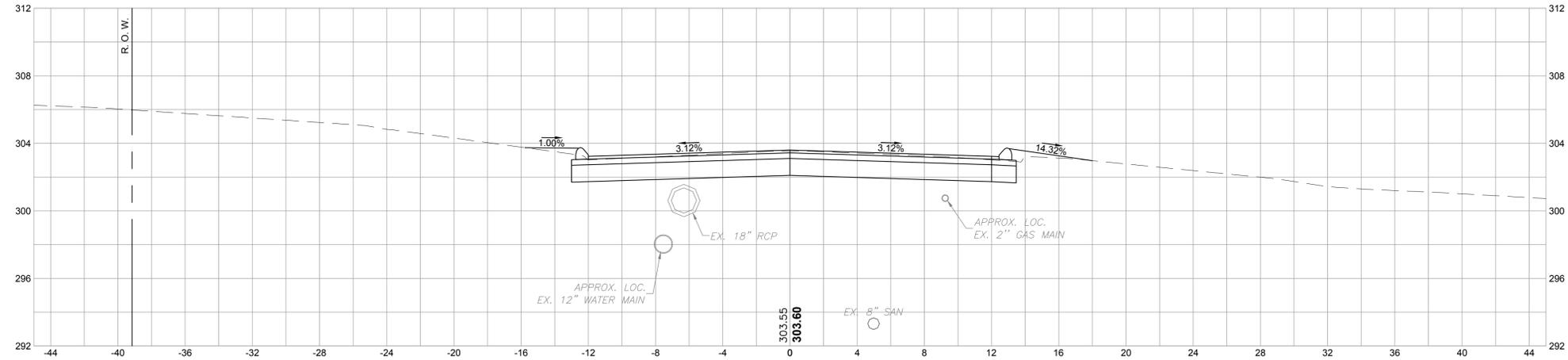
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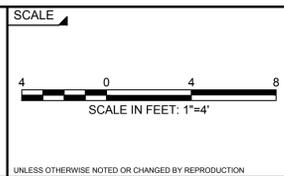
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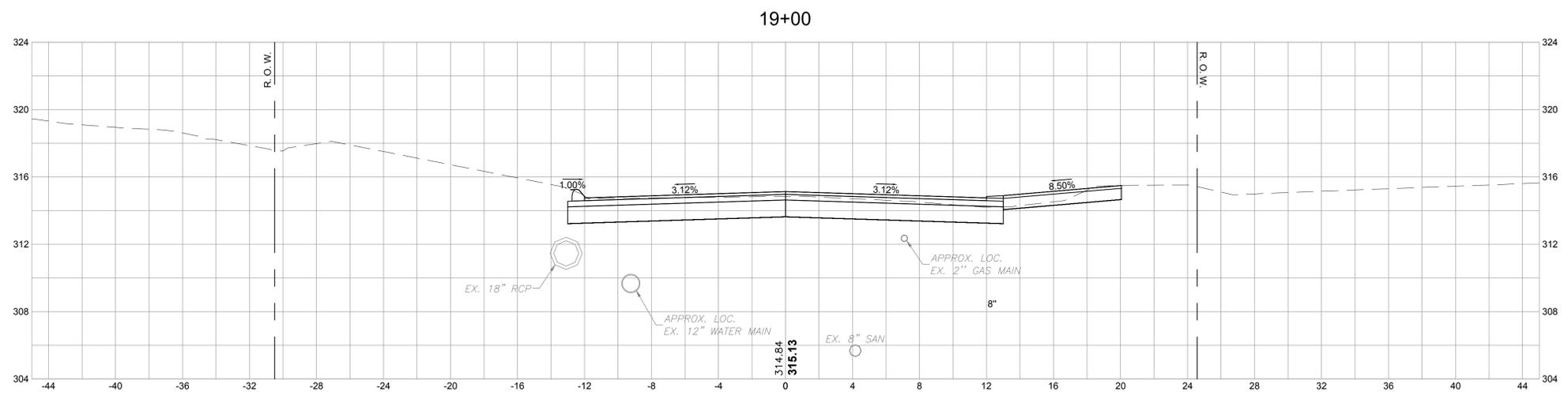
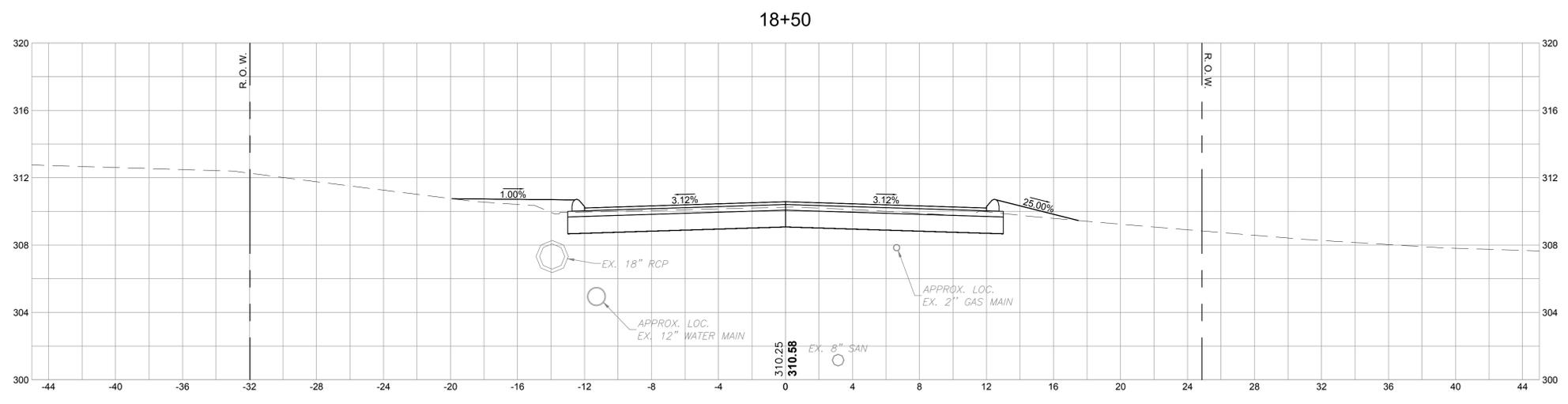
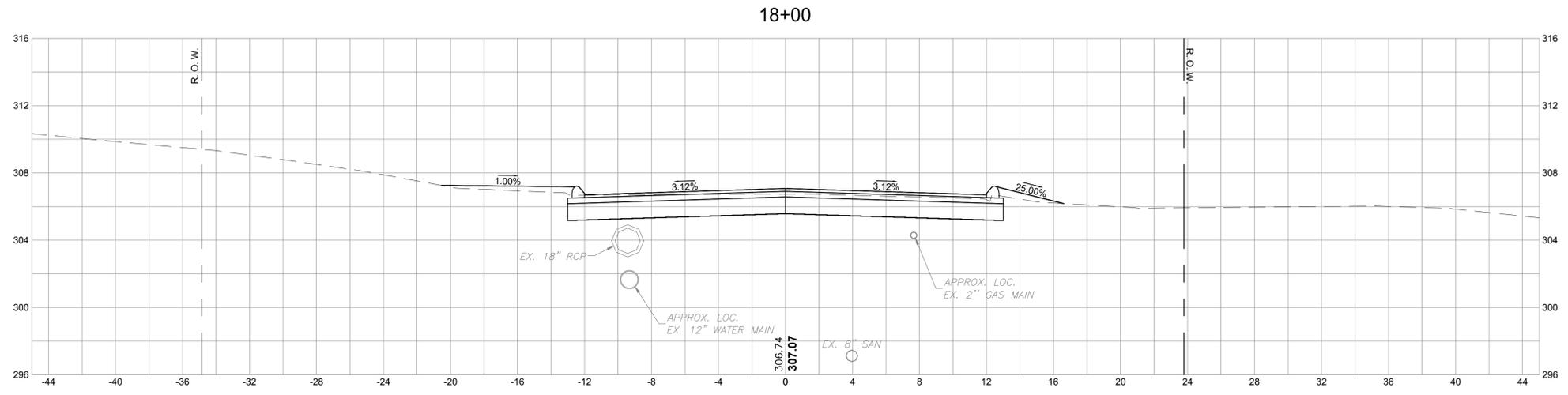


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**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
CROSS SECTIONS
PLANS**

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. XS-12
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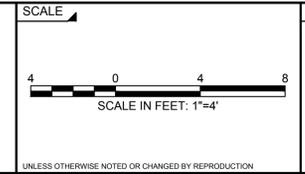
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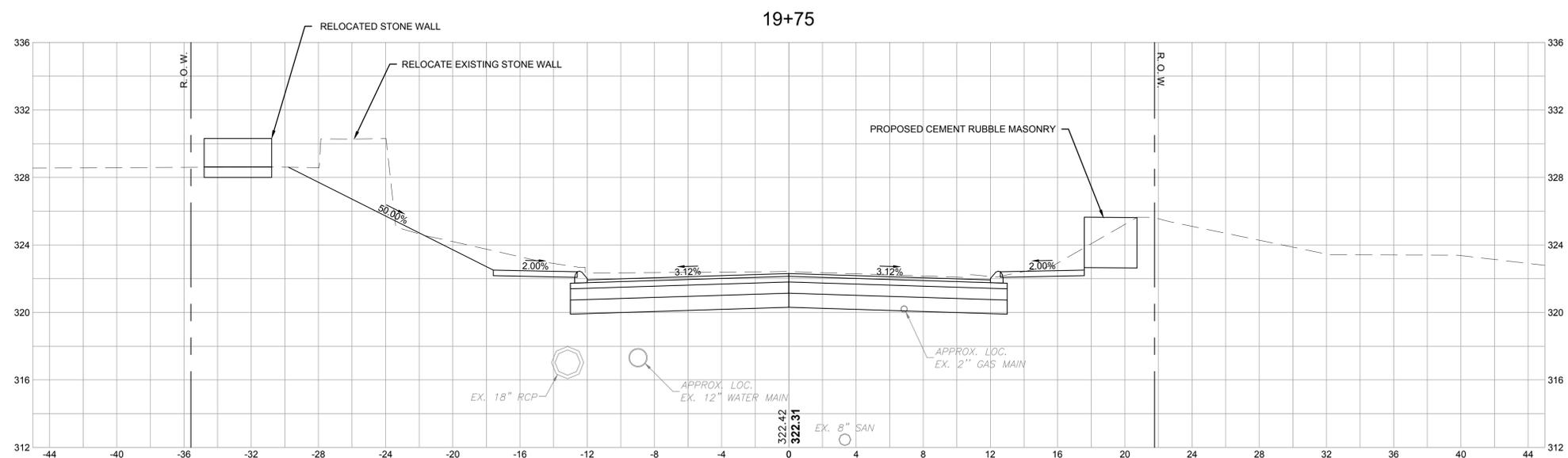
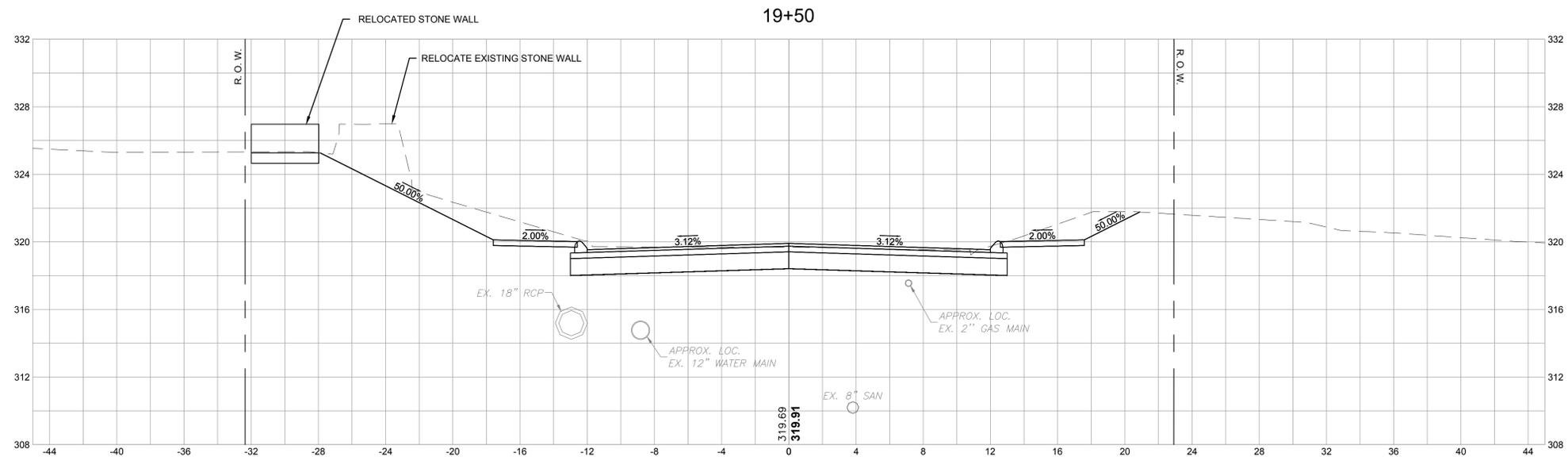
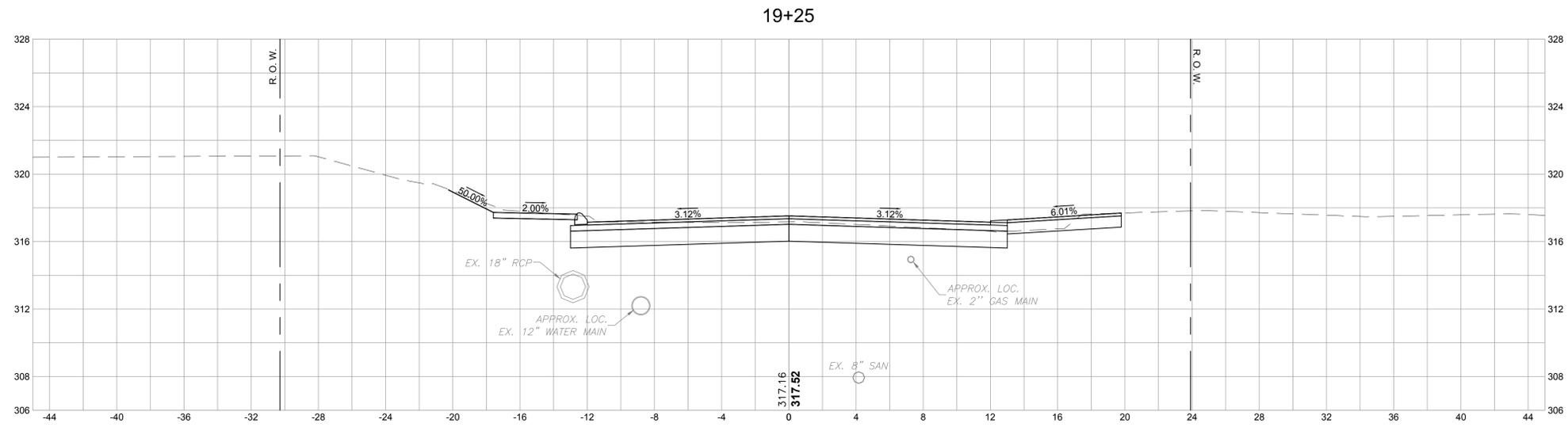
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CHESTNUT HILL ROAD RECONSTRUCTION TRUMBULL, CT

CROSS SECTIONS PLANS

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
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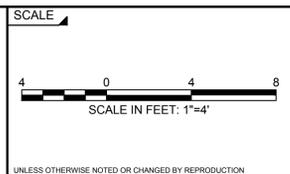
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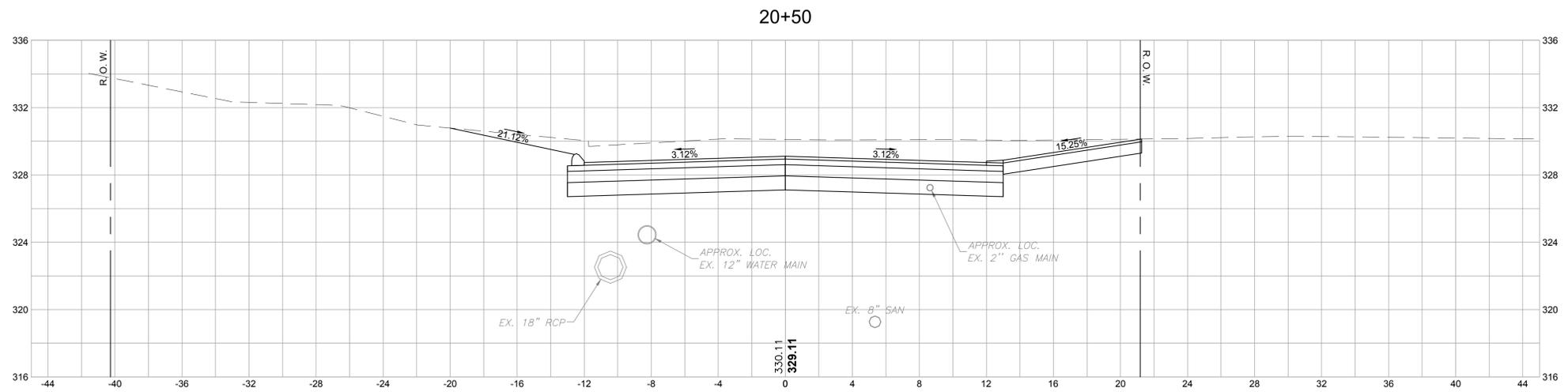
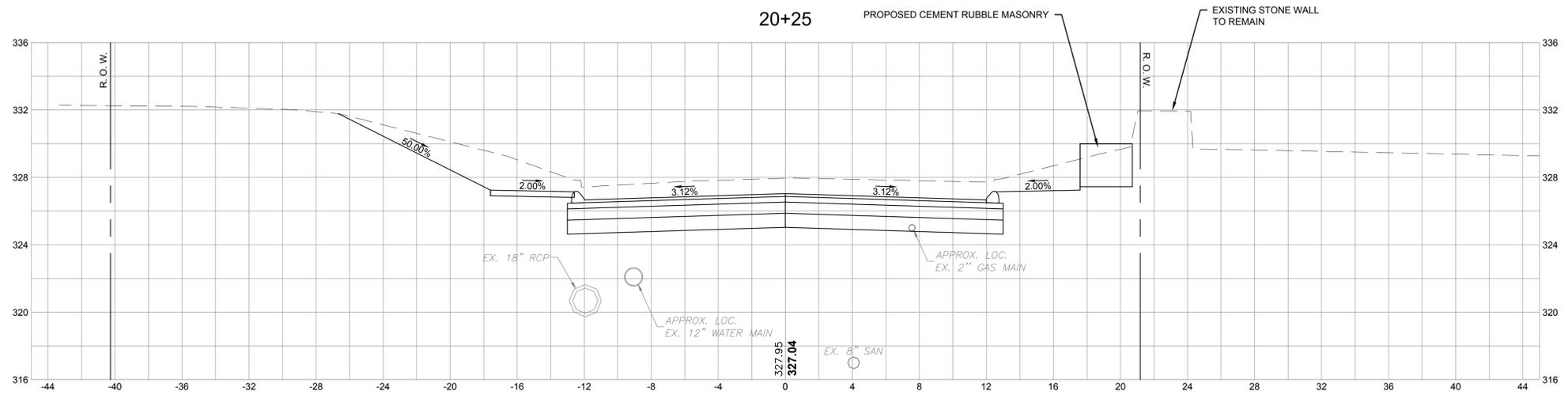
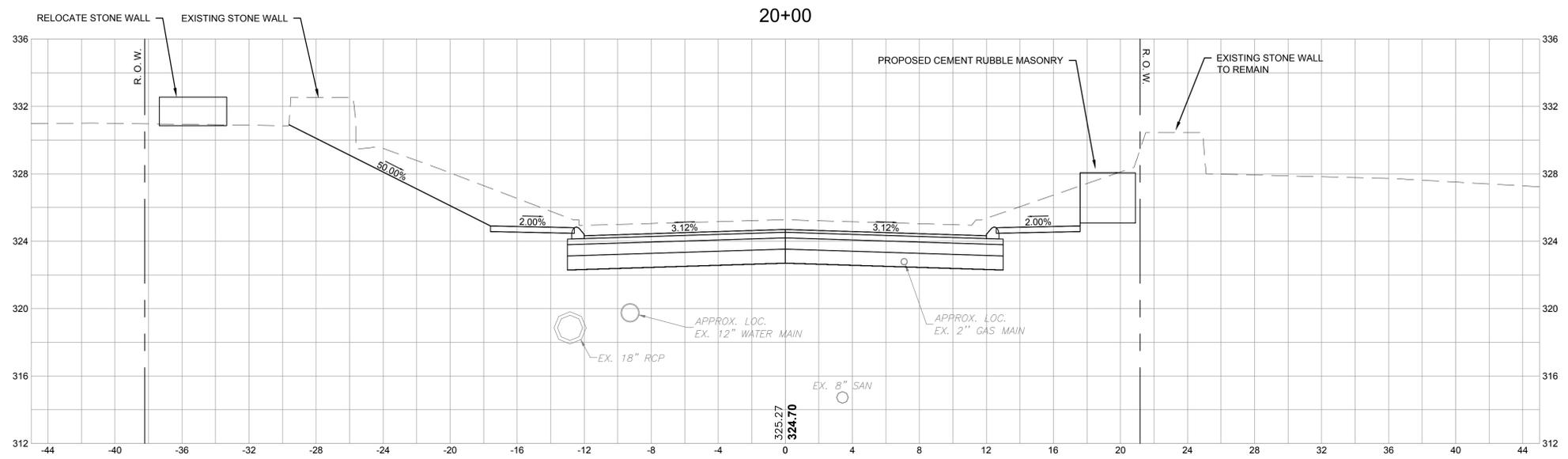


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CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
CROSS SECTIONS
PLANS

BETA JOB NO. 5658
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SHEET NO. **XS-14**
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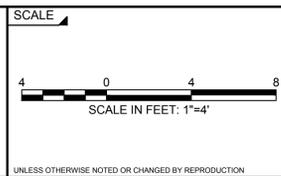
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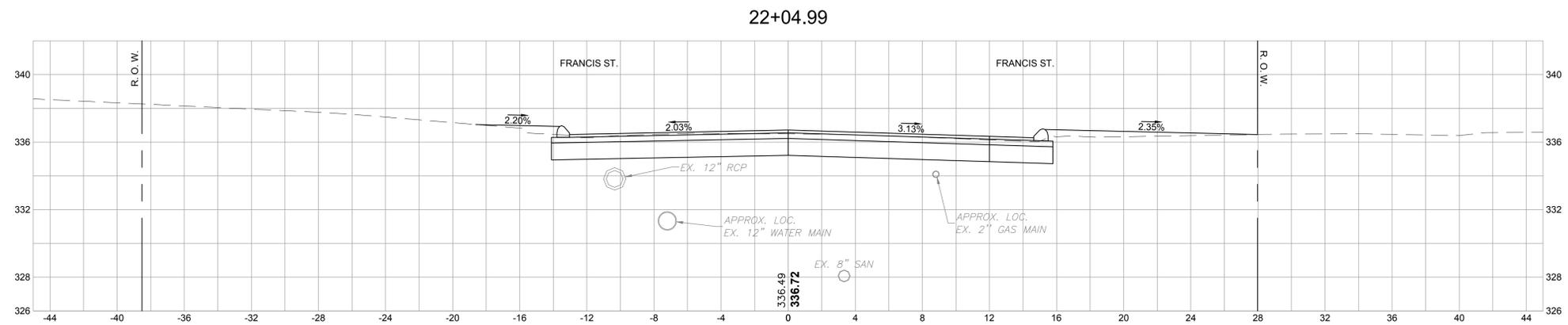
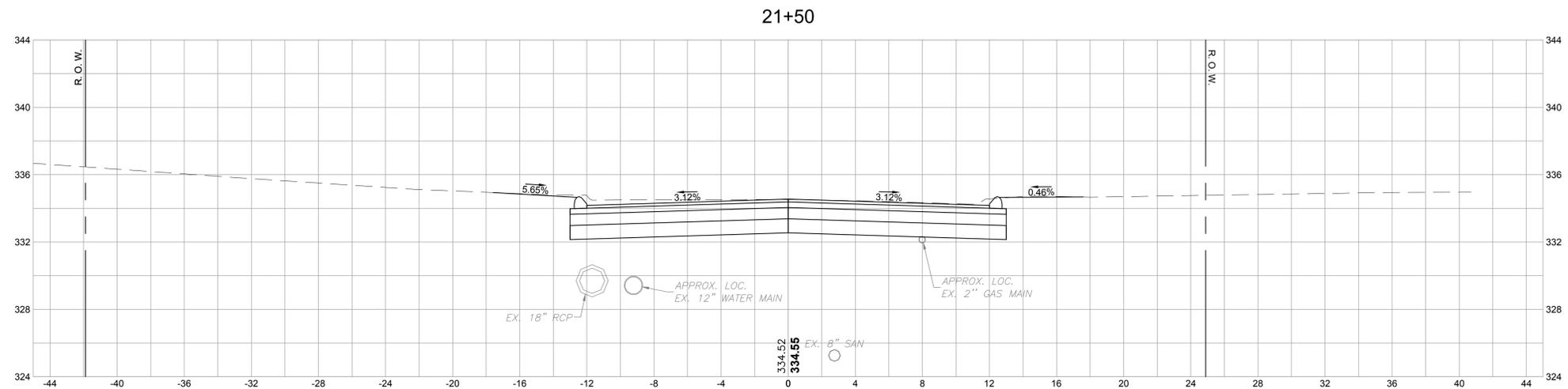
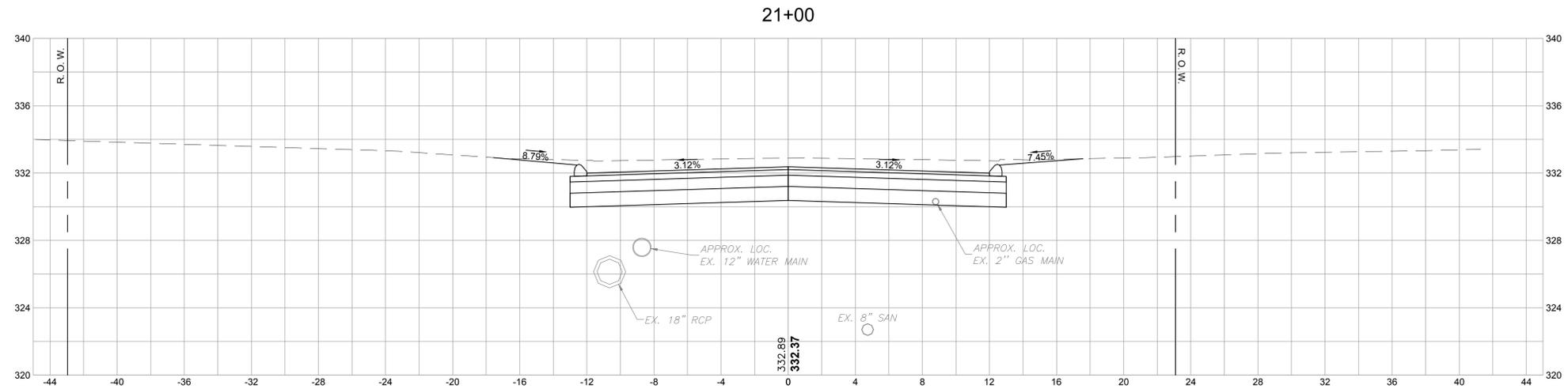
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CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
CROSS SECTIONS
PLANS

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. **XS-15**
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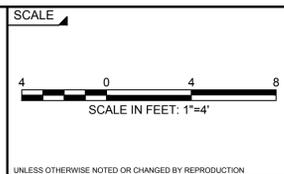
NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS

DRAWN BY:
C.D.
DESIGNED BY:
R.S.
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J.B.

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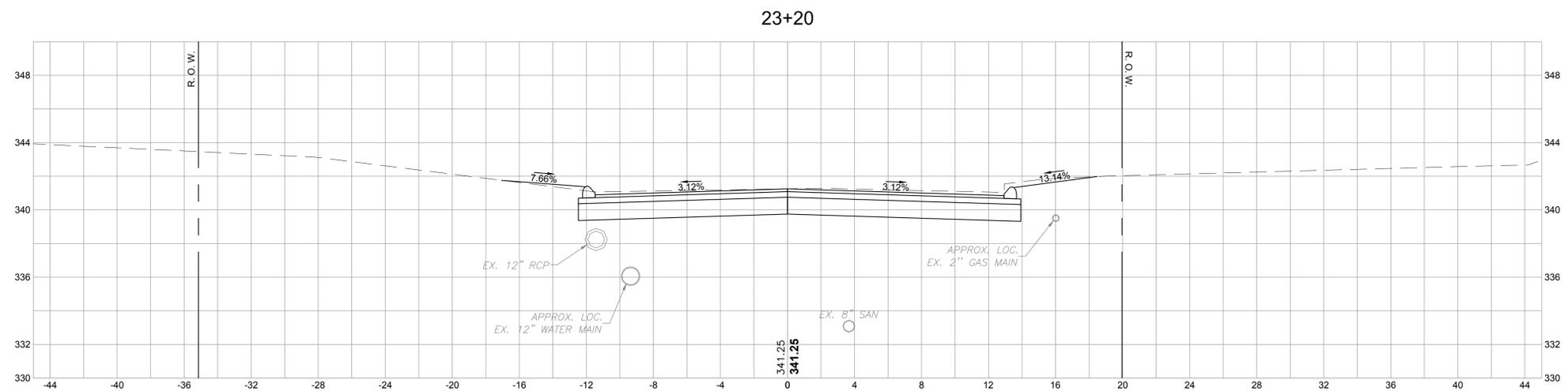
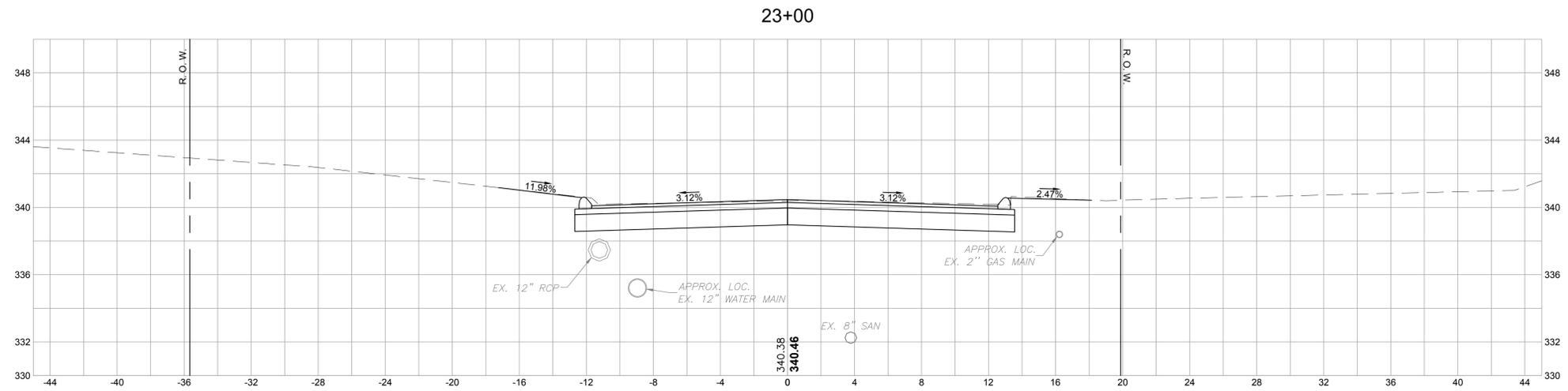
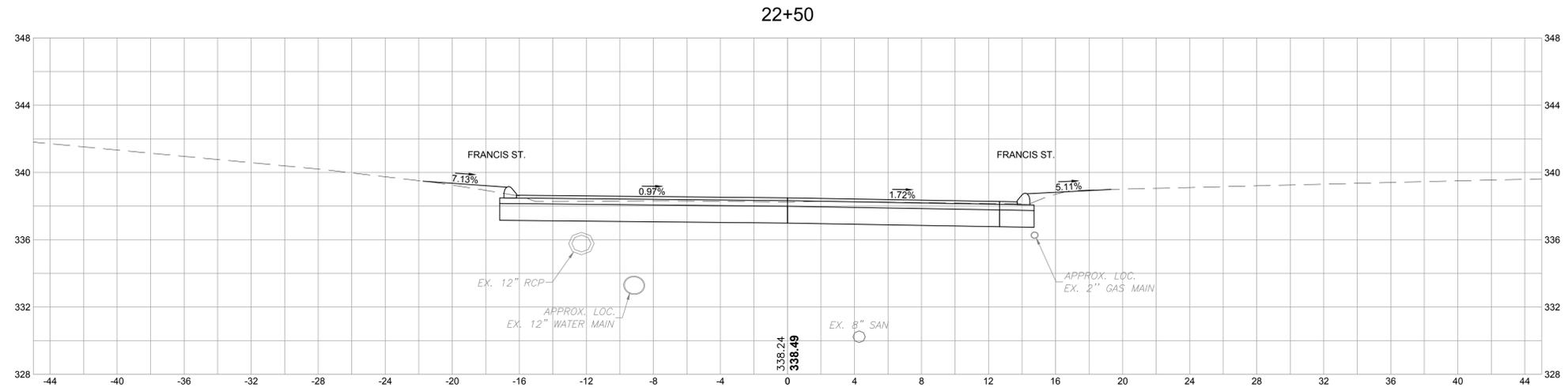


TITLE

**CHESTNUT HILL ROAD RECONSTRUCTION
TRUMBULL, CT
CROSS SECTIONS
PLANS**

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. XS-16
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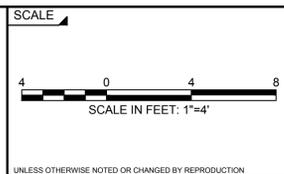
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TRUMBULL, CT
CROSS SECTIONS
PLANS**

BETA JOB NO. 5658
ISSUE DATE 1/16/2019
SHEET NO. XS-17
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